# GREAT WESTERN RAILWAY

# GENERAL APPENDIX

TO THE

# RULE BOOK

To come into operation on August 1st, 1936.

JAMES MILNE, General Manager.

#### BRITISH RAILWAYS

(Western Operating Area)

# Passenger & Freight Train Classification

On and from Monday, 5th June, 1950, a standard classification for passenger and freight trains will be introduced, and from that date existing headlamp and block bell codes shewn on pages 139 and 140 of the General Appendix to the Rule Book will be superseded by the following:—

	Engine Head-		Block Bell Code.				
Classi- fication.	code (white fights or discs).	Description of Train.	Beats on Bell or Gong.	How to be given.			
A	<u>.</u>	Express passenger train, newspaper train or breakdown van train or snow plough going to clear the line or light engine going to assist disabled train Officers' Special train not requiring to stop in section Express Diesel Car	4 4 8	Consecutively. Consecutively. 4 pause 1 pause 3.			
В	0	Ordinary passenger train, mixed train, or breakdown van train NOT going to clear the line, or loaded rail motor train \$\frac{1}{2}\$ Ordinary passenger train Ordinary passenger or parcels Diesel Car	4 4 9	3 pause 1. 1 pause 3. 5 pause 1 pause 3			
С	0	Parcels, fish, fruit, horse, livestock, meat, milk, pigeon or perishable train composed entirely of vehicles conforming to coaching stock requirements Express freight, livestock, perishable or ballast train, pipe-fitted throughout, with the automatic brake operative on not less than half of the vehicles Empty coaching stock train (not specially authorised to carry "A" headcode), or empty rail motor train ‡	5	1 pause 3 pause 1 3 pause 1 pause 1. 2 pause 2 pause 1			
D	0	Express freight, livestock, perishable or ballast train partly fitted with the automatic brake operative on not less than one-third of the vehicles	5	Consecutively.			
Е	<u>Ö.</u>	Express freight, livestock, perishable or ballast train partly fitted with not less than four braked vehicles connected by vacuum pipe to the engine Express freight, livestock, perishable or ballast train with a limited load of vehicles NOT fitted with continuous brake	5	1 pause 2 pause			

Continued over

#### Passenger and Freight Train Classification-continued.

Classi-	Engine Head-		Block Bell Code.				
fication.	code (white lights or discs).	Description of Train.	Beats on Bell or Gong.	How to be given.			
F	<u></u>	Express freight, livestock, perishable, or ballast train NOT fitted with continuous brake	5	3 pause 2.			
G	Ö	Light engine or light engines coupled Engine with not more than two brake vans	5	2 pause 3.  1 pause 1 pause 3.			
н	٥	Through freight or ballast train not running under class "C," "D," "E" or "F" headcode	5	1 pause 4.			
J	٥	Mineral or empty wagon train	5	4 pause 1.			
,K	Ō.	Freight, mineral or ballast train stopping at intermediate stations Branch freight train Freight, ballast or Officers' Special train, requiring to stop in section	3 3 7	Consecutively. 1 pause 2. 2 pause 2 pause 3.			

<sup>†</sup> To be used only where authorised by the Operating Superintendent.

The classification of each train will be shewn in the appropriate column of the working timeables on and from the above date.

Paddington Station, May, 1950.

5/50. 42,500.

GILBERT MATTHEWS,
Operating Superintendent.

<sup>‡</sup> The term "rail motor train" includes "auto train."

For the use of Employees only.

BRITISH RAILWAYS

(WESTERN OPERATING AREA)

# Alterations and Additions to the General Appendix to the Rule Book

To come into operation on receipt.

The following instructions to be inserted on page 70:-

### INSTRUCTIONS FOR RUNNING AND WORKING OF THE LENNOX-LOMAX EARTH AUGER EQUIPMENT.

- 1. The machine must only be used by the staff authorised by the Signal Engineer.
- 2. The machine, which is provided with a special match truck, is capable of self-propulsion at a speed of 10 miles per hour (in forward or reverse gear) and, except when operating, must have the match truck coupled. The machine is fitted with three types of brakes, viz., Hydraulic, Transmission and hand screw wheel. The match truck has a hand brake only.
- 3. When travelling under its own power the machine or the match truck, as the case may be, must carry a white headlamp and a tail lamp, which must be lighted as necessary. A red flag must be displayed on the rear vehicle by day. The machine must be equipped with red and green handsignal flags, not less than 12 detonators, a hand lamp (lighted when necessary) and a sprag. A portable telephone or "walkietalkie" apparatus must also be available.
- 4 When boring operations are required to be carried out an absolute occupation of the line concerned must be arranged. The service on which the machine will be worked to the site of operations and the occupation required must be pre-arranged with the District Operating or District Traffic Superintendent concerned.
- 5. The machine must be worked to the agreed place near the site of work, or vice versa, by freight train carrying "F" or inferior headcodes and be marshalled next inside the Guard's brake van. The match truck may be leading or trailing. Should it be necessary to run the machine as a special train hauled by a locomotive a brake van must always be provided at the rear, in which a Guard must ride.
- 6. When the machine is proceeding under its own power to the site where it is required to work the match truck must always be attached and both the screw and chain couplings must be used for the purpose. Such movements must be confined to the shortest possible distance but if it is necessary for the machine to pass completely through one or more sections it must be signalled and dealt with as a Through Ballast Train. When running in a multiple-aspect signalling area automatic signalling must be suspended and the machine dealt with in accordance with T.C.B. Regulation 15. Where an Inner Home Signal is provided the "Is Line Clear?" signal must not be accepted until the Home signal can be lowered.
- 7. The machine must only be moved under its own power by the Driver who has been passed as competent by the Signal Engineer and he must be accompanied by a man who has been passed as competent by the Motive Power Department in (a) knowledge of the route and (b) protective duties of a Driver. A member of the crew of the machine who has been passed by the Operating Department as competent to carry out the protective duties of a Guard must always be present.
- 8. The machine must not be relied upon to actuate track circuits and Rule 55 must be observed in all cases by the man acting as Guard. When the machine is detained whilst waiting acceptance by the box in advance it must not draw forward to the signal controlling the entrance to the section ahead or to an Intermediate Block Home signal but must be held opposite the box. No train must be allowed to follow the machine towards the Intermediate Block Signal until "Train out of Section" has been received.

- 9. A good look out must be kept when approaching level crossings.
- 10. No movement must be made past stop signals otherwise than with the consent of the Signalman.
  - 11. Protection at the site of boring operations must be in accordance with Rule 217
- 12. A portable telephone or "walkie-talkie" sets must be provided to enable contact to be maintained between the site of operations and the signal box in the rear.
- 13. When the machine is working, trains may pass on an adjoining line without restriction except as provided for in Clause 14.
- 14. Should boring between running lines be required to be carried out or should there be any possibility that the operations will obstruct the opposite or adjoining line, prior notification must be given to the Operating Department and a responsible member of the Operating Department must be present and the operations must not be commenced without his permission. Before giving such permission he must ascertain from the Signalman in the rear that no train is approaching on the opposite or adjoining line and the Signalman, before agreeing to obstruction of such line, must comply with the provisions of Block Regulation 13. The line affected must, in addition, be protected by a Handsignalman in accordance with Rule 217.
- 15. When the opposite or adjoining line is clear, the Signalman must be advised and such line must not be further obstructed until the provisions of Clause 14 have again been complied with.
- 16. At the conclusion of boring operations the machine and match truck will be removed from the section under the power of the machine. If the machine is returned to the signal box in rear, the Driver must bring it to a stand before reaching the detonators protecting the work. The conductor must proceed on foot to obtain the Signalman's Wrong Line Order authorising return to the signal box. The Signal Department person in charge must give the Signalman an assurance that the section is clear of obstruction. (G.A.32.Op—10/56. OM12832)

#### S. G. HEARN,

October, 1956.

Chief Operating Superintendent.

Each member of the Staff receiving a copy of this Circular is required to read carefully and note the contents, and, if supplied with a copy of the General Appendix to the Rule Book to alter or cancel in lnk the present instructions on the subject appearing therein, afterwards pasting the amendments in their proper places in the General Appendix to the Rule Book.

Station and Depot Masters are responsible for seeing that copies of the General Appendix to the Rule Book supplied to Signal Boxes, etc., under their supervision, are corrected in accordance with this Circular.

B.R. 30145/33 G.A.32.Op.

(This form must be detached and forwarded to the Head of Department.)	1
Station	1956
RECEIVED copy of Circular G.A.32.Op., dated October, 1956, containing alter and additions to the General Appendix to the Rule Book.	rations

PRIVATE.—For use of the staff concerned only

# BRITISH RAILWAYS

(WESTERN OPERATING AREA)

# NOTICE

OF

# SPECIAL TRAIN ARRANGEMENTS

EDINBURGH to FILTON JUNCTION (Via SHREWSBURY, HEREFORD and SEVERN TUNNEL)

BRISTOL (T.M.) to PADDINGTON

FRIDAY, 6th NOVEMBER, 1953

THIS NOTICE, WHICH WILL BE DISTRIBUTED BY THE DISTRICT OPERATING SUPERINTENDENTS CONCERNED TO ALL STAFF AFFECTED IN THEIR RESPECTIVE DISTRICTS, MUST BE ACKNOWLEDGED TO THE DISTRICT OPERATING SUPERINTENDENTS IMMEDIATELY ON RECEIPT BY TELEGRAM AS FOLLOWS:—"ARNO FIFTY."

THE INSTRUCTIONS CONTAINED IN RAILWAY EXECUTIVE CIRCULAR O/RR, DATED DECEMBER, 1950, UNDER THE HEADING "SPECIAL INSTRUCTIONS TO BE OBSERVED IN CONNECTION WITH THE WORKING OF TRAINS DESIGNATED BY THE CODE WORD 'DEEPDENE'" MUST BE APPLIED TO THE TRAINS IN THIS NOTICE WHICH ARE MARKED "DEEPDENE."

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TIME TABLE OF "DEEPDENE" TRAIN-EDINBURGH TO WESTERLEIGH STABLING POINT (VIA LONDON MIDLAND REGION, SHREWSBURY, MORNING. FRIDAY TUNNEL)-EARLY SEVERN HEREFORD AND 6th NOVEMBER, 1953.

The Engine to carry EXPRESS PASSENGER TRAIN HEAD CODE.

The Train will carry TWO TAIL LAMPS from Shrewsbury to Stabling Point. (Important—see paragraph 2 of R.E. Circular O/RR, dated December, 1950.) The Engine Head Lamps and the Tail Lamps must be lighted before leaving Shrewsbury.

The Train will run on the Main Line to Westerleigh West Junction; via the Down Platform Line at Hereford (Barr's Court) and Pontypool Road.

#### WORKING OF TRAIN ENGINE.

The Engine (No. 5073) to work the "Deepdene" Train forward from Shrewsbury Station to leave Coleham Shed, chimney towards Hereford, at 11||45 p.m. (5th November), and arrive Turntable Siding, Severn Bridge Junction at 12||0 night.

#### FORMATION (FROM ENGINE) LEAVING SHREWSBURY :-

LOUMAI	(BRAKE FIRST (Brake End Leading)			No.	5154	
	SALOON (Kitchen End Leading)	 		"	45006 Ar	Approximately
		 		**	45005	183 tons.
	SALOON (Kitchen End Trailing)	 		,,	45000	
	(VAN THIRD (Brake End Trailing)	 		22	26287 )	
			000		10 inches	

309 feet 10 inches. Length of Train (excluding Engine) ..

The distance from the centre of the footplate of the engine to the centre of the principal door of Saloon No. 45005 is approximately 152 feet 2 inches. The distance from the centre of the principal door of Saloon No. 45005 to the buffer face of the rear coach (Van Third No. 26287) is approximately 182 feet 13 inches.

#### THE PERMANENT AND TEMPORARY SPEED RESTRICTIONS MUST BE STRICTLY OBSERVED.

Dista fro Harle Cross	m scott	PRINCIPAL STATIONS AND INTERMEDIATE SIGNAL BOXES.	TIMES.	REMARKS.			
Mls.	Chs.	Harlescott Crossing pass	a.m. 12.25	From L.M.R.			
2	0	Crewe Junction ,,	-	NEW WIND A REPORT OF THE			
2	7	SHREWSBURY (Up and Down Plat- { dep. form)	12.30 12.55	Change Engines and Guards. Gas and Water as required L.M.R. Engine to be detached and run at once to Coleham Shed. W.R. Engine No. 5073 to work forward.  For detailed instructions for dealing with the "Deepdene" Train at Shrewsbury, see Chester District Operating Superintendent's Notice.			
2	61	Coleham pass	12.58	On SENSONS SMINISTERS SHEET WITH			
2	77	Sutton Bridge Junction ,,	THE SE	sarvipaque anithabab Tolalato			
6	33	Condover,	VIT 3	BY TELEGRAM AS FOL			
8	41	Dorrington ,,	-				

# TIME TABLE OF "DEEPDENE" TRAIN FROM EDINBURGH TO WESTERLEIGH STABLING POINT, FRIDAY, 6th NOVEMBER, 1953—

continued.

Dista fro Harles Cross	scott	PRINCIPAL STATIONS AND INTERMEDIATE SIGNAL BOXES.	TIMES.	REMARKS.
Mls.	Chs. 30	Leebotwood pass	a.m.	
	-	Dudgeley Signals "	_	Intermediate Block Signals—Down Distant and Down Home.
14	70	Church Stretton "	1.21	
22	3	Craven Arms & Stokesay ,,	1.31	
27	31	Bromfield ,,	-	
28	43	Water Troughs ,,	_	No Up trains must be in the section from Ludlow to Bromfield while the Royal Train is passing through the section from Bromfield to Ludlow.
29	50	Ludlow ,,	1.42	AT A SHORE THE THIRD AND A SHORE
29	53	Ludlow Tunnel ,,	_	
34	18	Woofferton,	1.481	
40	43	Leominster ,,	$1.57\frac{1}{2}$	See along the section of the section
44	741	Dinmore Tunnel ,, (Up and Down)	-	CED WED, Soul was not at all
51	33	Shelwick Junction "	2.13	
52	29	Barr's Court Junction ,,	2.15	
53	10	Hereford (Barr's Court) arr.	2,18	To run via Down Platform Line, Engine to take Water.
56	36	Red Hill Junction pass	2.29	
57	40	Red Hill Tunnel "	_	
59	62	Tram Inn "	_	
62	14	St. Devereux ,,	_	
65	49	Pontrilas "	$2.42\frac{1}{2}$	
70	58	Pandy "	_	
73	7	Llanvihangel "	2.55	
76	6	Abergavenny Junction "	_	
77	8	Abergavenny ,, (Monmouth Road)	3. 1	
79	63	Penpergwm "		
82	32	Nantyderry ,,		
85	1	Little Mill Junction ,,	-	
86	44	Pontypool Road ,,	$3.14\frac{1}{2}$	To run via Down Platform Line.
88	4	Panteg Junction "	-	
89	22	Lower Pontnewydd ,,	-	
90	49	Llantarnam Junction ,,	$3.20\frac{1}{2}$	

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# TIME TABLE OF "DEEPDENE" TRAIN FROM EDINBURGH TO WESTERLEIGH STABLING POINT, FRIDAY, 6th NOVEMBER, 1953—

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continued.

Distances from Harlescott Crossing.		PRINCIPAL STATIONS AND INTERMEDIATE SIGNAL BOXES.	TIMES.	REMARKS.			
Mls. 91	Chs.	Llantarnam pass	a.m.				
92	23	Ponthir ,,	_				
93	66	Caerleon ,,	_				
95	59	Maindee Junction North ,,	3.29				
, 96	18	Maindee Junction East ,,	3.31				
103	58	Water Troughs ,,	-	No Down train must be in the section from Und Crossing to Magor while the "Deepdene" Train passing through the section from Magor to Und Crossing.			
105	31	Severn Tunnel Junction ,,	3.44	To run from Up Main to Up Tunnel Line at Sever Tunnel Junction East,			
106	20	Severn Tunnel West ,,	3.46	and the state of t			
111	$12\frac{1}{2}$	Severn Tunnel East ,,	4. 5	-1 ASSESSED OF 18			
111	154	Ableton Lane Tunnel ,,	1	an han to put a trained had be			
114	14	Patchway Tunnel ,,	-				
115	73	Patchway ,,	4.16				
117	22	Stoke Gifford ,,	4.19				
119	10	Winterbourne ,,	4.231				
120	40	Coalpit Heath ,,	4.27				
121	67	Westerleigh West Junc- ,, tion Signal Box	4.32	A Tarana an again			
122	15	STABLING POINT arr. (opposite the redundant Westerleigh North Junetion Signal Box.)	4.37	For detailed instructions for dealing with the "Deedene" Train at Stabling Point, see Bristol Distri. Operating Superintendent's Notice.			

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# TIME TABLE OF "DEEPDENE" TRAIN FROM WESTERLEIGH STABLING POINT TO FILTON JUNCTION STATION, FRIDAY, 6th NOVEMBER, 1953.

The Engine to carry EXPRESS PASSENGER TRAIN HEAD CODE.

The Train will carry TWO TAIL LAMPS from Stabling Point to Filton Junction Station. (Important—see paragraph 2 of R.E. Circular O/RR, dated December, 1950.) The Engine Head Lamps and the Tail Lamps must be lighted before leaving Stabling Point.

#### WORKING OF TRAIN ENGINE.

The Engine (No. 5000) to work the "Deepdene" Train from Stabling Point to Filton Junction Station, to leave Bath Road Shed, Tender leading, at 3||50 a.m. for Stabling Point, arrive 4||43 a.m. To be attached to Van Third No. 26287 and steam heat the train until departure at 10.0 a.m.

#### FORMATION $(FR\ OM\ ENGINE)$ LEAVING STABLING POINT :-

L.M.R. Stock.	(VAN THIRD (Brake End Leading) SALOON (Kitchen End Leading)	 9	No. 26287 ,, 45000	
	SALOON (Principal Door Trailing)		,, 45005	Approximately 183 tons
Deoca.	SALOON (Kitchen End Trailing) BRAKE FIRST (Brake End Trailing)	 	,, 45006 ,, 5154	
	Length of Train (excluding Engine)	 309 f	eet 10 inches.	

The distance from the centre of the footplate of the engine to the centre of the principal door of Saloon No. 45005 is approximately 206 feet  $7\frac{1}{2}$  inches. The distance from the centre of the principal door of Saloon No. 45005 to the buffer face of the rear coach (Brake First No. 5154) is approximately 127 feet  $8\frac{1}{4}$  inches.

## THE PERMANENT AND TEMPORARY SPEED RESTRICTIONS MUST BE STRICTLY OBSERVED.

Distances from Stabling Point.		PRINCIPAL STATIONS AND INTERMEDIATE SIGNAL BOXES.	TIMES.	REMARKS.				
Mls.	Chs.	STABLING POINT dep.	a.m. 10. 0	For detailed instructions for dealing with the "Deep- dene" Train at Stabling Point, see Bristol District Operating Superintendent's Notice.				
-	28	Westerleigh West June- pass tion Signal Box	10. 3	A good of PROPER WHITE AND THE LAND				
1	55	Coalpit Heath ,,	10. 6	And the Stone at the Control of the Control of				
3	5	Winterbourne ,,	10. 9	AT 15th Athensis one estimation and explosion of				
4	73	Stoke Gifford,	10.14					
6	4	FILTON JUNCTION arr. STATION (Down Main Platform)	10.20	For detailed instructions for dealing with the "Deep- dene" Train at Filton Junction Station, see Bristol District Operating Superintendent's Notice.				

#### WORKING OF EMPTY L.M.R. STOCK-FRIDAY, 6th NOVEMBER, 1953.

#### FILTON JUNCTION STATION TO DR. DAY'S SIDINGS, BRISTOL.

# THE PERMANENT AND TEMPORARY SPEED RESTRICTIONS MUST BE STRICTLY OBSERVED.

PRINCIPAL STATIONS AND INTERMEDIATE SIGNAL BOXES.		TIMES.	REMARKS.
Filton Junction Station (Down Main Platform)	dep.	a.m. 10†26 <b>RL</b>	To carry "A" Head Code and be signalled accordingly. To be given a clear run. To carry one Tail Lamp from Filton Junction. Engine No. 5000. For detailed instructions for dealing with the Empty
Stapleton Road	pass	10†34	Train at Filton Junction Station, see Bristol District Operating Superintendent's Notice.
Dr. Day's Sidings	arr.	10†40	For detailed instructions for dealing with the Empty Train at Dr. Day's Sidings, see Bristol District Operating Superintendent's Notice.

The L.M.R. coaches (five) to be conveyed "light" formed next engine of the 1.50 p.m. ordinary train from Bristol (T.M.) to Paddington (due 4.10 p.m.) and Empty Stock thence from Paddington Arrival Side (dep. about 4†25 p.m.) to Old Oak Common. Special Empty Stock train with L.M.R. coaches (five) to run at following times:—

Class " A " Head Code.			arr.	dep.	
			p.m.	p.m.	6th November.
Old Oak Common Yard	 			7†20	
Old Oak Common East	 			7†30	
North Pole Junction	 		7†	33	
Kensington (Olympia)	 		7140 EG	N 7†53	N—Reverse.
North Pole Junction	 		8†	0	For Wolverton, via Willesden
		100			Junction.

L.M.R. Engine and Guard to work forward from Kensington (Olympia).

L.M.R. Van Third No. 26287 to be gangwayed to Main Train at Dr. Day's Sidings, but corridor doors to be kept locked during journey from Bristol (T.M.) to Paddington and Old Oak Common.

The regular Compo and 2 Thirds normally formed next behind Buffet Car on 1.50 p.m. Bristol (T.M.) to Paddington not to work on this date. Load 12= approximately 415 tons Bristol to Reading, and Load 11= approximately 380 tons Reading to Paddington.

For detailed arrangements, see Bristol and London District Operating Superintendents' Notices.

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# TIME TABLE OF 4.15 p.m. BRISTOL (T.M.) TO PADDINGTON—TO RUN AT AMENDED TIMES UNDER "DEEPDENE" INSTRUCTIONS—BRISTOL (T.M.) TO PADDINGTON—FRIDAY, 6th NOVEMBER, 1953.

The Engine to carry EXPRESS PASSENGER TRAIN HEAD CODE.

The Train will carry TWO ORDINARY TAIL LAMPS from Bristol (T.M.) to Paddington. (Important—see paragraph 2 of R.E. Circular O/RR, dated December, 1950.) Slip Tail Lamps will not be carried. The Engine Head Lamps and the Tail Lamps must be lighted before leaving Bristol (T.M.).

The Train will run on the Main Line throughout the journey.

#### WORKING OF TRAIN ENGINE.

E

The Engine (No. 6001) to work the "Deepdene" Train from Bristol (T.M.) to Paddington to leave Bath Road Shed at 2||55 p.m. for Malago Vale to be attached to coaches for steam heating prior to departure with stock at 3†35 p.m. for Bristol (T.M.) arrive 3†40 p.m.

FORMATION (FROM ENGINE) LEAVING BRISTOL (T.M.) :-

For Royal	BRAKE	COM	PO* (B	rake E	nd Lead	ing)	 	No. 7377	
Party.	HSALOON			itchen	End Le	ading)		,, 9001	
	VAN TH	IRD	(Brake	End I	Leading)			,, 0001	
	FIRST						 		
n 1	FIRST						 		Bristol
Regular	FIRST						 		(T.M.)
Coaches.	BUFFET	CAR					 		to
Ordinary	THIRD						 		Paddington.
Passengers.	THIRD						 		
r assengers.	THIRD						 		
	THIRD						 		
	VAN THI	RD.	Brake	End T	railing)		 		
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\* Tables to be fitted in all compartments.

H-To be gangwayed to Main Train, but corridor doors to be kept locked.

NOTE.—The Regular Bristol to Reading Slip Coach not to be attached. Train to call specially at Reading to set down and to carry Two Ordinary Tail Lamps throughout. Clause 31 of the Slip Carriage Working Instructions will not apply.

Load 13 Coaches = approximately 455 tons (not to be exceeded).

The distance from the centre of the footplate of the engine to the centre of the principal door of Saloon No. 9001 is approximately 150 feet 7 inches.

# 4.15 p.m. BRISTOL (T.M.) TO PADDINGTON (AMENDED TIMES). THE PERMANENT AND TEMPORARY SPEED RESTRICTIONS MUST BE STRICTLY OBSERVED.

Distances from Bristol (T.M.).		PRINCIPAL STATIONS AND INTERMEDIATE SIGNAL BOXES.	TIMES.	REMARKS.	
Mls.	Chs.	BRISTOL (T.M.) dep. (No, 9 Platform)	p.m. 4.15	For detailed instructions for dealing with the "Deep- dene" Train at Bristol (T.M.), see Bristol District Operating Superintendent's Notice.	
-	62	North Somerset Junction pass	-	Participant of the state of the	
1	31	Bristol East Depot,	_	The Land State State of the Sta	
1	52	St. Anne's Park ,,	1 kin		
1	60	Fox's Wood Tunnel No. 2 ,,	-		
2	$3\frac{1}{2}$	Fox's Wood Tunnel No. 3 ,,	_		
3	63	Water Troughs ,,	_	No Down train must be in the section from Keynsham	
4	46	Keynsham,	_	West to Fox's Wood whilst the "Deepdene" Train is passing through the section from Fox's Wood to Keynsham West.	
6	43	Saltford Tunnel "	_		
7	0	Saltford "	_		

# TIME TABLE OF 4.15 p.m. BRISTOL (T.M.) TO PADDINGTON (AMENDED TIMES), FRIDAY, 6th NOVEMBER, 1953—continued.

Distances from Bristol (T.M.).		PRINCIPAL STATION AND INTERMEDIAT SIGNAL BOXES.		TIMES.	REMARKS.	
Mls.	Chs.			p.m.	Committee of the second second second	
9	13	Twerton Tunnel .	. pass	-		
10	35	Oldfield Park	. ,,	-		
10	79	Bath Goods	. ,,	-	SHIP SHIP SHIP SHIP SHIP SHIP SHIP SHIP	
11	38	Bath Spa <	dep.	4.32 4.36		
11	75	Sydney Gardens W. Tur	-	_		
12	01	Sydney Gardens E. Tu	m	BOTES	C NIVARI PROPERTY OF THE PROPE	
13	59	Bathampton				
16	604	Middle Hill Tunnel .	. "			
			. "			
17	30	Box Tunnel	. ,,			
24	32	Chippenham	• ,,	4.55		
35	38	Wootton Bassett .	. "	-	The 4.35 p.m. Chippenham to Swindon must work punctually to Wootton Bassett, and after detraining passengers at Wootton Bassett, it must be back shunted to the Up Avoiding Line at Wootton Bassett. When the train has come to a stand inside the catch point the latter must be restored to the open position. The 4.35 p.m. Chippenham to Swindon must not be allowed to leave Wootton Bassett East until the "Train out of Section" signal has been received by	
				der er	Wootton Bassett East from Hay Lane for the "Deep- dene" Train.	
36	66	Studley Signals	. ,,	_	Intermediate Block Signals (Colour Light)—Up Distant	
41	4	Swindon Junction .	. "	5,13½	and Up Home.  The 4.43 p.m., Minety to Swindon must work punctually and run to No. 8 Platform at Swindon.  The 3.50 p.m. Cheltenham to Paddington to run to No. 5 Platform at Swindon with Points Nos. 33 and 69 set for the Up Loop.  The 3.40 p.m. (Auto) Westbury to Calne The 3.40 p.m. Bath Spa to Chippenham	
	400				The 3.55 p.m. Bristol (T.M.) to Melksham punctually.	
61	66	Steventon	٠,,	$5.32\frac{1}{2}$	AUTOIR S	
64	52	Foxhall Junction .	. "	ML	The 12.0 noon Minehead to Paddington. The 12.30 p.m. Wolverhampton to Paddington. The 11.10 a.m. Milford Haven to Paddington, The 4.20 p.m. Swindon to Paddington	
65	18	Didcot	. ,,	$5.35\frac{1}{2}$		
69	71	Cholsey & Moulsford .	. ,,	-		
71	61	South Stoke Signals .	. ,,	_	Intermediate Block Signals (Colour Light)—Up Distant	
73	48	Goring & Streatley .	• ,,	_	and Up Home.  No Down train must be in the section from Pangbourne to Goring & Streatley whilst the "Deepdene' Train is passing through the section from Goring &	
74	48	Water Troughs	. ,,	_	Streatley to Pangbourne.	
75	11	Basildon Signals .	. ,,	_	Intermediate Block Signals—Up Distant and Up Home	
76	65	Pangbourne	. ,,	_		
78	18	Purley Signals	,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,		Intermediate Pleak Simple Up Distant and P. W.	
		Tilehurst	. "		Intermediate Block Signals—Up Distant and Up Home	
79	57		. ,,			
81	30	Reading West Junction	"	- 1		

# TIME TABLE OF 4.15 p.m. BRISTOL (T.M.) TO PADDINGTON (AMENDED TIMES), FRIDAY, 6th NOVEMBER, 1953—continued.

fro Bri	ances om stol	PRINCIPAL STATIONS AND INTERMEDIATE SIGNAL BOXES,	TIMES.	REMARKS.
Mls.	Chs.	D 3: M : I: W-4	p.m.	
82	30	Reading Main Line West pass  Reading General { arr. dep. }	5.53 5.55	To set down passengers only.  The 4.46 p.m. Aldermaston to Paddington must work
		Platform)		punctually.  The 4.20 p.m. Swindon to Paddington to leave Reading at advertised time, 5.30 p.m.  The 5.20 p.m. Didcot to Reading must work punctually and be cleared promptly from the Up Relief Line Platform at Reading General.  The 3.50 p.m. Cheltenham to Paddington to run via the Relief Line from Reading Main Line West and be dealt with at the Up Relief Line Platform at Reading General. To run via the Main Line from Reading Main Line East.
87	27	Twyford pass	6. $2\frac{1}{2}$	
94	9	Maidenhead "	6. 9	
99	72	Slough ,,	6.141	The 4.20 p.m. Swindon to Paddington to leave Slough at advertised time, 5.55 p.m.
105	10	West Drayton ,,	-	The 5.45 p.m. Paddington to High Wycombe must work punctually to West Drayton. To run via the Relief Line from West Drayton East, preceding the 5.33 p.m. Paddington.  The 5.48 p.m. Uxbridge to Paddington (Suburban) must work punctually.
109	22	Southall ,,	6.23	Between Southall West Junction and Hanwell Absolute Block Working by Bell must be applied to the signalling of the train preceding the "Deepdene" Train and for the "Deepdene" Train itself in accordance with Regulation 15 of the Regulations for Train Working on double lines where Block Apparatus is not provided in Multiple Aspect Signalling Areas—Notice L.K. 245. The 5.48 p.m. Slough Depot to Paddington (Suburban) to run via the Relief Line to Subway Junction, closely following the 5.48 p.m. Uxbridge and preceding the 6.12 p.m. Auto Greenford to Ealing Broadway.
112	52	Ealing Broadway "	-	
115	43	Old Oak Common East ,,	-	
117	8	Westbourne Park,	6.31	
118	28	PADDINGTON arr. (No. 8 Platform)	6.35	For detailed arrangements for dealing with the "Deep- dene" Train at Paddington, see London District Operating Superintendent's Notice.

#### TRAIN OPERATING.

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The Trains shewn in this Notice must be reported from the usual reporting points to the District Operating Superintendent, who, in turn, must take steps to see that the information is passed forward to the Operating Superintendent's Office, Paddington.

Operating Superintendent,
PADDINGTON STATION.

3rd November, 1953. (T.39,620.G.)

### **BRITISH RAILWAYS**

(WESTERN OPERATING AREA.)

#### DIESEL ELECTRIC ENGINE P.W.M.650.

The Civil Engineer's Diesel Electric Engine No. P.W.M.650 may operate under the following instructions:—

#### General

- (I) It is Engineering Department plant and is to be used for Engineering purposes only under the general supervision of the Crane Relaying Inspector.
- (2) It will be operated by a man appointed by the Engineering Department who will be instructed by a Motive Power Department Headquarters Inspector in rules applicable to a driver employed by the Motive Power Department so that when operating in sidings he can follow the normal procedure applicable thereto.
- (3) It will operate track circuits.
- (4) The maximum permitted speed under its own power is 20 m.p.h. and when conveyed with gear disengaged 25 m.p.h., and in neither case may it travel more than 25 miles without intermediate stop for examination.

#### Movement (5) and Operating Instructions

- Machine P.W.M.650 normally is only permitted to work on lines completely occupied by the Civil Engineer or in Engineering Department Sidings and in such circumstances the Engineering Department Operator will operate to the instructions of the groundman or supervisor who will be required to see the track is clear and points correctly set for the movement. In the above circumstances a Motive Power Department conductor is not required.
- (6) (a) It may be permitted to travel under its own power on running lines and sidings by prior arrangement with the Operating and Motive Power Departments, but must in all such instances, in addition to the Engineering Department Operator, be accompanied by a conductor provided by the Motive Power Department who will be responsible for carrying out all protective rules.
  - (b) As a light engine under its own power in charge of an Engineering Department Operator with a Motive Power Department conductor. To be dealt with as a light engine and signalled "G" headcode (2-3).
  - (c) Working, in charge of an Engineering Department Operator, with a Motive Power Department conductor, under its own power, an Engineering Department train with freight brake van and guard. To be signalled as a ballast train under "H" headcode (I-4) conditions.
  - (d) Under its own power, coupled to a track relaying crane to an occupied section in charge of an Engineering Department Operator with a Motive Power Department conductor. A freight brake van and guard to be provided unless otherwise decided by the Engineering Department representative In charge. To be signalled as a ballast train requiring to stop in section "K" headcode (2-2-3).
  - (e) Hauled by a steam locomotive, with the gear disengaged with freight brake van and guard. To run under "H" headcode conditions and be signalled (1-4). To be accompanied by an Engineering Department representative.
  - (f) When forming part of an ordinary freight train it must be marshalled next in front of the rear freight brake van. To be restricted to "H" and inferior headcode trains and signalled accordingly. To be accompanied by an Engineering Department representative.
  - (g) When forming part of an Engineering Department special train at the discretion of the Engineering Department representative in charge, may be marshalled in any position and to be accompanied by an Engineering Department representative.

- (7) On a Permissive line when running under its own power, the next following train admitted to such line must be brought to a stand at the Signal Box and the driver verbally told to proceed with caution and that he must take care not to buffer up to the machine.
- (8) When in a siding unattended it is to be left with gear disengaged, with the brake on, and the switches are to be set and clipped to prevent any movement to the siding, or a wheel stop placed in position in order to protect the plant from contact with any shunting movements in the siding.
- (9) All shunting movements with the machine must be carried out with care and in every case the machine must be accompanied by an Engineering Department representative.

M. G. R. SMITH.

Civil Engineer.

W. N. PELLOW.

Motive Power Superintendent.

GILBERT MATTHEWS.

Operating Superintendent.

B.R.361/5.

PADDINGTON STATION. August, 1953.

# THE RAILWAY EXECUTIVE

### PROCEDURE TO BE ADOPTED

and

### STANDARD INSTRUCTIONS

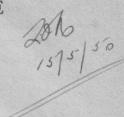
in connection with

TRAIN AND TRAFFIC CONTROL

BR 87223

#### THE RAILWAY EXECUTIVE

(BRITISH RAILWAYS)



### PROCEDURE TO BE ADOPTED

and

### STANDARD INSTRUCTIONS

in connection with

TRAIN AND TRAFFIC CONTROL

#### THE RAILWAY EXECUTIVE

#### (BRITISH RAILWAYS)

#### FOREWORD

In the interest of uniformity throughout the Regions forming British Railways and in conformity with the integration of practices which is one of the essential features of nationalisation, consideration has been given to the adoption of a standard procedure for Train and Traffic Control and this Booklet has been compiled which sets out the basic principles to be observed in the endeavour to maintain efficient train and traffic operating. The fundamental principles outlined will be supplemented by Instructions local to the District and these will be issued by the respective Regional Operating Superintendents.

The primary duty of a Control Office is to ensure that effect is given to the principles defined. As regards train control, there would be no necessity for a Control organisation if the service given completely followed the service scheduled, but all practical railwaymen appreciate that this ideal can never materialise, but it is hoped that, as the re-organisation and integration of the railway service continues, we shall, at least, get nearer to this objective.

The Control organisation represents a service to provide for "a place for everything and everything in its place"; the former calls for accurate and practical timing and diagramming and the latter for a good organisation primarily to ensure adherence to the pre-arranged plan but also, in the event of untoward incidents, to take such action as will ease the immediate difficulties until the service is once more back to normality and to provide for adjustments in the booked service to meet fluctuations in flows of traffic.

The danger in the past has been the tendency for the Control Organisation to degenerate into a recording agency and insufficient time has been given to the study of the train service and its regulation both from the train and traffic viewpoint; in particular, regard should be paid to the clearance of yards and the most effective use of motive power and train crews.

Whilst records must be maintained, this is not the primary function of the Control Office. The records of the daily performance throughout the week in a particular district are now readily available by use of the weekly card and this record should be studied day by day with a view to special steps being taken to eradicate any consistent bad working.

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The Control organisation, which will operate continuously on weekdays and, when necessary, on Sundays, is, in effect, the "watch dog" of the District Superintendent; this Officer, who should be regarded as the Chief Controller, must be served faithfully by three officials designated "Deputy Chief Controllers" directly responsible to their District Officer and deputising for him during their shifts of duty.

The principal duties of the staff operating the District Control Office are as follows:—

#### (i) Deputy Chief Controller

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The senior man in charge of each shift in the Control is called the Deputy Chief Controller.

The duties of the Deputy Chief Controller are to ensure that in accordance with the District Operating Superintendent's direction, the objects of control as defined in the Standard Control Instructions are achieved, to supervise the work of the Control staff, and to deal personally with all important matters.

#### (ii) Section Controllers

Each District is sub-divided into suitable territorial sections, due regard being paid to the intensity and flow of traffic and the avoidance of unnecessary exchange of information between sections.

It will be the practice to divide control offices into sections of a size which can be adequately administered by a Section Controller who does his own recording.

The Section Controller should fulfil all the defined train and traffic control functions on his section, collaborating with the Trainmen's Relief Controllers about the matters which are their concern, and referring, when necessary, to the Deputy Chief Controller on important matters.

#### (iii) Trainmen's Relief Controllers

The Controllers responsible for watching the hours of trainmen, providing relief when necessary, and arranging the supply of men for special working, are designated "Trainmen's Relief Controllers."

In many districts the Trainmen's Relief Controllers will deal also with the district responsibility for unbalanced power; where there is a large amount of this work the appointment of a Motive Power Controller may be necessary.

#### (iv) Assistant Controllers.

In most districts Assistant Controllers are appointed to do the routine work, such as the exchange of train information with other districts, recording the particulars of engines and trainmen, the collection from the Section Controllers and the transmission to Headquarters of traffic stock summaries and selected train running records, preparing and filing train cards and records and relieving for meal breaks.

#### (v) Office Relief Controllers

The number of staff under this heading will be determined by the number of positions to be covered and the calculated needs for relieving the various grades to cover holidays, rest days, sickness and vacancies.

These instructions define the standard principles and practices that are to be followed, and it is hoped that all concerned will do their utmost to make the Control Organisation a real and vital force in the operation of British Railways. At the same time the Control staff must appreciate that the measure of their success will largely depend upon the degree of co-operation which they receive from the outside staff. To achieve this co-operation and the enthusiasm of the outside staff to the maximum degree, the Control staff will, in normal circumstances, leave the initiative in the hands of the outside staff, so that the fullest use may be made of their skill, ability and experience. The outside staff, will, in turn, look to the Control staff for guidance and leadership.

InBarinstin - Ward.

222, Marylebone Road, LONDON, N.W.1 HAIR AND TRAFFIC COSTROL DISTRICTORS

OBJECTS OF CONTRACT

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#### TRAIN AND TRAFFIC CONTROL INSTRUCTIONS

#### OBJECTS OF CONTROL

- I. The fundamental principle of efficient train and traffic operating is that the scheduled timings of trains in the working time tables, notices, etc. be maintained, the booked workings for Guards, Enginemen and engines be adhered to, and that the instructions relating to the classification, marshalling and loading of freight trains be complied with.
- 2. The main objects of Control are to maintain the booked arrangements to the maximum possible extent, to guide the working back to normal when out-ofcourse, and to modify the arrangements when necessary to meet fluctuations in traffic.
- 3. The Control in conjunction with other Operating and Motive Power staff engaged in train and traffic working will have the following general aims:—
  - (a) To ensure the expeditious working of traffic including empty stock.
  - (b) To plan and organise the current working of Passenger and Freight trains so as to avoid delay.
  - (c) To obtain the maximum work from engine power and trainmen by :-
    - (i) Punctual working.
    - (ii) Using the fewest locomotives possible.
    - (iii) Securing the maximum authorised loading.
    - (iv) Incurring the minimum amount of light mileage or unrequired assistance.
    - (v) Releasing engines promptly after completion of work.
    - (vi) Making the best use of unbalanced engines.
  - (d) To regulate the working of trainmen to ensure economical working and avoid excessive hours.
- 4. All staff associated with the working of trains and the movement of traffic must carry out instructions given from the Control and must co-operate at all times to the fullest extent, by information, consultation and suggestion to overcome difficulties.
- 5. Station Masters, Yard Masters, Supervisors and the Staff under them are not relieved of their responsibility and must use their initiative in carrying out laid down arrangements.
- Departures from scheduled arrangements and the provision of power and trainmen, for any purpose other than booked, must be made through the Control.

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#### TRAIN CONTROL

7. The detailed regulation of trains in running must be carried out by the Signalmen (or Regulators) from information received in regard to the actual running, the working at their signal boxes and their knowledge of the requirements of the line ahead, taking into consideration the margins available for each type of train.

Signalmen will when necessary receive instructions from the Control as to ultimate requirements, thus assisting them in carrying out the detailed regulation. In certain circumstances, however, the Control Staff will give definite instructions to the Signalmen on questions of regulation.

Signalmen or others responsible for regulation must consult the Control in all cases of difficulty.

8. When necessary and in conjunction with other Control Offices concerned, the Control will make arrangements for the alteration or cancellation, of booked freight trains and the running of special trains, due regard being had to requirements for balancing or return working.

In emergency the Control will modify the working of passenger trains.

The Control will be expected to make suggestions for altered working and, to assist in this direction, current copies of the graphical train diagrams will be made available for the use of the Control.

- 9. The loading and movement of Freight trains, including Departmental trains, and the movement of specified Passenger trains falling within the following categories must be reported to the Control as shown in the local instructions:—
  - (a) Certain main line passenger trains.

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- (b) Important connecting passenger trains.
- (c) Suburban trains operating in particular localities.
- 10. Late starts, delays, loss of time in running, with the cause, must be reported currently to the Control by the points at which delay takes place except where a modification or exemption is granted. When engines are late off shed, steps must be taken in collaboration with the Motive Power Depot to adjust the immediate arrangements and subsequent working as may be deemed necessary.
- II. All orders for special Departmental trains and light engines will be received in the Control. Special power may be unnecessary, in which event the Control must arrange suitable transits by booked services in agreement with the Department concerned. When Departmental trains are working on the Line, the Control must co-operate with the Signalmen or other Staff in order that the maximum facilities may be given.

#### TRAFFIC CONTROL

- 12. The detailed position of traffic on hand must be telephoned to the Control in accordance with local instructions.
- 13. When traffic is stabled at places not specified in the local instructions for reporting traffic on hand, the person stabling the traffic should advise the Control of any wagons containing perishable or other important traffic. In addition, the Station Master or other person in charge must, at intervals, direct the attention of the Control to such traffic until it is cleared.
- 14. The Control must be advised also, at other than the times specified, of exceptional quantities of freight traffic passing or about to pass, and when it is seen that there will be a surplus or shortage of traffic for any of the booked services, so that the necessary arrangements can be made.

Urgent special freight traffics, such as livestock, meat and fruit, for which no previous arrangements have been made must be advised to the Control so that suitable services can be arranged.

- 15. When it is necessary to stop or restrict temporarily the flow of any particular traffic, the Control will make the appropriate arrangements.
- 16. The Control must be advised of exceptional numbers of passengers for regular or special services.

#### USE OF MOTIVE POWER

- 17. The fundamental principle of the use of motive power is the preparation and maintenance of efficient engine diagrams. Adherence to the diagrammed workings to the maximum possible extent on the part of the Motive Power Depots and Controls is essential.
- 18. Motive Power Depots will give immediate advice to Control of anticipated unavoidable departures from diagrammed engine workings and Control must advise the Motive Power Depot as soon as it can be foreseen that a special engine is required, or a booked engine will be cancelled, or that an engine cannot reach the Motive Power Depot in time to be got ready for its next booked working. Full consultation between the Motive Power Depots and Control must take place in all matters of this nature, in particular no work must be added to the booked diagrammed work of an engine without prior agreement with the Motive Power Depot.

Cancellations of freight trains owing to non-availability of locomotives, also the total number and extent of late starts owing to late provision of locomotives must be recorded by the Control currently for each 24 hours.

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19. Engines must not be used on workings allocated to any Depot other than that to which the engine belongs without authority from the Control.

20. Unbalanced engines must be closely supervised by the Control and it will be their responsibility to arrange in advance for such engines to be returned to their home depots, as quickly as possible, in the most productive and economical way.

21. Engines belonging to a Motive Power Depot in one operating district working into another district must be treated as 'foreign' and their use when unbalanced must be at the directions of Headquarters Control to whom the position must be reported by the District Controls.

Engines belonging to a Motive Power Depot in a particular operating district and working within that district should be treated as 'local' and their use when unbalanced must be at the direction of the District Control except where instructions are issued to the contrary.

22. Unbalanced engines on hand must be reported to the Control currently; for those under repair, an advice as to when they will be ready for work must be given as soon as this is known.

At a stipulated time each day Motive Power Depots must advise the Control the power position which must include information in regard to engines away from the Depot without booked or pre-arranged return working, unbalanced engines on hand, and other Depots' engines on hand under repair.

23. At Headquarters, and in Districts where the volume justifies it, the system of recording unbalanced engines and the use made of them, should be by card index. In Districts, where the volume of work does not warrant such a system, the necessary records will be made on suitable forms.

### CONTROL OF TRAINMEN

24. It is the responsibility of the Control to see that Enginemen and Guards do not incur excessive hours. Particulars of the booking on times and home stations of trainmen reported by Motive Power and Goods Guards Depots must be recorded by the Control; similar details must be given as to trainmen available for relief purposes, spare or becoming spare.

The Control will use the available staff to the best advantage by anticipating requirements and will record the use made of the relief staff.

Trainmen, when relieved, must be informed where practicable by what means they must proceed to the signing off point.

Particulars of trainmen travelling as passengers to another district must be advised to the Control concerned.

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#### DISTRIBUTION OF FREIGHT BRAKE VANS

25. The distribution of freight brake vans must be based on a planned allocation arrived at by balancing incoming and outgoing vans for every yard and station where vans are required for freight train working.

The allocation represents the minimum number of freight train brake vans required to be on hand at any yard at 8.0 a.m. each day to cover the working for the next 24 hours after an allowance has been made for the number of brake vans to arrive on incoming trains (with marginal allowance for late running based on the average working) and for special and departmental trains.

The total of the yard and station allocation is the allocation for the District at 8.0 a.m.

- 26. The actual number of freight brake vans on hand at all points, whether there is an allocation or not, must be recorded at 8.0 a.m. daily including those due to arrive by 8.0 a.m. but running late, provided it is known they are en route but excluding the following:—
  - (i) Crippled brake vans.
  - (ii) Brake vans which should have left by 8.0 a.m. but whose departures have been delayed.
  - (iii) Brake vans on trains at intermediate points on their journey.

The number of brake vans recorded on the foregoing basis and the number short of, or surplus to, the 8.0 a.m. allocation, must be reported to the Control.

- 27. The District Officer will arrange with the principal C. & W. Shops to report daily at 8.0 a.m. the number of freight brake vans which it is anticipated will will be ready for use that day.
- 28. The daily freight brake van position for the District, which will be arrived at by taking the number of brake vans on hand and comparing it with the allocations, must be telephoned promptly to Headquarters Control.

The shortage or excess of brake vans must be adjusted between yards by Control and between Districts by Headquarters; all disposal instructions so given must be fully and promptly carried out.

When instructions are received from Headquarters that freight brake vans must be sent to another District, the number of vans forwarded must be recorded together with particulars of despatch.

29. Cancellation of freight trains also the total number and extent of late starts waiting for goods brake vans during the previous twenty-four hours must be recorded.

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#### RELEASE OF COAL AND COKE FOR SHIPMENT

- 30. Except in those cases where free movement from pit to port is authorised, coal and coke for shipment must not be worked forward until released by the District Control.
- 31. The designated responsible person at the port, after ascertaining the requirements of the shipping agents and after taking into account the availability of vessels and the amount of coal already on hand to meet demands, should advise the District Control of the release with the following information:—
  - (i) Originating colliery or works.
  - (ii) Description and tonnage of coal or coke.
  - (iii) Name of vessel or shipping agent.
  - (iv) Date and time required at port.
- 32. The District Control will release the traffic from the originating points in that District and arrange for it to be worked forward to the port.

Traffic arising in a district other than the one in which the port is located will be released by the District Control advising the appropriate originating District Control.

A record must be kept of the releases and of the time each consignment is worked from the originating point and the service given.

#### GENERAL INSTRUCTIONS

33. Prompt attention to the telephones and accuracy and conciseness of messages to and from the Control are essential.

On telephone circuits not reserved exclusively for control purposes, messages to and from the Control must have precedence over all others excepting those between Signalmen in regard to the running of trains.

34. Adverse weather conditions such as fog or snow, likely to give rise to difficulty, and subsequent developments must be reported to the Control. Stations calling out fogmen are to advise the time they are called, on duty, and recalled.

Accidents, engine failures or other occurrences causing, or likely to cause, interruption to train or traffic working must be reported immediately to the Control who in turn must advise Headquarters Control.

In the event of accidents, adverse weather conditions or other occurrences making it necessary, the Control must arrange to curtail, divert or cancel trains or reduce loads to minimise, to the greatest extent possible, the adverse effect upon the working.

Control must also inform all stations and neighbouring Districts affected so that intending passengers can be acquainted. Alternative road services must be arranged where necessary.

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#### CONTROL PROCEDURE

35. The running and working of freight trains must be recorded on weekly cards of the approved design except for freight trains which have not a regular daily working when a one day card may be used.

In certain cases, working of a local character, usually confined to short distance working within one control section, may be recorded on daily sheets.

The movement of passenger trains, where reported, must be recorded on daily sheets of the approved design.

- 36. The position of freight trains should be indicated currently on the geographical boards provided.
- 37. In the case of inter-district trains, the necessary particulars including those relating to the engine number and trainmen, must be passed promptly to the next Control.
- 38. Reports as to traffic stocks, line position and the general situation in each District must be given periodically as required.

The Headquarters Control will co-ordinate the working between districts giving decisions on matters affecting inter-district working and will take the initiative in overcoming difficulties which arise, including serious difficulties local to a district.

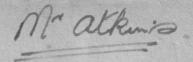
The District Control must therefore consult Headquarters Control freely and obtain their consent before making alterations to inter-district train working, such as alterations to timing, marshalling and classification, cancelling of booked trains and the running of specials.

In the case of alterations to regular inter-district freight trains or the running of special inter-district freight trains between two neighbouring districts, these can be arranged between these districts and the Headquarters Control be kept advised.

It will not be necessary to advise Headquarters Control in respect of alterations, etc. to the running of local trip trains over district boundaries.

- 39. Control must arrange through Headquarters Control, services for live-stock and other important traffic when the inter-district booked services do not meet requirements.
- 40. Any stop or restriction on traffic must be arranged through Headquarters Control.

Railway Printing Works



# BRITISH RAILWAYS (WESTERN OPERATING AREA)

# Alterations and Additions to the General Appendix to the Rule Book

To come into operation on receipt.

Reference to the following to be made on page 2:-

#### COLOUR LIGHT DISTANT SIGNALS.

Where a colour light Distant signal is placed below a semaphore stop signal, no light will be exhibited in the Distant signal when the semaphore arm above it is in the danger position, but a yellow or green light will be exhibited when the semaphore arm is in the clear position.

At night no green light will be exhibited by the semaphore signal when placed to the clear position.

(G.A.30 Op.—9/45 L.65733/33)

#### ADDITIONS TO THE STANDARD RULES-Page 14.

The following to be inserted on page 14:-

#### Signal Post Signs.

All concerned to please note the following new signal post signs will be exhibited where applicable:—

Letter "T" in black on white diamond sign.

Letter "T" in black on white rectangular plate. Designation of signal in black figures on white rectangular plate. Fixed on signal to rear of which track circuit is provided and a telephone to signal box is provided in addition.

Telephone to signal box provided (no track circuit).

Provided in colour light signalling areas and for all colour light Intermediate Block Signals.

(G.A.30 Op.—9/54 L.72404/283)

#### Rule 35 clause (c).

The instruction shown under this heading to be amended to read:-

On the Western Region this clause will only apply to the Multiple Aspect Signalling area in the London District.

(G.A.30 Op.--9/54)

#### Rule 133 .- The amplification of this rule to be amended to read:-

Rule 133.—Tail lamps of passenger, empty stock, perishable, parcels trains and "C" Headcode freight trains assisted in the rear uncoupled need not be removed before ascending the incline.

(G.A.30 Op.-9/54 L.K.1/-)

#### Rule 149. exception vii.-Propelling Ballast Trains.-page 20.

The second paragraph of this instruction to be amended to read:-

The restriction in regard to propelling on falling gradients steeper than I in 200 may be withdrawn in the case of Engineers light inspection trains completely composed of vacuum stock coupled throughout also vacuum fitted Hopper ballast wagon trains equipped with fully vacuum fitted or piped brake vans, provided the Engineer has occupation of the line and also that there is a brake van at the end which is leading with someone riding in it who can apply the hand brake and/or the vacuum brake.

(G.A. 30 Op.—9/54 L.K.1/11729/365)

The following to be inserted as the fourth paragraph:-

Weed killing trains which are not fully vacuum fitted throughout or on which there are less than four fully vacuum fitted vehicles attached to the engine must not be propelled on falling gradients steeper than I in 260.

G.A.30 Op.—9/54 L.K.1/10661/417)

Rules 189 to 208 and 217 Arrangements during Permanent Way Operations and Signal Alterations.—page 21.

The following to be inserted as the third paragraph of the instructions under heading "Between Trains' Occupations":—

"A ballast train must not be set back into a section where intermediate block signals are provided."

(G.A.30 Op.-9/54 L.K.1/11874/420)

Rules 215-217 and 234-page 21:-

"Clause (h) of Rule 234" amend to read "Clause (i) of Rule 234."

(G.A.30 Op.—9/54)

Rule 240.—Conveyance by Goods Train of Explosives and Dangerous Goods.—page 22.

The amplification of Clause (9) to be cancelled.—See standard Rule 240 clause (9).

The General Note shown in supplement G.A.8 to be cancelled.—See standard Rule 240—last paragraph.

Clause (10). The existing amendment to be deleted and the following substituted:-

Rule 240.—Clause (10). The restriction on the number of vehicles containing explosives which may be conveyed by any one train at any one time to a maximum of five does not apply in the case of explosives conveyed on account of:—

- (i) The Government (Admiralty, War Office, Air Ministry and Ministry of Supply).
- (ii) A Trader for transit to a Government establishment.
- (iii) A Trader for transit to a Trader on account of the Government.

The marshalling arrangements laid down in the Rule must, however, be observed.

(G.A.30 Op .- 9/54 L.K.1/E)

Reference to the following to be made on page 73:-

#### TRAINS NOT COMPLETELY WITHIN FIXED SIGNALS.

- (a) In cases where-
  - (i) an outlet signal is provided to control movements from a siding to a running line, and
  - (ii) a shunting signal is provided to control set-back movements

Drivers must regard such signals as controlling the movement, although the engine may be standing on the wrong side of the signal, and must not move their engines until they are satisfied the signal is off. Where, however, the Driver cannot see the signal and the movement is accompanied by a Shunter, the latter must advise the Driver when the signal is lowered.

If for any reason the signal cannot be lowered, the Shunter or person in charge will be responsible for ascertaining from the Signalman that it is in order for the movement to be made and for satisfying himself that the points are in the proper position. In the case of a light engine unaccompanied by a Shunter, this duty must be carried out by the Fireman.

(b) On platform lines or other running lines when an engine is ahead of the signal controlling the starting of trains owing to the length of the train, a Driver must regard that signal as controlling his movement. When the Driver cannot see such signal, or back indication where provided, or the signal cannot be lowered owing to the engine occupying a track circuit or other apparatus which prevents the signal being lowered, the Driver must not proceed until he receives a green handsignal from the Signalman or verbal intimation to do so from the person acting under the instructions of the Signalman. The Driver must not be authorised to proceed until it has been ascertained that any points concerned have been correctly set.

In cases, however, where Absolute Block Working is not in operation and the signal has been lowered to enable the train to draw forward for station duties the signal must be placed at Danger in accordance with Rule 68a (ii) and (iii). In such circumstances the signal must not be regarded as controlling the further movement of the train. If it is necessary for the Signalman to allow a conflicting movement to take place ahead of the standing train he must not do so until the Driver of such train has been advised of what is about to be done. After the conflicting movement has been completed and when the train is ready to continue its journey, the Driver must not proceed until he has received a verbal intimation to do so from the Signalman or the person acting under the Signalman's instructions, in addition to the Guard's "right away" signal.

(c) When the fixed signals referred to in clauses (a) and (b) lead to more than one running line, the Driver should satisfy himself by observation which line he is travelling over, but the person in charge of the movement mentioned in clause (a) or the Signalman or person acting under his instructions as mentioned in clause (b) must, whenever practicable, also inform the Driver over which line he is about to travel.

(G.A.30 Op.-9/54 476/E)

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Reference to the following to be made on page 74:-

#### LIMITED CLEARANCE—WARNING TO STAFF.

The attention of all concerned is directed towards the need for exercising care when working at places where there is a restricted clearance between the running line or siding and adjacent structures, or between running lines and/or sidings. This applies to Footplate Staff and Guards as well as to staff working on the ground.

At certain places the limited clearance may be indicated by a Red and White chequered board bearing the words "Warning-Limited Clearance."

(G.A.30 Op .- 9/54 L.K.1/10483/364)

#### DEFECTIVE SIGNALS AND POINTS.—Page 74.

Delete the instructions under the above heading and substitute the following:-

#### Defective Signals and Points.

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If it is found that any signals or points do not respond to the working of the controlling lever or that it is not possible to correctly set up a route, the Signalman must first replace the levers and operate them again, provided it is safe for him to do so; if this is not successful it is probable that the cause is due to some hold up in the mechanism, such as a stone in the points or other obstruction in the wire or rodding. The Signalman should then endeavour to locate the fault and, if possible, remove the obstruction.

If the Signalman is unable to leave his box for the purpose of tracing the fault he should obtain assistance from a member of the station or yard staff.

Should the Signalman be unable to trace or remove the obstruction he must send for the

No attempt should be made by the Signalman to interfere in any way with electrical signalling

(G.A.30 Op.-9/54 476/E)

#### EMERGENCY OPERATION OF ELECTRIC POINT MACHINES.—Page 75.

The following to be inserted as the fourth and fifth paragraphs:-

The crank handle must not be restored to the circuit controller if the Signalman has given permission for a train to pass over the points, until such train has cleared the points.

When the failure has been rectified, and the points set in a position corresponding to the lever in the frame, i.e. normal or reverse, the crank handle must be placed in the circuit controller and a test made to ensure that the points are working correctly.

(G.A.30 Op.-9/54 O.M.12594)

#### AUTOMATIC TRAIN CONTROL SYSTEM IN USE ON THE G.W.R.—page 80.

The above heading amended to read:-

#### AUTOMATIC TRAIN CONTROL SYSTEM IN USE ON THE WESTERN OPER-ATING AREA.

The second paragraph of Clause 12 to be amended to read:-

The automatic train control system is in operation on the undermentioned sections of the line (double line sections except where otherwise indicated),

Paddington and Didcot (Four lines.) West Ealing and Greenford.

Henley Branch.

Reading and Penzance via Westbury.

Didcot and Newbury.
Thingley Junction and Bradford Junction.

Bathampton and Westbury

Castle Cary and Weymouth.

Newton Abbot and Paignton.

Didcot and Taunton via Bath.

Swindon and Severn Tunnel Junction via Gloucester.

Wootton Bassett and Bristol and Severn Tunnel Junction via Badminton.

Severn Tunnel Junction and Fishguard.

Skewen and Llandilo Junction. (Swansea District Line.)

Old Oak Common and Saltney Dee Junction via Birmingham.

Didcot and Aynho Junction.
Oxford and Wolverhampton via Worcester.

Worcester and Newport.

Fairford Branch. (Single Line.)

Handsworth Junction and Stourbridge Junction.

Tyseley and Gloucester.

Shrewsbury and Hereford.

(G.A.30 Op.-9/54 LKI/11041/232)

The following to be added at the end of these instructions on page 82:-

Action to be taken if bell indication received when the Distant Signal is at 'Caution.'

Should a Driver receive the bell indication "Proceed" at a ramp, but find that the Distant signal controlling the ramp is at "Caution" he must stop at the Signal Box to which the Distant Signal applies and Inform the Signalman who must advise the Signalman at the Signal Box in rear in order that subsequent trains passing through the section may be stopped and the Drivers advised that the A.T.C. ramp is defective. The Signalman first advised of the failure must promptly call the Lineman and the District Operating Superintendent or District Traffic Superintendent must be notified immediately.

The Distant Signal concerned must be maintained at "Caution" until the Signalman to whom the failure was first reported is advised by the Lineman that the ramp is in order when he must notify the Signalman at the Signal Box in rear that normal working may be resumed.

An entry must be made by each Signalman in his Train Register Book shewing the time the defective A.T.C. ramp is reported. The Signalman at the Signal Box to which the Distant Signal applies must upon being advised that the failure has been rectified notify the Signalman at the Signal Box in the rear. Each Signalman must then enter the time in the Train Register Book, and the entry in the Train Register Book at the Signal Box to which the Distant Signal applies must be countersigned by the Lineman.

(G.A.30 Op.-9/54 O.M./12652).

#### TRACK CIRCUITS.—Pages 82-83.

The following to be added at the end of these instructions:-

Sand-use of by Enginemen.

Enginemen must, as far as practicable, avoid the use of sand when standing on or passing over track circuits, lock bars, points or crossings. If, however, it is essential that sand be used in these circumstances the minimum quantity necessary should be used.

(G.A.30 Op.-9/54.)

The "General Instructions" at the end of these regulations amended to read:-

#### General Instructions.

Signalmen must watch the action of track circuit indicators each time a train or engine passes on to or off that portion of the line to which each indicator applies, and if it fails to give a correct indication, immediately advise the Lineman.

If an indicator fails to show when a train or vehicle is on the track circuit the lineman must be advised by the most expeditious means, and arrangements made for the vehicles to be examined as soon as possible and, except as shown below, they must be taken out of traffic and held until such examination has been made.

Should the vehicle or vehicles be loaded and undue delay would be caused to the contents, or in the case of an empty vehicle specially required for traffic, a wire must be sent to the destination or exchange station requesting that an examination be made there.

In every case of failure of a vehicle to operate a track circuit a detailed report must be sent to the District Operating Superintendent or District Traffic Superintendent from the Station at which the failure occurred.

An entry must be made in the Train Register Book of all irregularities in the working of track circuits.

2. In case of emergency during the time a track circuit is out of order, a lock may be released by the Lineman after an understanding with the Signalman, to allow a signal to be restored to normal, or points moved to either normal or reverse, on the written authority of the Station Master, Assistant Station Master or District Inspector.

An entry must be made in the Train Register Book and signed by the Signalman and Lineman.

- 3. In no circumstances must a lock be released to allow a signal to be lowered or a block instrument be released to allow "LINE CLEAR" to be given when the track circuit controlling either is out of order.
- Referring to Rule 83; if, in consequence of a derailment, permanent way operations, signalling alterations or other exceptional causes, one or more track circuits are occupied for an extended period, certain movements cannot be signalled even though they would be clear of the obstruction, the Lineman at the request, IN WRITING, of the Station Master, Assistant Station Master or District Inspector may, after satisfying himself that the obstruction is clear of the movement to be made, give the necessary release. The Signalman must make use of the lever collars or other similar devices where provided and points both facing and trailing which are locked by the track circuit concerned must be secured by clip an except huntil the movement is completed. circuit concerned must be secured by clip or scotch until the movement is completed.

#### A separate release must be given for each Movement.

After each movement has been complexed the Lineman must restore the full track circuit The time and details of the release and the restoration of the controls must be entered in the Train Register Book on each occasion and signed by the Signalman and Lineman and also the Station Master, Assistant Station Master or District Inspector as the case may be.

5. Enginemen must not throw cinders on track circuited portions of the lines.

6. An Engineer's trolley, motor trolley or velocipede must not be relied upon to operate track circuits and protection must be provided in accordance with the Rules or Instructions applicable.

7. Before a trolley is placed upon, or run over, any portion of a line that is track circuited, the permission of the Signalman in whose box the track circuit in indicated must be obtained.

(G.A.30 Op .- 9/54 LKI/-)

Reference to the following to be made on page 83:-

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#### Facing Points controlled by Track Circuits.—Emergency Release.

When track circuits are provided in lieu of Facing Point Lock Bars, in certain cases emergency releases will be provided and the following instructions must be carried out in the event of a failure of the track circuit:—

1. When the track circuit fails and the facing point lock lever cannot be restored to normal, the Emergency Release provided in the signal box must be used and the following instructions must be carried out by the Signalman in the order shown:—

(a) Satisfy himself by personal observation, or by assurance from a responsible person, that no track circuit controlling the Facing Point Lock lever is fouled by any vehicle and that it is in order for the facing points to be reversed or restored to normal as the case may be.

(b) Break the glass on the front of the box containing the release plunger.

(c) Operate plunger and restore F.P.L. lever to normal, thus releasing the points.

(d) The facing points can then be operated normally and when the F.P.L. lever is reversed the appropriate signal, if not locked by the track circuit which has failed, can be lowered, provided the detection is intact.

(e) An entry must be made in the Train Register Book with the time whenever the glass is broken. The entry must be signed by the Signalman, who must promptly report the circumstances to the Station Master and Lineman.

(f) While the track circuit lock is out of use it will be necessary to press the emergency plunger on each occasion when the F.P.L. lever is to be restored to normal.

(g) While the track circuit remains out of use a Handsignalman or competent man must be appointed as required by Rule 77(e).

(h) When the track circuit is out of use the Signalman must, before restoring the F.P.L. lever to normal, satisfy himself by personal observation, or by assurance from the Handsignalman or competent man, that any train or engine signalled to pass over the facing points has passed clear of same.

2. The Station Master must specially report to the District Operating Superintendent or District Traffic Superintendent every occasion on which the glass front has been broken and must see that it is renewed immediately normal working is resumed, and that the paper label fixed to the new glass bears the Lineman's signature and the date replaced. An entry must be made in the Train Register Book showing the time the track circuit is again in order and normal working resumed, and this must be signed by the Lineman.

3. The Emergency Release Plunger is for use in connection with the failure of the track circuit only.

(G.A.30 Op.—9/54 L66326/145)

### "THREE SHOT" DETONATOR MACHINE WORKED IN CONJUNCTION WITH TRAILING POINTS.—Page 86.

The third paragraph of the above instructions to be deleted and the following substituted:—

The Station Master must see that three fresh detonators are placed in the machine every month and a note to the effect that the detonators have been changed, must be made in the Train Register Book. Where the change is not made by the Signalman the person appointed to change the detonators must countersign the entry. The old detonators taken out of the machine in this way must be returned by the Station Master to the Stores Department on the 1st June and 1st December each year with the name of the sending station.

(G.A.30 Op.—9/54 LKI/10730/363E)

#### SLIP CARRIAGE WORKING .- Page 91.

The following additional paragraphs to be added to Clause 4.

(d) The rear vehicle of the main train to which the slip coach is to be attached and from which it is to be slipped must always be a Western Region or B.R. Standard stock type vehicle. Other Region's vehicles are prohibited from working as the rear vehicle of the Main Train and if it is necessary for such vehicle to be conveyed on the train it must be marshalled inside the rear Western Region or B.R. standard vehicle of the Main Train or arrangements must be made for the train to stop at the Slipping Station. (e) When a B.R. Standard coach is the last vehicle on the Main Train, a special gangway door, special steam pipe safety chain bracket, and coupling safety clip for use on the buckeye draw hook, must be used for the attachment of the slip portion.

(G.A.30 Op.-9/54 LKI/8247/2/Gen.)

#### REGULATIONS FOR WORKING THE VACUUM BRAKE.—Page 98.

Clause I (c) of these instructions to be amended to read:-

The vacuum train pipes at the ends of "fitted" vehicles are painted red, the vacuum train pipes at the ends of "piped" vehicles are painted white.

(G.A.30 Op.-9/54 RI/5662)

#### INSTRUCTIONS TO GUARDS AND TICKET COLLECTORS ON CORRIDOR TRAINS .- Page 115.

The existing paragraph I of Clause I to be deleted and the following substituted:-Corridor and gangway doors should be left unlocked so as to provide free access Locking of through the train, except as shown below:-

corridor and gangway doors

- (I) Gangway doors at the extreme ends of the trains. Care must be taken to ensure that when vehicles are detached from a train en route that the gangway doors at the point of detachment are locked.
- Brake Vans or Luggage Vans at the extreme ends of the train. Where, however, a guard is riding in the van or it is empty, the doors should be unlocked. Should the guard have occasion to leave his van whilst the train is in motion he must lock the door.
- (3) Brake Vans or Luggage Vans intermediate in the train. Where, however, there is a restaurant or buffet car on the train or a guard is riding in the van or the latter is empty, the doors should be unlocked.
- Where the gangway connections cannot be made.
- The gangway doors at both ends of sleeping car accommodation on trains. Where, however, it is necessary to admit passengers to their berths or to give access to a restaurant car during the time the restaurant service operates, the the doors should be unlocked.

In laying down the marshalling of trains, arrangements should be made, if possible, to avoid a passenger carrying vehicle being isolated from the remainder of the train by being marshalled between the brakevan in which a guard is not riding and the sleeping car accommodation. In cases where, in the interests of the working, this is not desirable the door leading to the sleeping car must be

The Guard will be responsible for carrying out these instructions but Travelling Ticket Staff, where provided, should assist. In the case of Sleeping Cars, the Sleeping Car Attendant will be responsible.

The first paragraph of Clause 7 amended to read:—

In the event of it becoming necessary to obtain access to the locked lavatory in a coach of the former G.W.R. design in the case of an emergency, this can be done by removing the indicator plate, which is secured by four screws, when the end of the spindle becomes exposed. This spindle will take the standard gas key and, by using the same, the door can be readily opened.

The following to be added as the third paragraph of Clause 7:-

When it is necessary to release a lavatory door lock (with indicator set at "Engaged") belonging to a British Standard Carriage, the three screws which secure the indicator plate should be withdrawn, the plate removed, and the indicator driving pin will then become exposed. Upon sliding this pin towards the lock handle it will be possible to gain access by turning the lock handle.

(G.A.30 Op.—9/54 LKI/6624/Gen. E.)

#### DAMAGE TO CARRIAGE WINDOWS, ETC.-Page 117.

The following to be substituted for the list of amounts that must be collected from persons responsible for the breakage of windows or carriage fittings, if possible at the time of the incident, otherwise the identity and address of the person(s) should be established and reported.

			WIN	DOWS		
Old Type Coaches		S.	d.		S.	d.
Top side light		6	0	Large light in corridor of saloon		
Ordinary door light		16	0	coaches under 34 inches wide	35	0
Corridor door light		14	0	Large light in corridor of saloon		
Ordinary quarter light		22	0	coaches, 34 inches wide and not		
Corridor quarter light		17	0	exceeding 50 inches wide	52	6
Lavatory drop light		8	6	Large light in corridor of saloon		
Lavatory fixed light		10	6	coaches over 50 inches wide	56	6
Quarter light "Smoking"		22	0	Ventilators, large in Auto cars	86	0
Door light frame including the	lights	69	6	Ventilators, small in Auto cars	52	6

New Type Coaches		s.	d.		s.	d.
Door light frame (with glass)		71	6	Window under ventilator, com-		
Fixed window, corridor side		70		partment side	52	6
First Class		72	6	Window side of ventilator, com-		
Fixed window, corridor side, Third Class		62	. 6	partment side Fixed window, corridor partition,	10	0
Drop window, corridor side,	•••	02	. 0	First and Third Class	16	6
First and Third Class		17	6	Door window, sliding door,		ŭ
Door drop window		19	0	corridor partition	19	6
				Window, sliding ventilator, com-		
Blinds, First Class				partment side	10	0
Door 19½ inches		12	0	D 1 F: . C:		
Quarter light Sliding door		9	0	Rack netting, First Class	28	0
sliding door		10	0	Rack netting, Third Class Wash basin, old type	27 75	0
Blinds, Third Class	•••			Wash basin, N.P. without	13	·
Door 19½ inches		12	0	pedestal	78	6
Quarter light		10	6	Pedestal N.P. without wash basin	72	0
Sliding door		10	0	Pedestal and wash basin N.P		
				complete	144	0
Carriage Fittings		21	,	Cushions, Third Class corridor	138	0
Curtains, Third Class Curtains, First Class	***	26	6	Cushions, Third Class, non corr. Cushions, First Class corridor	220	0
Electric light bulbs		25	6	£	74	6
Window straps, First Class		12	6	Cushions, First Class corridor		ŭ
Window straps, Third Class		- 5	6	4 per compartment	128	0
Mirrors, Lavatory O.P. (inc. frai	me)	33	0			
Mirrors, Lavatory O.P. (Glass or	nly)	8	0			
Mirrors, N.P.		17	6			
Mirrors, Compartment		19	0			
The same amount should a				e event of breakage of windows and carri	age fitti	ings
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STEAM HEATING OF PASSENGER TRAINS-Page 117.

Clause I of the existing instructions to be cancelled and the following substituted:-

The following dates are those which normally must be followed in the application and discontinuance of steam heat for passenger trains: Commencement.

All steam heater pipes to be fitted and coupled for use by 25th August.

Heat to be applied: Sleeping Car trains 1st September Other Express trains while running after 5.0 p.m. and before 10.0 a.m. All other passenger trains 1st October

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Heat to be discontinued: All trains except as shown below 1st May Sleeping Car trains 15th June Other express trains while running after 5.0 p.m. and before 10.0 a.m.

(ii) Pipes to be removed as soon as possible after 15th June The foregoing dates for the application and discontinuance of steam heating are to be regarded as a general guide, but guards and others concerned must use their discretion in the event of (G.A.30 Op.—9/54 T33320 G/2)

Reference to the following to be made on page 121:-

#### FIRES IN PASSENGER TRAINS.

unusual climatic conditions.

If any member of the staff, either in the employ of the British Transport Commission or the British Transport Hotels and Catering Services, becomes aware of a fire on the train on which he is travelling he should take appropriate steps to extinguish it. If, however, he is unable to do so promptly he must arrange for the train to be stopped immediately so that suitable action can be

After the train has been stopped the services of a Carriage and Wagon Examiner should be obtained, if possible. Attention is also drawn to the fact that if the fire occurred in a roof with a ceiling, it may be necessary to remove part of the ceiling to ensure that the fire is properly extinguished. When the train proceeds on its journey a member of the staff should, if possible, travel in the affected compartment or carriage for the purpose of observation and he should be perpared to deal with any subsequent outbreak of fire. Should a member of the staff not be available to ride in the compartment or carriage, the Guard should examine the affected compartment or carriage as frequently as possible.

Passengers should not be permitted to travel in the vehicle concerned until it has been passed (G.A.30 Op .-- 9/54 LK1/11343/365E) fit for service by a Carriage and Wagon Examiner.

### EMERGENCY APPLIANCES, &c., IN PASSENGER TRAINS .- Page 121.

The instructions under this heading to be cancelled and the following substituted:---

### EMERGENCY APPLIANCES, &c., IN PASSENGER & PARCELS TRAINS.

Emergency appliances and first aid requisites are provided in vehicles on passenger and parcels trains for use in the event of fire or accident in accordance with the following:-

Description		Where provided
HAND FIRE EXTINGUISHERS	Sleeping Cars	Two water/CO.2 pressure extinguishers, one at each end of corridor, or alternatively the two extinguishers in a recess at one end of the corridor.
	Restaurant Cars	
	Twin Units	One foam and one CO.2 gas extinguisher in kitchen. One water/CO.2 pressure extinguisher in corridor at end of kitchen and one water/CO.2 pressure extinguisher in corridor at end of dining car.
	Single Units	One foam and one CO.2 gas extinguisher in kitchen, and one water/CO.2 pressure extinguisher in corridor at end of car.
	Corridor Brake Vans	Standard Stock—One water/CO.2 pressure extinguisher and two buckets in brake compartment. One water/CO.2 pressure extinguisher in corridor.  Non-Standard Stock—One water/CO.2 pressure extinguisher in brake compartment.
	Other Corridor Vehicles	One water/CO.2 pressure extinguisher or W.R. No. 4 "Hand Cup" extinguisher at end of corridor.
	Non-Corridor Brake Vans	Standard Stock—One water/CO.2 pressure extinguisher and two buckets in each brake compartment.
		Non-Standard Stock—One water /CO.2 pressure extinguisher in each brake compartment.
	Diesel Cars	One water/CO.2 pressure extinguisher and one W.R. No. 5 "Vessel and Syringe" C.T.C. extinguisher.
	Post Office Vans	Two W.R. No. 4 "Hand Cup" extinguishers and two buckets of sand.
SET OF TOOLS &c.	One case in the g vehicle.	uard's compartment of each passenger brake
FIRST AID REQUISITES	One cabinet in the vehicle.	guard's compartment of each passenger brake

Note-The fire extinguishers and tools must only be used in the case of fire, or in the event of a train accident.

Inspectors and others.

- Special I. In the event of an accident to a train conveying passengers, it is of the first importInstructions to ance, after protection as required by the Rules, that immediate steps be taken to relieve Station Masters, any who may have sustained in intermediate adopt all precausing the requirement which the any who may have sustained injury, and to adopt all precautionary measures which the circumstances may render desirable in the direction of limiting the effects of the occurrence. The following further important points which are supplementary to the instructions of the Rule Book must be borne in mind:-
  - (a) Examine train to see if there is any sign of fire from live engine coal or broken gas cylinders or connections, and if so take prompt steps to extinguish it by means of the fire extinguishers provided on the train, or any other available means.

Immediately call for assistance from the nearest Fire Brigade, who should, if possible, be informed the exact situation of the train and the means of access. The correct method for calling the brigade by telephone is included in the Fire Notice exhibited at every station, yard and signal box. (b) Ascertain where the greatest damage has occurred and take instant steps to release any passengers who may be entangled in the wreckage.

Hand out tools and other appliances provided in the brake compartments to the most experienced men available.

(c) Ensure prompt telegraph or telephone communication with the local police officer and with the nearest likely places for doctors, nurses, ambulances, refreshments, etc., according to circumstances.

Obtain the assistance of all available first aid men and make the best possible use of the first aid cabinets provided in the brake compartments.

If a serious train accident occurs in the vicinity of a station, the station master or person in charge must despatch immediately as many staff qualified to render first ald as possible, with equipment.

- (d) Enlist the services of any uninjured and willing passengers or other persons to convey messages, and to assist generally, under the direction of the railway staff.
- (e) Use cushions taken from the coaches in such a manner as to form comfortable couches for any injured persons until such persons can be removed.
- (f) Advise the nearest station master and other officials, also the permanent way men, at the earliest possible moment.

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(g) Should gas be escaping from a broken pipe, bend the pipe away from timber or inflammable wreckage clear of any source of ignition and, if practicable, in the direction to which the wind is blowing. If possible, the pipe should be hammered flat to prevent the escape of gas.

Should the gas be ignited at a broken pipe end and it is not possible to flatten it, remove all inflammable material from the vicinity of the flame. If the end of the pipe is not accessible the adjacent woodwork should be wetted. Provided persons are not trapped in the wreckage, the flame may be extinguished by directing a stream of liquid from a water /CO.2 pressure extinguisher to the end of the pipe in the SAME DIRECTION as the issuing flame. Naked lights should not be allowed in the vicinity until the gas has been dispersed.

#### 2. Fire Extinguishers

The W.R. No. I pressure extinguisher is fitted with a quick-acting press valve, which is controlled by thumb pressure and closes automatically to reserve the liquid.

New Type Extinguishers The following proprietary extinguishers are provided in passenger vehicles for use as shown:—

Name	Туре	Use	
Conquest 55	Water/CO.2 pressure	All fires except electric	cal, petrol and oil
Firesnow	do.	do.	do.
Waterloo	do.	do.	do.
Phomene	Foam	Petrol, oil and fat fires.	
Kidde (Lux)	CO.2 gas	Electrical, petrol, oil an	
Pyrene CO.2	do.	do.	do.

Instructional Booklet.

In addition to the instructions shown on each appliance, booklet B.R.7006—"Portable Fire Extinguishers on Trains" illustrates the various types of extinguishers and their methods of operation.

All train staff must be in possession of this booklet and be fully acquainted with its contents.

Seals on Extinguishers.

Lead seals marked "B.R.(W)" and "S.F.B." are provided on W.R. No. 1 water/CO.2 pressure, W.R. No. 4 "Hand Cup," C.T.C. and CO.2 gas extinguishers.

Arrangements for Instructing Train Staff. Guards, ticket collectors, sleeping car attendants, restaurant car attendants and travelling carriage cleaners will be instructed in the use of each type of extinguisher, as may be appropriate according to the types they are likely to encounter. Where possible they will have actual experience in handling the appliances.

The periodical examination of trainmen in the rules and regulations must include the use of fire appliances.

Train staff must not on any account test the appliances by operating them and drawing off small quantities of liquid. When seals are provided they must not be broken unless the extinguishers are required for a fire or when permission for doing so is given by an authorised instructor.

#### 3. Tool Cases.

#### Description,

The tool cases are painted red with the words "FOR EMERGENCY USE ONLY" in gilt lettering, black edged, on the front of the cases and a rectangular piece of reinforced glass is inset in the lid to enable the contents to be seen. The lid is taped and sealed at the top and bottom on one side, and in the event of an accident it is only necessary to cut or break the tape in order to open the lid and obtain the tools and appliances.

### of Cases.

Each tool case in Western Region stock contains the following articles:-

Saw, hand, 26 inches, rough tooth. Saw, hand, 22 inches, rough tooth.

Felling Axe.

I Axe, hand, small.

2 Crowbars, 3ft. 5in. and 2ft. 5in. long respectively.

I Extension Tube.

Chain, 12ft. Oin. long, with hook at one end and ring at the other.

Spade.

Set of Splints (4 to set).

I Metal Sleeve (for extension of splints)

Tool cases in B.R. standard stock contain the following articles:-

Saw, hand, large. Saw, hand, small.

Felling Axe.

Axe, hand, small

Crowbar, 3ft. 0in. long. Crowbar, 4ft. 6in. long (in clips on left-hand side of tool case).

Crowbar Extension Tube.

Sledge Hammer.

2 Steel Wedges.

Coil of Rope, approx. 13 yards long and lin. diameter.

Coil of Rope, approx. 12 yards long and ½in. diameter. Inspection Lamp, electric, fitted with 200ft. flexible lead and plug.

2 Lamps, hand, paraffin.

### 4. Examination and Maintenance of Fire Extinguishers and Tool Cases.

The Chargeman Carriage & Wagon Examiner or other authorised member of the Carriage & Wagon Engineer's staff at the various depots will be held responsible for seeing the fire extinguishers and tool cases are in their proper places and that seals (where provided) are intact.

Extinguishers and replacements for Tool Cases.

**Appliances** 

intact.

to be in proper places with Seals

Should an extinguisher be damaged or missing or be found with a broken or missing lead seal when it should have one, such extinguisher must at once be replaced. A supply of spare extinguishers and tool case replenishments will be kept on hand for renewal purposes at the following depots:-

Fire Extinguishers. Aberdare H.L. Aberystwyth

Barry

Birmingham, S.H. Bridgend

Bristol (Dr. Days)

Caerphilly (To draw on Cardiff Queen St.)

Cardiff General Cardiff Queen Street Carmarthen

Cheltenham St. James

Exeter Goodwick Helston Hereford Leamington Liskeard Marylebone Neasden Shops

Neath Newport (Ebbw) Carriage Shed

Newquay Neyland

Old Oak Common Oswestry Shops

Oxford Paddington Penzance Plymouth Pontypool Road

Pontypridd (To draw on Cardiff Q. St.)

Pwllheli Reading Ruabon St. Blazey St. Erth

Severn Tunnel Junction

Slough Southall

Stratford-upon-Avon Swansea High Street

Swindon Taunton Trowbridge Truro Tyseley Westbury West London

Wolverhampton Cannock Road

Wolverhampton L.L.

Worcester Wrexham

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St.)

LY' Aberystwyth Barry Bridgend Bristol Cardiff General Carmarthen Cheltenham St. James Fishguard & Goodwick Hereford Marylebone

Neasden Shops Neath

Tool Cases.

Newport (Ebbw) Carriage Shed

Neyland Old Oak Common Oswestry Penzance 11

Plymouth Pwllheli Swansea High Street

Taunton Tyseley West London

Wolverhampton Cannock Road

Wolverhampton L.L.

Worcester

Duties of Guards, Sleeping Car and Restaurant Car Attendants.

Guards in charge of trains must satisfy themselves that extinguishers are installed in accordance with the introductory paragraph to these instructions and that the seals on extinguishers (where provided) and tool cases are intact. In the case of restaurant and sleeping cars the attendants will be responsible for examining the extinguishers and reporting defects, etc., to the guard.

Carriage Cleaning Staff.

Carriage cleaning staff observing deficiencies, damage or broken seals when carrying out interior cleaning, must advise the Carriage & Wagon Engineer's staff, so that replacements can be installed with a minimum of delay.

Appliances damaged or missing-Guards to report.

The guard will be responsible for reporting to the Carriage & Wagon Examiner on duty at the station where the train terminates its journey any cases of extinguishers being damaged, missing or having broken seals. Where restaur formed in the train the guard must confer with the attendants. Where restaurant or sleeping cars are

In cases where the guard does not proceed with the train to its destination he must report any defects, etc., to the guard who relieves him, and the latter will be responsible for advising the Carriage & Wagon Examiner at the terminating station.

Any such report made by the guard must be recorded on his journal and the District Operating or District Traffic Superintendent in whose office the journal is filed must immediately pass an extract of the report to the District Outdoor Carriage & Wagon Engineer. The latter must then report any special matters or defects in connection with fire appliances to the Carriage & Wagon Engineer, Swindon.

Replacing Extinguishers.

If an extinguisher is damaged or the seal provided is broken or missing it must at once be exchanged and the defective appliance returned in one of the crates provided to the Fire Station, M. & F. Engineer's Department, Swindon; the Chargeman, Carriage & Wagon Examiner or other authorised member of the Carriage & Wagon Engineer's staff at the depots mentioned being responsible for this duty.

#### First Aid Cabinets.

of Cabinets.

The First Aid Cabinet is marked "BR(W)-FIRST AID No. 3" and contains the following requisites:-

Sterilised Dressing (small)			 2
" " (medium)			 2
" " (large)			 2
Mines Dressing (large)	***		 6
" " (Medium)			 3
Sterilised Cotton Wool, ½oz.		***	 3
Antiseptic No. 5-2oz. bottle		•••	 1
Sal Volatile-2oz. bottle with	screw cap		
(also to be used as sm	elling salts)		 1
Triangular bandages (individua	ally wrappe	ed)	 9
Roller bandages 2ins. x 2 yar	ds		 6
Safety pins—set of 6			 !
Splints, with junctions-set			 1
Lint-white, ½oz. packet			 3
Eye drop No. IA (castor oil)	-loz. bott	tle	 1
Card of Instructions			
Report Form			

Use of Cabinet. How sealed. The First Aid Cabinet is primarily intended for use in "Train Accidents."

The lid of the cabinet is sealed but the seal may be broken in event of emergency as indicated above, when the contents will be obtainable.

### 6. Examination and Maintenance of Train First Aid Cabinet.

Responsibility. Station and Yard Masters will be held responsible for ensuring that the examination, replenishment and sealing of First Aid Cabinets is carried out by the Carriage Cleaning Staff.

Cabinets' requiring attention.

Should a First Aid Cabinet be damaged or missing, or found with a broken or missing seal, it must be replaced or replenished without delay and a supply of spare First Aid requisites will be maintained for this purpose at certain Stations and Depots. When a First Aid Cabinet has been replenished and sealed a small coloured adhesive label must be affixed to the lid to indicate when the contents were last inspected. The labels will be forwarded annually, without requisition, to District Officers by the Stationery Department and will be used in the following sequence:—

Colour	For use during 3 months commencing
Pale Blue	January 1st
Brick	April 1st
Apple Green	July 1st
White	October 1st

### 7. Train First Aid Cabinets-Replenishment Cupboards.

Stations at which Replenishment Cupboards are provided.

Operating District	Station or Depot	Grade	Operating District	Station or Depot	Grade
LONDON	Didcot Henley-on-Thames Old Oak Common Oxford	COAC	CARDIFF	Barry Bridgend Caerphilly Cardiff (Bute Road)	CBCC
,	Paddington Reading Slough Southall West London	BBCCA		Cardiff (Carriage Sdgs) Cardiff (General) Cardiff (Queen St.) Dowlais (Caeharris) Llantrisant Maerdy	04000000000000
BRISTOL	Bristol (Dr. Days) Chippenham Frome Swindon Trowbridge Westbury Weston-Super-Mare	A C C B A C B B	SWANSEÄ	Penarth Town Pontypridd Porthcawl Rhymney Tondu Treherbert Aberavon Town	000000 0
EXETER	Weymouth Yeovil (Pen Mill)  Exeter (St. Davids) Kingswear Paignton Newton Abbot Taunton	C C B C B B	SVVANSEA	Carmarthen Fishguard Harbour Llanelly Neath (Carriage Sidgs) Neath (General) Neyland Pembroke Dock Swansea (High St.)	BCCBBCCA
PLYMOUTH	Bodmin Helston Kingsbridge Laira Liskeard Newquay Penzance Plymouth (Millbay)	CCCCCCBA	WORCESTER	Swansea (Victoria) Whitland Evesham Kidderminster Worcester (Shrub Hill) Banbury	BC CCB C
GLOUCESTER	St. Blazey St. Ives Truro Cheltenham (St. James)	CCB		Birmingham (Snow Hill) Leamington Spa Stourbridge Jct. Stratford-upon-Avon Tyseley (Carriage Sdgs)	BCAC
GLOOCESTER	Gloucester (Central) Lydney Ross-on-Wye	0000		Wolverhampton (Cannock Road) Wolverhampton (L.L.)	B A

Operating District	Station or Depot	Grade	Operating District	Station or Depot	Grade
NEWPORT	Aberbeeg Aberdare (High Level) Abergavenny Junction Ebbw Jct. (Carriage Shed) Merthyr	C	CHESTER	Hereford Ruabon Shrewsbury Wellington (Salop) Wrexham	BCBCC
	Newport (High Street) Pontypool Road Severn Tunnel Junction Tredegar	0000	CENTRAL WALES	Aberystwyth Brecon Oswestry Pwllheli	0000

### Replenishment Cupboards-Details of Stock to be held.

Details of the stock to be held should be pasted on the inner side of the replenishment cupboard door and the stock must be maintained as near as possible to the quantities stated.

Excessive stocks must not be held.

Spare empty cabinets when required should be obtained from the nearest Carriage Repair Depot. Replenishments to be ordered on form "BR.9016."

Standard Item Number	Description of Material		Stock Unit		permi Station	
		7		Grade	of St	ation
				"A"	"B"	"C"
02-204 02-207 02-211	Bandages Roller 2in. x 2 yards Bandages Triangular—Wrapped Blocks Wood		 doz. each	4 72 1	2 36 1	18
102-222 102-223 102-224 102-225 102-226 102-228 102-237 102-238 102-245 102-255 102-255 102-255 102-255 102-259	Dressings Sterilised Compressed, Mines type, large Compressed, Mines type, medium Ordinary, Finger Ordinary, Large, wound Ordinary, Medium, wound Drops Eye, No. IA (castor oil) Forms 5042 (Guard's Report) Lint, White ½oz. packets Lotion, Antiseptic, 2oz. bottles Pins, safety (sets of six) Sal Volatile—2oz. bottles Seals—Lead ½in. Splints—with joints Tape, Sealing Wool, Absorbent Cards of Instructions		 " " doz. each set each each set doz. yds. each "	48 24 16 16 16 8 24 2 8 8 8 8 150 3 4 24 8	24 12 8 8 8 4 12 1 4 4 4 100 2 3 12 4	12 6 4 4 4 2 6 1 2 2 2 2 2 2 2 6 1 2 6 6 1 1 2 1 2

Duties of Guards.

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8. Guards in charge of trains must satisfy themselves that the seals of the First Aid Cabinets are intact and if any of the equipment is damaged or missing, or the seals broken or missing, must report the matter to the person in charge at the station at which the train terminates its journey, who must advise the Carriage Cleaning Staff that a First Aid Cabinet requires attention.

In cases where a Guard does not proceed with the train to its destination he should report any defects to the Guard who relieves him and the latter will be responsible for advising the person in charge as stated above.

Entry on Guard's Journals. Any such report must be recorded on the Guard's journal and the District Operating Superintendent or District Traffic Superintendent in whose office the journal is filed must ensure that the necessary replenishment and re-sealing is carried out.

Record of

In the event of a First Aid Cabinet having been used the Guard must make an appropriate entry on the form provided and leave the form inside the Cabinet for the information of the Carriage Cleaning Staff at the terminating station or depot.

#### 9. Pilferage from First Aid Cabinets.

Numerous losses from First Aid Cabinets in trains have been brought to notice and there is reason to believe that these occur during the night or whilst the vans are standing in isolated sidings.

Guards when taking charge of trains should satisfy themselves that the Cabinets are in order and call attention of the Station or Yard Master to any irregularity, which should also be reported on the journal.

Station and Yard Masters should report all irregularities to the District Operating Superintendent or District Traffic Superintendent and, if necessary, the B.T.C. Police.

The co-operation of the staff is desired to prevent pilferage from First Aid Cabinets.

(G.A,30 Op.-9/54 A1/7 1163/102 S2/AMB/7)

#### GENERAL INSTRUCTIONS TO BE OBSERVED IN CONNECTION WITH AUTO-CAR SERVICES.—Pages 128-131.

The following to be substituted for the table shown in paragraph 19 on page 130;-

	Auto Engines		
Gradlent	2-4-0 T	2-6-2 T	
	0-4-2 T	0-6-0 T	
1 :- 40	Tons	Tons 90	
I in 40 I in 50	72 96	120	
I in 60	120	150	
l in 80	144	180	
I in 100	168	210	

(G.A.30 Op.-9/54 T.D./P.R.P.)

### WORKING OF DIESEL CARS-Page 132.

The following additional paragraph to be inserted after the instructions headed "Use of emergency coupling-Cars I to I7 inclusive":—

### Tail Traffic.

Tail traffic may only be hauled by Diesel Cars numbered 18 upwards.

The maximum tail tonnage in all circumstances must not exceed 60 tons. In the case of the Twin Diesel Units formed with an intermediate coach, the maximum tail tonnage must not exceed 30 tons.

The tail load must be restricted to 30 tons where there is a rising gradient of I in 60 or steeper, except where the diesel car is used for Engineering Department tunnel inspections when the restriction applies on rising gradients of I in 40 or steeper.

When steam heating is in use not more than one passenger carrying vehicle may be attached to the Diesel Car or Twin Diesel Units.

(G.A.30 Op.-9/54 LKI/12009/372 T.D.288)

The following to be inserted at the end of the clause headed "Instructions to Guards":-

All passenger Diesel Cars, with the exception of Diesel Car No. I, may carry additional passengers up to a number not exceeding half the normal seating capacity.

#### Exceptions:

- (a) The normal seating of Diesel Car No. I (69 passengers) must not be exceeded.
- (b) The loading of Diesel cars passing through the Severn Tunnel must not exceed the seating capacity.
- (c) The maximum permissible loads in Luggage compartments of Diesel Cars must not exceed the maximum indicated on the Notice exhibited in the Car.

(G.A.30 Op .- 9/54 T.39,119 G/I)

The following to be added after the word "run" in line two of the last paragraph of these instructions (See G.A.25):—

".....or if a Diesel Car is required to work in a service which is normally scheduled to be worked by a train or auto-car on a route over which Diesel Cars are authorised to be worked....."

(G.A.30 Op.-9/54 LKI/8847/372)

# DIAGRAM SHOWING CLASSIFICATION, HEAD LAMPS AND CORRESPONDING BELL SIGNALS.—Page 139.

Insert § against D headcode trains and the following at the foot of page 140:

§ The proportion of vehicles on which the automatic brake must be operative to be based on the equivalent load, Class 3 traffic.

(G.A.30 Op.—9/54 E.84669 H(2-C)).

Engine Head Codes.

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The following to be added to the "description of Trains" carrying "E" Head Codes:-

Weed killing trains when both running and spraying

Beats on Bell

How to be given
1 pause 2 pause 2

(G.A.30 Op.—9/54 LKI/10661/417E).

### TIMING OF EMPTY COACHING STOCK TRAINS .- Page 141.

The following to be added at the end of the 2nd paragraph:-

"on sections of the line where the ruling gradient is less than 1 in 100 rising."

(G.A.30 Op.-9/54 T.38964 G/7).

Reference to the following to be made on page 141:-

### WORKING OF GAS TURBINE ENGINES.

When a gas turbine engine is required to work over a section of line where it is not normally scheduled to run, prior advice must be issued to all concerned, including the staff of other Departments, e.g., Permanent Way men, of the intention to make such movement.

When the scheduled working of a gas turbine engine has been suspended for a short period, i.e., not exceeding seven days, in connection with repairs, etc., it will not be necessary for a special advice of resumption of normal working to be issued to all concerned.

When the scheduled working of a gas turbine engine has been suspended for a period in excess of seven days, a notice to all concerned must be issued before normal working is resumed.

If it is absolutely necessary for a gas turbine engine to work over a section of line where it is not normally scheduled to run, or if a gas turbine engine is required to work in a service which is normally scheduled to be worked by a steam locomotive on a route over which the gas turbine engines are authorised to be worked and a printed or stencilled notice cannot be issued in sufficient time to ensure that 48 hours' notice is given to all concerned, the Drivers of such engine must be notified of the circumstances and must then sound the siren when entering and emerging from tunnels, also when approaching curves, level crossings, barrow crossings, overbridges, gangers' huts and other buildings adjacent to the line upon which the gas turbine engine is run.

(G.A.30 Op.-9/54 LKI/8847/372)

### ENGINE WHISTLES-Page 141.

The table and instructions under this heading to be deleted and the following substituted:-

### STANDARD CODE OF ENGINE WHISTLES

The following code of engine whistles applies at all stations, junctions and sidings, not otherwise specially provided for in the local Code of Engine Whistles shown in the table following the standard codes.

In order to avoid annoyance to passengers at stations and residents in the neighbourhood of the Railway, Drivers are requested not to make more frequent use of the engine whistles than is absolutely necessary to ensure safe and efficient working in compliance with the Rules and Regulations

Note: The term "Slow line" includes Relief line.

\*Main or Fast lines ... ... 1 long
\*Line next to Main line (Slow or Goods)
\*Line next to Slow or Goods ... 3 long

(One additional long whistle to be given for each additional line farther away from the Main line.)

\*These codes to be given when approaching signals at Danger or when necessary to indicate when ready to proceed on same line.

### Approaching Geographical Junctions and requiring to proceed through Junction.

+On Main line and requiring to proceed to left		I long I short
On Main line and requiring to proceed to right		I long 2 short
On Slow or Goods line and requiring to proceed		2 long I short
†On Slow or Goods line and requiring to proceed	to right	2 long 2 short
†The appropriate route code whistle to be given at	Signal Boxes	enumerated in the local
Appendices.		

Appendices.			
To or from Goods line or Slow line or Loop and	d Main lin	e	5 short
To cross from Main to Main			4 short
To or from Bay or Platform lines			I crow I long
Down Main or Fast, Slow or Goods or Loop to Dow			1 crow
Down Main or Fast, Slow or Goods or Loop to Up Sic			2 short pause 3 short
Up Main or Fast, Slow or Goods or Loop to Up	Sidings		3 short pause I short
Up Main or Fast, Slow or Goods or Loop to Do	wn Siding		3 short pause 2 short
Up Sidings to Down Sidings or vice versa			3 short pause 3 short
Train ready to leave Sidings			2 short pause I short
Shunt from Sidings to Main Line			2 short pause 2 short
To or from Loco			2 short
Express trains requiring fresh engine at next stoppi			3 crows
‡Fire on lineside			I crow I long I crow
To be repeated when passing next Permanent W	av men. S	tation.	Signal Box or Crossing
Keeper's hut.			· ·
Engine requiring water			I long pause 3 short
To indicate light engine is clear of points which requi	re to be tu	rned	I short
To indicate that train or light engine has been shu	nted clear	of	
points leading from one running line to another—(			I crow I short
To indicate that train or light engine has been sho	inted clea	r of	
all running lines—(Rule 69)			3 short
Before starting train assisted by engine in rear—(Ru	le 133 claus	se c)	2 crows
perore searching cram assisted by engine in real (rea			

(G.A.30 Op.-9/54 LKI/9593/E).

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### WORKING OF ENGINES IN STEAM COUPLED TOGETHER .- Page 142.

The instruction under heading "(B)" Over Royal Albert Bridge, Saltash, to be cancelled and the following substituted:—

(I) The maximum speed of all engines passing over the structure is 15 m.p.h.

(2) The following "Red" engines may be assisted by any of the "Blue" or "Red" engines shown:-

	"Red"		Ass	isting	Engin	e	
	Standard "Castle"	CI.7	49xx, 68xx 78xx	59xx,	69xx,	79xx	
68xx			43xx,	53xx,		73xx,	93×

(3) The following "Red" engines may be assisted by any of the "Blue" engines shown:-

1000 Class 40xx 40xx 43xx, 53xx, 63xx, 73xx, 93xx 41xx, 51xx, 61xx, 81xx

Both groups of engines may also be assisted by engines of the "Yellow" and "Uncoloured" classes.

Assisting tender engines with a leading bogie may be coupled either in front of the train engine or between the train engine and the train. In all other respects the general instructions for assisting or double heading of trains apply.

(G.A.30 Op.—9/54 TD95/41)

# INSTRUCTIONS TO BE OBSERVED WHEN ARRANGING THE ASSISTING OR DOUBLE-HEADING OF TRAINS—Page 144.

2. Passenger Trains.

The following to be added as paragraph (iv) to Clause (b):-

(iv) Engines of the 41xx and 51xx 2-6-2T types and of the 43xx 2-6-0 type may assist in front of any authorised engine between the following points:—

Par-Newquay Newquay-Par.

(G.A.30 Op.—9/54 TD.95/166).

Reference to the following to be made on page 145:-

#### WORKING OF LOCOMOTIVES WITH TENDER LEADING.

Tender locomotives must not exceed a speed of 45 m.p.h. when running with the tender leading either when attached to a train or when running light. (G.A.30 Op.—9/54 LKI/E).

# RESTRICTIONS ON THE WORKING OF WESTERN REGION WIDE STOCK —Pages 150–154.

Restrictions as to Working of Stock over Certain Branch Lines.-Page 150.

The following to be added to the present restrictions respecting stock 73ft. 0in. long by 9ft. 0in. wide:—

Branch Station
Severn Valley Line Bridgnorth

Restriction

Must not come back from Down Main Line into the Down Siding at the Highley end of the Station. (This also applies to stock 66ft. 8in. long by 8ft. 11in. wide).

(G.A.30 Op.-9/54 R6/-)

Stock 73ft. long by 9ft. wide.-Page 151.

DELETE "Standish Junction" in the second line and substitute "Abbots Wood Junction." INSERT after "Yate Section—"via Cheltenham and Gloucester South Junction only."

(G.A.30 Op.—9/54 R.6/5515).

The last sentence to be amended to read:-

"......and on the Central Wales Section may be worked only between Whitchurch and Aberystwyth, Aberystwyth and Carmarthen, Dovey Junction and Pwllheli (speed restriction of 25 m.p.h. through tunnels Aberdovey to Dovey Junction).

(G.A.30 Op.—9/54 R.17/355W.).

Stock 66ft. 8in. long by 8ft. Ilin. wide.-Page 151.

The entry under heading "L.M.S. Railway Lines prohibited" in respect of Swansea Victoria Station to be deleted.

(G.A 30 Op.—9/54 R6/-)

Stock 63ft. 6in. long by 9ft. 53in. and 9ft. 7in. wide.—Page 151.

The entry under heading "Central Wales" in respect of Pwllheli to Dovey Junction to be amended to read:—

Pwllheli to Dovey Junction (Speed restricted to 25 m.p.h. through tunnels Aberdovey to Dovey Junction).

(G.A.30 Op.—9/54 R.17/355W).

Stock 60ft. long by 9ft. wide.-Page 153.

The following to be added to the entry under heading "L.M. & S. Company (Western Section)":

Swansea Victoria and Craven Arms (provided the length over buffers does not exceed 63ft. and the width over projections does not exceed 9ft. 4in.) subject to the following prohibitions:—

Swansea Victoria-Wash road in the Carriage Sheds.

Builth Road-Bay Platform.

(G.A.30 Op.-9/54 R.6/-).

# SHUNTING INSTRUCTIONS—SHUNTING ETC. IN STATIONS AND STATION YARDS.—Page 155.

The following to be substituted for the fifth paragraph of these instructions:-

When coaching stock is propelled on running lines or from running lines to sidings, the Guard or Shunter should ride on the leading vehicle or the nearest suitable vehicle, keep a good lookout and be prepared to hand signal to the Driver or Fireman. If this is impracticable the Guard or Shunter should place himself in such a position on the ground that he can plainly signal to the Driver.

(G.A.30 Op.-9/54 O.M.12725).

### CONTROL AND DISTRIBUTION OF ROLLING STOCK .- Page 166.

The instructions in G.A.9 under this heading to be cancelled and substituted by the following:—

The existing instructions for the Supply and Distribution of Freight Stock are shown in booklets BR.87237 and 87238 "Standard instructions to Stations and Standard instructions to District Officers" respectively.

(G.A.30 Op.-9/54 R.).

Reference to the following to be made on page 168:-

#### DRIVERS' TICKETS.

It is very important that Drivers' Tickets are correctly and fully completed as the information is the basis of Railway Statistics called for by the Transport Act of 1947. There is evidence to show that Column 13 "Description of Trip" is not being accurately compiled in that "loaded" or "empty" freight trains are not being properly described. It is important to remember that if there are one or more loaded wagons on a train, that train must be regarded as a "loaded" train.

Guards are responsible for informing Drivers as to the "Description of Trip" (Column 13 of Drivers' Ticket) and the number of wagons and the equivalent load (Column 21 of Drivers' Ticket). It is imperative that care should be taken to see that the Driver is properly informed as to:—

- (a) Whether the train is "loaded" or "empty".
- (b) Number of vehicles and equivalent number of wagons at starting point.
- (c) Any alterations to load en route.

(G.A.30 Op.-9/54 E.).

#### CONVEYANCE OF PASSENGER TRAIN STOCK ON FREIGHT TRAINS,-Page 168.

The following to be substituted for clauses 1, 3 and 4:-

1. Coaching Stock exceeding 65ft. in length may be conveyed on a through freight train, with "F" or inferior headcode only, subject to being formed next in front of the rear Goods Brake Van, with an additional Goods Brake Van or 15-ton open Goods (TUBE) wagon, or other vehicle 24ft. over headstocks or longer, formed next in front of the coaching stock.

Freight trains conveying such coaching stock must not enter sidings not normally used for such stock, and the stock must not be conveyed on trains passing by routes over which the stock is prohibited except when notified by the Operating Superintendent giving Civil Engineer's Conditions of passage.

This instruction does not apply to stock passing from private building works, particulars of which are notified by the Operating Superintendent as out of gauge or exceptional loads with the applicable conditions of passage.

- 3. If essential for traffic reasons 8-wheel passenger train vehicles when conveyed by freight trains must be formed immediately in front of the brake van except in the case of local trains and pilot trips conveying not more than equal to 25 wagons of Class I traffic, when they may be placed in any position on the train.
- 4. If essential for traffic reasons 4 and 6-wheel passenger train vehicles may be formed in any position on freight trains.

(G.A.30 Op.-9/54 Ex.5299/52)

The following to be added as clause 6:-

6. Empty Parcels Vans Siphons 'F', 'G', 'H' or 'J' must not be conveyed on freight trains except where essential for traffic requirements on Branch Line services as arranged by the local District Officer.

(G.A.30 Op.-9/54 E.1088H (2-C) R.8/-)

### INSTRUCTIONS FOR WORKING BALLAST TRAINS.—Page 169.

The instructions under this heading to be cancelled and the following substituted:-

# INSTRUCTIONS FOR WORKING OF CIVIL ENGINEERING DEPARTMENT BALLAST AND MATERIAL TRAINS.

Additional instructions relating to the working of fully fitted Ballast trains are shown under separate heading "Fully fitted trains with Ballast loaded in Engineering Department Hopper Wagons and Plough or Freight Brake Van from Quarries to unloading sites and return of empty Hoppers."

Arrangements for running.

The District Operating Superintendent or District Traffic Superintendent from whose District the trains will start will make all arrangements for the running, and advise the other District Operating Superintendents concerned. In his requirements, the District Engineer must indicate the approximate duration of the Engineering Department work at site or sites to enable the Operating Department to arrange relief of trainmen when necessary.

Any emergency arrangements after office hours will be made by the respective Control Offices.

Engine Power.

The District Operating Superintendent will arrange with the District Motive Power Superintendent for the provision of the necessary engine power.

When making application for the running of these trains or engines in connection with relaying, reconstruction of bridges, or other Engineering Department work, the District Engineer must advise the District Operating Superintendent or District Traffic Superintendent (giving type of engine or engines if necessary), stating whether during any interval, the engines may be utilised for other purposes, and, whenever possible, name the time when it is expected an engine will be available to return from the site of the work.

Trains picking up men for unloading.

When these trains are required to call at places en route to pick up men for unloading the District Operating Superintendent or District Traffic Superintendent must be advised at the time arrangements are made for the running of the train.

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Instructions to Guards. and Signalmen as to site for unloading. The section or station and mileage of the site with a description of the line at which such a train is to be worked must be shown in the Civil Engineer's weekly ballast programmes, Speed and Engineering Notice, or other notice, and the guard of the train must be instructed accordingly. Where it is necessary to unload some or all the material on another site and time will not admit of the Guard being notified in the usual way, information must be given by the Permanent Way Inspector or Ganger to the Signalman at the commencement of the section affected, and he in turn must inform the Guard of the train accordingly.

Ballast not to be deposited on points, rodding wires, connections, or A.T.C. Ramps.

When ballast or other material is being unloaded, great care must be exercised to see that it is not deposited on point rodding wires or connections, or automatic train control ramps.

Wagons put off for repairs. When an empty wagon is put off for repairs it should be labelled by the Guard to the home station, and when a loaded wagon is detached for repairs it is to be labelled to the destination of the train.

Should any loaded wagons be put off for repairs, etc., the Guard must give particulars in writing to the man responsible for unloading at destination, in order that he may be aware that the complete train has not arrived, thus obviating the risk of error in the certification of the advice note.

The guard must show on the back of the engine ticket (form No. 2009), Engineering Department service vehicles, wagons put off for repairs, etc., giving the name of station and wagon numbers, and stating if loaded or empty and how labelled.

Guards to examine trains.

Guards must examine their trains immediately after unloading or on taking charge, also before proceeding to or leaving a Contractor's temporary road, and in the event of a wagon having been damaged, a report of the circumstances must be sent to the District Operating Superintendent or District Traffic Superintendent.

Reversing on incline.

Trains which have to reverse on a steep incline must be provided with a brake van at each end. The provisions of Rule 151 must be observed.

Propelling on incline.

The amplification of Rule 149, exception (vii) as shown on page 20 of the General Appendix to the Rule Book to apply.

Marking of wagons.

The marking of Engineering Department wagons is not to be altered without authority from the Civil Engineer.

Guard's Journal. The Guard of each train must send a sectional journal on form B.R.87210 to the Operating Superintendent of each District through which his train passes.

Journals need not be rendered to the Engineering Department.

Engine Ticket. The Guard of each train must prepare engine ticket (form No. 2009), including on the back thereof particulars of the loading of the train, and immediately the last journey for the day is completed, it must be forwarded to the District Engineer in whose District the train originated.

The Guard must also give an engine ticket (form 2009A) to each Driver for the time the Driver was on the train.

Each Driver must obtain from the Guard an engine ticket and attach it to his daily record.

Guards are responsible for rendering an engine ticket to the Driver after the completion of the work, covering the whole of the time the engine has been in traffic. The Driver must communicate with the Guard after completion of the work in order to obtain this engine ticket.

Engines returning home.

When engines which have been used for working loaded trains are not required on the return journey and return home light, the time will be debited to the Engineering Department, but should they be utilised for ordinary traffic working no charge will be made.

Guard to inform Driver formation and number of wagons. Before starting, the Guard must inform the Driver in charge the total number of wagons on the train, and what proportion of it is composed of vacuum fitted wagons, the brakes of which can be applied from the engine. VACUUM-FITTED VEHICLES MUST BE FORMED NEXT TO THE ENGINE.

Vacuum pipes out of use.

On arrival at site when these vehicles are disconnected the Guard must see that any loose vacuum pipes are placed on the stop plugs provided.

Headcodes to be carried. Engineering Department trains must carry "F" headcodes except in the case of trains fully or partially vacuum fitted which should carry "C," "D" or "E" headcodes respectively.

Stopping at Stop Boards to pin down brakes, and at foot of inclines.

ALL TRAINS WITH THE EXCEPTION OF-THOSE CARRYING "C" HEADCODE, MUST COMPLY WITH THE INCLINE INSTRUCTIONS AS SHOWN ON PAGES 178 AND 180 OF THE GENERAL APPENDIX TO THE RULE BOOK, MUST STOP AT STOP BOARDS AND PIN DOWN BRAKES AS REQUIRED IN ACCORDANCE WITH THE "GENERAL INSTRUCTIONS FOR WORKING INCLINES."

Co-operation between Operating and Engineering Departments.

It is of importance that close co-operation should exist between the District Operating Superintendents and District Engineers so that Engineering Department trains shall be scheduled at a time when they will cause least interference with the ordinary booked train services. This is especially necessary where the trains have to work on, or pass over, single lines.

Running of trains during Summer.

On main passenger train routes the running should be curtailed as far as possible throughout the period of summer traffic (during Bank Holiday periods, etc.) especially on Mondays, Fridays and Saturdays.

(G.A.30 Op .- 9/54 E.8267 H(12-C. ).

Reference to the following to be made on page 171.

# FULLY FITTED TRAINS WITH BALLAST LOADED IN ENGINEERING DEPARTMENT HOPPER WAGONS AND PLOUGH OR FREIGHT BRAKE VAN FROM QUARRIES TO UNLOADING SITES AND RETURN OF EMPTY HOPPERS.

Loaded trains must run under the following conditions:-

- (i) Carry "C" headcode.
- (ii) Maximum speed-50 m.p.h.
- (iii) Engine provided to have not less than 5ft. 8in. diameter coupled wheels.
- (iv) Vacuum brake on all wagons to be operative, also plough or freight brake van to be fitted or piped.
- (v) It will not be necessary to observe incline instructions.
- (vi) Maximum load not to exceed 22 20-ton loaded Engineering Department hopper wagons. On rising gradients the standard loading for Class I traffic shown in the Service Time Tables must not be exceeded. Engineering Department 20-ton hoppers loaded with ballast to be calculated as "Two equals Three 10-ton wagons of Class I traffic."
- (vii) Instanter couplings must be in short position.

Procedure to be adopted in arranging trains.

The Civil Engineer will issue a weekly programme to all concerned on Tuesdays showing the requirements for fully fitted trains of ballast to run from the following Monday to Sunday (both days inclusive).

The District Operating Superintendent or District Traffic Superintendent concerned with the despatch of the ballast must confer with the District Engineer and agree departure time of trains, according to the unloading point for all the trains listed and advise other District Operating or Traffic Superintendents concerned by telegram on Wednesdays full details of the trains arranged for the following week.

The District Engineer involved with the unloading of the ballast to consult the District Operating Superintendent or District Traffic Superintendent in whose District the unloading will take place, on Wednesdays and agree at what time this can be accomplished. The District Operating Superintendent or District Traffic Superintendent will order a fresh engine, if this is necessary, to undertake the work of discharging the ballast and bringing back the empties, and arrange departure time for a special train conveying the empty hoppers back to the Quarry.

Train Schedules. Standard schedules will be laid down for these trains, where practicable, and must be adhered to. In the case of journeys where a standard schedule cannot be applied, throughout timings must be pre-arranged in the case of loaded trains to arrive at unloading site by 7.30 a.m. unless otherwise specified in the Civil Engineer's weekly programme.

District Operating Notices.

Details of the loaded and empty trains required to run including timings must be issued in the District Operating or Traffic Superintendent's notices, copies of which must be sent to the Operating Superintendent's T.D. Section.

Engine Power and trainmen.

In general where the transit is within 100 miles of the quarry, an engine and trainmen must be diagrammed to cover the loaded and empty journeys throughout, although in the case of branch lines it may be necessary to utilise a smaller type of engine from the junction in which case the engine used to convey the ballast from the Quarry, or nearby marshalling yard, must be available for working back the empty hoppers to the Quarry.

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Res of to hop In the case of longer distance transits or where it is considered guaranteed arrivals at unloading sites within reasonable limits are uncertain, it is desirable that ballast should be despatched from Quarries as soon as possible after loading has been completed, and be stabled at a convenient point near to unloading site. The Operating Superintendent will diagram engine and trainmen to work to stabling point and back to the Quarry.

A fresh engine and trainmen will be diagrammed to carry out the unloading of the ballast unless the District Engineer has made other arrangements.

Prior arrangements must be made in order to ensure that enginemen and guards are available to work the loaded and empty trains at the recognised changing points en route.

Return of empty hoppers to Quarries.

Owing to limited supply of hopper wagons for ballast traffic, it is important no delay occurs in returning these wagons, when empty, to Quarries in order that the programmed supplies of ballast may be maintained, and movement must be undertaken by special trains.

Empty hopper trains.

Empty trains may run under "C" headcode freight train conditions not exceeding 50 20-ton hoppers. Engineering Department hopper wagons being calculated on the basis of 20/21 ton wagons shown in the Service Time Tables.

### WORKING OF HOPPER BALLAST WAGONS AND PLOUGH.

Guard to understand working of hopper trains. Whenever practicable, a loaded hopper ballast train must be worked by a Guard who from previous instruction and experience, is qualified to take charge.

Working of plough to be undertaken by Guard.

The working of the hopper ballast plough for spreading the ballast is to be undertaken by the Guard, under direction of the Permanent Way Inspector or Sub-Inspector.

Where ploughs are not to be used.

The hopper ballast plough is not to be used in station yards.

Materials carried by hopper trains. Hopper wagons are only to be loaded with material as authorised by the Civil Engineer and the plough must not be employed for spreading material other than crushed slag or stone, gravel and ashes suitable for top ballast.

Discharging ballast from hopper trains.

When required to be unloaded on lines already opened for traffic, the Permanent Way Inspector of the district will give instructions where the ballast is to be discharged, but the Guard of the train and the Ganger of the length must satisfy themselves that there are no fittings, catch points, guard rails, signal wires, locking gear rods, detonator machines, automatic train control ramps, level crossings, or other works with which the plough can possibly come in contact on the lengths of line to be ballasted.

Responsibilities of Guards in charge of hopper train.

The Guard will be held solely responsible for the proper working of the train and for satisfying himself that:—

- (a) Hopper wagons and plough vans are in good order and examined at the usual examining points en route.
- (b) When not engaged in unloading, the plough is in its normal position by being raised to the fullest extent, and the two keys inserted and secured by padlock.
- (c) When starting on a journey with a train which has to be unloaded in a tunnel, the plough is in the right direction, also that the hoppers are arranged so that all the levers are one side of the train, and that where unloading has to be performed in a double line tunnel, all levers face the 6-ft. way.
- (d) Upon arrival of the train at the place where it is intended to unload, and after it has been placed in position the plough is lowered to the rails, but not screwed hard down. That the French keys in the centre and right-hand levers of the wagons are then removed and the key of the left-hand lever kept in.

That the hopper wagon next to the engine is discharged first, the door being opened by removing the key from the left-hand lever, and the engine started at a speed of not more than four miles per hour. Before the whole of the contents of the first wagon to be discharged are run out, that the hopper door of the next wagon to it is lowered, and so on through the train.

The Permanent Way Inspector or Ganger will assist in the discharge of the train, one being on one side of the train and the Guard on the other.

- (e) When the contents of the whole train have been spread, and before resuming the journey, the plough is cleaned with the scrapers which are kept in the plough van, then raised to the fullest extent and secured by means of the keys and padlock.
- (f) Before proceeding with the empty train the doors of the hoppers of the wagons are closed and secured with the French keys which are to be properly fixed in the three levers, the keys being inserted in the holes from the left-hand side.

Lubrication.

The lubrication, which includes doors, pins and slides of hopper ballast wagons and plough vans will be done by the C. & W. Engineer's Department staff at the loading points.

Closing doors of hopper wagons.

Should any difficulty be experienced in closing the doors of hopper wagons, force must not be used, but the doors swung back against the stop on the solebar to dislodge any fine ballast from the ledge on the "Z" iron.

(G.A.30 Op.—9/54. E.82672H. (12-C.).

# WORKING OF VACUUM AND PARTLY VACUUM FITTED FREIGHT TRAINS—page 172

### (A) Freight Trains carrying "C" Headcodes.

Clause 4 amended to read:-

4. Piped or fitted freight train brake van must be provided. (G.A.30 Op.-9/54. E.1562.W.).

### WORKING OF VACUUM AND PARTLY VACUUM FITTED FREIGHT TRAINS—Page 173

The following to be inserted immediately after note "Z":-

Note. For the purpose of giving effect to this instruction the minimum number of wagons to be coupled with the vacuum pipe to the train engine to be based on the equivalent load for Class 3 traffic, e.g., should a load of 44 wagons of Classes 2 and 3 traffic be equal to 60 wagons of Class 3 traffic the minimum number of wagons to be coupled with the vacuum pipe to the engine to be 20 and not 15.

(G.A.30 Op.-9/54. E.84669H. (2-C.)).

Reference to the following to be made on page 174:-

### RUNNING OF SPECIAL FREIGHT TRAINS.

When an Inter-District Special freight train is required (other than those pre-arranged by notice or other means) the request to be passed by the originating District Control to Head-quarters Control, Paddington, with details of headcode, load to be conveyed and time the Special train is required to start. The Headquarters Control to make the necessary arrangements with the other District Controls including points at which Carriage and Wagon examination is to be carried out and agree or otherwise with the District Control making the application whether or not the Special train can run.

As long notice as possible, minimum 4 hours, must be given to the Motive Power Department to enable preparation to be made in the provision of power. A serial number will be given by Headquarters Control to all Special freight trains agreed and this number to be quoted in all messages, telephonic or telegraphic, sent in connection with the train concerned.

(G.A.30 Op.-9/54. E.81019 H(7-B.)).

### WORKING OF FREIGHT TRAINS.—Page 174.

The instructions under the above heading to be cancelled and the following substituted:—

### **EXAMINATION OF FREIGHT TRAINS.**

Unless otherwise specially agreed between the Operating Superintendent and the Carriage & Wagon Engineer, freight trains must not run more than the following distances shown under the various headcodes without stopping for Carriage and Wagon examination.

Class of Freight Train	Maximum Distance in Miles, train is allowed to run without full Carriage & Wagon Examination	Type of Axle Box
"C" "E" "E" "F" and below. "F"	160 125 125 125 85 85	Oil Axle Box Oil Axle Box. Oil Axle Box. Grease Axle Box. Oil and Grease Axle Box. Wagons (loaded or empty). Oil or Grease Axle Box empty wagons only*

<sup>\*</sup> Full train loads of empty wagons capable of being run under "C", "D" or "E" headcode conditions to be so arranged.

(G.A.30 Op .-- 9/54. E.81019. H(7-B.)).

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### GENERAL INSTRUCTIONS FOR WORKING INCLINES .- Page 178.

Descending Inclines.

Clause 15 amended to read:-

15. All ballast and freight trains, including those consisting partly of vacuum stock but excluding those which are fully vacuum fitted, must stop at the foot of inclines as well as at the top.

(G.A.30 Op.—9/54. E.82672. H.(12-C.)).

# BRAKING VACUUM-FITTED AND PARTLY VACUUM-FITTED FREIGHT TRAINS DOWN INCLINES.—Page 180.

Clause I—The words "Guard's Brake Valve" to be substituted for "Brake Setter" in the last sentence of this clause.

(G.A.30 Op.-9/54. E.1562W.)

# BRAKING VACUUM-FITTED AND PARTLY VACUUM-FITTED FREIGHT TRAINS DOWN INCLINES.—Page 181.

Clause 5 amended to read:-

5. These instructions do not apply to Ballast Trains partly composed of vacuum fitted stock which must work under the Incline Instructions.

(G.A.30 Op.-9/54. E.8267 H.(12-C.)).

Reference to the following to be made on page 182:-

### REGULATIONS AS TO PRIVATE OWNERS' WAGONS.

- I. The Owners of all new or rebuilt vehicles intended to work upon the Western Region must, before they are brought into use, communicate with the Carriage & Wagon Engineer, Swindon, so that he may have them inspected without unreasonable delay, and if built, or rebuilt, in accordance with the Railway Clearing House Standard Specification and addenda thereto, register plates, as described in the Specification, shall be forthwith affixed to each.
- 2. The name and address of the Owner or Lessee, the wagon number, and the tare, shall be painted conspicuously on both sides of the wagon; the maximum load must also be clearly indicated on both sides of the wagon.

When wagons are let on hire the Lessee will, for the purpose of these Regulations, be regarded as the Owner.

Provided that when the hire is for a term of not less than three months, the name and address of the Lessee shall be painted or exhibited on a board or plate on both sides of the wagon, and that when the hire is for less than three months, the name and address of the Lessee shall either be so painted, or exhibited on a card (other than the wagon label), on both sides of the wagon.

- 3. The Owners or Lessees, as the case may be, shall keep their wagons in good working condition, and have them properly lubricated and examined and put into good repair before being tendered to the Western Region for transit.
- 4. The British Transport Commission may remove the register plates from any wagon if wheels, axles, or any other materials of less dimensions or strength than those provided for by the Railway Clearing House Standard Specification are afterwards substituted in contravention of the conditions of the said Specification.
- 5. If in transit any defect shall be observed, which for the proper and safe working it is necessary to repair before the vehicles are allowed to proceed further, the British Transport Commission may, with the consent of the Owners, make such repairs, and charge them with all expenses incurred in effecting the same.
- 6. In pursuance of the rules made by the Board of Trade under the provisions of the Railway Employment (Prevention of Accidents) Act, 1900, when it is necessary in the ordinary course of business that any label or direction as to destination or consignee shall be placed upon any railway wagon, such label or direction must be placed on both sides of such wagon; and no Private Owner's Wagon will be accepted for conveyance by the Western Region unless so labelled or directed on both sides.

All wagon Owners, representatives of wagon Companies and their repairers, when labelling defective wagons at railway stations and depots, shall clearly set forth on the labels the station or siding from and to which the wagons are required to travel, and hand in a proper Consignment Note or written forwarding instructions.

The British Transport Commission agrees, without prejudice, to dispense with a Consignment Note or written forwarding instructions in the case of wagons painted with a yellow star in accordance with the provisions of the Commuted Empty Haulage Scheme, labelled to Repair Works.

Note-Not applicable to wagons working in Scotland.

7. When wagons, for the purpose of repair, are required to be shunted into and out of Sidings belonging to the British Transport Commission and/or into and out of premises in the occupation of Private Wagon Repairers, a charge of 2s. 8d. per wagon will be made for such services, except where a higher charge is now made, in which case such higher charge shall be the maximum charge under these regulations.

Siding rent will be chargeable to the owner, or his agent, in respect of standing room for any wagon detained at a station or siding for repairs at the rate of 1s.  $5\frac{1}{2}$ d. per wagon per day, which will be calculated from the expiration of three days, exclusive of the date of the advice note, Sundays and Bank Holidays, from the time the wagon is placed at the disposal of the Owner or Repairer, and to terminate when the wagon is labelled for despatch after repair.

The foregoing charges do not apply when the owners are parties to the Commuted Shunting and Siding Rent Charge Scheme, in which case the wagons are plated or stencilled "C.C."

- 8. Any authorised servant of the British Transport Commission may detain any wagon which may appear to him unfit to run until it has been put into proper repair and passed by an authorised person of the Western Region Carriage & Wagon Engineer's Department.
- 9. The British Transport Commission will not be responsible for any damage to Private Owners' Wagons left unprotected in an imperfect state by the Owners, nor for any injury that may occur to wagon repairers, who will be required to execute an indemnity before they are allowed to work on British Transport Commission's premises.
- 10. Private Owners' Wagons running over the British Transport Commission lines must not, apart from a reasonable description of the contents of the wagon, be used for advertising purposes, but the British Transport Commission will not object to a description (to be approved by them) of a product of the Owner's manufacture being painted thereon.
- 11. Nothing contained in these Regulations shall prejudice or affect any legal liability to each other of the actual Owners or Lessees of Wagons and the British Transport Commission.

(G.A.30 Op.-9/54. E.86585 H.10-F.).

### WAGONS LABELLED TO PASS ON SLOW TRAINS .- Page 184.

The instructions under this heading to be cancelled.

(G.A.30 Op .-- 9/54 E.86585H.).

# RAILWAY COMPANIES' OR PRIVATE OWNERS' WAGONS LABELLED FOR REPAIRS. —Page 184.

The instructions under this heading to be cancelled and the following substituted:—

### WAGONS LABELLED FOR REPAIRS.

Wagons fitted with oil axleboxes and bearing green "FOR REPAIR" labels, indicating defects other than those appertaining to axleboxes, axleguards, wheels and axles may be conveyed by trains running under "D" or inferior headcodes.

Wagons fitted with grease axleboxes and bearing green "FOR REPAIR" labels, indicating defects other than those appertaining to axleboxes, axleguards, wheels and axles may be conveyed by trains running under "F" or inferior headcode conditions.

Wagons bearing green "FOR REPAIR" labels indicating a defective axlebox, axleguard, wheel or axle, the nature of which does not call for the wagon to be loaded upon another vehicle, should only be conveyed by trains running under "H" or inferior Headcode conditions.

(G.A.30 Op.-9/54. E.86585H.)

### TRANSIT OF STEAM AND HAND TRAVELLING CRANES .- Page 186.

The following to be added as clause (c):-

A special train conveying a 45 ton steam crane complete with match truck and Stokes bogies, which are piped, when travelling within the Western Operating area to or from the site of Engineering Department Operations, may carry "B" headcode, provided the Crew's van is also piped and the other vehicles comprising the train are fully vacuum fitted. Such trains must not exceed a speed of 45 m.p.h. at any point.

G.A.30 Op.-9/54. LKI/12204/417. EX.3509/53.).

#### PREVENTION OF ACCIDENTS.—Page 266.

The following to be added to paragraph 5 respecting the whitening of platform edges:—
"The edges of the ramp must not be whitened."

(G.A.30 Op.-9/54. LKI/9678 Gen.).

# INSTRUCTIONS FOR REQUISITIONING, STORAGE AND DISTRIBUTION OF PETROLEUM FOR TRAFFIC AND GOODS DEPARTMENTS.—Page 280.

Amend the word Extension in the last line of the second paragraph to read "Extinction." (G.A.30 Op.—9/54).

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# GLASSES FOR LAMP CASE INTERIORS .- Page 285.

Delete the entry in respect of Route Indicator and Permanent Speed Indicator Lamps and insert the following:

> Route Indicator Lamps 43in. x 54in. Permanent Speed Indicator Lamps Front glasses 57in. x 5in. Side glasses 3in. x 21/in.

(G.A.30 Op .- 9/54 LKI/10676/26).

# HANDLING OF MOTOR CYCLE TRAFFIC .- Page 288

The following to be inserted as clause (e):-

At stations where special motor cycle loading boards are provided they must always be used for loading or unloading motor cycles into or out of vans. The boards must be carefully handled and after use they must be returned to their storage point.

(G.A.30 Op.-9/54. A2/154/5S.).

## EXAMINATION, MAINTENANCE, TESTING AND WORKING OF LIFTING AND HAULING APPLIANCES.—Page 293.

## Instructions to Staff respecting Working and Transit.

The following to be added as clause 13a:-

Staff not to expose to danger.

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Staff must see that they do not expose themselves to danger during lifting operations by standing where the load may fall, be lowered on to them, or strike them during motion, or where they may be struck by rotation of crane handles which, through some defect, cannot be removed or disconnected as required by Clause 10. When withdrawing chains or slings clear of loads, care must be taken to ensure that they do not catch in the load and cause it to overturn.

Staff must not ride on a hook or load, nor interfere with a load except to guide or prevent it swinging.

(G.A.30 Op.-9/54. C.R.O.-W.36027/5).

### Chains-Annealing and Testing.

Amend the first sentence of clause 56 to read:-

Form 2432-1 must always be rendered in duplicate when a chain is forwarded for testing.

(G.A.30 Op.-9/54. C.R.O.-W.36027/5).

Amend the first sentence of clause 57 to read:-

Loose lifting tackle required for temporary use should be requisitioned on form 2432 rendered in duplicate amended as necessary, and the numbers quoted as appearing in the diagram of "Standard Lifting Tackle.

(G.A.30 Op.-9/54. C.R.O.-W.36027/5).

## CLEANING, TRIMMING AND LIGHTING OF TRAIN LAMPS AND MODE OF WORKING .-- Page 310.

The first paragraph of these instructions to be amended to read:-

Side, Tail and Hand Lamps .- Only petroleum must be used in these lamps; great care must be taken in trimming the lamps to ensure that an adequate quantity is poured into the tank but that the tanks are not filled above the level of the bottom of the burner collar. The tanks must be wiped dry before being placed in the lamp, every part of which must be well cleaned especially the burners and vent holes.

(G.A.30 Op .-- 9/54 LKI/10688/229).

GUARDS AND ENGINEMEN TRAVELLING FROM P	OINT TO POINT TO RELIEVE
TRAINMEN AND TRAINMEN PROCEEDING TO	THEIR HOMES AFTER BEING
RELIEVED en route FOR REST, ETC.—Page 338.	

The instructions under this heading to be deleted.

(G.A.30 Op.-9/54 S.57368 M.).

September, 1954.

GILBERT MATTHEWS,
Operating Superintendent.

Each member of the Staff receiving a copy of this Circular is required to read carefully and note the contents, and, if supplied with a copy of the General Appendix to the Rule Book to alter or cancel in ink the present instructions on the subject appearing therein, afterwards pasting the amendments in their proper places in the General Appendix to the Rule Book.

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B.R. 30145/29 G.A.30.Op.

(This form must be detached and forwarded	to the Head of Department.)
Station.	1954
RECEIVED copy of Circular G.A.30.Op., dated Sept additions to the General Appendix to the Rule Book.	tember, 1954, containing alterations and

G.A.31.Op

# BRITISH RAILWAYS

(WESTERN OPERATING AREA)

# Alterations and Additions to the General Appendix to the Rule Book

To come into operation on receipt.

### ADDITIONS TO THE STANDARD RULES.

Rule 215-Use of Trollies where track circuiting is provided .- Page 21.

The third paragraph under this heading to be deleted.

(G.A.31-7/56. O.M.12789)

### INTERMEDIATE BLOCK SIGNALS-Pages 71 and 72.

The following to be inserted as Clause (h) paragraph on page 72:-

### (h) Trolley Working.

Where Intermediate Block Signals are provided, before a trolley is placed upon or run over a track circuit, the permission of the Signalman to whose box the track circuit is connected must first be obtained. In such cases a telephone is provided which may be used by the Ganger to communicate with the Signalman.

Where the overlap track circuit of the Intermediate Block Home Signal does not extend to the Home Signal of the signal box in advance and a trolley is required to be placed on the line in advance of the overlap track circuit of the Intermediate Block Home Signal, the Handsignalman, when going back to protect such trolley must, if he should reach the Intermediate Block Home Signal, telephone to the Signalman and, after acquainting him with the position, continue to go back protecting the trolley until reaching the  $\frac{1}{4}$  mile point as required by the Rule. He must then act in accordance with Rule 215, Clause (d).

When the trolley proceeds the Handsignalman upon following must, when he comes to the telephone at the Intermediate Block Home Signal, advise the Signalman that the trolley has proceeded. The Signalman must be similarly advised if the trolley is removed from the line before the Handsignalman has passed in advance of the Intermediate Block Home Signal.

The Signalman, upon being advised by the Handsignalman, must keep the Intermediate Block Signals in the "On" position until such time as he receives intimation that the trolley has either gone forward to a point  $\frac{1}{4}$  mile in advance of the Intermediate Block Home Signal or has been removed from the line.

(G.A.31-7/56. O.M.12789)

### PUBLIC LEVEL CROSSINGS LOCKED BY PADLOCK.—Page 73.

The entry under this heading to be deleted.

(G.A.31-7/56 O.M.12763)

### DETONATORS, PERIODS KEPT IN STOCK, PAINTING OF DETONATORS.—Page 84.

Reference to "Rules 56 to 58" in the first paragraph of these instructions to be deleted and substituted by Rule 58.

(G.A.31-7/56 L.K.1/12963/418)

(G.A.31-7/56 L.K.I/12351/420)

### WATER TROUGHS, G.W. AND JOINT LINES .-- Page 89.

The following to be added :-

Water Troughs, Clearance of Ice.

- 1. The Length Ganger will be responsible for keeping ice broken and cleared from the troughs.
- 2. The Length Ganger must use his discretion when the ice thickens as to when the troughs should be closed, but must not allow the ice to become more than  $\frac{1}{8}$  in. thick before arranging for the troughs to be closed. If the temperature is so low that the water freezes as quickly as it is cleared the troughs must be closed.
- 3. If ice collects in the four foot to a depth of 1 in. due to spillage the troughs must be closed.
- 4. When the Length Ganger finds it necessary to close the troughs as set out in Clause 2, he must advise the nearest Signalman who will notify the District Controller. In turn the District Controller will immediately notify the following:—

Chief Mechanical and Electrical Engineer's Outdoor Machinery District Mechanical Foreman or Local Mechanical Chargeman and responsible Water Fitter whichever is most convenient.

Motive Power Depots.
District Motive Power Superintendent.
Headquarter's Control.
Chief Mechanical and Electrical Engineer's Works Manager.
District Engineer.

The Length Ganger must also advise his Permanent Way Inspector.

- 5. When a thaw sets in after prolonged frost the Chief Mechanical and Electrical Engineer's Outdoor Machinery District Mechanical Foreman or Local Mechanical Chargeman or responsible Water Fitter, whichever is most convenient, after consulting with the Ganger, will say when normal taking of water can be resumed, and will advise the nearest Signalman and arrange for the water to be turned on. The Signalman must, in turn, advise the District Controller, and he will notify the Departments listed in Clause 4 that normal working of the trough can be resumed.
- 6. The District Operating Superintendent to notify promptly the Chief Operating Superintendent, who will issue the necessary instructions in regard to the taking of water while the water trough is out of use.

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### ROBBERIES FROM LUGGAGE VANS .-- Page 117.

Paragraph 2 of the instructions under this heading to be deleted and substituted by the following:—

2. The Guard in charge must see that the inside sliding doors of luggage compartments, where such are provided, and the steel grilles of luggage compartments fitted in British Railways Standard Stock, are kept closed and locked whilst the train is travelling.

(G.A.31-7/56-T.50, 639G/7 L.K.1/13138/420)

# EMERGENCY APPLIANCES, ETC., IN PASSENGER AND PARCELS TRAINS.-Page 121.

The following to be inserted after "Diesel Cars" in the table in the first paragraph:-

New Type Diesel Cars.

Power Cars.—One water/CO.2 pressure extinguisher in the brake compartment and two CO.2 gas extinguishers in the driving cab.

Note.—A chlorobromomethane (C.B.) extinguishing system is fitted to each diesel engine, with detectors to give the driver warning of fire and indicate which motor is affected.

Trailer Cars.—One water/CO.2 pressure extinguisher at one end of the corridor.

(G.A.31-7/56 A.1/72034/102)

7. Train First-aid Cabinets-Replenishment Cupboards.

The entry against LONDON-Old Oak Common should be amended to read "A x 3."

(G.A.31-7/56 S.2/AMB/7)

### WORKING OF DIESEL CARS .- Page 132.

The following to be added at the end of the second paragraph under the heading "Instructions to Signalmen.":—

"The Train Entering Section" signal for the Diesel Car must be sent when the car leaves the signal box controlling the Intermediate Block Signal.

(G.A.31-7/56 L.K.1/12510/372)

Reference to the following to be made on page 141:-

### STANDARD WHISTLE CODE.

Drivers requiring Assistant Engine or Fresh Engine en route.

In order to minimise delays to trains because of partial or total engine failure, the attention of all Drivers and Signalmen is directed to the standard instructions in the Sectional Appendices which provide for the use of the following whistle code:

Express trains requiring a fresh engine at next stopping place ...... 3 crows.

The sounding of this whistle code will denote to Signalmen that another engine is required at the next station, if available there, or at the next Motive Power Depot, and

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Signalmen must immediately pass forward an advice, giving title and head code of train in difficulty to the appropriate Control Office, who will decide what action is necessary and issue instructions accordingly. In cases where a fresh engine is required from a Motive Power Depot, an advice must be passed forward by the Control without delay, and, on receipt of such an advice, Motive Power Depot staff must take steps at once to provide the required fresh engine.

(G.A.31-7/56 T.38, 113.G.1 7.38, 417.G.1 M.1953)

### WORKING OF ENGINES IN STEAM COUPLED TOGETHER.-Page 142.

The instruction under heading (B) "Over Royal Albert Bridge, Saltash" (see Supplement G.A.30) to be cancelled and the following substituted:—

- (1) The speed of all engines passing over the structure must not exceed 15 m.p.h.
- (2) Not more than two permitted engines may work coupled together.
- (3) The instructions shewn on pages 144 and 145 of the General Appendix to the Rule Book in respect of the assisting or double heading of trains must be observed.

(G.A.31-7/56. T.D.95/41)

### RESTRICTIONS ON WORKING OF WESTERN REGION WIDE STOCK .- Page 151.

66 ft. 8 ins, long by 8 ft. 11 ins. wide.

Delete the following entry from the list of lines prohibited:—
Brynamman Branch.

(G.A.31-7/56 R6/-)

#### Passing Restrictions.

The last paragraph of these Restrictions dealing with the Newport Engineering District as listed in G.A.26 Op. to be deleted and the following inserted:—

These coaches are prohibited from passing other coaching stock and out-of-gauge loads between the following points in the Newport Engineering District:—

Maesycwmmer Station (inclusive) to Pengam Station.

The existing restrictions so far as other Regions are concerned will continue to apply as already shown in G.A.18.

63 ft. 61 ins. long by 9 ft. 3 ins. wide.-Page 152.

Delete the following from the list of lines over which this stock is prohibited:— Vale of Glamorgan.

(G.A.31-7/56 R.6/3926)

Reference to the following to be made on Page 158:-

#### BRAKE STICKS

All brake sticks on hand at stations and in yards must be examined during the first week of June and December and any which shew signs of having become defective must be returned to Swindon Stores. Requisitions for replacements must be submitted through the usual channels.

(G.A.31-7/56-L.K.1/13121/420)

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ek be he "INSTANTER" PATENT COUPLER.—Page 158.

The paragraph following figure 2 to be amended to read:-

The coupling can be used for shunting operations as an ordinary loose coupling as shewn in figure 2. When the centre link is required to be placed into the short or close-coupled position the following procedure to be observed.

(G.A.31-7/56 L.K.1/9003/Gen.E.)

### CONVEYANCE OF PASSENGER TRAIN STOCK ON FREIGHT TRAINS.—Page 168.

The following to be added as paragraphs 7 and 8:-

- Eight-wheel ex-passenger stock converted as tool vans, mess vans, etc., when conveyed
  on freight trains must be formed immediately in front of the brake-van or with other
  8-wheel vehicles so formed.
- 8. Four- or 6-wheel ex-passenger stock converted as tool vans, mess vans, etc., may be formed in any position on freight trains, but unless piped or fitted with the automatic vacuum brake they cannot be conveyed on fully fitted trains, nor in the fitted portion of a partially fitted train.

(G.A.31-7/56 L.K.1/12693/232)

### WORKING OF VACUUM AND PARTLY VACUUM FITTED FREIGHT TRAINS .- Page 172.

B-Freight Trains carrying "D" headlamps.

The Note following Clause (1) to be deleted.

(G.A.31—7/56—E.2109H. (1-B))

STATION INSTRUCTIONS—Pages 262-344.

Cleaning Petroleum Lamps-Page 281.

The instructions under this heading to be deleted.

(G.A.31-7/56 B.R. 29611)

Long Burning Signal Lamps-Pages 282-285.

The instructions under this heading to be deleted and substituted by the following :-

### LONG BURNING SIGNAL LAMPS-EQUIPMENT.

The undermentioned articles may be ordered by Signal Lampmen from the Stores Department on Requisition Form No. 224 through the Station Master at their Home Station, and arrangements should in future be made accordingly:—

Glasses for Lamp Case Interiors.

Standard Pattern Interior Circular Tank.

Front,  $4\frac{3}{4}'' \times 4\frac{1}{4}''$ . Back,  $3\frac{1}{4}'' \times 2\frac{3}{4}''$ 

Old Pattern Interior Shallow Circular Tank.

Back and Front, 37" × 33".

Semaphore 9" and 11" Cases.

Back and Front,  $5\frac{1}{2}$ "  $\times$  5". (Bottom corners cut away.)

Repeater 9" and 11" Cases. Back and Front, 43" × 43".

Route Indicator Lamps.  $4\frac{7}{8}'' \times 5\frac{1}{4}''$ .

Permanent Speed Indicator Lamps. Front glasses,  $5\frac{7}{8}'' \times 5''$ . Side glasses,  $3'' \times 2\frac{1}{4}''$ .

Taff Vale Section "B" Type.  $5\frac{1}{4}$ "  $\times 4\frac{1}{2}$ " thin.

Taff Vale Section "C" Type.  $4\frac{7}{8}" \times 4\frac{1}{8}"$  thin.

Barry Section Signal. Curved, 53" high × 43".

Barry Section Disc. Curved, 41" high × 25".

Rhymney Section. 23" × 33" thin.

Rhymney Section. Elevated Disc,  $5\frac{1}{2}'' \times 3\frac{7}{8}''$  thin.  $5\frac{1}{2}'' \times 3\frac{1}{8}''$ .

 $5\frac{1}{2}$ " ×  $3\frac{3}{8}$ ".

B. & M. Section Signal.

55"×51".

Wicks for Ordinary and Disc Long Burning Lamps. Ditto for Standard "Serex" (T.V.) Lamps. Feeding, Round, 11" long.

Wicks for Repeater Long Burning Lamps.

Burning,  $12'' \times \frac{3}{8}''$  wide. Feeding,  $6'' \times \frac{1}{2}''$  wide.

Tins, Safety, for Methylated Spirit.

Scissors, Lamp.

Tins, Pocket, wick.

Polish, Metal, for cleaning reflectors.

Special attention is directed to the fact that metal polish instead of whitening may be ordered for cleaning lamp reflectors.

Leather washers should be used on the screw stoppers of these lamps, and if any are required at any time they can be requisitioned by Station Masters through the District Signal Inspectors.

Long Burning Lamps for Speed Indicators and Route Indicating Signals.

The burners of these lamps are 5 wide, and wick of similar width is supplied for use in the lamps.

(G.A.31-7/56 B.R. 29611)

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# INSTRUCTIONS FOR USING PARAFFIN VAPOUR LAMPS—"TILLEY" TYPE.— Pages 285-287.

The instructions under this heading to be deleted and substituted by the following:-

#### PARAFFIN VAPOUR LAMPS--" TILLEY" TYPE-SPARE PARTS.

The Operating Department Staff can order the following, but care must always be taken to quote the correct number of the respective part when ordering:—

Part No. Name of Part.

123-90 Globes, Clear (outside lighting).

123-91 Globes, Clear and Frosted (inside lighting).

133-40 Mantles, Inverted, Large, No. 2.

133-42 Mantles, Inverted, Small, No. 1, Indoor Lamp (I.L. 37).

133-39 Mantles, Inverted, Floodlight Proiector.

133-79 Washers, Vaporiser, Black, No. 153.

133-19 \*Caps, Screw, Filler "F."

133-25 \*Connections, Pump, 15".

133-37 \*Lighters, Paraffin.

Part No. Name of Part.

133-46 \*Pumps, without connections.

133-74 \*Vaporisers, complete.

133-81 Washers, XN Cock Black, No. 160.

119-31 \*Cans, Oil, with spout.

119-165 \*Angle Funnels, Tin, Gauze strainer.

133-22 \*Carriers, Wire, "A," for P.L. 55 and 56 lamps.

133-23 \*Carriers, Wire, "B," for all other lamps.

130-10 Balls, 32", No. 147.

109-31 Special Brush for cleaning burner.

\*These items are obtainable only on Repairs Requisitions (Form No. 224), and the old articles must be returned to Swindon.

Spare Parts must be kept by the person in charge of the place where the lamps are installed, i.e., Station, Yard, Depot or Signal Box.

(G.A.31-7/56 B.R. 29611)

# CLEANING, TRIMMING AND LIGHTING OF TRAIN LAMPS, AND MODE OF WORKING. --Page 310.

The instructions under this heading to be deleted.

(G.A.31-7/56 B.R. 29611)

### S. G. HEARN,

Chief Operating Superintendent.

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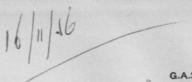
B.R. 30145/31 G.A.31.Op.

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(This form must be detached and forwarded	to the Head of Department.)
Station.	1956
RECEIVED copy of Circular G.A.31.Op., dated July,	1956, containing alterations and additions
to the General Appendix to the Rule Book.	



### BRITISH RAILWAYS

(WESTERN OPERATING AREA)

# Alterations and Additions to the General Appendix to the Rule Book

To come into operation on receipt.

The following instructions to be inserted on page 70:-

# INSTRUCTIONS FOR RUNNING AND WORKING OF THE LENNOX-LOMAX EARTH AUGER EQUIPMENT.

- 1. The machine must only be used by the staff authorised by the Signal Engineer.
- 2. The machine, which is provided with a special match truck, is capable of self-propulsion at a speed of 10 miles per hour (in forward or reverse gear) and, except when operating, must have the match truck coupled. The machine is fitted with three types of brakes, viz., Hydraulic, Transmission and hand screw wheel. The match truck has a hand brake only.
- 3. When travelling under its own power the machine or the match truck, as the case may be, must carry a white headlamp and a tail lamp, which must be lighted as necessary. A red flag must be displayed on the rear vehicle by day. The machine must be equipped with red and green handsignal flags, not less than 12 detonators, a hand lamp (lighted when necessary) and a sprag. A portable telephone or "walkietalkie" apparatus must also be available.
- 4 When boring operations are required to be carried out an absolute occupation of the line concerned must be arranged. The service on which the machine will be worked to the site of operations and the occupation required must be pre-arranged with the District Operating or District Traffic Superintendent concerned.
- 5. The machine must be worked to the agreed place near the site of work, or vice versa, by freight train carrying "F" or inferior headcodes and be marshalled next inside the Guard's brake van. The match truck may be leading or trailing. Should it be necessary to run the machine as a special train hauled by a locomotive a brake van must always be provided at the rear, in which a Guard must ride.
- 6. When the machine is proceeding under its own power to the site where it is required to work the match truck must always be attached and both the screw and chain couplings must be used for the purpose. Such movements must be confined to the shortest possible distance but if it is necessary for the machine to pass completely through one or more sections it must be signalled and dealt with as a Through Ballast Train. When running in a multiple-aspect signalling area automatic signalling must be suspended and the machine dealt with in accordance with T.C.B. Regulation 15. Where an Inner Home Signal is provided the "Is Line Clear?" signal must not be accepted until the Home signal can be lowered.
- 7. The machine must only be moved under its own power by the Driver who has been passed as competent by the Signal Engineer and he must be accompanied by a man who has been passed as competent by the Motive Power Department in (a) knowledge of the route and (b) protective duties of a Driver. A member of the crew of the machine who has been passed by the Operating Department as competent to carry out the protective duties of a Guard must always be present.
- 8. The machine must not be relied upon to actuate track circuits and Rule 55 must be observed in all cases by the man acting as Guard. When the machine is detained whilst waiting acceptance by the box in advance it must not draw forward to the signal controlling the entrance to the section ahead or to an Intermediate Block Home signal but must be held opposite the box. No train must be allowed to follow the machine towards the Intermediate Block Signal until "Train out of Section" has been received.

- 9. A good look out must be kept when approaching level crossings.
- 10. No movement must be made past stop signals otherwise than with the consent of the Signalman.
  - 11. Protection at the site of boring operations must be in accordance with Rule 217.
- 12. A portable telephone or "walkie-talkie" sets must be provided to enable contact to be maintained between the site of operations and the signal box in the rear.
- 13. When the machine is working, trains may pass on an adjoining line without restriction except as provided for in Clause 14.
- 14. Should boring between running lines be required to be carried out or should there be any possibility that the operations will obstruct the opposite or adjoining line, prior notification must be given to the Operating Department and a responsible member of the Operating Department must be present and the operations must not be commenced without his permission. Before giving such permission he must ascertain from the Signalman in the rear that no train is approaching on the opposite or adjoining line and the Signalman, before agreeing to obstruction of such line, must comply with the provisions of Block Regulation 13. The line affected must, in addition, be protected by a Handsignalman in accordance with Rule 217.
- 15. When the opposite or adjoining line is clear, the Signalman must be advised and such line must not be further obstructed until the provisions of Clause 14 have again been complied with.
- 16. At the conclusion of boring operations the machine and match truck will be removed from the section under the power of the machine. If the machine is returned to the signal box in rear, the Driver must bring it to a stand before reaching the detonators protecting the work. The conductor must proceed on foot to obtain the Signalman's Wrong Line Order authorising return to the signal box. The Signal Department person in charge must give the Signalman an assurance that the section is clear of obstruction. (G.A.32.Op—10/56. OM12832)

#### S. G. HEARN,

October, 1956.

Chief Operating Superintendent.

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B.R. 30145/33 G.A.32.Op.

(This form must be detached and forwarded to the Head of Department.)
RECEIVED copy of Circular G.A.32.Op., dated October, 1956, containing alterations and additions to the General Appendix to the Rule Book.

13 AUG 1951

THE RAILWAY EXECUTIVE (WESTERN REGION)

District Operating Superintendent's Office, Rolling Stock Section, BRISTOL (TEMPLE MEADS).

August 11th, 1951.

G. X. 4571.

Dear Sir,

### STANDARDISATION OF PASSINGER COACHES.

In connection with future building programmes, further British Standard Coaches, similar to those now working in the "Merchant Venturer", are expected from Shops, but for the present they must not be worked to other Regions, without special authority.

Dimensions of the Coaches are as follows :-

- (1). 63' 6" long over headstock by 9' 0" wide and 12' 10" high from rail level, with bogic centres not more or less than 46' 6"
- (2). 57' long over headstock by 9' wide and 12' 10" high from rail level, with bogie centres not more or less than 41' 0"

Passenger carrying Stock, except for the Open firsts and Open thirds, that work with the Eitchen cars, will bear five figure numbers prefixed by the letter 'W', 'H', 'E', 'S' or 'SC'.

The rollowing restrictions and prohibitions apply to the Standard British Railway Coaches working over the Western Region, and I shall be glad if you will make the necessary arrangements for these restrictions and prohibitions to be strictly observed.

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## LINES TOTALLY PAORIBITED.

Highworth Branch. Oulm Valley Branch. Looe Branch. Liskeard - Looe Branch. North Roskear Branch. Trenance Branch. Keyhan - H.H. Dockyard. Burry Port and Gwendreath Valley line (Burry Port - Cwmmawr). Shipston-on-Stour. Pontcysyllto Branch. Cleobury Mortimer and Ditton Priors Light Railway. Clechill Branch. Criggion Branch. Vron Branch. Wrexhan and Minera Line (Brymbo-Terminus). Grymawr (Blaenavon Bay). Syansea Valley Line and Branches. Swansea Victoria Station. Brynnawr (MTA Line) Up Line through Station.

# LINES OVER WEICH RESTRICTIONS ARE TO BE OBSERVED.

M. & S. W. Jon. Line ) Swinden - Andover.

Valley Lines on Cardiff and Newport District, Passing
In South Wales.

Beauford Rd. S3 to Ebbw Valo Jon.

Will you kindly issue the necessary instructions to all staff under your control and acknowledge receipt.

Yours truly,

Lo. Eccumands

# W.R. LOCOMOTIVE STOCKING SHEWING TYPE CODE AND MOTIVE POWER CLASSIFICATION.

CLASS	OF ENGINE.	W. A. TYPE CODE.	ENGINE NOS.	M.P. CLASS.
4-6-0	King Castle	2	60xx 100, 111, 4000, 4016, 4032, 4037, 4073-99,	7 P. 6 P.
	Star Saint Hall Grange Manor County	34568	50xx, 70xx. 40xx (except 'Gastles') 29xx 49xx, 59xx, 69xx, 79xx 68xx 78xx 10xx	5P. 4 P. 5 M.T. 5 M.T. 5 M.T. 6 M.T.
4-4-0	Bulldog Duke & 90xx	8	35xx, 34xx 90xx	3 P. 2 P.
2-6-0	28xx 47xx R. O. D. W. D.	14 15 15 89	28xx, 38xx 47xx 30xx (90xxx) *	8 F. 7 F. 7 F. 8 F.
2-6-0	Abordare 45xx	17	26xx 43xx, 55xx, 63xx, 73xx, 93xx.	1 F. 4 M. T.
2-4-0	M. & 3.V.	9	1354-36	1 M.T.
Con 6+0	2351 ) 2301 ) Cam. )	18	22xx, 32xx 23xx, 24xx, 25xx 844 - 896	3 M.T. 2 M.T.
2.8.2T	72xx	33		8 F. T.
2-8-0T	42xx	31	( 5205-64 ( 42xx, 5200 - 04	8 F.T. 7 F.T.
2-6-2T	33.50-81xx	33	( 3150-90,41xx, 51xx ( 61xx, 81xx.	4 M. T. T.
	5100 45xx A. D. 44xx V. of R.	34 35 35 36 29	3100-04 45xx, 55xx 1205-06 44xx (Narrow gauge)	4 M. T. T. 4 M. T. T. 4 M. T. T. 3 M. T. T.
2-4-0T	Metro	25	3561,3582/6/8,3592/9	1 P.T.

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	OF ENGINE.	CODE.		CLASS.
J-6-8T	Sexx Rhy. Cardiff Barry T. V. B. & M.	40 41 26 41 41 41 26 41 41	30-75 76 - 83 155 231, 240-277 203-220*236,278,299 303-322*335-399 422-428 * 431-436 *	4F. T. 3P. T. 4F. T. 3F. T. 4F. T. 4F. T. 2F. T. 3F. T.
0-6-CT	57xx	42	36xx, 46xx, 96xx, 37xx, 57xx ) 67xx, 77xx, 87xx, 97xx	4F. T.
*	417±1 1701	44	907,1705-31,1752-64 1799,18xx 2721-99 15xx,1742-47, 1773-89	3 F. T. 2 F. T.
	1501) 4'1½" 1331 ) 19xx )	45	2702-19 1331 992, 19xx, 20xx	O F. T. 2 F. T.
	37 8" ) }	46	2100-60, 2181-90 ) 1361-65 1366-71	0 F.T. 1 F.T. 1 P.T.
	54xx 64xx 74xx 94xx	23 22 57 43		2 P. T. 2 F. T. 4 F. T.
	Const. V.& L. Barry N. & B.	29	(Narrow gauge) 784 1715	0 F.T.
	S.H.T. B.P.	fr fr fr	1146-1147 2162 - 2168 2176/92/96/98 2193-95, 2197 28, 29 90 - 96 *	1 F.T. 2 F.T. 1 F.T. 0 F.T. 2 F.T. 4 F.T.
	A.D. T.V. L. & M.M.	n n	666, 667 193 - 195 803 359	3 F.T. 1 F.T. 2 F.T. 1 F.T.
	Oardiff W.O.P.	11	681-684	4 F. T. 0 F. T.
0-4-2T	517,14xx	28	3574/5/7, 14xx 58xx	
0-4-0T	1101 Cardiff S.H.T.	48 49 11	1101 - 06 1338 1142 1140/1/3/4/5	5 F. T. O F. T. O F. T.
C-6-0 Diesel	Electric	99	15100-06 of re-numbering to series shew	U
0-4-0T 0-6-0	517, 14xx 1101 Cardiff S.H.T.  P. & M. Electric	28 48 49 11	3574/5/7, 14xx E8xx 1101 - 06 1338 1142 1140/1/3/4/5 1150 - 1153	1 P.T. 5 F.T. 0 F.T. 0 F.T. U

# BRITISH RAILWAYS

(WESTERN REGION)

(For the use of the Executive's employees only.)

### NEW GROUND FRAME — BRABAZON LEVEL CROSSING FILTON WEST JUNCTION.

On Monday, November 15th, 1948, between the hours of 8.0 a.m. and 12.0 noon. or until completion, the Signal Engineer will be engaged in bringing into use a new twolever Ground Frame to be known as B.A.C. Ground Frame and located in the Bristol Aeroplane Company's Gate Control House which is situated on B.A.C. property on the Down side of the Down Main Line between Filton West Junction and Henbury, at approximately 113m. 28ch.

The Ground Frame will be electrically released by Interlocking Lever No. 24 from Filton West Junction Signal Box and will work the level crossing gates which open

parallel to the railway lines as indicated in the sketch shown overleaf.

Telephonic communication will be provided between Filton West Junction Signal Box, the Level Crossing Gate Control House and the B.A.C. Flying Control Tower.

District Inspector Old, Bristol, to make all arrangements for safe working in accordance with Rule No. 77, and provide any necessary handsignalmen.

### Instructions for Working.

The Level Crossing is provided to allow the passage of Aircraft to and from the runway which is on the Up side of the railway line.

A B.A.C. employee will be in charge at the Level Crossing Gate House and will

operate the levers controlling the level crossing gates.

The Flying Controller will advise the Filton West Junction Signalman by telephone as long in advance as possible the approximate time it is anticipated that aircraft will require to pass over the Level Crossing. When the aircraft have been manoeuver adjacent to the level crossing gates the Flying Controller will telephone to the signaln advising him that the aircraft are ready to cross the railway line. When the signalman is a position to allow the level crossing to be used he must operate the Interlocking Le (No. 24), which releases the Ground Frame levers in the Level Crossing Gate Con House and must advise the Flying Controller that this has been done. The Flying Controller that this has been done. Controller will telephone the necessary advice to the B.A.C. employee in charge at the Level Crossing Gate House to open the level crossing gates. When the Level Crossing is again clear for the passage of trains and the gates have been closed the Flying Controller will advise the signalman by telephone and the Interlocking Lever (No. 24) must be restored to its normal position.

Should aircraft come to a stand on the level crossing through failure, the Flying Controller will immediately advise the signalman by telephone in order that the

necessary additional protective measures may be taken.

The above will be included in the next supplement to the appendix to No. 4 Section of the Service Time Tables.]

ACKNOWLEDGE RECEIPT TO HEAD OF DEPARTMENT.

TEMPLE MEADS STATION, Bristol, November, 1948.

R. G. POLE, Superintendent of the Bristol Division.

Department.
Station.
·Signature.

SIGNAL BOX TO BRISTOL & PADDINCTON 0 T C FILTON WEST JC. CATE BOLT FROM AVONMOUTH

COPY .

6/-

Ext 2417.

Chief Goods Manager's Office, Paddington Station. LONDON, W. 2.

WT/X.61935.

Examination of Privately Owned Locomotives at Exchange Points.

examine at the inter-Company Junctions before acceptance all privately owned locomotives, travelling on their own wheels, either dead or in steam.

It has now been decided by the L.M.S., L.N.E and G.W Companies that, provided the locomotives have been examined and passed as fit to travel on their own wheels and authority obtained for passage before acceptance at forwarding point, for the examination at the inter-Company Junctions to be dispensed with.

This arrangement will not apply to the Southern Railway owing to their electrified system. Private locomotives therefore, passing to that Company will be examined by them before being worked forward from the exchange junction.

These instructions are also applicable to railway owned locomotives on loan to War Department or other Government Departments or private Firms.

This for your information.

In all other respects instructions laid down in the General Appendix to the Rule Book operate.

For A.Maynard, signed: - R.J.S.

R.G.Barefoot Esq., Bristol.

Ref . A1/64705.

Dist.Oper.Supt's Office, Bristol T.H.

14th August, 1950.

Dear Sir,

Instructions respecting the Conveyance of Continental Train Ferry Wagons by Passenger Services.

The Railway Executive have agreed to the conveyance of Continental Train Ferry Wagons on Passenger Train Services, subject to the following conditions:-

(1) Under an International agreement these wagons must be accepted at the Ports, and providing they conform to the coaching stock requirements shown under item (2), and receive a satisfactory examination by the Eastern, North Eastern and Southern Regions as they pass through the Ports, they may run under the same conditions as British Railway Stock marked 'XP'.

## (2) Coaching Stock requirements.

- (1) Fitted with oil lubricated axle boxes or roller bearings.
- (2) Bearing springs hung in brackets with links and bolts, or on hangers with auxiliary springs, or resting on shoes fitted with retaining bolts.

(3) Fitted with automatic vacuum brake or through pipe.

- (4) Fitted with screw couplings and long buffers.
  (5) Wheel base to conform to coaching stock conditions applicable to 'XP' wagons (if the wheel base is less than 15ft. the vehicles will automatically be restricted to a maximum speed of 60 m.p.h. under the same ruling as applied to British Railways Stock marked 'XP'.)
- (6) The vehicles should also conform to the restrictions and prohibitions imposed by the Civil Engineers on account of bridges and load gauge clearances.
- (3) The only prohibitions at present in force in the Bristol District are that these wagons must not work over the Highworth Branch and must not be shunted into the Dairy Sidings at Frome.
- (4) Special care should be taken in the movement of these wagons over sidings and other lines where gradients exist, and in no circumstances should these wagons be permitted to stand at rest in sidings where gradients occur.

Please note and advise all concerned.

four L. Edwards

## L.M.R. SECTIONAL APPENDIX (CREWE AND SOUTH THEREOF), DATED 1937.

Page 150.—WAENAVON.

Delete last paragraph re Waen Nantyglo Colliery Co's. New Clydach Colliery Siding.

Page 162 (page 42 of Supplement No. 5).—Propelling vehicles on running lines outside station limits. Rule 149.

Delete Brynmawr No. I to Waenavon Station.

Page 183.-Sidings connected with running lines, etc.

Delete New Clydach Colliery.

Page 188.—Trains returning from intermediate sidings or stations or stations on Single Lines of railway to the token or staff station in the rear.

Delete New Clydach Colliery.

#### CONVEYANCE OF TANK WAGONS.

Clause I of the instructions on page 177 of the General Appendix to the Rule Book to be amended to read as follows:

Private Owners' tank wagons having one star painted on each side of each tank, former G.W.R. tank wagons and Messrs. Allsopps (Beer) tank cars, (loaded or empty), also Road/Rail and Demountable tanks (loaded or empty), loaded on wagons specially constructed for their conveyance with one star painted on the rail chassis only, may be conveyed on trains scheduled to carry "E" or inferior to "E" headcodes, irrespective of the class of traffic the tanks contain, as indicated on the wagon label.

(E. 76159/5.W.)

#### "INSTANTER" PATENT COUPLER.

The following to be substituted for the last paragraph of the instruction in regard to "Instanter" couplings shewn on page 159 of the General Appendix to the Rule Book:

#### Instanter Couplings.

A number of vehicles are fitted with Instanter couplings which can be in either of two positions, viz.:

A-Short position.

B-Long position.

Such vehicles may be attached to trains as shewn below:

### (a) Trains Conveying Passengers.

One vehicle only, fitted with Instanter Couplings, may be attached to trains conveying passengers provided it conforms in other respects to coaching stock requirements and subject also to the instructions relative to the conveyance of four-wheeled vehicles on passenger trains. In such circumstances the screw couplings of the adjacent vehicles must be used. This does not modify the authority (where given) to attach at the extreme rear one vehicle not conveying passengers and not fitted with the continuous brake or through pipe.

### (b) Coaching Stock Trains Not Conveying Passengers,

Vehicles fitted with Instanter Couplings and conforming to coaching stock requirements may be conveyed in trains composed of coaching stock, other than those conveying passengers, but the screw couplings of the vehicles on each side of the Instanter-fitted vehicle must be used. When two or more vehicles fitted with Instanter couplings are marshalled together in such trains, the screw couplings of the vehicles on each side of the Instanter-coupled vehicles must be used and the Instanter coupling, or couplings, in use, must be in the short position.

The instructions respecting the conveyance of four-wheeled vehicles will apply in respect of the running of Instanter-fitted vehicles on empty coaching stock trains.

#### (c) Freight Trains.

The couplings must be in the long position during shunting operations, and also when used in ordinary freight trains except when the vehicles are conveying cattle when the couplings of the cattle wagons must be in the short position.

Vehicles with Instanter Couplings can be used in fitted freight trains in exactly the same way as screw-coupled vehicles, but when used in the fitted portion of the train the screw coupling of the adjacent vehicle or vehicles should be used if possible. If this is not possible the Instanter Coupling must be in the short position.

Under no circumstances must an Instanter Coupling be used for attaching to vehicles fitted with the Buck-eye Coupling, and a screw coupling must be used.

## APPENDIX TO No. 12 SERVICE TIME TABLE.

Loose Runaway Catch Points or Dead End Catch Sidings.

The following entry on page 24 to be deleted:

Station	Up or Down Line	Where situated	Gradient One in	Remarks	
Colwall	Up	Ledbury end of Up Platform 356 yards to rear of Up Main Inner Home Signal.	No.	148	*

## LIMIT OF SHUNT BOARD.

The instructions shewn on page 48 to be deleted.

GILBERT MATTHEWS

Operating Superintendent

Paddington, 2nd August, 1950.

14th. November, 1945.

Ref: - A1/41,192.

Dear Sir,

Conveyance of "Warflats" and "Warwells" on "C" Headlane Freight Trains.

With reference to my circular letter of January 3rd, 1945 under the heading of "Coupling of Vacuum of full trains of Warwells".

# (1). Loaded vehicles.

Although there is no objection to the whole vacuum pipes being coupled up to the engine on special Freight trains run with A.F.V's loaded on Warflats or Warwells, the speed of the trains must be limited to 30 miles per hour (see R.H.C. booklet of instructions re A.F.V's) and in such circumstances the trains should carry "B" headlamps although the train may be composed wholly or partially of vacuum braked vehicles. Loaded vehicles may not be conveyed on trains

booked to carry "C" or "D" headlamps.

# (2). Emoty Vehicles.

Empty Warflats or Warwells may be conveyed on "o" or "D" headland trains and Clause 3 of the instituctions shewn on page 176 of the General Appendix will in due course be altered to read:-

Empty Great Western and Empty "Foreign" Companies Oil axle-box special wagons of types enumerated in Clauses land 2 must not be conveyed on any freight train carrying superior than "E" headlamps, except the following in temporary use:-

WARFLAT

WAR ELL.

Yours truly,

For Refree

Local Language

Loca

PRIVATE AND NOT FOR PUBLICATION.

# GREAT WESTERN RAILWAY.

Divisional Superintendent's Office, BRISTOL(T.W.) G.W.7. lith May, 1939.

## WORKING OF STREAMLINED CARS.

Referring to the instructions contained on page 133 of the General Appendix, amended by circular G.A.5, dated March 1939, Car No.15 has been supplied with electrically lighted tail lamp and the following instructions will apply:-

"These cars will not carry the G.W.standard head or tail lamps. "A" headlamps are provided by two white lights (horizontally at platform level) and "B" headlamps by one white lamp (at top centre), and these must be exhibited at the leading end when required. The tail lamp will be a G.W. standard tail lamp case fitted with an electric bulb and provided with a short length of cable and two pin connecting plug. A corresponding connecting socket is fitted to the body of the car immediately above the tail lamp bracket and when the tail lamp is placed in position, the Guard must ALWAYS connect the connecting plug to socket and press the pins well home.

"meadlamps, which must be exhibited after sunset, during fog or falling snow or before passing through a tunnel, must be switched on by the Driver.

"The Guard, or man acting as Guard, will be responsible for placing the tail lamp in position, making the electrical connection and switching on the light after sunset, during fog or falling snow, and before passing through a tunnel.

"The electrically fitted lamps are branded on the chimney cap thus:- "Rail Car Electric".

"A spare oil tail lamp is to be carried on each car for use in emergency and when standing in sidings."

for 7. 9. Pole

ALL CONCERNED TO NOTE.

PRIVATE AND NOT FOR PUBLICATION.

# GREAT WESTERN RAILWAY. NOTICE NO.Al/-

Divisional Superintendent's Office, BRISTOL(T.M.) G.W.7. 19th April, 1939.

WORKING OF STREAMLINED CARS.

Referring to the instructions contained on page 133 of the General Appendix, amended by circular G.A.5, dated March 1939, cars Ros.9 and 10 have been supplied with electrically lighted tail lamps and the following instructions will apply;-

"These cars will not carry the G.W.standard head or tail lamps. "A" headlamps are provided by two white lights (horizontally at platform level) and "B" headlamps by one white lamp (at top centre), and these must be exhibited at the leading end when required. The tail lamp will be a G.W. standard tail lamp case fitted with an electric bulb and provided with a short length of cable and two pin connecting plug. A corresponding connecting socket is fitted to the body of the car immediately above the tail lamp bracket and when the tail lamp is placed in position, the Guard must ALWAYS connect the connecting plug to socket and press the pins well home.

"neadlamps, which must be exhibited after sunset, during fog or falling snow or before passing through a tunnel, must be switched on by the Driver.

"The Guard, or man acting as Guard, will be responsible for placing the tail lamp in position, making the electrical connection and switching on the light after sunset, during fog or falling snow, and before passing through a tunnel.

"The electrically fitted lamps are branded on the chimney cap thus:- 'Rail Car Electric'.

"A spare oil tail lamp is to be carried on each car for use in emergency and when standing in sidings."

0,00

BRISTOL (T.II) . G.W.7.

Ref:- A1/41,831.

1st. June, 1946.

Dear Sir,

# Running of Four-wheeled Vehicles in Express Trains.

Referring to my circular letter of February 17th, 1945, the Chief mechanical Engineer has been instructed by the General Lanager to remove "XP" markings from a number of G.W. four-wheeled vehicles until certain structural modifications have been carried out. The obliteration of the markings is being carried out immediately at outlying points throughout the Line.

The vehicles concerned have a wheel base of less than 15 feet. (Passenger Cattle Vans with a wheel base of 16 feet), and are as under:-

Horse Boxes.

Passenger Cattle Vans ("BEETLES C").

Hotor Car Trucks ("DA OS B").

Goods Vans. ("VANFITS").

Passenger Fruit Vans. ("PASFRUITS" and "PASFRUITS C").

Banana Vans (with 12 feet wheel base).

Goods Fruit Vans (with 10 ft. wheel base & normal carrying capacity of 6 tons).

Onen Carriage Trucks ("SCORPIONS" and "SCORPIONS D").

The modifications referred to above will be carried out as soon as practicable; as and when such are effected the "XP" markings will be restored. For the purpose of distinguishing the vehicles dealt with, the restored "XP" lettering will be enclosed in a square consisting of a white line:-

X. P.

The vehicles can then again be used in accordance with instructions in respect of "XP" vehicles contained in General Appendix Supplement G.A.4 and 16.

The whole of our stock of Horse Boxes, Passenger Cattle Vans, Open Carriage Trucks, and "DAIOS B", also the majority of our Passenger Fruit Vans are to be so dealt with. It will, therefore, be necessary forthwith, until the modifications have been effected and "XP" markings have been restored, for these vehicles to be dealt with as laid down for "Non-XP" Vehicles in General Appendix Supplements G.A.4 and 16. So far as the modifications are concerned, priority is being given to Horse Boxes, some of which have already been turned out by Swindon with the new markings

The following "XP" four-wheeled vehicles, with a wheel base of less than 15 feet, are already equipped with suitable springs, and their dimensions are such that it is not proposed to remove the "XP" markings from these:-

Container Trucks 920
Fish Vans 45
Covered Goods Vans. 1,216
Goods Cattle Wagons. 2,093
Heat Vans. 813
Goods Fruit Vans. 329

The remainder of our four-wheeled stock, all of which have a wheel base of more than 15 feet, and which are suitable for "XP" running, and will remain so marked are :-

Fish Yans ("BLOATERS").

Covered Carriage Trucks' ("PYTHONS A" and "PYTHONS B")

Louvre Sided hilk Trucks. ("SIPHONS C").

Passenger Fruit Vans. ("FRUITS D").

Passenger Well Trucks ("HYDRAS", "HYDRAS C" and "HYDRAS D").

Goods notor Trucks ("ASHOS" & DAHOS A").

The loading of all G.W. four-wheeled "XP" vehicles with a wheel base of less than 15 feet is to be restricted to 6 tons, except when to be conveyed by freight train service, and a suitable G.A. Instruction is being issued shortly to cover this feature.

The 2-20p.m. (SX), 2-38p.m. (SO), Parcels Paddington to Plymouth is scheduled to carry 'A' headlamps Paddington to Didcot, and "C" headlamps beyond. Commencing on Londay next, June 3rd, this train to carry 'C' headlamps throughout Paddington to Plymouth and special attention given to the working accordingly with a view to maintaining, as far as practicable, the present scheduled times.

The 5-10pm Milford Haven (9-10pm Cardiff) to Paddington is scheduled to carry 'A' headlarms throughout. This train must henceforth only convey vehicles coming within the revised "K.P" category, as outlined above. Normally the train is composed of G.W. "BLOATERS" and foreign fish vans, which are suitable.

It is appreciated that the decision to prohibit the rolling stock mentioned from Class 'A' headlamp trains, which has been arrived at on the grounds of safety after exhaustive investigation, will entail considerable difficulty in providing satisfactory services in many cases, particularly in respect of horses and live stock, but the best will have to be done in the circumstances during the transition period.

These instructions apply to G.W. stock only, and Foreign / Companies' vehicles carrying "XP" markings will be dealt with as at present.

Please arrange accordingly so far as you are concerned, and acknowledge receipt.

Yours truly, Poly

# GREAT WESTERN RAILWAY

# GENERAL APPENDIX

TO THE

# RULE BOOK

To come into operation on August 1st, 1936. Corrected to GAIL Corrected to GAIY. CONTENTS Dec. 1944 Corrected to GA18 SECTION I Corrected to GAIA NOV. 1948 1949 1949 (a) Additions to the Standard B PAGES 2-22 (b) Extracts from Regulations for on Double and Single Lines of Enginemen, Guards and 25 - 61(c) MATTERS RELATING MAINTENANCE OF POINTS 65 - 86SECTION II GENERAL INSTRUCTIONS AFFECTING OF TRAINS 89-137 (a) Passenger trains (b) Passenger and freight trains 139 - 166(c) Freight trains 168-186 SECTION III GENERAL INSTRUCTIONS AFFECTING THE LOADING AND CONVEYANCE OF MERCHANDISE TRAFFIC, ALSO LIVE STOCK BY PASSENGER AND FREIGHT TRAINS 187 (a) Loading and conveyance of merchandise 189 - 246(b) Loading, conveyance, etc., of live stock 248-258 by passenger and freight trains SECTION IV INSTRUCTIONS CONCERNING STATION WORK 262-344

> JAMES MILNE, General Manager.

### RULES IN FORCE ON JOINT LINES.

The following list showing which parent Company's Rules apply on the Joint Lines named below is given for the guidance of the staff concerned:—

is given for the guidance of the st	an cone	cerned					Con	npanys'
	i	Joint I	ine.				F	Rules icable.
Abersychan and Talywain				 	 			G.W.
Brecon and Merthyr and L.N.W.				 	 			G.W.
Brynmawr and Western Valleys				 	 	• •		G.W.
Chester and Birkenhead				 	 			L.M.S.
Clee Hill Branch				 	 			G.W.
Clifton Extension				 	 			L.M.S.
Easton and Church Hope				 	 			G.W.
G.W. and G.C. Joint Committee				 	 			Joint
								L.M.S.
Halesowen				 	 			
Hammersmith and City				 	 			G.W.*
Nantybwch No. 2 and Rhymney	Bridge	(inclu	sive)	 	 			L.M.S.
Rhymney Joint and Rhymney B								G.W.
Severn and Wye—	arage (	020100	,					
Portion maintained by L.M.	S Rails	wav			 			L.M.S.
Portion maintained by G.W.		-						G.W.
			**		 			G.W.
Shrewsbury and Hereford		**	**		 			G.W.
" " Wellington	ijou.			 				G.W.
,, ,, Welshpool	dio 14	48	Sin	 	 			G.W.
				 	 			G.W.
Vale of Towy	N. I.O.			 	 **			u.w.
West London			-	 	 			G.W.*
West London Extension	3046				 			G.W.
Weymouth and Portland			8 6					G.W.
Wrexham and Minera—	1							
Brymbo (exclusive) to Coed	Talon				-			L.M.S.
Drymbo (exclusive) to Coed	Laion							

G.W. staff working over the above-named Joint Lines on which the G.W. Company's Rules do not apply must be in possession of a copy of the instructions affecting G.W. staff working over other Companies' systems.

<sup>\*</sup> The Rule Book of the Metropolitan Railway or Underground Railway is issued in addition to the G.W. Rule Book.

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# SECTION I (a).

ADDITIONS TO THE STANDARD RULES.

## ADDITIONS TO CERTAIN OF THE STANDARD RULES IN THE RULE BOOK TO BE OBSERVED ON THE GREAT WESTERN RAILWAY.

Rule 14.—In addition to the Instructions shown in this rule, the Great Western Staff are hereby informed that the Company and amalgamated constituent and subsidiary Companies have had special powers conferred upon them by Acts of Parliament in regard to trespassing, by which it is enacted by the G.W.R. (Additional Powers) Act, 1923, that any person who shall trespass upon any of the lines of railway, or in the case of the Rhymney Company Act, 1887, or the G.W.R. Act (No. 1), 1882, any person who shall trespass upon any of the railways or stations in such a manner as to expose himself to danger, or risk of danger, shall, without having received any personal or other warning, be liable to a penalty of 40s., provided that a Notice Board giving public warning is exhibited at the nearest station and at the public road level crossing (if any). (See page 271.)

Rule 16.—The glossary of terms included in this rule will also apply, except where otherwise provided, to Regulations and other instructions.

The following additional term will apply, except where otherwise provided, to rules, regulations

and other instructions.

Includes Freight train 1-1-50 GAZ4 Goods train. Token Station Signal Box. See New Rule

Refe

Rule 35, clause (b):-

( (X) Colour light signals ; 2 (N) Banner type signals;

3 (it) Small semaphore arms for calling-on, warning, and shunt-ahead signals. See diagram, pages 4, 8, 11 and 12.

## COLOUR LIGHT SIGNALS.

Colour light signals are provided on Main and Relief Lines at various places. The indications given correspond with the lights exhibited by the present semaphore signals, viz.:—

Yellow .. .. Caution. Distant Signals Green Clear. . . Red Danger. .. Stop Signals .. Green Clear.

Where colour lights replace "Siding" and "Backing" semaphore arms, small red (Danger) or green (All right) lights will be shown. See reference to colour light repeating signals in instructions re "Banner" Repeating Signals.

## COLOUR OF ARMS AND LIGHTS OF "DRAW-AHEAD," "CALLING-ON." "SHUNT-AHEAD" AND "WARNING" SIGNALS.

The standard colours of the arms and lights of these signals are as follows:-

The face of the arm is painted white with red horizontal stripes at top and bottom (see diagram, page 4) and except in the case of the "draw-ahead" signal (see paragraph below) the indication of the character of the arm will be given by the respective one of the following letters at the side of

the character of the arm will be given by the respective one of the following the temperature of the lluminated indicator:

"C" for "Calling-on." (Rules 40 and 44 re Calling-on Signals.)

"S" for "Shunt-ahead." (Rule 46 re Shunting Signals.)

"W" for "Warning." (Rules 41 and 45 and Regulation 5 of the Regulations for Train Signalling re "Warning." arrangement.)

No letter will be shown at the side of the lamp or on the illuminated indicator in the case of the "Draw-ahead" signals.

The back of the arm is painted white with a vertical black band in the usual position as at present. The normal light indication when the arm is in the horizontal position is white in each case,

and when the arm is lowered a green light is shown.
"Calling-on," "Shunt-ahead" and "Warning" signals which do not conform to the above will be altered accordingly as opportunity occurs.

Where colour-light signals are provided, "Calling-on," "Shunt-ahead" and "Warning" indications are shown in diagrams on pages II and I2, but in the case of "Draw-ahead" signals no letter appears on the opal plate.

"Calling-on" Signals.-Such signals will in future be provided under stop signals whether such stop signals control the entrance to the section or not. See Rule 44.

"Draw-ahead" Signals.—Such signals are at present provided below stop signals not con- he trolling the entrance to the section ahead. These signals will be substituted by "calling-on" signals, and in the meantime the following instruction is applicable to such signals:-

"Draw-ahead" signals apply when lowered as far as the line is clear towards the next  $^{\rm hn}$ signal only, but the lowering of such signals does not authorise the next signal to be passed  $\,^{\rm h}$ (G.A.24.—11/49.)

- grown agan and the instructions in the preceding paragraph will also apply to colour light repeating signals.

"Banner" repeating signals of the type shewn on page 8 are provided at certain places. The Banner repeating signal is illuminated, the "arm" being shown in black on a white ground.

Banner repeating signals, where provided, will indicate to Drivers the position of the signal they are approaching, the view of which is obscured by a bridge, tunnel or other obstruction. The Banner repeating signal may be passed in the "on" position, and when in this position will indicate that the relative stop signal or distant signal, as the case may be, is also in that position. A Banner repeating signal when lowered indicates that the relative signal ahead has been lowered, but the provision of these Banner repeating signals does not relieve Drivers of the necessity of observing the position of the respective signal ahead of the Banner repeating signal. Where colour light repeating signals are provided the "On" position will be indicated by a yellow

light and the "All right" position by a green light and the instructions in the preceding paragraph will also apply to colour light repeating signals.

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#### BUILE 9.

Staff, other than those required to travel in Brake Vans in the course of their normal duties, may be permitted to travel in passenger or goods brake vans if in possession of :--

(a) Endorsement by Operating Superintendent on periodical pass;

(c) Special Van pass issued by the Operating Superintendent; (c) The holder of an "All Line" gold medal, issued by the Railway Executive.

Except as provided in the following paragraph no member of the Public will be permitted to mide in a brake van unless authorised in writing by the Operating Superintendent and provided the Commercial Superintendent has arranged for a suitable indemnity to be signed.

In cases of emergency, or serious overcrowding, discretion is given to Station Masters, Supervisors in charge and Guards, when the facts must be recorded in the Guard's Journal. Travelling state to the van in which the Guard is riding, although in cases of serious difficulty chattended vans may be used if a responsible member of the staff can be put in charge.

(G.A.24.—11/49. S.4978 O.M.)

# - GAZH See New Rule

Rule 17 (v).—Station Masters and District Inspectors, when making their periodical inspection, must sign the Train Register Book across the page immediately beneath the last entry.

(G.A. 10. 3/42. O.M. Min. 11953).

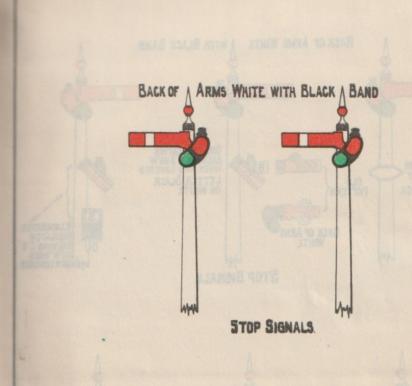
Reference to the following to be made on page 2:-

## COLOUR LIGHT DISTANT SIGNALS.

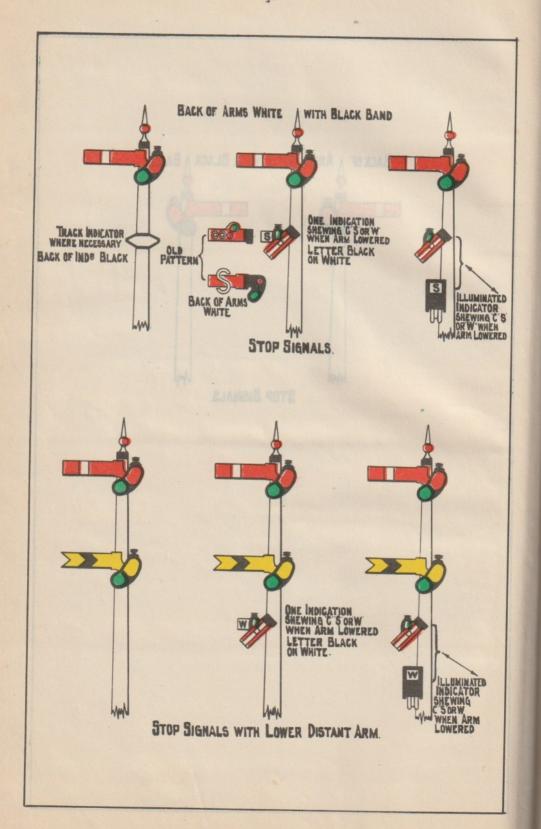
Where a colour light Distant signal is placed below a semaphore stop signal, no light will be exhibited in the Distant signal when the semaphore arm above it is in the danger position, but a yellow or green light will be exhibited when the semaphore arm is in the clear position.

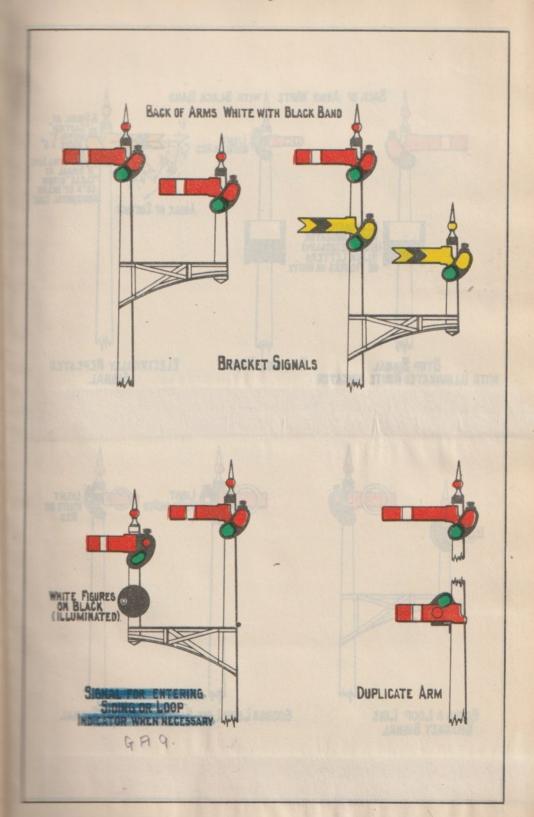
At night no green light will be exhibited by the semaphore signal when placed to the clear sition.

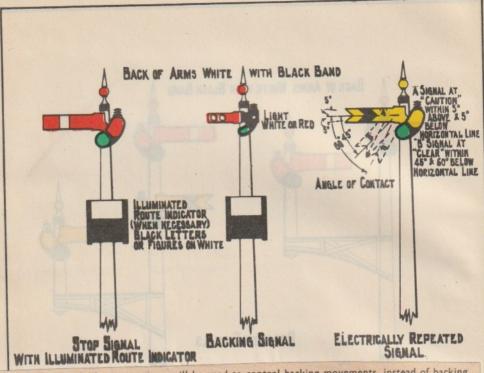
(G.A.30 Op.—9/45 L.65733/33)



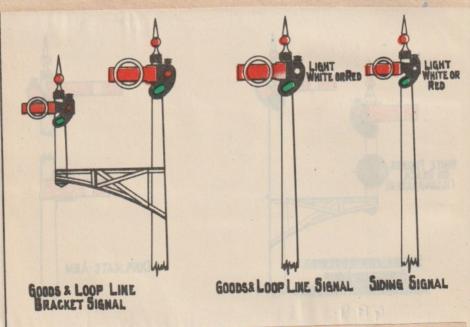
OTOP SIGNALS WITH LOVER DISTART ARM







Discs, elevated where necessary, will be used to control backing movements, instead of backing signals. The revised arrangement will apply in connection with new signalling schemes, signal alterations and renewals. (G.A.25,—1/50. R.E. Stand:—L.56267/89.)



New pattern signals will not carry rings on the arms. The revised arrangement will apply in connection with new signalling schemes, signal alterations and renewals.

(G.A.25—1/50. R.E. Stand:—L.56267/89.)

BACK OF ARMS WHITE WITH BLACK BAND.



"DANGER"





"CAUTION"

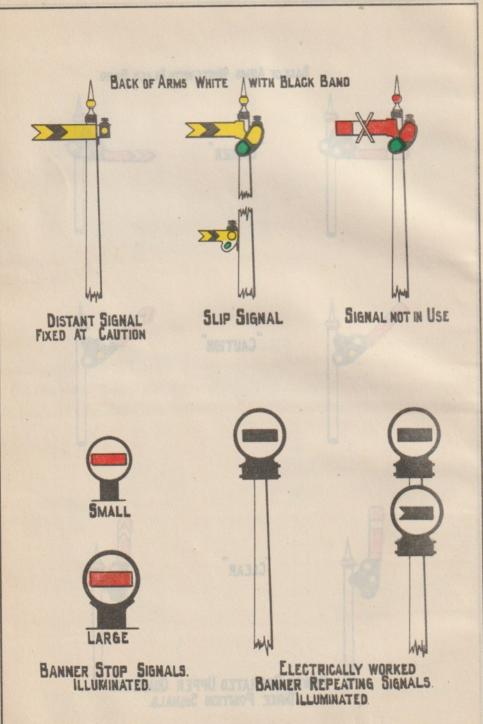




"CLEAR"



POWER OPERATED UPPER QUADRANT THREE POSITION SIGNALS.



## POINT DISCS







INDEPENDENT DISCS. OLD PATTERN LIGHT WHITE OR RED.





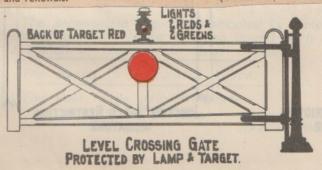


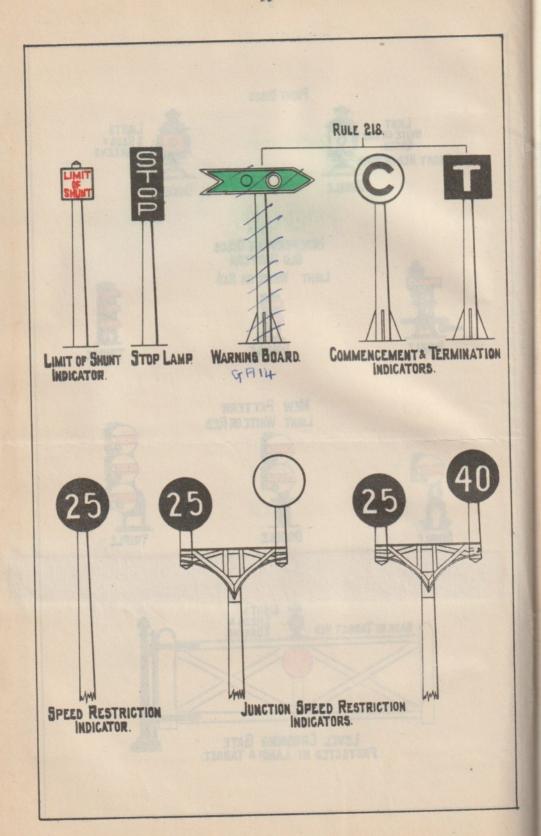
NEW PATTERN LIGHT WHITE OR RED



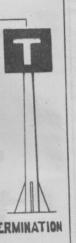
The illustrations headed "New Pattern" to be amended to show either a red band with red light or yellow band with yellow light and the heading to read "New Pattern. Light Yellow or Red." The above revised arrangement will apply in connection with new signalling schemes, signal alterations and renewals.

(G.A.25.—1/50. R.E. Stand:—L.56267/89.)





Page I De the follo

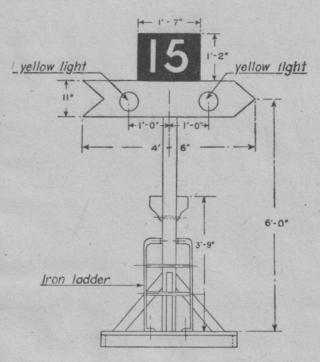




Page 10

Delete the illustration and description of a Warning Board as shewn in G.A.14 and substitute the following :—

NEW TYPE ARROW WARNING BOARD WITH ILLUMINATED SPEED INDICATOR.



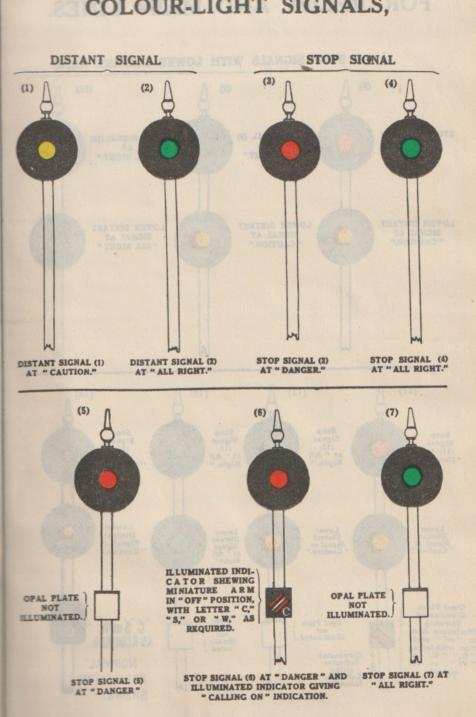
The permissible Speed is indicated by lamp placed over the Arrow Warning Board with a stencil in front of an Opal Screen.

The Board to be painted Yellow.

The Post to be painted White.

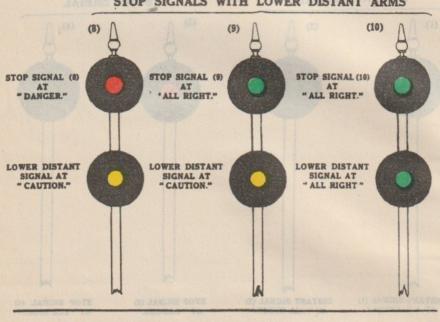
(G.A.23-7/49. L.K.1/8601/32.)

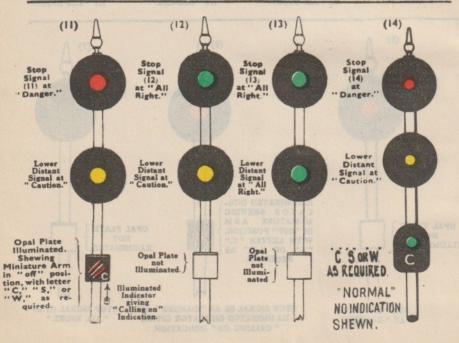
# COLOUR-LIGHT SIGNALS,



## FOR RELIEF AND MAIN LINES.

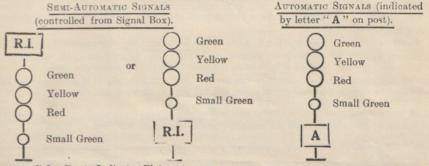
## STOP SIGNALS WITH LOWER DISTANT ARMS





## COLOUR LIGHT SIGNALLING INSTALLATION. UP AND DOWN CARRIAGE LINES BETWEEN PADDINGTON AND OLD OAK COMMON.

Semi-automatic and automatic colour light signals are provided on the Up and Down Engine and Carriage lines between Paddington and Old Oak Common East Signal Box. The signals are in the form shewn below, automatic signals being indicated by a letter "A":—



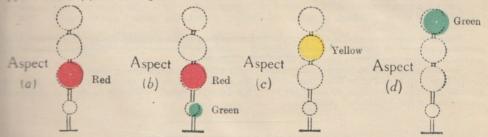
R.I.-Route Indicator Plate.

Each signal is provided with a number plate for identification purposes. Semi-automatic colour ight signals are controlled from the respective Signal Boxes concerned, in conjunction with track-circuiting, but the automatic running signals are worked entirely by the passage of trains on the track-

circuited portion of the line in advance of the signal.

The small Green light is a "Calling-on" signal, and is operated by a short length of track circuit about 60 feet in length) to the rear of the signal. The "Calling on" signal shows a "Green" indication when the section ahead is occupied, and the provisions of Rule 44 (a) for "Calling-on" signals must be observed, and the Driver must be prepared to stop at any point in the section irrespective of whether the signal in advance may be showing, or changed to aspects (b) small green under red, (c) yellow, or (d) green, mentioned below.

Each of the Light Signals consists of a group of three lamps with a smaller lamp underneath and the Light Aspect exhibited at any one time will be either (a) a Red, (b) a Small Green under a Red, (c) a Yellow or (d) a Green Light as shown below:—



The meanings of the Colour Light Signals applicable to the Engine and Carriage Lines are as

		As	SPECT.		MEANING.
			nder Red		Stop. Section ahead occupied. Proceed cautiously at low speed prepared to stop short of any obstruction.
(c)	Yellow			 	Line is clear to next signal only.
(6)	Green			 	Line is clear two sections ahead.

The aspects applying to movements from the Engine and Carriage lines to other lines will be Red

The aspects applying to movements from the Engine and Carriage lines to other lines will be Red

The aspects applying to movements from the Engine and Carriage lines to other lines will be Red

The aspects applying to movements from the Engine and Carriage lines to other lines will be Red

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The aspects applying to movements from the Engine and Carriage lines to other lines will be Red

The aspects applying to movements will indicate that the road is right for trains

The aspects applying to movements will indicate that the road is right for trains

The aspects applying to movements will be Red

The aspects applying to movements from the Engine and Carriage lines to other lines will be Red

The aspects applying to movements from the Engine and Carriage lines to other lines will be Red

The aspects applying to movements from the Engine and Carriage lines to other lines will be Red

The aspects applying to movements from the Engine and Carriage lines to other lines will be Red

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The aspects applying to the Engine and Carriage lines will be Red

The aspects applying the Engine and Carriage lines will be Red

The aspects applying the Engine and Carriage lines wi

The aspects applying to movements to the Engine and Carriage Lines from other lines will be red and the green light for such movements will indicate that the road is right for trains become to the Engine and Carriage Line, but not that the line is clear. Drivers must be proceed to the next Engine and Carriage Line Signal and be prepared to stop short of any

#### COLOUR LIGHT SIGNALLING-continued.

During fog or falling snow, when visibility is such that enginemen cannot see far enough ahead to ensure stopping short of any train or engine in front, they must stop at the automatic or semi-automatic light signals unless or until the yellow aspect (c) or green aspect (d) is shown, and if neither of these aspects appears in a reasonable time, the Driver must, in the case of an Automatic Signal, send his Fireman ahead to ascertain the state of the line or the aspect of the signal in advance. In the case of a Semi-Automatic Signal the Driver must obtain the instructions of the Signalman before passing the signal.

Should a Driver in clear weather find one of the Automatic Signals (which are distinguished by an illuminated letter "A" on the post) showing a red light only, or no light at all, but he can see that the line ahead is clear, he must stop dead at the signal, and then proceed cautiously, prepared to stop short of any obstruction in the section, irrespective of whether the signal in advance may be showing or changed to aspects (b) small green under red, (c) yellow, or (d) green. He must stop at the next signal box and report the failure to the signalman.

In no circumstances must a Driver pass a Semi-Automatic (or controlled) signal when shewing a

Red light only or no light, until the authority of the Signalman has been obtained.

Rules 35, clause (b/iii), 47, 48 and 49.—White Lights in Ground Signals (Disc or Dwarf), Siding Signals and Backing Signals, in place of Red Lights.

Siding Signals, Backing Signals and Ground Signals (Disc or Dwarf) which may be passed in the "On" position are provided with white or yellow lights. When such signals are provided with red lights they must not be passed in the "On" position unless verbal instructions are received from the Signalman, Guard, or Shunter.

Note.—The lights of Calling-on, Warning, Shunt-ahead, Loop and Backing Signals are smaller (G.A.24.-11/49.) than the lights of ordinary Running Signals.

Rule 36. Distant Signals. Distant Signals are painted yellow with black stripe on the GAZW facing side of the arm, and white with black on the reverse side, yellow lights are shown in these Signals when the Signal is in the "caution" position.

Keeping Distant Signals at "Caution" where Permanent Reduction of Speed is ordered.-Whereever there is a permanent reduction of speed to 15 miles an hour or less, at places situated between a Distant and a Home, Starting, or Advanced Starting Signal, the Distant Signal must be kept at "Caution" permanently.

Where it may be desirable at other places for the purpose of reducing the speed of trains over cortain points on the road, the Divisional Superintendents or District Traffic Managers will give the needful orders for the Distant Signal to be kept at "Caution."

Privers are instructed to report any instances in which this order is not observed.

Distant Signals at Facing Junctions.—Where the speed over one or both of the diverging lines at a facing junction is less than 40 miles per hour, the Distant Signal applicable to the line or lines is maintained in the "Caution" position.

## Rules 47 and 49.—" BACKING" SIGNALS.

"Backing" Signals, where provided, control backing operations over the line in the Wrong

direction, and are distinguished thus.—(See diagram—page 6.)

When a "Backing" Signal is lowered for a movement to be made with a Train or Engine and Vehicles, the Driver, if he has also received verbal authority or a hand signal to do so, or received intimation by Klaxon horn or other mechanical appliance, must proceed past the Signal. In the case also of Light Engines, verbal authority or a hand signal must be given where practicable. In all cases the Driver must regard the lowering of the arm only as an indication that the points over which he may have to pass are in the proper position, and that the Signals for Trains approaching in the right direction along the same line are at Danger. The Driver must not expect that the line will be clear as far as he may require to run, and he will be held responsible for stopping short of any obstruction or upon the exhibition of a hand Danger Signal by the Guard or Shunter who, whenever necessary, must accompany the Train or Vehicles.

Under no circumstances must a Train or Engine be set back outside the Home Signal, applicable to Trains approaching in the right direction, unless a special Signal for a Wrong Line movement is provided, or the Driver has received instructions to do so from the Signalman, who must previously have "Blocked Back" to the Signal Box in rear, or obtained permission for an Engine or Engine and Vehicles to travel in the Wrong direction to the Box in the rear at those places where such working is

authorised.

The following to be inserted on page 14:-

Signal Post Signs.

All concerned to please note the following new signal post signs will be exhibited where applicable:—

Letter "T" in black on white diamond sign.

Letter "T" in black on white rectangular plate. Designation of signal in black figures on white rectangular plate.

Fixed on signal to rear of which track circuit is provided and a telephone to signal box is provided in addition. Telephone to signal box provided (no track circuit). Provided in colour light signalling areas and for all colour light Intermediate Block Signals.

(G.A.30 Op.-9/54 L.72404/283)

RULE 33 (b).-Page 14. The following to be substituted for the existing entry:-

On the Western Region the time signal is signalled daily at 11.0 a.m. and where not received the Station Master must obtain the precise time from the Guard of the first stopping train com-mencing its journey after 11.0 a.m. (G.A.24.—11/49.)

Tule 35 clause (c).

The instruction shown under this heading to be amended to read:-

On the Western Region this clause will only apply to the Multiple Aspect Signalling area in the London District. (G.A.30 Op.-9/54)

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A RESIDENCE previously. Engine and withing is RULE 50 (c).—On the Western Region a white hand signal is used as follows:

To acknowledge Guard's white hand-signal—(see addition to Rule 147 in the General Appendix and amplification of Regulations for signalling White light held steadily trains and engines by Permissive Block System over Goods Running Loop Lines and Permissive Lines).

by Signalman.

To indicate to Signalman that train or engine is in clear of loop points (see addition to Rule 147 in the General Appendix and amplification of Regulations for signalling trains and engines by Permissive Block System over Goods Running Loop Lines and other Permissive Lines).

White light held steadily by Guard or Fireman.

Kules 59 and 60.—The amendments advised in Circular G.A.10 to Rule 59 clause (a) and tirst paragraph clause (b) also to Rule 60 first two paragraphs, to be deleted as the Rules have now been amended by Rule Book Supplement No. 13.

(G.A.12. 4/43. O.C.Min.2562.) R.B.Sup.13.

Rule 59 to be amended as shewn below :-

Clause (a) to read :-

When one or more detonators are exploded by a train at stop signals or signal boxes, the Driver must immediately bring his train to a stand, whether a hand signal is exhibited or not, unless he is satisfied the detonator is intended to indicate that a distant signal under a stop signal is at caution or he receives a green hand signal waved slowly from side to side in which case he must reduce speed as laid down in the third paragraph of Rule 60. Should he receive a yellow hand signal at a three-aspect signal he must reduce speed and be prepared to find the next stop signal at Danger.

Clause (b) the first paragraph to read:-

When a detonator is exploded at a distant signal and a yellow hand signal is exhibited, the Driver must be prepared to stop at the home signal. If, however, the Driver receives a hand Danger signal at a distant signal he must bring his train to a stand as quickly as possible and not proceed until he receives authority to do so, except that in daylight and provided the weather is clear he may proceed cautiously towards the place of obstruction or until he receives a further signal for his guidance. (G.A. 10. 3/42. Op. Com. Min. 2562).

Rule 60.—The following to be substituted for the first paragraph of this rule :-

When one or more detonators are exploded by a train other than at a fixed signal or a signal box and no hand signal is exhibited, the Driver must, in clear weather, bring his train under such complete control as to enable him to stop at once if required and then proceed cautiously towards the place of obstruction or until he receives a signal for his guidance: during fog or falling snow he must bring his train to a stand and then proceed cautiously

towards the place of obstruction or until he receives a signal for his guidance.

If, however, the Driver receives a hand Danger signal, he must bring his train to a stand as quickly as possible and not proceed until he receives authority to do so, except that in daylight and provided the weather is clear he may proceed cautiously towards the place of

obstruction or until he receives a further signal for his guidance.

(G.A. 10. 3/42. Op. Com. Min. 2562).

The following to be inserted on page 15:

Rule 68 (b).—A stop signal, after being lowered for the passage of a train, may be replaced at Danger before the train has passed it when the Signalman cancels the "Is Line Clear?" in accordance with the second paragraph of clause (c) of the "Switching In" instructions in Block Regulation 24.

(G.A.18. 11/47. O.M. 12354.)

Rule 74. In the case of Long Burning Signal Lamps the lamp which fails should be replaced by a sparelamp. GAZLA Sec New Rule (G.A.18. 11/47. L.K. 1/-,)

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#### ADDITIONS TO THE STANDARD RULES.

Rule 55.—Reminder Flaps on Three position Disc Block Instruments. 1. Movable brass flaps are ided over the "Line clear" and "Train on line" keys of the disc block instruments. The normal tion of these flaps is turned up.

2. Should a signalman have a train or vehicle standing outside his Home Signal (the "Train on

"key being of course pegged down), he must turn down the flap over the key, which will display the words "Train at Signal," and lock the key in the "Train on line" position.

3. The flap over the "Line clear" key when turned down will display the words "Clearing point.

Men this flap is turned down the "Line clear" key when this flap is turned down the "Line clear" key cannot be depressed.

4. At places where the "Warning" signal is authorised to be sent, and it is desired to accept a

bollowing train under Block Regulation 5 (Warning Arrangement), the "Clearing point fouled" flap hould, if it has been placed in the turned down position in accordance with the preceding clause, be all raised to enable the "Line clear" key to be operated.

5. On every occasion when a flap is turned down it must not be restored to the normal position the line is clear to the Home Signal in the case of the flap over the "Train on line" key, and between the Home Signal and the clearing point in the case of the flap over the "Line clear" key.

6. Signalmen will be held responsible for always making use of these reminders as shown in the

umstances mentioned.

Reminder Flaps on Tyer's Block Instruments. Where reminder flaps are provided on Tyer's block instruments the following instructions must be observed:-

## Instruments Showing Three Indications.

A brass flap bearing the words "Line blocked" is fitted at the side of the button releasing the commutator. The flap can be turned on to the button in such a position as to prevent the button being worked and so make it impossible to alter the indication without the flap being removed.

The normal position of the flap is away from the button.

The flap must be turned on to the button in either of the following circumstances:—

(1) When a Train or Vehicle is standing outside the Home Signal.
(2) When the clearing point is fouled.

After the flap has been turned on to the button it must not be taken off until the line is clear to Home Signal, or between the Home Signal and the clearing point, as the case may be.

At Boxes where the "Warning" signal is authorised to be sent, and it is desired to accept a follow-Train under Block Regulation 5, the flap may be moved to enable "Train out of section" to be for this purpose, but where this is done the flap must be replaced on the button immediately the Signal is sent and must not be placed in its normal position until the line is clear to the standard dearing point.

#### Instruments Showing Two Indications.

A brass flap bearing the words "Train at Signal" is fitted at the side of the "Train arrived" The flap can be turned over on to the plunger in such a position as to prevent the plunger worked, and so make it impossible for the "Train arrived" indication to be shown. The normal section of the flap is away from the plunger.

The flap must be turned on to the plunger in either of the following circumstances:—

(1) When a Train or Vehicle is standing outside the Home Signal.

(2) When the "Blocking Back" Signal (2-4) has been accepted by the Box in the rear.

After the flap has been turned on to the plunger, it must not be taken off until the line is clear Home Signal, or between the Home Signal and the clearing point, as the case may be.

Reminder flaps are not intended to cancel the use of lever collars or other similar devices, provided as additional safety appliances.

The Signalman must make an appropriate entry in the Train Book whenever a Guard, Shunter, or Fireman arrives at a Signal Box to carry out Rule 55; be made across the page and not in the "Remarks" column.

Bail Cars must not be relied upon to operate track circuits and the Guard or Shunter cut Rule 55, whether track circuiting is provided or not. (See page 132.)

55 (b) — Except where instructions are issued to the contrary, when a train or engine is the apposite line, Rule 55 (clause b) must be carried out, whether track circuiting is ARLH See New Rule

Desarrangement of Interlocking or Disconnection of Facing Points or movable elbows.

The possible arrange to keep the detector locks on the nearest to the facing junction working as usual, and the facing points or movable elbows to the facing junction working as usual, and the facing points or movable elbows as usual, and the facing points or movable elbows as the more position for every train. The Stop Signals may then be worked as a mover-run, and the provisions of Bule 77 must be observed.

The possible to keep the detector locks working, in addition to the precautions are stop Signals in each direction affected by the disarrangement in the

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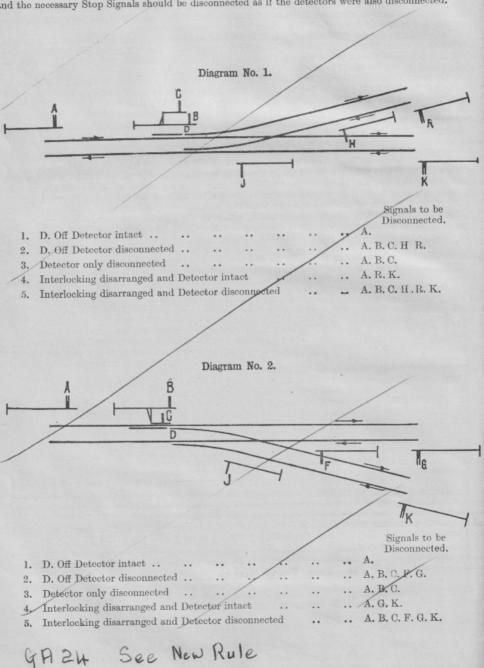
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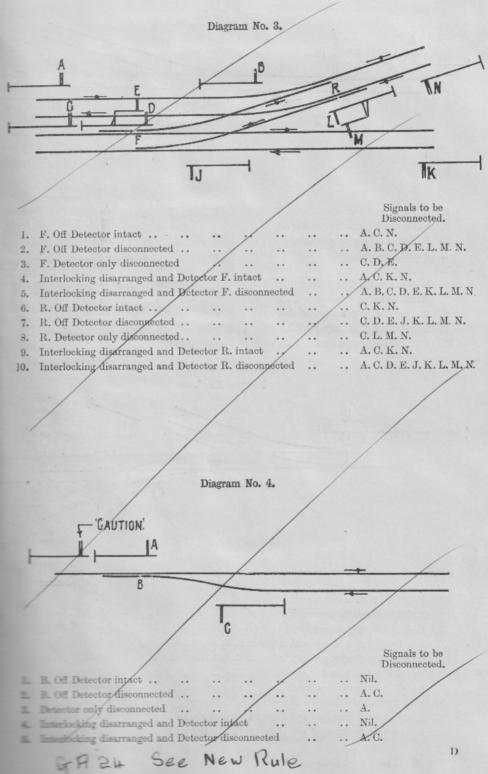
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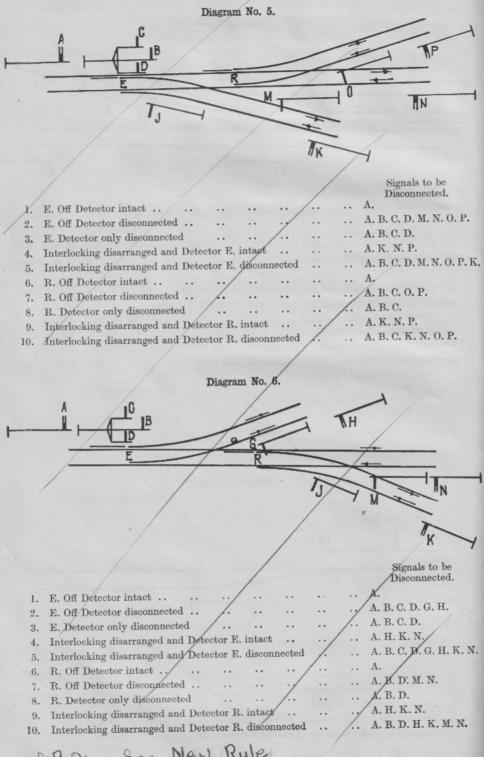
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locking or disconnection of the points, must also be disconnected and kept at danger, and while these Stop Signals are out of use all trains must be hand signalled past them until the repairs are finished and ordinary working can be resumed.

The following diagrams illustrate typical cases where Points are disconnected where mechanical detection exists. Where Electrical detection only exists, the detection must be considered ineffective and the necessary Stop Signals should be disconnected as if the detectors were also disconnected.







GAZH See New Rule

#### RULE 77.

## Signal Engineer's Occupations.

When the locking is disarranged no points may be moved, signals lowered, or hand signals given to Groundmen stationed at points and signals without the permission of the District Inspector, Reliefman or Signalman in charge of the work.

In cases where the Distant Signal disconnected in connection with Rule 77 is the lower arm on the post of the stop signal of the Signal Box in the rear and is not the standard distance out in accordance with Regulation 4A of the Regulations for Train Signalling on Double and Single Lines, the outermost Distant Signal must also be disconnected.

District Inspector to make all necessary arrangements in accordance with Rules 77 and 78 and the amplification of Rule 77 on page 15 of the General Appendix where the latter applies.

(G.A.22-2/49. LK1/9581/20.)

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Rule 83.—The signalman is not authorised to request the lineman to release the locking except on authority from the Divisional Superintendent or District Traffic Manager.

Whenever the locking is released the signalman, having obtained the necessary authority and made arrangements with the lineman, must enter in the train register "Lock on Lever No. ....... released" and the time. This entry must be signed by both men. Should a change of signalman take place prior to the restoration of the lock, the man coming on duty must countersign the entry. When the lock is restored the time must be entered in the train register and the entry signed by the signalman and the lineman.

(G.A.13. 9/43. O.M. 12152.)

Rule 91 (f).—The provisions of the first paragraph of this clause will also apply to Draw-Ahead signals.

The following to be inserted on page 19:-

Rule 120.—For the purpose of this Rule it must be understood that fully fitted freight trains which are not required to carry side lights are those classified "C" and signalled by 5 beats on the Block Bell Code, viz., 3 pause 1 pause 1.

(G.A.27.Op. 1/51. LK1/E.)

Diagram No. 7. B! CAUTION! Signals to be Disconnected. 1. B. Off Detectors intact B. Off Detector B1 disconnected A. B. Off Detector B2 disconnected 4. Detectors only disconnected A. C. 5. Interlocking disarranged and Detectors intact Nil. 6. Interlocking disarranged and Detector B1 disconnected A. C. 7. Interlocking disarranged and Detector B2 disconnected A. C. Diagram No. 8. GAUTION. GAUTION! Signals to be Disconnected 1. B. Off Detector intact .. NI. 2. B. Off Detector disconnected A. C. 3. Detector only disconnected A. · 4. Interlocking disagranged and Detector intact NI! 5. Interlocking disarranged and Detector disconnected A. C. Sea New Rule GH ZL

Rule 77 (c) and 81 (c).—The instruction in these clauses as to the disconnection of Distant Signals when facing points are disconnected is to be understood as applying also when trailing points are disconnected.

Rule 81.—A Signal which is electrically repeated in the Signal Box must be regarded as defective the repeater is out of order and the Signalman is unable to satisfy himself that the Signal arm is working properly or the lamp burning satisfactorily.

Rule 84.—Particulars are shown in the respective Appendices to the Service Time Tables, or other local notices, of the places at which the last sentence of this Rule applies.

Rule 110, clause (b) second paragraph.—The prohibition against loose shunting of or against webicles containing passengers, includes any person except the Executive's employee in charge of the hand brake.

(G.A.24.—11/49.)

Rule 110 clause (c).

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The toning of wagons by road motor vehicles which are not specially provided for this purpose is prohibited authorised by the Superintendent of the Line, Chief Goods Manager or Chief Docks Manager. (G.A. 15-12/44 L.K.17684)

Rule 121. Side lamps are not provided on G.W. passenger, empty coaching stock, parcels, perish-

In the case of a Mixed train with a Goods Guard's brake van as the rear brake van, side and tail Samme must be carried as for a Freight train. (See page 143.)

RULE 121 .- page 20.

The existing entry to be deleted. (See New Rule.)

The word "adjoining" in clauses (b) and (c) includes a line running in the same direction where another line used in the opposite direction intervenes. (G.A.24.—11/49.)

## Rule 122.—Tail Lamps on Light Engines.

Chimney First.

Tail lamp to be carried on top lamp stand of tender or bunker when not lighted, but when alight it must be carried on the stand in centre of buffer plank.

Tender or Bunker First.

Tail lamp to be carried on stand at bottom of chimney during the time lamp is not lighted; but when alight it must be carried on the stand in centre of buffer plank.

Rule 129, clause (iii).—Great Western Goods Guards, in addition to carrying the articles named in this clause must have with them a brake stick.

Rule 133.—The amplification of this rule to be amended to read:—

Rule 133.—Tail lamps of passenger, empty stock, perishable, parcels trains and "C" Headcode freight trains assisted in the rear uncoupled need not be removed before ascending the incline.

(G.A.30 Op.-9/54 L.K.1/-)

Rule 131.—With regard to clause (g) of this rule, Signalmen must in the case of coupled trains, after sending the "Train entering section" signal on the Block Instrument, send the word "coupled" on the Telephone.

Freight, mineral, or ballast trains, wholly or partly composed of vacuum-fitted stock, must not be coupled to trains unprovided with the vacuum brake (on account of the difference in braking power) except in cases of great emergency, when the vacuum brake pipe must be disconnected from the engine and both trains worked as ordinary non-vacuum fitted trains,

All such cases of coupling vacuum fitted and non-vacuum fitted trains must be reported to the

Divisional Superintendent or District Traffic Manager when they occur.

The combined load of coupled freight trains must not, except as shewn below, exceed 100 wagons. Whenever it is found that the combined load of two trains which it is proposed to couple exceeds 100 wagons, the trains must be worked forward singly unless the load can be reduced.

In certain exceptional cases the Divisional Superintendent or District Traffic Manager may permit

the combined load to exceed 100 wagons, but in no case more than 120 wagons.

Note.—The "wagons" referred to above are ten-ton wagons or equivalent thereto, and the load for each train must be calculated on a ten-ton wagon basis.

Rule 149, exception (vii).—Propelling Ballast Trains. This exception will cover ballast or material trains, cranes, and any other Engineering Department trains (including trains wholly or partially formed of coaches conveying railway workmen) when working in a section—except on a falling gradient steeper than 1 in 200—to nearest convenient point at which engine can run round train. Maximum speed 15 miles per hour. Such trains formed of more than 20 vehicles must not be propelled unless the Driver can keep the whole of the train in view or a look-out man is provided in the centre of the train to transmit hand signals.

The restriction in regard to propelling or falling gradients steeper than 1 in 200 may be withdrawn in the case of Engineers' light inspection trains completely composed of victum stock coupled throughout, provided the Engineer has occupation of the line and also that there is a brake van at the end which is leading with someone riging in it who can apply the hand brake.

Rule 153.—Particulars of the places where freight trains (not being merely shunting operations) are allowed to run on a passenger line from one point to another without a brake van in the rear, appear in the respective Appendices to the Service Time Tables, or other local notices.

Rule 153, clause (b) .- Steam and hand travelling cranes with the jib properly secured on a specially constructed match truck fitted with roller on which the jib can rest and traverse may be permitted to travel with the jib leading or trailing. Other steam or hand travelling cranes must, when practicable, travel with the jib trailing. Travelling cranes must be conveyed from place to place, as far as possible, by slow freight trains. (See pages 186 and 295.)

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Rule 149. exception vii.-Propelling Ballast Trains.-page 20.

The second paragraph of this instruction to be amended to read:—

The restriction in regard to propelling on falling gradients steeper than I in 200 may be well arm in the case of Engineers light inspection trains completely composed of vacuum so coupled throughout also vacuum fitted Hopper ballast wagon trains equipped with fully vacifitted or piped brake vans, provided the Engineer has occupation of the line and also that the strain at the end which is leading with someone riding in it who can apply the hand brand/or the vacuum brake.

(G.A. 30 Op.—9/54 L.K.1/11729/36

The following to be inserted as the fourth paragraph:—

Weed killing trains which are not fully vacuum fitted throughout or on which there are than four fully vacuum fitted vehicles attached to the engine must not be propelled on fagradients steeper than 1 in 260.

G.A.30 Op.—9/54 L.K.1/10661/41

Table 114 (c) - Placing Trains or Vehicles Outside Home Signals on falling gradients.

Trains or vehicles must not be placed outside—(a) Outer Home signals or Home signals, or (b) the signal next in advance of an Outer Home signal or where more than one Home signal is provided outside the second Home signal in the normal direction of where the line is on a falling gradient towards the Signal Box in the rear, except as shewn below :-

(I) On any gradient.

(i) An engine, or an engine with one or two brake vans.

(ii) Trains or vehicles, provided the engine is at the lower end.

(2) On gradients not steeper than I in 260.

Trains or vehicles, provided the vehicle at the lower end is a brake van in which a guard or shunter is riding.

(3) On gradients steeper than I in 260.

Only where authorised by the Operating Superintendent or as shewn in clause (1) above.

In any of the above-mentioned cases the setting back movement must not be made beyond a point which will bring the train or vehicles immediately outside the signal referred to unless the movement is required to pass through a connection beyond that (G.A.27.Op.-1/51. LK1/10428/Gen.E.)

Rule 122.-Tail Lamps on Auto Engines.

Tail lamp to be carried on stand at bottom of chimney during the time the lamp is not lighted and in the centre of buffer plank when alight. (G.A.12, 4/43, O.M.12070.)

132.—Driver and Fireman to be on Engine when in motion.

On the Great Western System and where the passage of engines from one Depot to another will be facilitated by the arrangement, two light engines in steam (or any greater number which may be authorised on particular routes by the Engineer) may run coupled Two engines . . . . Driver and Fin

Driver and Fireman on leading engine; one Driver or Fireman on trailing engine.

Three or more engines, coupled Driver and Fireman on leading engine; one as permitted. Driver or Fireman on each intermediate engine, Driver and Fireman on last engine.

Not more than five engines to be so worked, all to be in steam, closely coupled and the vacuum pipes connected throughout. (G.A. 10. 3/42. LK 1/6543/6).

Rule 138. Loading and Unloading Rails by the "End-on" Method.

When an Engineer's materials train is engaged in loading or unloading rails by the "end-on" method, the Engineering Department man in charge of the operations will give the necessary hand signals to the Driver so far as the movement of the train is concerned. The Guard will apply hand-brakes as may be necessary on the portion of the train which is separated and stationary, but the operation of the brake on the vehicle on which rails are being loaded or unloaded, will be performed by the Permanent Way man appointed by the Inspector or Ganger in charge. When loading or unloading operations are not being performed, the train will be under the control of the Guard.

(G.A.15-12/44 L.K.1/7496/5A)

RULE 147-page 20.

The following to be substituted for this instruction :-

"When a train is admitted to a loop, goods line or siding, before the last vehicle has passed the Box the Guard (or Fireman in the case of a light engine) must advise the Signalman by the quickest during fog or falling snow, to indicate that the train is in clear. The Guard's hand signal must be given from the brake van and the Fireman's from the footplate. The Guard (or Fireman) must continue to exhibit the hand signal until it is acknowledged by the Signalman, who will, at night or during fog or falling snow, exhibit a white light held steadily. (G.A.24.-11/49.)

= 149, exception (vii)-page 20.

The following to be inserted as the third paragraph :-

The restriction may also be withdrawn in the case of vehicles provided for the accommodation of workmen accompanying an engine and snow plough when there is no alternative, but for such vehicle returning from the site of obstruction to be propelled over the obstructed line. The vehicle provided for the accommodation of the workmen must be either a vacuum fitted goods brake van or a steam heated passenger brake vehicle.

(G.A.23-7/49. L.K.1/9218/Gen.)

Reference to the following to be made on page 20 :-

Page 151.—When a crane and match wagon working with a breakdown train are detached the man in charge of the breakdown van train staff will be responsible for satisfying was the crane and match wagon are secured and the crane properly scotched. (G.A.23-7/49. R.E. Stand. L.K.1/9263/13.)

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The following to be inserted on page 21:

Rule 205, Clause (b).—On single lines worked in accordance with E.T.T. Regulations cases may arise in which pilot working in accordance with Regulation 25 is in operation for a prolonged period, but the Token stations between which such working operates are required to be closed during the hours of darkness. Where in such cases the same Pilotman is on duty throughout, but a fresh Signalman is on duty the following morning at one or both of the Token stations it will not be practicable for the late-duty Signalman of the previous day to notify the Signalman taking duty of the arrangements in force. In such circumstances the Station Master must sign the form of the late-duty Signalman and arrange for this to be handed to the early-turn Signalman the following morning, but where circumstances make this course impracticable a suitable entry must be made across the Train Register, by the Signalman going off duty, to the effect that pilot working is still in operation and the pilot working form must be left in the Train Register.

(G.A.18. 11/47, L.K. 1/8780/38.)

## Rule 215-Permanent Way Trolleys-page 21.

The following to be added as an additional paragraph:

When a permanent way trolley is required to be worked over any sidings for which an occupation has not been obtained, the Ganger or man in charge of the trolley, before allowing it to be put on the rails, must come to a clear understanding with the man in charge of the working over such sidings as to the proposed work to be carried out and the movements of the trolley required to be made and the latter must then take the necessary steps to advise all concerned.

(G.A.16. 5/46. LK1/7940/5A)

## RULES 189-208.—SINGLE LINE WORKING WHERE MORE THAN ONE RUNNING LINE IS AVAILABLE.

1. Where there are more than two running lines, and all the lines in one direction are blocked by accident or are out of use owing to Engineering Department occupation, leaving two or more lines free for traffic, Single Line Working must be put into operation over one of the unobstructed lines for all trains running in the wrong direction. The other line or lines must be used only by trains in the normal direction.

2. The Rules applicable to Single Line Working must be observed in regard to the line over

which Single Line Working is put into operation.

3. Where practicable trains travelling over the line or lines used in the normal direction should be turned on to these lines at some point before reaching the Single Line Working and allowed to run to a point beyond the Single Line Working, in order that they shall not conflict with trains being worked over the line used in the wrong direction.

4. The Station Master or person in charge who puts the Single Line Working into operation will be responsible for advising the Signalmen at Boxes beyond the limits of the Single Line Working when it is necessary to turn trains at their Boxes to and from the parallel line used in the proper

75. Where this method of working is adopted the Pilotman must instruct Drivers of trains passing over the line used in the wrong direction not to change their headlights at night or during for or falling snow or when passing through a tunnel.

6. The passage of trains over the line used in the wrong direction must be controlled by :-

(a) the block instruments at all Signal Boxes between which Single Line Working is in operation and applicable to the obstructed line nearest to the line being used in the wrong direction, and

(b) the signals at intermediate Signal Boxes and applicable to the obstructed line nearest to the line being used in the wrong direction.

7. In the event of the block apparatus having failed the provisions of Block Regulation 25 must be observed, but it will not be necessary for the Pilotman to accompany every train as shown in Rule 198 (a).

8. Drivers may be instructed in certain cases to bring engines to a stand at a particular signal or point to wait further instructions, and they must be careful to clearly understand and carry out any such instructions.

9. When this method of working involves turning Passenger trains in the normal direction over Goods lines the instructions shewn in clause 5 (b) of the Regulations for signalling trains and engines by Permissive Block system over Goods Running Loop lines and other Permissive Lines will apply.

(G.A.24.—11/49.) Reference to the fol

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Rules 192 (b) and 202 (a).—When a Wrong Line Order is issued in accordance with Rule 192 (b) the first sentence of Rule 202 (a) will not apply. (G.A.26 Op.-5/50.) Rule 217. Clause (a)—Provision of Handsignalmen in connection with the use of Litting Jacks and Jim Crows.—Accidents having occurred owing to permanent way men having been unable to remove lifting jacks and jim crows from the line on the approach of trains, special attention is directed to the instructions in this clause as to sending out a handsignalman if from any cause the line is unsafe.

It must be clearly understood by all concerned that whenever there is the slightest possibility of a train approaching too quickly to permit of any obstruction being removed from the line a handsignalman must be provided, and wherever the sight of approaching traffic is limited at places where the line is being lifted by means of a jack, a look-out man must be provided, if necessary, to give ample warning to the gang, particularly on lines over which trains run at high speed. In those cases where in the opinion of the Ganger or man in charge it is not necessary to post a handsignalman, the Ganger or man in charge must nevertheless appoint one man to stand by each appliance and must instruct this man that his duties are to remain by the lifting jack or jim crow whenever it is placed in position near the rail, and continuously to look out for approaching trains and remove the appliance clear of the line in good time.

(G.A. 5.-2/39. O.M.11868.)

Reference to the following to be made on page 21 :-

#### RULES 189 to 208 and 217.—ARRANGEMENTS DURING PERMANENT WAY OPERA-TIONS AND SIGNAL ALTERATIONS.

#### Single Line Working and "Absolute Occupation" Arrangements.

The District Inspector or Station Master will appoint the Pilotman and Handsignalmen for Single Line working in every case, but Handsignalmen may be drawn from the Engineering Department Staff if necessary.

The line on which the Engineers have to carry out their work must not be occupied by them until written permission is given by the person in charge of the arrangements. Before trains are allowed to resume running on such line the Engineering Inspector or man in charge of the work must give to the person in charge of the traffic arrangements if present, or if he is not present, to the Signalman at the Signal Box nearest to the spot where the occupation is given up, a written certificate that the line which has been occupied is in proper condition for the passage of trains.

## "Absolute Occupations" not involving Single Line Working.

When Single Line working is not put into operation, for example, where trains are diverted over other lines, Rule 217 will apply and the Handsignalmen will be provided by the Engineering Department.

The line on which the Engineers have to carry out their work must not be occupied by them until written permission is given by the person in charge of the arrangements or by the Signalman in charge of the Signal Box in the rear, and before trains are allowed to resume running on such line, the Engineering Inspector or man in charge of the work must give a written certificate that the line which has been occupied is in proper condition for the passage of trains.

## "Absolute Occupation" by Engineering Donate of Single Lines worked by Electric Train Token.

worked by Electric Train Token, the man in charge of the work must, unless he has in his possession the token or occupation key for the Section occupied, arrange for the obstruction to be protected accordance with Rule 217. The time at which the token is handed to and received from the Ganger or man in charge of the work must be recorded in the Train Register.

A certificate to the effect that the line is safe for the passage of trains must be furnished by the Engineering Inspector or man in charge of the work after the work has been completed in all cases where the Engineering Department have occupation of Single Lines for Relaying, or other work ffecting the safety of the line.

# \*\* Absolute Occupation "by Engineering Department of Single Lines when Electric Train Token Working is Suspended.

During the period of such Absolute Occupation the instructions in the Electric Train Token Regulations may be regarded as suspended and Drivers of Engineering Department trains may

Rules 189 to 208 and 217 Arrangements during Permanent Way Operations and Signal Alterations.-page 21. The following to be inserted as the third paragraph of the instructions under heading

the setting back movement is subject to the instructions at any

" 'Between Trains' Occupations":-

"A ballast train must not be set back into a section where intermediate block signals are

crams must stop where ordered to do so by the Engineering Department.

provided." (G.A.30 Op.—9/54 L.K.1/11874/420) proceed on to the Single Line without being in possession of the token, provided they are instructed to do so by the Ganger, man in charge of the work, or, in his absence, by the Signalman controlling the entrance to the single line in occupation by the Engineering Department. Drivers must proceed with caution and be prepared to stop short of any obstruction. Guards must keep a good look-out and be specially on the alert.

A certificate to the effect that the line is safe for the passage of trains must be furnished by the Engineering Inspector or man in charge of the work after the work has been completed in all cases where the Engineering Department have occupation of Single Lines for Relaying, or other work affecting the safety of the line.

The setting back of ballast trains in the wrong direction into a block section is prohibited where the gradient is steeper than I in 200, falling towards the Box in the rear except where special authority has been previously obtained. Where the line is on a falling gradient towards the Box in the rear with the setting back movement is subject to the instructions shewn on page 20 of the General Appendix

der heading "Rule 149, exception (vii) Propelling Ballast Trains "being complied with.

When Trainmen working a ballast train are authorised by the Signalman to set back in the wrong

rection into a block section exceeding  $\frac{3}{4}$ -mile in length at a time when the Engineering Department ve "Between Trains" occupation of such section, the setting back movement must terminate a point not less than  $\frac{1}{2}$ -mile in advance of the most advanced stop signal of the Signal Box next in

The Engineering Department will provide Handsignalmen in accordance with Rule 217 in con-

ction with "Between Trains" occupations.

All ballast trains must stop where ordered to do so by the Engineering Department.

tions and Signal Alterations Notice, or in any other Notice that may be issued are only to be granted in clear weather and must be cancelled if owing to fog or falling snow they are likely to interfere with the working of the Trains.

(G.A. 25—1/50. L.K.1/9581/20.)

**RULE 218.** 

Disconnecting Distant Signals in connection with Restrictions of Speed.—Page 21. The instructions under the above heading to be cancelled.

(G.A. 22-2/49. R.E. Stand. Op. Com. Min. 38.)

PERMANENT RESTRICTION OF SPEED INDICATORS.—Page 143.

The following paragraph to be added;

Where a temporary speed restriction is super-imposed on the whole or part of a length of track which is subject to a permanent speed restriction, it will not be necessary for the Permanent lestriction of Speed Indicator where provided to be obscured, but Drivers must regard the temporary estriction of speed laid down as superseding the Permanent Restriction.

(G.A. 22.-2/49. L.60164/89.)

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Rule 158. - "Out of gauge" Traffic and "Exceptional loads." In order that Signalmen may be reminded of the fact that "out of gauge" traffic or "exceptional loads" are being conveyed by a particular train, when signalling trains conveying such traffic, they must, after forwarding the special Is line-clear for train conveying out of gauge or exceptional load?" bell code for the train, send the words "out of gauge" or "exceptional load," as the case may be, on the speaking instrument. The tmost observation and care must be exercised when dealing with such trains.

The person responsible for putting such traffic on the train must ensure that the Signalman at

the starting station is properly advised.

Rule 175, clause (b).—The Handsignalman need not join the train until it arrives at the Signal Pox at the entrance to the block section in which it has to work.

Rule 215.—Brake sticks must be carried on each trolley for use in cases of emergency. In the event of its being necessary to put more than one trolley on the line at the same time, care must be taken to see that they are kept far enough apart to avoid the possibility of collision.

Permanent Way Trellies.—The Ganger or man in charge of a trolley, whether manually or mechanismly propelled, must always approach a station with care and not exceed a speed of 10 m.p.l. on the Distant Signal, and be prepared to stop in case of shunting going on between the Distant Home Signals or other duties being performed.

Rule 215.—Use of Trollies where track circuiting is provided.—A track circuit must not be relied upon to protect a trolley, but the Ganger must arrange for its protection in accordance with Rule 215.

Except in the case of track circuits provided in connection with Intermediate Block Signals, trolleys may be run over track circuits but not be allowed to stand on them while work is being performed mless the permission of the Signalman has previously been obtained.

Where Latermediate Block Signals are provided before a trolley is placed upon or run over a frack creuit, the permission of the Signalman to whose Box the track circuit is connected must first be braided. In such cases a telephone is provided which may be used by the Garger to communicate with the Signalman. CTA 31

Rules 215, 217, and 234.—The references to "occupation token" in clause (n) of Rule 215, clause a) of Rule 217, and clause (1) of Rule 234 are to be understood as applying to "occupation key." GA30

Rules 215-217.-Provision of Handsignalmen. Each Permanent Way Inspector must see that regularly employed man in the length gangs and relaying gangs in his district is supplied with a copy of the "Forms of Examination of Look-Out Men, Handsignalmen and Fogsignalmen." He also be held responsible for instructing and examining the men in their duties, and no man must be allowed to act as Handsignalman who has not been passed by the Permanent Way Inspector as exempetent to act in that capacity.

New entrants to the regular staff in length and relaying gangs must be examined within six months

of the date of their attachment to such gangs.

A red flag held steadily in the hand must be exhibited by the Handsignalman to indicate to the Ganger man in charge that the protection required under Rules 215 and 217 has been properly carried out.

A green flag held steadily in the hand must be exhibited by the Ganger or man in charge to indicate to Handsignalman in the rear that the trolley has been removed from the running line or that the obstruction that was being protected in accordance with Rule 217 has been cleared. (G.A.9 7/41. O.M. 12004.)

man must be provided, and wherever the sight of approaching traffic is limited at places where is being lifted by means of a jack, a look-out man must be provided, if necessary, to give warning to the gang, particularly on lines over which trains run at high speed.

Rule 217, clause (g). - In the event of the half mile point (or such farther necessary distance) se to a Distant Signal, the Handsignalman must proceed at least 200 yards to the rear of the der that there may be no risk of his hand signal conflicting with the Distant Signal.

218.—Disconnecting Distant Signals in connection with Restrictions of Speed.—In connection restrictions of speed during Engineering Department operations, the following instructions must

In cases where the Distant Signal disconnected in connection with the observance of Rule 218 is where are distant and is not the correct distance out in accordance with Regulation 4A, for train Double and Single Lines, the Outer Distant Signal must also be disconnected and so In cases where independent Distant Signals are disconnected for warning purposes, beginning the time the Box is switched out.

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irection over s and engines es will apply. ..24.—11/49.)

(ule 192 (b) 0p.-5/50. The following to be inserted on page 21:—

Rule 178 (a).—The following to be added to the second paragraph of this rule:—

In the case of a Driver becoming aware that his train has been accidentally divided and he is unable to see the rear portion, and the circumstances are such that it is safe for the front portion to be brought to a stand or he is brought to a stand at a signal at Danger, the Fireman must, before proceeding to meet the Guard, place 3 detonators opposite his engine on any adjoining line in the opposite direction. If the Driver does not consider it safe to stop, he must endeavour to attract the attention of the Driver of any train approaching on the opposite line, such as by sounding the engine whistle.

(G.A.27.Op.—1/51. L.K.1/E.)

Rule 218 Provision of "Arrow" Warning Boards.—"Arrow" warning boards will be provided irrespective of the length of time the restriction is expected to be in force, and will be fixed on the commencing date of temporary speed restrictions shown in the Weekly Reduction of Speed and Engineering Arrangements Notice or other special potice. A Handsignalman will be appointed during the first week except on certain specified branch lines, who will act in accordance with clauses (q) and (h) of Rule 217.

Drivers must be prepared at all times to act upon hand signals exhibited by a Handsignalman,

whether a "warning "board is provided, in addition, or not.

Rule 218. Provision of Arrow Warning Boards.

Arrow Warring boards will be provided irrespective of the length of time the restriction is expe be in force, and will be fixed on the commencing date of temporary speed restrictions shewn in the Fort Reduction of Speed and Engineering Notice or other special notice. Drivers must be prepared at all times to act upon band signals exhibited by a handsignalman, a Warning Board is provided in addition or not.

When particulars of the speed restriction are shewn in the Fortnightly Speed and Engineering a handsignalman will be posted for the first 48 hours who will act in accordance with clause (g) c a handsignalman will be posted for the first 48 hours who will act in accordance with clause (g) c Rule 217. When the advice of a speed restriction is given by a special natice or telegraphic advinot shewn in the Fortnightly Speed and Engineering Notice, a handsignalman will be posted for 48 hours, after which, unless shewn in the Notice to the contrary, he will be withdrawn, upon the Engagement being informed by the Divisional Superintendent or District Traffic Manager that the act been received by all concerned. The Handsignalman will not, in any case, be employed for more

Tomporary Restrictions of Speed on Branch Lines.

Handsignalmen will not be posted on certain specified lines when particulars of the speed restriction are shewn in the Fortnightly Speed and Engineering Notice, and the positions of the Warring Board indicated in the notice as required by Rule 218. The words "Handsignalmen will not be posted" with the notice as required by Rule 218.

When the advice of a speed restriction is given by telegraphic advice or a special notice, and not win in the Fortnightly Speed and Engineering Notice, a handsignalman will be posted until the Engineering Notice, a handsignalman will be posted until the Engineering Notice, a handsignalman will be posted until the Engineering Notice and the Engineering Notice and the Engineering Notice and Indiana t

The Branch lines to which this arrangement applies are scheduled in the Appendices to Service more than one week.

Time Tables or other local notices,

NOTE: -On Goods and Mineral lines where a permanent speed restriction of 15 miles bosted i less is in force, further speed restrictions will not be imposed nor handsignal posted i \*connection with Engineering work, unless arrangements are made between Engineer and Divisional Superintendent or District Traffic Manager. (G.A. 14. 1144. 11764)

The examination of look-out men as required by Rule 234 (e) will be conducted in the case of staff employed in the Signal and Telegraph Department by the Signal and Telegraph Inspector concerned. Look-out men employed in Swindon Works will be examined by the Foreman in charge, but those employed by the Chief Mechanical Engineer's Department in connection with the cleaning of water troughs must be examined by Permanent Way Inspectors.

See also page 21 re Rule 217 clause (a), with regard to the provision of look-out men where neces-

sary, in connection with the use of lifting jacks.

Rule 234. Clause (e)-Amplified as follows:-

No man must be appointed as Look out man unless he has previously been passed as competent to act in that capacity by the Permanent-way Inspector or other person authorised by the Engineer or Signal Engineer.

Rule 240.—Conveyance by Goods Train of Explosives and Dangerous Goods.—Where consignments of explosives or dangerous goods are forwarded under a Secretary of State's Warrant under which the Company is indemnified, the limitation in clause (10) of Rule 240 that "not more than five vehicles containing explosives must be conveyed by any one train at any one time " will not apply. On receipt of such a Warrant, the Station Master or Goods Agent must immediately wire the Chief Goods Manager, giving particulars of the explosives, etc., tendered for conveyance.

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Rule 231.-Provision of "Look-out" Men.-The warning given by a "Look-out" man is to be regarded as an intimation to the men he is protecting to stand clear immediately and the "Look-out" man must exercise every vigilance to ensure the warning being heard and acted upon by the men.

Rule 234 provides for the posting of more than one look out man as may be necessary. Particular attention is necessary in the case of work of such a character as to necessitate more than ordinary length of warning to the men engaged upon it, or where the look-out man might not have a good and distant view of signals, e.g., during the erection of steelwork near the line, painting of structures from ladders. The requirements of safety must be adequately met and consideration be given to whether Handsignalmen should be posted under Rule 217.

Authorised representatives of the Signal and Mechanical Departments will, when necessary, arrange with Permanent Way Inspectors for the provision of look-out men to protect men of either of those Departments when engaged in operations on or near the track. The Chief Mechanical Engineer's Department will provide look-out men during cleaning of water troughs, and as required in

Swindon Works. When a look-out man is protecting men whose normal hours of duty or meal intervals differ from his own, he must not withdraw his protection while any of the men continue to work. Arrangements are to be made by the Permanent Way Inspector or other person responsible for the provision of look-

out men for the necessary alterations in meal times or hours of duty. Every regularly employed man in the permanent way length gangs and relaying gangs must be supplied with a copy of the "Forms of Examination of Look-out men, Handsignalmen and Fogsignalmen."

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# Rule 240.—Conveyance by Goods Train of Explosives and Dangerous

Clause 8.—The distinctive label for explosives will be as shewn on page 148 of the General Appendix. Wagons bearing such labels must be treated as containing explosives.

Rule 240.—Conveyance by Goods Train of Explosives and Dangerous Goods.—page 22. The amplification of Clause (9) to be cancelled.—See standard Rule 240 clause (9).

The General Note shown in supplement G.A.8 to be cancelled.—See standard Rule 240—last paragraph.

Clause (10). The existing amendment to be deleted and the following substituted:-

Rule 240.—Clause (10). The restriction on the number of vehicles containing explosives which may be conveyed by any one train at any one time to a maximum of five does not apply in the case of explosives conveyed on account of:-

- The Government (Admiralty, War Office, Air Ministry and Ministry of Supply).
- A Trader for transit to a Government establishment.
- - A Trader for transit to a Trader on account of the Government.

The marshalling arrangements laid down in the Rule must, however, be observed.

(G.A.30 Op.—9/54 L.K.1/E)

#### Rule 240 Conveyance by Goods Train of Explosives and Dangerous Goods-page 22.

Clause 9 to be inserted as amplified below:

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In addition the Guard must inform the Driver and Fireman of the train engine and of any assisting engine the number of wehicles containing such traffic and their position on the train.

Clause 10, to be amended as follows:

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Maximum Number and halling picles (10) Not more than five vehicles containing explosives must be conveyed by any one train at any one time. Vehicles containing explosives must be marshalled as near the middle of the train as possible. Trains conveying not more than five vehicles containing explosives must have at least two vehicles either empty or containing non-explosive traffic marshalled between the engine and the first vehicle containing explosives, except that for short distances between depots or private and marshalling yards these two vehicles need not be provided.

Note.—The foregoing paragraph does not apply in the case of Explosives conveyed on account of:

- (i) The Government (Admiralty, War Office, Air Ministry and Ministry of Supply).
- (iii) A trader for transit to a Government Establishment.
- (a) A trader for transit to a trader on account of the Government.

that where possible odd wagons of explosives should be marshalled near the middle of

Tank wagons containing highly inflammable liquids or compressed liquified gases must not, when travelling for short distances between depots or private sidings and marshalling be placed next to the engine, but at least one vehicles either empty or containing non-marshalled between the engine and the first tank wagon.

(G.A.16. 5/46, LK1/6133/6)

Reference to the following to be made on page 22 :-

Bute 218. Clause (e). The "I" Board will normally be located at termination of affected in exceptional circumstances such as where multiple unit stock is in use or where in of Engineer it is desirable for safety reasons, the "T" Board will be put further out.

(G.A.23—7/49. R.E. Stand:—C.E. K.1/57,293

220. Each Ganger (or in his place another member of the gang or other appointed for the nurpose as authorised by the Engineer) must carry eight ors instead of six as provided for in this rule.

Deleted GRZL (G.A. 8.—5/41. LK1/6233/10.)

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ADDITIONS TO THE STANDARD RULES.

The following to be inserted on page 22 :-Rule 234. Clause (d)—New entrants to the regular staff in length and relaying gangs must be examined

Permanent Way Inspectors within six months of the date of their attachment to such gangs.

(G.A. 1. 3/37. C.E.O. J2/34395.)

# SECTION I. (b).

# EXTRACTS FROM REGULATIONS FOR TRAIN SIGNALLING ON DOUBLE AND SINGLE LINES

for the guidance of Enginemen, Guards and others concerned.

# SECTION I. (b.)

# EXTRACTS FROM RECULATIONS FOR TRAIN SIGNALLING ON DOUBLE AND SINGLE LINES

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## EXTRACTS FROM RECULATIONS FOR TRAIN SIGNALLING ON DOUBLE AND SINCLE LINES FOR THE CUIDANCE OF ENGINEMEN, CUARDS AND OTHERS CONCERNED.

(Regulations not included herein do not directly affect Drivers, Firemen or Guards.)

Notes .- Any Additions to, or Alterations in, these Regulations differing from the AGREED RAILWAY CLEARING HOUSE STANDARD ARE SHEWN IN THIS SPECIAL TYPE.

Wording which is not an actual extract from the above Regulations is printed in italics.

## EXTRACTS FROM REGULATIONS FOR TRAIN SIGNALLING ON DOUBLE LINES.

Absolute Block System.—The object of absolute block signalling is to prevent more than one train being in a block section between two signal boxes on the same line at the same time.

Regulation 5. Section clear but Station or Junction blocked. (Warning arrangement).—The method of conveying to Drivers that they are required to proceed into the Section in accordance with the provisions of this Regulation is dealt with in Rule 41.

Regulation 5.—Section Clear but Station or Junction Blocked (Warning arrangement). Clause (d) to be amended to read :-

DURING FOG OR FALLING SNOW A TRAIN MUST NOT BE DRAWN TOWARDS THE SIGNAL CONTROLLING THE ENTRANCE TO THE SECTION AHEAD TO AWAIT ACCEPTANCE FROM THE SIGNAL BOX IN ADVANCE, EXCEPT WHEN A FOGSIGNALMAN IS ON DUTY AT THE SIGNAL, UNLESS THE SIGNALMAN IS ABLE TO SATISFY HIMSELF THAT THE SIGNAL IS AT DANGER, AND THE LINE TO THE REAR OF SUCH SIGNAL IS TRACK-CIRCUITED OR A TRAIN STANDING AT THE SIGNAL WILL BE WITHIN HIS VIEW. (G.A.19—10/48. L.K.1/8617/31.)

VIDED) MAY BE LOWERED.

(g) Where intermediate block signals controlled from the signal box in rear are provided, should the "Is Line Clear?" signal be acknowledged by the Signalman at the box in advance in accordance with this Regulation 5, the intermediate block home signal must be maintained at Danger and the Signalman, after he has been informed by the Driver or Fireman that the train is at a stand at that signal, must instruct the Driver that the section is clear to the next home signal but that the station or junction ahead is blocked, after which the signal may be lowered.

If, after the Driver or Fireman has communicated with the Signalman the train is accepted under the Warning arrangement, the signal may be lowered, and the lowering of such signal will indicate to the Driver that the train may proceed in accordance with clause (b) of Rule 41, but if the telephone has failed and the Signalman is unable to satisfy himself that the train is at a stand at the signal it must not be lowered and the Driver will act in accordance with clause (e) of the General Instructions

regard to Intermediate Block Signals. (See page 71.)

Regulation 6. Engine assisting in rear of Train.—An engine must never assist a passenger TRAIN AT THE REAR EXCEPT WHERE AUTHORISED BY THE SUPERINTENDENT OF THE LINE AND THE CHIEF MECHANICAL ENGINEER,

Regulation 9. Trolley going into or through Tunnels.—(a) When it is necessary for a trolley to into or through any of the tunnels specially enumerated in the appendices to the service time as as coming within the application of this Regulation, it must be signalled on the block ments in accordance with the prescribed "Is Line Clear?" signal.

(b) Should the trolley, after passing into or through the tunnel, be removed from the rails before

seehing the next signal box, the Ganger or man in charge must go forward and inform the Signalman the trolley is clear of the line and the Signalman must then send the "Train out of Section"

If, however, time would be saved, the Ganger or man in charge must return to the signal box in and inform the Signalman that the trolley is clear of the line; the Signalman must then send the "Cancelling" signal. TROLLIES PASSING THROUGH ALL OTHER TUNNELS MUST BE PROTECTED SOLELY BY THE PERMANENT-

WAT MEN'S OWN SIGNALS.

(c) TEOLLIES PROPELLED BY SUCH MEANS AS ENABLE A FAST SPEED TO BE ATTAINED MUST BE SENALLED IN ACCORDANCE WITH THIS REGULATION WHETHER THERE BE A TUNNEL IN THE SECTION EXCEPT WHERE INSTRUCTIONS EXIST TO THE CONTRARY, SUCH TROLLIES MUST NOT BE PUT THE PAILS EXCEPT AT A SIGNAL BOX, AND MUST ALWAYS BE RUN THROUGH THE WHOLE BLOCK SECTION.

## EXTRACTS FROM BLOCK REGULATIONS-DOUBLE LINES-Continued

Regulation 10. Train out of Section. -(c) When the last vehicle of a train does not pass the the signal box before it has been shunted into a siding, or when a train has been brought to a stand with a the home signal, and it is necessary to give the "Train out of Section" signal before the train passes the signal box, the Signalman must, before giving the signal, ascertain from the Guard or Shunter in charge of the train that the whole of the train, with tail lamp attached, has arrived, and the Guard or Shunter will be held responsible for giving this information to the Signalman, the Fireman being similarly responsible in the case of a light engine.

Regulation 11. Course to be pursued when a Train is an unusually long time in a Section.—Should an unusual time elapse after the "Train entering Section" signal has been received, the Signalman, unless he is satisfied that the train is approaching, must, if the weather is clear and there is no tunnel in the section, stop the first train proceeding in the opposite direction, inform the Driver of the circumstance and, after the necessary signals have been sent and acknowledged, instruct him to proceed with caution; during fog or falling snow or where there is a tunnel in the section in which the train is running, he must prevent any train in the opposite direction (except an engine as provided for in Regulation 14A, Examination of Line), proceeding on its journey until he has ascertained that the line on which it has to run is clear.

Regulation 12A. Animals on Line.—(a) Should a Signalman become aware that cattle, horses or other animals are on the line and likely to cause an obstruction, he must at once inform the Signalman at the opposite end of the section affected of the circumstances on the speaking instrument and take the necessary steps to have the line cleared. Until it has been ascertained that the line is clear, all trains allowed to proceed must be brought to a stand and the Driver verbally informed of the circumstances and instructed to proceed cautiously.

(b) Should there be a tunnel in the obstructed section, the Signalman at each end of the section must verbally explain the circumstances to the Driver and the Guard and instruct them that although the train may proceed into the section it must not enter the tunnel until they have ascertained that

the tunnel is clear.

Regulation 13. Blocking Back.—(b) Unless special permission is given by The DIVISIONAL SUPERINTENDENT OR DISTRICT TRAFFIC MANAGER no train or vehicle must be placed outside a home signal where the line is on a falling gradient towards the signal box in the rear, AND THEN ONLY IF THERE IS A BRAKE VAN AT THE REAR AND A MAN WITH IT.

Regulation 14. Section obstructed by Accident or by Disabled Train. (a) Should a Signalman (a) Should a Signalman receive information from the Guard or Fireman of a disabled train or from the Signalman at the box in advance that an assisting train or engine is required to enter the section to assist the disabled train, or should it be necessary for the breakdown van train to enter a section obstructed by accident or otherwise, the ASSISTING TRAIN OR ENGINE or the breakdown van train, as the case may be, may, after having been brought to a stand and the Driver AND GUARD informed of the circumstances, be allowed to enter the section under the following arrangements:

(i) The Signalman must inform the Signalman at the box in advance of the circumstances, give the "Train entering Section" signal to that box, and after it has been acknowledged, note the circumstances in his train register book. He may then allow the ASSISTING TRAIN OR ENGINE to enter the section and must instruct the Driver to pass at Danger the signal controlling the entrance into the section ahead, as provided for in Rule 38 clause (b). The assisting train ORENGINE MUST BE RUN AT REDUCED SPEED AND GREAT CAUTION OBSERVED BY ALL CONCERNED. The Signalman in advance must also note the circumstance in his train register book, and must not give the "Train out of Section" signal until both trains have arrived.

The Guard or Fireman of the train requiring assistance will ride on the ASSISTING ENGINE OR on the engine of the ASSISTING TRAIN, and point out to the Driver the position of the disabled train. After the "Train out of Section" signal has been received and permission obtained for another train to proceed, such train must be stopped at the signal box in rear, and the Driver instructed to proceed cautiously through the section.

- (ii) Except during fog or falling snow, it will not be necessary for the Signalman in rear of the obstruction to detain the assisting train OR ENGINE until the arrival of the Guard or Fireman of the disabled train at his box if information has been received from the Signalman in advance that the Guard or Fireman is coming back. On receipt of this information the Signalman in rear may allow the assisting train or engine to enter the obstructed section after the Driver has been informed that the Guard or Fireman of the disabled train is coming back, and has been instructed to keep a look-out for such Guard or Fireman. The Guard or Fireman of the disabled train must ride on the assisting engine or the Engine of the assisting train, and pilot it to the rear of the disabled train.
- (iii) If there is a tunnel in the obstructed section the Driver of the assisting train or ENGINE must not enter such tunnel until the Guard or Fireman of the disabled train has come back and met the ASSISTING TRAIN OR ENGINE or it has been ascertained that the tunnel is clear. The Driver of the assisting train or engine must be instructed accordingly by the Signalman.

The Signalman in advance must stop any train proceeding in the opposite direction, and instruct the Driver to travel through the tunnel at reduced speed.

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- instruct the Driver to travel through the tunnel at reduced speed. (b) When the line is clear to the home signal but is occupied by a train or otherwise obstructed within the home signal, and assistance is required from the rear, OR IT BECOMES NECESSARY TO WOR TRAINS TO SOME POINT ADJACENT TO THE OBSTRUCTION AND WITHIN THE PROTECTION OF THE SAME
- SIGNALS, AND WHERE THERE IS A CROSSOVER ROAD OR REFUGE SIDING OVER WHICH OR INTO WHICE SUCH TRAINS MAY BE SHUNTED, the Signalman in rear must be informed of the circumstances In the case of a train requiring assistance, THE DRIVER OF THE ASSISTING TRAIN OR ENGINE MUST BE VERBALLY TOLD THE POSITION OF MATTERS AT THE SIGNAL BOX AHEAD. If the disabled train or the obstruction is within the authorised clearing point the assisting train or engine must be accepted (G.A.3.—12/37. O.M.11795.) under The Warning Arrangement.

# EXTRACTS FROM BLOCK REGULATIONS—DOUBLE LINES. REGULATION 12—"OBSTRUCTION DANGER" SIGNAL,—page 26.

The following to be inserted:

Regulation 12. Obstruction Danger Signal.—(h) If after the "Obstruction Danger" signal has been sent it is necessary to ascertain which line or lines are obstructed, an engine may be allowed to enter the section for this purpose at either end in accordance with Regulation 14A, clause (c)...

(G.A.18. 11/47. R.C.H. Op. Supts. Min. 78.)

Regulation 13.—Blocking Back—page 26.

Delete clause (b) under this heading and substitute the following :-

(b) Unless special permission is given by the Operating Superintendent no train or vehicles, other than an engine or engine with one or two brake vans, must be placed outside a Home signal where the line is on a falling gradient steeper than I in 260 towards the signal box in rear unless there is an engine at the lower end. WHERE SUCH PERMISSION IS GIVEN THERE MUST BE A BRAKE VAN AT THE LOWER END WITH A MAN IN IT. (G.A.27.Op.—I/51. L.K.I/10428/Gen.E.)

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REGULATION 17.-STOP AND EXAMINE TRAIN.-page 27.

The following to be inserted as clause (d):

IF AT THE SIGNAL BOX AT WHICH THE "STOP AND EXAMINE TRAIN" SIGNAL IS RECEIVED IT IS NOT POSSIBLE TO DETACH THE VEHICLE IN RESPECT OF WHICH THE EMERGENCY SIGNAL WAS SENT, OR TO RECTIFY THE DEFECT, OR OTHERWISE DEAL WITH THE EMERGENCY, AND IT IS CONSIDERED THAT THE TRAIN CAN SAFELY BE ALLOWED TO PROCEED TO A SIGNAL BOX IN ADVANCE WHERE THE MATTER CAN BE DEALT WITH AS OCCASION MAY REQUIRE, THE TRAIN MUST BE SIGNALLED FORWARD BY THE APPROPRIATE BELL CODE, THE "STOP AND EXAMINE TRAIN" SIGNAL BEING SENT IMMEDIATELY FOLLOWING THE ACKNOWLEDGMENT OF THE "TRAIN ENTERING SECTION" SIGNAL.

(B.R.7. 11/47. O.M. 12182.)

The following to be inserted as clause (g):

Should the rearmost portion of a divided train arrive within the home signal at the signal box in advance, complete with tail lamp and accompanied by the Guard, the Signalman may, after ascertaining from the Guard that the rear portion is intact and so informing the Signalman at the signal box in rear, give the "Train Out of Section" signal, in accordance with Regulation 10, but the first train requiring to travel over an adjoining line in the opposite direction must be stopped and the Driver informed of the circumstances and instructed to proceed cautiously.

The Signalman receiving the "Train Out of Section" signal must, in such circumstances, stop the first train requiring to proceed over the line on which the divided train has travelled and on any adjoining parallel line, and inform the Driver of the circumstances and instruct him to proceed cautiously.

(G.A.18, 11/47. Op. Com. Dec. 757.)

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#### EXTRACTS FROM BLOCK REGULATIONS-DOUBLE LINES-Continued.

TRAINS TO SOME POINT ADJACENT TO THE OBSTRUCTION AND WITHIN THE PROTECTION OF THE SAME SIGNALS, AND WHERE THERE IS A CROSSOVER ROAD OVER WHICH OR INTO WHICH SUCH TRAINS MAY BE SHUNTED, the Signalman in rear must be informed of the circumstances. In the case of a train requiring assistance, THE DRIVER OF THE ASSISTING TRAIN MUST BE VERBALLY TOLD THE POSITION OF MATTERS AT THE SIGNAL BOX AHEAD. If the disabled train or the obstruction is within the authorised clearing point the assisting train or engine must be accepted under The Warning Arrangement,

Regulation 14A. Examination of Line .- (a) When it is necessary in accordance with the emergency Clause (a) to be amended to read:

(a) When it is necessary in accordance with the emergency regulations, viz. Train an Unusually Long Time in a Section; Obstruction Danger; Stop and Examine Train; Train Passed Without Tail Lamp; Train Divided; Vehicles Running Away on Wrong Line; and Vehicles Running Away on Right Line, to ascertain if the line or lines are clear, an engine may be allowed to enter the section for the purpose on any other line in the same or opposite direction provided speaking communication exists between the two signal boxes which will admit of a proper understanding being arrived at between the two Signalmen and the Train out of section signal has been received for the previous train, but in connection with clauses (c) and (d) the engine may be allowed to travel through the section over any line. The circumstances must be explained to the Driver, and he must be instructed to pass at Danger the signal controlling the entrance into the section ahead as provided for in Rule 39, clause (b), and proceed cautiously through the section, prepared to stop short of any obstruction. Where practicable, the engine must be accompanied by a Station Master or other competent person. After sunset, during fog or falling snow, or where a tunnel intervenes, the engine must always be so accompanied.

Clause (b) amended to read:

(b) The Signalman at the Box at which the engine enters the section must not send the "Is Line Clear?" signal, but must inform the Signalman in advance of the circumstances under which the engine is entering the section, and the "Train Entering Section" signal must then be sent and acknowledged; the block indi-cator must be maintained at "Train on Line" until the engine has arrived at the Box in advance or has returned to the Box in rear and the person accompanying the engine, or the Driver, as the case may be, has reported which line or lines are safe for the passage of trains. Should the engine return in the wrong direction under the authority of a "Wrong Line" order (Form A, B or D) and the line be safe for the passage of trains, the "Cancelling" signal must be sent.

After arrangements have been made for an engine to enter the section in accordance with this clause,

the Signalman at the Box in advance may occupy the line ahead of the Home signal.

The following to be inserted as clauses (c) and (d):

(c) Should a Signalman receive information which in his opinion necessitates the examination of the line he must immediately advise the Signalman at the opposite end of the section or sections affected, and no train must be allowed to proceed through such section or sections until the line has been examined and is safe for the passage of the train; an engine may, however, be allowed to enter the section or sections affected

in the same or opposite direction, as laid down in clauses (a) and (b).

Should, however, the "Obstruction Danger" signal have been sent and it is necessary for an engine to enter the section at the Signal Box in rear in accordance with this clause, the Signalman who sent the signal may, after coming to a proper understanding with the Signalman at that Signal Box and provided the next adjoining line is not occupied between the two Signal Boxes concerned, send the "Obstruction Removed" signal but the block indicator must be maintained at the "Train on Line" position. After an engine has been allowed to enter the section in accordance with these conditions, no train must be allowed to enter the section in the same or opposite direction on an adjoining line until the engine has passed through the section and it is known that the line is clear.

(G.A.18, 11/47, R.C.H. Op. Supts, Min. 78.)

(d) In connection with the "Stop and Examine Train" signal for the purpose of ascertaining if the line or lines are clear, an engine may also, in accordance with the instructions laid down in clauses (a) and (b), be allowed to enter the sections, both in rear and in advance of the Signal Box from which the "Stop and Examine Train" signal was sent, on the same line and in the same direction as the train concerned has proceeded.

SHOULD IT BE NECESSARY FOR THE LINE TO BE EXAMINED CONSEQUENT UPON INFORMATION HAVING BEEN RECEIVED THAT A PASSENGER HAS FALLEN FROM A TRAIN, A BRAKE VAN, IN WHICH A GUARD MUST RIDE, MAY BE ATTACHED IN REAR OF THE ENGINE, BUT THE SIGNALMAN AT THE BOX IN ADVANCE MUST BE ADVISED ACCORDINGLY. WHEN THE STATION MASTER OR OTHER COMPETENT PERSON IS ACCOMPANYING THE ENGINE AND BRAKE VAN HE MUST RIDE ON THE ENGINE.

(G.A.18, 11/47. R.C.H. Op. Supts. Min. 15.)

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(e) In connection with "Train Divided," when it is required to ascertain if the line is clear after the divided train or front portion thereof has arrived, a train may be allowed to enter the section on the opposite or parallel line for this purpose provided the weather is clear, no tunnel intervenes, and there are no catch points in the section through which the divided train or portion thereof has travelled, except that a train conveying passengers must only be allowed to enter the section under these conditions during daylight.

Regulation 17. Stop and Examine Train .- (a) Should the Signalman who receives the "Stop and Examine Train" signal be able to ascertain from the Trainmen after examination of the train that the opposite line is not obstructed he may allow trains to proceed. Should, however, he be unable to ascertain any reason for the signal being given he must inform the Driver of the first train travelling in the opposite direction of the circumstances and instruct him to proceed cautiously to the next signal

(b) Should the "Stop and Examine Train" signal have been sent on account of a door being open on a passenger train, the Signalman sending the signal must advise the Signalman in rear and trains running in the same or opposite direction between these signal boxes need not be detained until evidence has been obtained that the line is not obstructed, but the first train in each direction must be stopped, the Drivers informed of the circumstances, and instructed to proceed cautiously to the next signal box, keeping a good look out.

Regulation 19. Train passed without Tail Lamp.—(b) Should a train pass without a tail lamp, or the Signalman be unable to satisfy himself whether or not the tail lamp is on the train, he must immediately place or maintain his signals at danger to stop the first train going in the opposite direction, inform the Driver what has occurred and instruct him to proceed cautiously so as to avoid danger in the event of any portion of the train having fouled the line on which his train is running.

(d) Should a train pass without a tail lamp and there are catch points or a tunnel in rear, or during fog or falling snow, no train (except an engine as provided for in Regulation 14A (Examination of Line) must be allowed to enter the section in either direction until it has been ascertained that the

line on which it is about to run is not obstructed.

Regulation 20. Train Divided .- (c) If the divided train is assisted by an engine in rear, or is running on a falling gradient, or where the line is level, or between short sections, where the stoppage of the first portion would risk a collision with the second portion, the Signalman, provided permission has been obtained from the box in advance for the train to proceed, must not exhibit the Danger signal to stop the first portion, but must exhibit to the Driver a green hand signal waved slowly from side to side

The existing clause (d) to be cancelled and the following to be inserted as clauses (d) and (e) :-

(d) If a Signalman considers that the stoppage of the first portion would cause a collision with the second portion and he has been unable to obtain permission for the divided train to proceed to the box in advance, he may, by the exhibition of a green hand signal waved slowly from side to side. authorise the Driver of the first portion to pass at Danger the signal controlling the entrance of trains to the section ahead for the purpose of avoiding or reducing the force of a collision, provided either of the following conditions obtains :-

(i) There is a rising gradient in the section ahead sufficiently long or steep to bring the second portion to a stand, and the Signalman considers that the time which has elapsed since the

previous train passed his box makes it safe to do so.

(ii) The weather is clear, there is no tunnel in the section, no passenger train has been accepted in the opposite direction on any adjoining line, and the Signalman considers that the time which has elapsed since the previous train passed his box makes it safe to do so.

(e) If the divided train is running on a rising gradient, or if permission has not been obtained for it to proceed to the box in advance and the conditions in clause (d) cannot be complied wish. the Signalman must exhibit the Danger signal to stop the first portion of the train, but a green hand signal must not be exhibited. The first portion when stopped must be shunted into a siding as expeditiously as circumstances will permit, or otherwise dealt with as may be necessary to prevent the second portion coming into collision with it.

The existing clauses (g), (h) and (i) to be re-lettered (h), (i) and (j) respectively. (G.A.24.-11/49.)

## EXTRACTS FROM BLOCK REGULATIONS—DOUBLE LINES—Continued.

(h) If it is necessary for a train to follow the first portion of a train which has become divided, such train must not be allowed to enter the section until the Signalman at each end is satisfied that the section is clear. The Signalman in rear must then advise the Signalman in advance of the train which is about to enter the section and give the "Train entering Section" signal. When this signal has been acknowledged he must inform the Driver of what had occurred, and instruct him to PASS AT DANGER THE SIGNAL CONTROLLING THE ENTRANCE INTO THE SECTION AHEAD IN ACCORDANCE WITH Rule 38, Clause (b) and to proceed cautiously. The same course must be adopted in the event of the second portion of the divided train following the first portion through the section ahead.

(i) Should a train that is appointed to convey a slip portion pass a signal box short of the proper slip tail signals or with wrong tail signals (unless it be the box immediately preceding the slipping point), the Signalman will assume that the slip portion has become detached, and immediately place or maintain the signals for the opposite line of in the same direction on any adjoining line at danger, and send to the signal box ahead the "Train Divided" signal. If he is advised by the Signalman in advance that the train has arrived incomplete, no train must be allowed to proceed in the opposite direction, except for the Examination of Line in accordance with Regulation 14A.

SHOULD THE SLIP PORTION BE AFTERWARDS SENT ON BY SPECIAL ENGINE IT MUST BE SIGNALLED

AS A PASSENGER TRAIN, AND CARRY THE SLIP TAIL SIGNALS TO DESTINATION.

Regulation 22. Train or Vehicles running away on Wrong Line.—(c) Should the runaway train or vehicles stop in the section and assistance be required from the box in rear, the provisions of Regulation 14 (Section obstructed by accident or disabled train), must be observed. If the train or vehicles are removed from the section other than under Regulation 14 (Section obstructed by accident or disabled train), the block indicator must be maintained at the "Train on Line" position, and when the next train requires to pass over the line affected the Signalman in rear must advise the Signalman in advance and give the "Train entering Section" signal. When this signal has been acknowledged the Signalman in rear must inform the Driver of what has occurred and instruct him to PASS AT DANGER THE SIGNAL CONTROLLING THE ENTRANCE INTO THE SECTION AHEAD IN ACCORDANCE WITH RULE 38, CLAUSE (b), and to proceed cautiously.

Regulation 23. Train or Vehicles running away on Right Line.—(c) Should the runaway train or vehicles stop in the section and assistance be required from the box in rear, the provisions of Regulation 14 (Section obstructed by accident or disabled train), must be observed. If the train or vehicles are removed from the section at the box in advance other than under Regulation 14 (Section obstructed by accident or disabled train), the block indicator must be maintained at the "Train on Line" position and when the next train requires to pass over the line affected the Signalman in rear must advise the Signalman in advance and give the "Train entering Section" signal. When this signal has been acknowledged the Signalman in rear must inform the Driver of what has occurred and instruct him to pass at danger the signal controlling the entrance into the section ahead in accordance WITH RULE 38, CLAUSE (b), and to proceed cautiously.

Regulation 24. Opening and Closing of Signal Boxes where Switches are provided. General Switching Rules.—(c) Except where instructions are issued by the divisional superintendent or district TRAFFIC MANAGER TO THE CONTRARY, WHEN A TRAIN CALLS AT A STATION WHILE THE BOX IS SWITCHED OUT, THE SIGNALS MUST BE PLACED AT DANGER FOR ITS PROTECTION. WHERE THE SIGNALS ARE NOT REQUIRED TO BE PUT TO DANGER WHEN A TRAIN IS STOPPED AT A SIGNAL BOX FOR AN ORDINARY PERIOD, THEY MUST NEVERTHELESS BE PLACED TO DANGER IF THE TRAIN REMAINS STATIONARY FOR AN UNUSUALLY LONG TIME FROM WHATEVER CAUSE IN ACCORDANCE WITH RULE 178 (c). WHENEVER IT IS REQUIRED TO ATTACH OR DETACH A VEHICLE DURING THE TIME THE BOX IS SWITCHED OUT, THE OPPOSITE LINE MUST NOT BE FOULED BY SHUNTING OPERATIONS; AND ANY VEHICLE DETACHED MUST FIRST BE SHUNTED INTO A SIDING CONNECTED WITH THE LINE ON WHICH THE TRAIN IS RUNNING. ON NO ACCOUNT MUST THE VEHICLE BE DETACHED ON THE RUNNING LINE.

(d) In the event of a box being kept open specially after the usual switching-out time, DURING FOG OR FALLING SNOW, UNLESS THE FOG-SIGNALMEN WORKING IN CONNECTION WITH THE BOX HAVE BEEN DEFINITELY INSTRUCTED TO REMAIN ON DUTY AFTER THE USUAL SWITCHING-OUT TIME, THE SIGNALMAN MUST IMMEDIATELY WORK UNDER THE SAME CONDITIONS AS THOUGH THE FOG-SIGNALMEN HAD NOT ARRIVED, AND MUST ALSO INSTRUCT THE SIGNALMAN ON EACH SIDE TO ADVISE DRIVERS OF ALL APPROACHING TRAINS THAT THE BOX IS STILL OPEN. THE SIGNALMEN ON EACH SIDE MUST GIVE THIS INTIMATION TO DRIVERS UNTIL THE BOX IS SWITCHED OUT, OR THEY ARE INFORMED THAT THE FOG-SIGNALMEN ARE REMAINING AT THEIR POSTS AND IT IS UNNECESSARY FOR THE DRIVERS TO BE

25. Failure of Instruments, Bells or Gongs. - In the event of any failure of the block instruments, bells or gongs, so that the necessary signals cannot be forwarded and received in the ordinary way, the following instructions must be observed :-

(a) (i.) A train must not in any circumstances be allowed to pass a signal box into that section of the line where the failure exists without having been previously brought to a stand and the Driver and Rear Guard, also the Driver of an engine assisting in rear, if any, advised of the failure.

The Driver or Drivers must in addition be instructed to proceed cautiously through the section.

Where intermediate block signals controlled from the signal box in rear are provided, should any failure of the block instruments, bells or gongs occur after a train has been allowed to proceed

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hat section id and the the failure. section. led, should to proceed The following to be inserted between the first and second paragraphs of clause (a) (i):-

When a Driver has been stopped at a Signal Box and advised by the Signalman of the failure of instruments or bells, or of a track circuit controlling the block instruments, the Driver must draw his train forward and bring it again to a stand with the brake van near to the Signal Box to enable the stallars. After the train has thus been brought to a stand for the Signalman to communicate the Guard and Driver of an engine assisting in rear, the Driver must not start again until he receives a green hand signal from the Signalman.

(G.A. 5.—2/39. O.M.11869.)

## EXTRACTS FROM BLOCK REGULATIONS—DOUBLE LINES—Continued.

# REGULATION 25.—FAILURE OF INSTRUMENTS, BELLS OR GONGS.—page 28.

The following to be substituted for the existing regulation:

- 25. Failure of Instruments and or Bells. In the event of any failure of the block instruments and or Bells so that the necessary signals cannot be forwarded and received in the ordinary way, the following instructions must be observed:
- (a) (i) A train must not in any circumstances be allowed to pass a signal box into that section of the line where the failure exists without having previously been brought to a stand and the Driver and Rear Guard, also the Driver of an engine assisting in the rear, if any, advised of the failure. The Driver, or Drivers, must in addition be instructed TO PASS AT DANGER THE SIGNAL CONTROLLING THE ENTRANCE INTO THE SECTION AHEAD IN ACCORDANCE WITH RULE 38, CLAUSE (b) and to proceed continued although the continued at the control of the section of cautiously through the section.

When a Driver has been stopped at a Signal Box and advised by the Signalman of the failure of instruments and/or bells or of a track circuit controlling the block instruments, the Driver must draw his train forward and bring it again to a stand with the brake van near to the Signal Box to enable the Signalman to verbally inform the Guard in rear and Driver of an engine assisting in the rear, if any, of the failure. After the train has thus been brought to a stand for the Signalman to communicate with the Guard and Driver of an engine assisting in rear, the Driver must not start again until he receives a green hand signal from the Signalman.

Where intermediate block signals controlled from the Signal Box in rear are provided, should any failure of the block instruments and/or bells occur after a train has been allowed to proceed to the intermediate block home signal, the train must be brought to a stand at that signal, and, after the necessary information has been given by the Signalman to the Driver by means of the telephone provided, the Driver must send his Fireman to advise the Rear Guard, also the Driver of an engine assisting in rear, if any, of the circum-

(ii) The Signalman at whose Box the block instruments and/or bells have failed must advise the Signalman at the box in advance of the failure by speaking instrument. When speaking instruments are not available TRAINS MUST BE DEALT WITH IN ACCORDANCE WITH CLAUSE (a) (iv) OF THIS REGULATION.

#### Time Interval Working.

(iv) When the speaking instruments are not available TIME INTERVAL WORK-ING MUST BE PUT INTO OPERATION. THE DRIVER OF THE FIRST TRAIN THAT IS BEING CAUTIONED MUST BE HANDED A COPY OF FORM 4981, PROPERLY FILLED UP AND SIGNED AND BE INSTRUCTED TO STOP AT THE SIGNAL BOX TO WHICH IT IS ADDRESSED AND HAND THE FORM THE SIGNALMAN THERE.

A train must not be allowed to follow another train until the time usually taken by the preceding train to clear the section, after allowing for the train having been stopped, has elapsed, but in no case with a less interval than three minutes. When a tunnel intervenes in a block section, an interval of not less than ten minutes must be allowed between two trains, unless the Signalman can satisfy himself that the tunnel is clear.

(v.) In the event of a partial failure where some means of communication between the boxes is available for signalling purposes, applying to one line only, block working must be maintained on the next adjoining line or lines, the instruments for which are in working order, but the Driver of the first train travelling on each of such adjoining line or lines must be advised of the circumstances and instructed to proceed cautiously through the section.

In cases where trains on one line have to be worked on the time interval system in consequence of the

through the section

In cases where trains on one line have to be worked on the time interval system in consequence of the failure of all communication, all trains on the next adjoining line or lines must be stopped and the Driver of each train instructed to proceed cautiously. Trains on the next adjoining line need not, however, be stopped and cautioned when the failure occurs on a line not worked in accordance with the Absolute Block Regulations

(b) Steps must be taken immediately to have the apparatus put into working order and when the failure has been remedied and the apparatus is again in working order, OR SHOULD SPEAKING COMMUNI.

CATION ONLY BE RESTORED, the Driver of the next train allowed to proceed through the section over the line, or lines, on which the failure existed must be cautioned and supplied with a notice on FORM 4810 intimating that the train carrying this notice will be the last train to work on THE TIME INTERVAL SYSTEM through the Section and he must also be instructed to stop at the next signal box and hand this wit notice to the Signalman. The Signalman receiving this notice must then give the "Train out of Section" signal in accordance with Regulation 10 and the ordinary method of signalling or signalling by speaking the structure of the signal in accordance with Regulation 10 and the ordinary method of signalling or signalling by speaking the structure of the signal in accordance with Regulation 10 and the ordinary method of signalling or signalling by speaking the structure of the signal in accordance with Regulation 10 and the ordinary method of signalling or signal in accordance with Regulation 10 and the ordinary method of signal in accordance with Regulation 10 and the ordinary method of signal in accordance with Regulation 10 and the ordinary method of signal in accordance with Regulation 10 and the ordinary method of signal in accordance with Regulation 10 and the ordinary method of signal in accordance with Regulation 10 and the ordinary method of signal in accordance with Regulation 10 and the ordinary method of signal in accordance with Regulation 10 and the ordinary method of signal in accordance with Regulation 10 and the ordinary method of signal in accordance with Regulation 10 and the ordinary method of signal in accordance with Regulation 10 and the ordinary method of signal in accordance with Regulation 10 and the ordinary method of signal in accordance with Regulation 10 and the ordinary method of signal in a

When the train is worked by two engines in front or is assisted by an engine or engines in rear, the notice must be shown to all Drivers on the train and carried by the rearmost Driver.

(d) When the block instruments and/or bells HAVE failed and there is a level crossing in the Section provided with block indicators and/or bells, but WHICH is not a block post, the Signalman in rear if telephonic communication with the crossing is available, must inform the person in charge of the crossing that the block indicators and/or bells, as the case may be, are not in working order.

If there is no telephonic communication with the crossing, or the telephone has failed, the Signalman must instruct the Driver of every train proceeding in the direction of the crossing during the failure to approach the level crossing cautiously, sound the engine whistle and be prepared to stop short of any obstruction at such crossing. The time interval must also be extended sufficiently to allow for the additional time likely to be occupied in carrying out this instruction.

WHEN IT IS KNOWN THAT THE ELECTRICAL COMMUNICATION AT A LEVEL CROSS-ING IS NOT IN WORKING ORDER THE DISTANT SIGNALS (IF SUCH ARE PROVIDED) PROTECTING THE CROSSING MUST BE KEPT AT CAUTION UNTIL INFORMATION HAS BEEN RECEIVED THAT THE COMMUNICATION IS AGAIN IN WORKING ORDER.

IF DISTANT SIGNALS ARE NOT PROVIDED, GREAT CARE MUST BE TAKEN TO ASCERTAIN, AS FAR AS POSSIBLE, THAT NO TRAIN IS APPROACHING THE CROSSING IN EITHER DIRECTION BEFORE THE GATES ARE OPENED TO THE PUBLIC.

(c) When trains are being worked on the time interval system all trains must be brought within the protection of the Home Signal as promptly as possible, and, to obviate a train standing with its rear portion outside the Home Signal, the Signalman must, if necessary, authorise the Driver to draw forward a sufficient distance to bring the rear portion within the home signal AS PROVIDED FOR IN RULE 38 (b) (iii).

If a train requires to stand outside a Home Signal for the purpose of attaching or detaching traffic or through any other cause, the Signalman must if practicable, obtain the assistance of a Handsignalman provided with the necessary detonators and hand signals, who must be sent out a sufficient distance from the rear of the train to afford protection. Until this Handsignalman has been provided, a train must not be stopped outside the Home Signal to attach or detach traffic.

(G.A.18. 11/47. O.M. 12044 and 12248.)

GAIR

### EXTRACTS FROM REGULATIONS FOR TRAIN SIGNALLING ON SINGLE LINES BY THE ELECTRIC TOKEN BLOCK SYSTEM.

NOTES .- ANY ADDITIONS TO OR ALTERATIONS IN THESE REGULATIONS DIFFERING FROM THE AGREED RAILWAY CLEARING HOUSE STANDARD ARE SHEWN IN THIS SPECIAL TYPE.

Wording which is not an actual extract from the above Regulations is printed in italics.

When the term "Token Station" is used in these Regulations it applies to a signal box or token station which is open.

Where the term "Signalman" is used it applies to the person in charge of the Token working

acting as Signalman.

Electric Token Block System.—(a) The object of the Electric Token Block System of signalling is to prevent more than one train being in a block section between two Token Stations at the same time, and, when no train is in the section between two Token Stations, to admit of a train entering the section from either end. This is accomplished by every train carrying a token, one token only being obtainable from the Token Instruments of the same section at the same time.

(b) The signalling of trains on the Electric Token Block System does not in any way dispense with the use of fixed or hand signals, or detonators, whenever and wherever such signals or detonators

may be requisite to protect obstructions on the line.

Drivers not to leave Token Station without Token and Proper Signals being exhibited.—(a) Except as provided in Regulation 14c, "Working of trains to and from point of chstruction," and 25, "Failure of Token apparatus," a Driver will render himself liable to dismissal if he leaves a Token Station without the Token for that section of the line over which he is about to run, unless the Token has been shewn to him as provided in the following clause.

(b) When a train has more than one engine in front, or is assisted by an engine or engines in rear, or when two or more engines are coupled together, the Token must be shewn to each Driver, and

delivered to, and carried by, the Driver of the rearmost engine.

(c) After receiving the Token, the Driver must not proceed until all the necessary fixed or other signals have been exhibited. He must keep the Token under his own charge, except as laid down in Regulations 14, "Section obstructed by accident or disabled train"; 14B, "Train or portion of train left on Single Line"; 14c, "Working of trains to and from point of obstruction"; and 34, "Sidings controlled by means of Token"; until he reaches the end of the section, when he must give it up to the Signalman or other duly authorised person, except as provided in Regulation 14B, "Train or portion of train left on Single Line.

(d) The Driver must be careful not to take the Token beyond the Token Station at which it

should be left.

(e) The person in charge of the Token working will render himself liable to severe punishment

should be contribute to any irregularity in the Token working.

(f) Each Token has engraved or marked on it the name of the Token Station at each end of the section to which it applies, and the Tokens of adjoining sections differ essentially in pattern or

Custody and Transference of Token.—(b) Except where some other person is specially appointed to the duty, the Signalman is the sole person authorised to receive a Token from, and deliver it to, the Driver or Fireman or to place the Token in or take it from exchanging apparatus where provided. When necessary, in the case of non-stopping trains, two competent men may be employed, one to receive and the other to deliver the Token. The Driver, while the Token is in his charge, must see that it is placed in a safe position on the engine.

In no circumstances, except as provided in Regulations 14, "Section obstructed by accident or disabled train"; 14B, "Train or portion of train left on Single Line"; 14c, "Working of trains to and from point of obstruction"; 23, "Token damaged or lost"; and 25, "Failure of Token apparatus"; must a Token be transferred from one train to another without being passed through the Token Instrument.

Working of Fixed Signals.—In the circumstances referred to in Regulations 14, "Section obstructed by accident or disabled train"; 14A, "Examination of Line"; 14B, "Train or portion of train left on Single Line"; 14c, "Working of trains to and from point of obstruction"; and 20, "Train Divided"; the signal controlling the entrance of trains into the section ahead must not be lowered, but such signal may be passed at "Danger" upon the Driver being instructed verbally to do so by the Signalman.

(c) When a shunting movement is being made on a loop line at a crossing place, the home signal applicable to a train approaching on the single line in the opposite direction must not be lowered, unless there are points which are set to prevent the shunting movement fouling the single line, until the shunting movement has come to a stand and the Driver has been instructed that no further

movement towards the starting signal must be made.

Similarly, when a home signal at a crossing place has been lowered for an approaching train a shunting movement must not be made towards the starting signal applicable to the opposite loop until the train approaching on the single line has passed clear on to the loop line, unless there are points which are set to prevent the shunting movement fouling the single line.

Regulation 5. Section clear but Station or Junction blocked. (Warning Arrangement.)-The method of conveying to Drivers that they are required to proceed into the section in accordance with the provisions of this Regulation is dealt with in Rule 41.

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DURING FOG OR FALLING SNOW A TRAIN MUST NOT BE DRAWN TOWARDS THE SCHOOL ON FALLING SNOW A TRAIN MUST NOT BE DRAWN TOWARDS THE SIGNAL CONTROLLING THE ENTRANCE TO THE SECTION AHEAD TO AWAIT ACCEPTANCE FROM THE SIGNAL BOX IN ADVANCE, EXCEPT WHEN A FOGSIGNALMAN IS ON DUTY AT THE SIGNAL MAN IS ABLE TO SATISFY HIMSELF THAT THE SIGNAL IS TRACK-CIRCUITED OR A AT DANGER, AND THE LINE TO THE REAR OF SUCH SIGNAL IS TRACK-CIRCUITED OR A TANDRING AT THE SIGNAL WILL BE MUTUAL LISTEN WITH THE SIGNAL WILL BE MUTUAL WITH THE SIGNAL WI TRAIN STANDING AT THE SIGNAL WILL BE WITHIN HIS VIEW. (G.A.19—10/48. L.K.1/8617/31.)

(f) AT TERMINAL STATIONS WHERE SPECIAL AUTHORITY IS GIVEN FOR TRAINS TO BE ACCEPTED IN ACCORDANCE WITH THE "WARNING ARRANGEMENT," WHILE VEHICLES ARE STANDING AGAINST THE SUFFER STOPS, THE HOME SIGNAL MUST BE KEPT AT DANGER IN SUCH CIRCUMSTANCES UNTIL THE IN-COMING TRAIN HAS BEEN STOPPED AT IT, AFTER WHICH IT (OR THE "CALLING-ON ARM WHERE PROVIDED) MAY BE LOWERED.

Regulation 6. Engine assisting in rear of Train.—An Engine must never assist a Passenger TRAIN IN THE REAR EXCEPT WHERE SPECIALLY AUTHORISED BY THE SUPERINTENDENT OF THE LINE AND THE CHIEF MECHANICAL ENGINEER. Officer's Spl. Train

Regulation 8. Ballast Train, Freight Train, Mineral Train or Inspection Train requiring to stop in Section. -(c) The Driver of a ballast train that has to do work in the section must be told, when receiving the Token, to which end of the section it is to be taken and at what time it is to be there, in order to clear the line for the next train:

(d) Should the Guard of the ballast train require his train to return to the Token Station in rear instead of going through to the Token Station in advance, he must obtain the verbal permission of the Signalman before the train enters the section. When the train has arrived back complete and the single line is again clear, the Signalman must restore the Token to the instrument and give the "cancelling" signal to the Token Station in advance.

(e) When a ballast train working in the section has to return to the Token Station in rear, no shunting outside the home signal at that end must be allowed until a man with hand signals and detonators has been sent out to protect such shunting.

Trains other than Ballast Trains requiring to return to Token Station in the Rear, without going through the whole Block Section.—In those cases where special authority has BEEN GIVEN BY THE SUPERINTENDENT OF THE LINE FOR CERTAIN TRAINS TO RETURN TO THE TOKEN STATION IN REAR WITHOUT PROCEEDING THROUGH THE WHOLE BLOCK SECTION, THE SIGNALMAN, WHEN DEALING WITH SUCH A TRAIN, MUST FIRST SEND TO THE TOKEN STATION IN ADVANCE A MESSAGE ON THE SPEAKING INSTRUMENT EXPLAINING THE CIRCUMSTANCES. HE MUST THEN SEND THE PROPER "IS LINE CLEAR?" SIGNAL AND THE SIGNALMAN AT THE TOKEN STATION IN ADVANCE MUST, IF THE SECTION BE CLEAR AS LAID DOWN IN REGULATION 5, "Section clear but station or junction blocked," GIVE PER-MISSION FOR THE TRAIN TO APPROACH UNDER THE "WARNING ARRANGEMENT." A TOKEN MUST THEN BE WITHDRAWN, AND GIVEN TO THE DRIVER, WHO MUST BE VERBALLY WARNED THAT ON THE RETURN JOURNEY HE IS TO REGARD THE LINE AS CLEAR TO THE HOME SIGNAL ONLY. WHEN THE TRAIN LEAVES, THE "TRAIN ENTERING SECTION" SIGNAL MUST BE SENT IN THE USUAL WAY, AND UNTIL THE TRAIN HAS RETURNED NO SHUNTING MUST BE ALLOWED OUTSIDE THE HOME SIGNAL,

Regulation 9. Working of Trolleys.—For the purpose of this instruction the term "Trolley" applies to ordinary trolley, motor or mechanical trolley and inspection car.

(i) Trollies going into or through Tunnels.

When it is necessary for a trolley to go into or through any of the tunnels specially enumerated in the appendices to the Service Time Tables as coming within the application of this Regulation, the Ganger or man in charge of the trolley must be in possession of the token or of the token or occupation key where the occupation key system is in OPERATION.

Should the trolley be removed from the rails before reaching the next token station, the Ganger or man in charge must take the token to the Signalman in advance and inform him that the trolley is clear of the line; the Signalman must then place the token in the instrument and send the "Train out of Section" signal. If, however, time would be saved, the Ganger or man in charge must return with the token to the token station in the rear and inform the Signalman that the trolley is clear of the line; the Signalman must then restore the token to the instrument and send the "cancelling" signal.

WHERE AN OCCUPATION KEY IS HELD THE TROLLEY WILL NOT BE SIGNALLED ON THE TOKEN INSTRUMENTS, BUT THE SIGNALS MUST BE WORKED. THE GANGER OR MAN IN CHARGE MUST, AFTER THE TROLLEY HAS BEEN REMOVED FROM THE RAILS, OR HAS PASSED OUT OF THE SECTION, RESTORE THE OCCUPATION KEY TO THE NEAREST OCCUPATION KEY INSTRUMENT, INFORMING THE SIGNALMAN THAT THE SECTION IS CLEAR.

ORDINARY TROLLIES PASSING THROUGH ALL OTHER TUNNELS MUST BE DEALT WITH AS FOR ORDINARY SECTIONS AND PROTECTED BY HANDSIGNALMEN IN ACCORDANCE WITH Rule 215, unless the token or occupation key is held. In the case of motor trollies THE GANGER OR MAN IN CHARGE MUST BE IN POSSESSION OF THE TOKEN OR OCCUPATION KEY

(ii) Trollies going through Section. WHEN IT IS NECESSARY FOR A TROLLEY TO PROCEED THROUGH A SECTION FROM ONE TOKEN STATION TO THE NEXT, THE GANGER OR MAN IN CHARGE MUST BE IN POSSESSION OF A TOKEN OR OCCUPATION KEY, AND THE INSTRUCTIONS CONTAINED IN CLAUSE (i) WILL APPLY.

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#### EXTRACTS FROM ELECTRIC TRAIN TOKEN REGULATIONS Continued.

(iii) Motor Trollies and Inspection Cars.

IN ADDITION TO THE INSTRUCTIONS CONTAINED IN CLAUSES (i) and (ii) SPECIAL INSTRUCTIONS ARE SHEWN ON PAGE 65 IN REGARD TO THE WORKING OF THE MOTOR TROLLIES AND INSPECTION CARS. MECHANICAL TROLLIES AND VELOCIPEDE CARS MUST BE WORKED IN ACCORDANCE WITH LOCAL INSTRUCTIONS.

(iv) Use of Motor or Mechanical Trolley or Inspection Car for conveying Pilotman through Section.

If, Owing to a failure, neither a token nor occupation key can be withdrawn, a motor or mechanical trolley or inspection car may be used for the purpose of conveying the Pilotman (delivering the pilot forms) through the section.

Regulation 10. Train out of Section.—(b) When the last vehicle of a train does not pass the signal box before it has been shunted into a siding, or when a train has been brought to a stand within a home signal, and it is necessary to give the "Train out of Section" signal before the train passes the signal box, the Signalman must, before giving such signal, ascertain from the Guard or Shunter in charge of the train that the whole of the train, with tail lamp attached, has arrived, and the Guard or Shunter will be held responsible for giving this information to the Signalman, the Fireman being similarly responsible in the case of a light engine.

Regulation 12A. Animals on Line.—(a) Should a signalman become aware that cattle, horses, or other animals are on the line and likely to cause an obstruction he must at once inform the Signalman at the opposite end of the section affected of the circumstances on the speaking instrument, and take the necessary steps to have the line cleared. Until it has been ascertained that the line is clear, all trains allowed to proceed must be brought to a stand and the Driver verbally informed of the circumstances and instructed to proceed cautiously.

(b) Should there be a tunnel in the obstructed section, the Signalman at either end of the section must verbally explain the circumstances to the Driver and Guard of each train entering the section and instruct them that although the train may proceed into the section it must not enter the tunnel

until they have ascertained the tunnel is clear.

Regulation 13. Release Token for Shunting or for Engineer's Occupation of Single Line.—(c) When a token has been withdrawn for the running of a trolley or for the protection of the single line during relaying or other permanent-way operations, as provided in Rules 215 (n) and 217 (a) respectively, and the single line is again clear and safe for the passage of trains the Token must be replaced in the instrument. If, however, time would be saved the Ganger or man in charge may take the Token to the opposite end of the section and the Signalman there, upon being informed that the line is clear and safe for the passage of trains, must place the Token in the instrument and advise the Signalman at the opposite end of the section affected accordingly.

Regulation 14. Section Obstructed by Accident or by Disabled Train.—(a) In the event of a train

(a) In the event of a train becoming disabled between two Token Stations, necessitating an ASSISTING TRAIN, ENGINE, or breakdown van train entering the section to render assistance, the Guard must make arrangements for the Fireman to obtain assistance and the latter, after obtaining an assurance from the Driver that he will not move his engine until assistance arrives, must take the Token to the nearer Token Station, and as he proceeds must place detonators on the rail as laid down in Rule 179; the Guard must proceed in the opposite direction, protecting his train as directed in that Rule. When the services of the Guard are not available the Fireman must protect the train in accordance with Rule 179 when proceeding to the nearer Token Station for assistance, but before doing so must protect the train in the opposite direction by placing on the rail three detonators, ten yards apart, at least 300 yards from the train. When the services of the Fireman are not available the Guard must perform the duties laid down for the Fireman.

On reaching the Token Station the Fireman must inform the Signalman of the circumstances and show him the Token. If an assisting train OR ENGINE is available at that end of the section, the Fireman must personally hand the Token to the Driver and conduct the assisting train OR ENGINE to the disabled train, and the Driver of the assisting train OR ENGINE must not allow the Token to pass out of his possession until the whole of the disabled train is removed clear of the section, except when the line is obstructed and special arrangements are made for the working in accordance with Regulation 14C, or in the circumstances described in Regulation 14B, Clause (d).

(b) If, however, on arrival at the nearer Token Station it is found necessary for an assisting train or engine to be obtained from the other end of the section, the Fireman, or Guard where the services of the Fireman are not available, must hand the Token to the Signalman who, after having come to a clear understanding with the Signalman at the other end of the section as to what is about to be done, must place the Token in the instrument to enable a Token to be withdrawn at the other end. The Signalman at that end when handing the Token to the Driver in such circumstances must make him clearly understand what he is required to do and instruct him to keep a look-out for the Guard or Fireman who will ride on the ASSISTING ENGINE OR on the engine of the assisting train and pilot it to the disabled train.

Should the Guard or Fireman not have been picked up before the assisting train or ENGINE passes over the detonators it must be brought to a stand and, except during fog or falling snow, it may proceed to the disabled train, but during fog or falling snow the Fireman of the assisting train or ENGINE must walk in front to pilot it to the disabled train.

(c) In the event of it being necessary to clear a section under Regulations 14B. "Train or portion of train left on Single Line"; 20, "Train Divided"; and 22, "Vehicles running away," the Signalman must, if the engine which is to remove the obstruction starts from that end of the section where the (e) When the line is clear to the home signal but is occupied by a train or otherwise obstructed within the home signal, and assistance is required from the rear, the Signalman in rear must be informed of the circumstances. In the case of a train requiring assistance, provided it has arrived with tail lamp attached, the "Train out of Section" signal must be given if this has not already been done, or where the "Obstruction Danger" signal has been sent, the "Obstruction Removed" signal must be given. The assisting train or engine or breakdown van train may then be accepted, and if the disabled train or the obstruction is within the authorized clearing point the assisting train or engine or breakdown van train must be accepted under the Warning arrangement. The DRIVER OF THE ASSISTING TRAIN OR ENGINE MUST BE VERBALLY TOLD THE POSITION OF MATTERS AT THE SIGNAL BOX AHEAD.

(f) Should it be necessary for the assisting train or engine to continue in rear of the disabled train through any Token section or sections in advance of the section obstructed, the Signalman in rear, when forwarding the "Is Line Clear?" signal for the disabled train, or if this signal has already been sent, must inform the Signalman in advance that the approaching train is being assisted in rear BY A TRAIN OR ENGINE.

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### EXTRACTS FROM ELECTRIC TRAIN TOKEN REGULATIONS-Continued.

Token is out of the instrument, hand such Token to the Driver. If, however, the assisting engine is to start from the other end of the section, then the Token must, after all arrangements are made, be placed in the instrument, so that one may be withdrawn at the other end of the section to enable the assisting engine to proceed to the disabled train or vehicles.

In either case, the Signalman concerned must communicate with each other and arrive at a clear understanding how the obstruction is to be removed, and when handing the Token to the Driver the Signalman must instruct him to proceed cautiously to the disabled train or vehicles and remove

them to the more convenient end of the section.

- (d) The assisting train OR ENGINE or the breakdown van train as the case may be, may, after having been brought to a stand and the Driver AND GUARD informed of the circumstances, be allowed to enter the section under the following arrangments, provided the Driver is in possession of the Token :-
  - (i) The Signalman must instruct the Driver to pass at Danger the signal controlling the entrance into the section ahead as provided for in Rule 38, clause (b).

The assisting train or engine, or breakdown van train must run at reduced speed and great caution must be observed by all concerned. After removing the disabled train to the more convenient end of the section, the Driver must deliver the Token to the Signalman.

(ii) If there is a tunnel in the obstructed section the Driver of the assisting train or ENGINE or breakdown van train, unless he is accompanied by the Fireman of the disabled train, must not enter such tunnel until he has met the Guard of the disabled train, or it has been ascertained that the tunnel is clear. The Driver of the assisting train OR ENGINE or breakdown van train must be instructed accordingly by the Signalman.

After the line is again clear and permission has been obtained for another train to proceed, such train must be stopped and the Driver instructed to proceed cautiously.

onen so accepted, an obstruction is within the authorised clearing point the assisting train or breakdown van train must be accepted under the Warning Arrangement. THE DRIVER OF THE ASSISTING TRAIN MUST BE VERBALLY TOLD THE POSITION OF MATTERS AT THE SIGNAL BOX AHRAD.

(f) Should it be necessary for the assisting train to continue in rear of the disabled train through any Token section or sections in advance of the section obstructed, the Signalman in rear, when forwarding the "Is Line Clear" signal for the disabled train, or if this signal has already been sent, must inform the Signalman in advance that the approaching train is being assisted in rear BY A TRAIN OR ENGINE.

(g) In the event of both loops at a crossing place being obstructed by accident, or other exceptional cause, inside the respective home signals but within the authorised clearing point, the Signalman at the Token Station on each side must be advised of the circumstances, and if it is necessary for trains to be worked to and from the point of obstruction, such trains approaching the obstruction must be accepted under the "Warning" arrangement. The Driver of each train allowed to go forward must be verbally informed by the Signalman of the circumstances.

Regulation 14A. Examination of Line.—(a) When it is necessary in accordance with Regulation 17 ("Stop and Examine") to ascertain if the line is clear, and speaking communication exists between the two Token Stations, which will admit of a proper understanding being arrived at between the two Signalmen, an engine may be allowed to enter the section provided the "Train out of Section" signal has been sent or received as the case may be, for the previous train. The circumstances must be explained to the Driver and he must be instructed to pass at Danger the signal controlling the entrance into the section ahead as provided for in Rule 38, clause (b), and proceed cauticusly through the section, prepared to stop short of any obstruction. Where practicable, the engine must be accompanied by a Station Master or other competent person. After sunset, during fog or falling snow,

or where a tunnel intervenes, the engine must always be so accompanied.

(b) The Token must not be placed in the instrument at either end of the section until the person in charge or Driver, as the case may be, has reported that the line on which the engine has travelled is safe for the passage of trains. If expedient the engine may return to the token station in the rear.

Regulation 14B. Train or Portion of Train left on Single Line.—(a) When a train has to be left or divided and the rear portion left, on the single line, owing to accident or inability of the engine to take the whole forward, or from any other cause, the following instructions must be observed:-

(i.) The man who divides the train must inform the Driver how many vehicles, if any, are being taken forward, and after sunset or during fog or falling snow, or if the division is made in a tunnel, must place on the rail three detonators, 10 yards apart, not less than 100 yards ahead of the portion left behind.

(ii.) The Driver must, before returning to the rear portion of the train, satisfy himself that

the front portion is complete.

(iii). The Guard must protect his train in rear in accordance with Rule 179.

(b) When a train or portion of a train has been accidentally left on the single line, the following instructions must be observed :-

(i.) Except as otherwise provided for in the following paragraph, the Driver must not return for the train or rear portion thereof until it has been ascertained that the whole of the vehicles left

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### EXTRACTS FROM ELECTRIC TRAIN TOKEN REGULATIONS-Continued;

in the section have come to a stand, and, if there is no Token Station or other point where speaking communication exists from which this information can be obtained, the Driver must send his

Fireman on foot for the purpose.

If the gradients of the line over which the train has run are such that it can be concluded that the train or rear portion is at a stand at the time when the engine requires to return, the Driver may return for his train or rear portion thereof, without having ascertained that the rear portion has been secured, but must proceed cautiously, and if approaching a Token Station he must bring his engine to a stand at the distant signal and the Fireman must walk in front from there to the home signal.

(ii.) The Driver must (except as described in paragraph (i.)) take the front portion forward

to the nearest place where it can be disposed of.

(iii.) The Guard must secure the train or rear portion thereof left behind and place on the rail three detonators, 10 yards apart, not less than 100 yards ahead of the portion left behind.

He must then protect his train in rear in accordance with Rule 179.

(iv.) If it is necessary to allow an engine to enter the section at the Token Station in rear of the divided train for the purpose of removing the obstruction to the more convenient end of the section, the working must be conducted in accordance with Regulation 14, "Section obstructed by accident or disabled train."

(c) The Driver when returning for the portion of the train that has been left behind must not

pass any Token Station without the permission of the Signalman.

(d) The Driver must retain possession of the Token until the whole of the train is removed from the section, unless it is necessary for another engine to remove the rear portion. If, however, it is necessary to dispose of the front portion at a station ahead of the next Token Station, the Driver must give up the Token and must not enter the occupied section for the second portion until he has again received the Token. The Token when given up by the Driver in these circumstances must on no account be placed in the instrument until the section is clear or it becomes necessary to do so in accordance with Regulation 14, "Section obstructed by accident or disabled train."

(e) A white light must be placed on the leading vehicle of the rear portion before that portion

is propelled to the Token Station in advance or drawn back to the Token Station in rear.

(f) Should a failure occur to an engine assisting a train in rear, or it become necessary to divide a train assisted in rear, and the train engine is required to take the front portion of the train to the Token Station in advance and afterwards return for the disabled engine or for the rear portion of the train, the Driver of the train engine must send his Fireman to the Driver of the assisting engine, and obtain from him THE TOKEN AND an order in writing authorising the Driver of the train engine to return from the Token Station in advance for the remainder of the train. The train engine must then proceed to the Token Station in advance, and after disposing of the front portion of the train, the Driver, after informing the Signalman what he is about to do, MUST SHEW HIM THE TOKEN, AND AFTER GIVING him the written order, must return and remove the disabled engine or rear portion of the train from the section.

If, however, it is necessary to allow an engine to enter the section at the Token Station in rear, the working must be conducted in accordance with Regulation 14, "Section obstructed by accident or

disabled train.'

Should the assisting engine fail or the train become divided and the front portion proceed, owing to the Driver of the train engine not being aware of the circumstances, the Fireman of the assisting engine must act as directed in Regulation 14, "Section obstructed by accident or disabled train," and the disabled engine must not be moved until the relieving engine has arrived.

(g) If the train is assisted by an engine in rear and the train engine becomes disabled so that it cannot be moved, the assisting engine must draw the train back to the Token Station but must bring it to a stand at the distant signal and the Fireman must walk in front of it from there to the home signal. The Fireman of the disabled engine must protect it by placing on the rail three detonators, 10 yards apart, at least 300 yards from the engine in both directions. If, however, it is necessary to take the train to the next Token Station the Driver of the assisting engine must give up the Token to the Signalman, Advising him of the circumstances, and must not enter the occupied section for the second portion until he has again received the Token. The Token when given up by the Driver in these circumstances must on no account be placed in the instrument until it becomes necessary to do so in accordance with Regulation 14, "Section obstructed by accident or disabled train," or the section is clear.

The provisions of Regulation 14, "Section obstructed by accident or disabled train," must be observed when the assisting engine returns to the disabled engine or it is necessary for another engine to enter

the section to render assistance to the disabled engine.

Regulation 14C. Working of Trains to and from Point of Obstruction.—(a) Should an accident or obstruction occur of such a nature as to block the line and traffic is likely to be stopped for any considerable time, special arrangements, as shewn below, must, if necessary, be made for working trains to and from the point of obstruction.

(i.) If a Token is out of the instrument it must be retained to work trains between the point of obstruction and the Token Station on one side; on the other side the traffic must be conducted

by Pilotman.

#### EXTRACTS FROM ELECTRIC TRAIN TOKEN REGULATIONS-Continued.

(ii.) If the obstruction is caused by a derailed or disabled train and the engine cannot proceed the Guard most put the Driver in charge at the point of obstruction, and give him a written order instructing him not to move his engine until authorised by the Pilotman. The Driver must then hand the Token to the Fireman, and the Guard must instruct the Fireman to which Token Station he must take it so that it can be used to work trains between that station and the point of obstruction; the Fireman must protect the train in accordance with Rule 179 as he proceeds with the Token to that station. The Guard must protect the train in accordance with Rule 179 on the other side of the obstruction, after which he must proceed to the Token Station on that side and arrange with the person in charge there for a Pilotman to be appointed.

(iii.) If the engine, or engine and front portion, of a disabled train can proceed to the Token Station in advance, this must be arranged, and on the way the Driver must stop to allow the Fireman to place three detonators, 10 yards apart, on the rail in rear of the engine, or engine and front portion at three-quarters of a mile from the obstruction; on arrival at the Token Station in advance the rerson in charge of the Token working must be advised of the circumstances, and the Token delivered to him to enable it to be used for the purpose of working up to the obstruction on that side. The Guard must protect the train or rear portion in accordance with Rule 179, on the other side, after which he must proceed to the Token Station on that side and arrange

with the person in charge there for a Pilotman to be appointed.

(iv.) During the whole time the line is obstructed the obstruction must be protected on each side in accordance with Rule 179, but, except as provided in paragraphs (ii.) and (iii.), the use of a detonator at quarter of a mile and another at half a mile from the obstruction may be dispensed with. The Guard and Fireman will be responsible for this being done until two men provided with hand signals and detonators are appointed specially to perform the duty.

(v.) After sunset, during fog or falling snow, or if the disabled train is in a tunnel, a red

light must be exhibited at both ends of such train.

(vi.) The person arranging working by Pilotman must fill up, sign and address the necessary forms (see page 41 for specimen form), which must be dealt with in accordance with Regulation 25, "Failure of Token apparatus," clause (c), except that where it is possible to provide a person in charge at the point of obstruction a form must be addressed to him and conveyed by the Pilotman when travelling with the first train thereto; and it will not be necessary to supply the Station Master and Signalman at the opposite end of the Token section with forms.

The Pilotman must wear the red armlet as shewn in Regulation 25, "Failure of Token apparatus," or until this is available a red flag round his left arm above the elbow and must

accompany each train to and from the point of obstruction.

(vii.) When the line is again clear no train must be allowed to pass the point where the obstruction existed without the Token. The Pilotman must accompany the first train through the section carrying the Token that has been used for working up to the point of obstruction. After the Token has been given up to the Signalman and all the forms issued for working by Pilotman have been collected by the Pilotman, ordinary working may be resumed.

In no case of obstruction away from a Token Station must the Token be restored to the

instrument at either end of the section until the section is clear.

(b) Should no Token be out of the instrument and the line become obstructed through the failure of a bridge or embankment, or other cause, arrangements must, when necessary be made for working by Pilotman as laid down in clause (a/v.) to be put into operation between the point of obstruction and the nearest Token Station on each side, no Token being withdrawn while the working by Pilotman remains in operation; a responsible man must be placed in charge at the point of obstruction.

The line on each side of the obstruction must be protected in accordance with Rule 217 by men specially appointed to perform the duty. After sunset, during fog or falling snow, or if the obstruction

is in a tunnel a red light must be exhibited on both sides of such obstruction.

When the line is again clear both Pilotmen must proceed together to one end of the section and after the forms for working by Pilotman have been withdrawn at that end, they must accompany the first train through the section which has been obstructed, and collect the remaining forms; on arrival at the opposite end of the section ordinary working may be resumed.

(c) All forms which have been issued for working by Pilotman must be cancelled by writing the word "Cancelled" across them, afterwards being sent to the Divisional Superintendent or District

Traffic Manager.

(d) If there is a level crossing in the section not situated at a Token Station, the Signalman, if telephonic communication with the crossing is available, must inform the Crossing Keeper of the circumstances.

The Signalman must instruct the Driver of each train proceeding in the direction of the crossing faring the time the Token instruments are out of use to approach the level crossing in each direction cautiously, sound the engine whistle and be prepared to stop short of any obstruction at such crossing. Where, however, a Pilotman is appointed the Signalman must request the Pilotman to convey this instruction to the Driver of each train concerned.

(e) Clauses (c), (d), (g) and (h) of Regulation 25, "Failure of Token apparatus," must also be

observed, except as otherwise provided for in this Regulation (14c).

## EXTRACTS FROM ELECTRIC TRAIN TOKEN REGULATIONS-Continued.

Regulation 16. Fouling Single Line for Shunting Purposes.—(b) Except as shown in paragraphs (i.), (ii.), (iii.) and (iv.), a train must not be allowed to foul the single line outside the home signal, unless the Driver is in possession of the Token for the section so fouled.

(i.) Where shunting tokens are provided interlocked with the Token instruments, a Driver, when in possession of the shunting Token, and when authorised by the Signalman to do so, may proceed with his engine on to the Single line at the end of the section to which the shunting Token applies, as far as it is necessary for shunting purposes at the Token Station, without being in possession of a Token for the section.

(ii.) Where shunting Tokens as described in paragraph (i.) are not provided, the Signalman may, if necessary, and provided permission has not been given for a train to approach from the opposite end of the section, allow the single line to be fouled outside the home signal, but before doing so he must send the "Blocking Back" signal, and until this signal has been acknowledged

the line outside the home signal must not be occupied.

The single line may, if necessary for shunting purposes, be occupied outside the home signal at both ends of the same section at the same time, provided no train is approaching in the section, and that the "Blocking Back" signal has been given to, and acknowledged by, the Signalman

at the opposite end of the section.

(iii.) If a train is travelling in the section away from the Token Station at which shunting operations have to be performed, and it is necessary to occupy the single line outside the home signal, this may be done without first giving the "Blocking Back" Signal to the Token Station at the opposite end of the section, but immediately the "Train out of Section" signal has been received for that train, the Signalman must, if the single line is still occupied outside the home signal, give the "Blocking Back" signal to the Token Station at the opposite end of the section,

which signal must be acknowledged.

(iv.) IF IT IS NECESSARY TO FOUL THE SINGLE LINE OUTSIDE THE HOME SIGNAL FOR SHUNTING PURPOSES, WHILE THE GANGER IS IN POSSESSION OF THE OCCUPATION KEY, THIS MAY BE DONE AFTER THE DRIVER, WHEN THE ENGINE IS LEADING, OR THE GUARD OR SHUNTER IN CHARGE WHEN VEHICLES ARE PROPELLED, HAS BEEN WARNED THAT A TROLLEY MAY BE APPROACHING, WITHOUT FIRST SENDING THE "BLOCKING BACK" SIGNAL TO THE TOKEN STATION AT THE CONSTRUCTION OF T OF THE SECTION, BUT IMMEDIATELY THE KEY IS RETURNED TO THE KEY BOX THE SIGNALMAN MUST, IF THE SINGLE LINE IS STILL FOULED OUTSIDE THE HOME SIGNAL, GIVE THE "BLOCKING BACK" SIGNAL TO THE TOKEN STATION AT THE OPPOSITE END OF THE SECTION, WHICH SIGNAL MUST BE ACKNOWLEDGED.

(f) Unless special permission is given by the Divisional Superintendent or District Traffic MANAGER, no train or vehicle must be placed outside a home signal where the line is on a falling gradient towards the Token Station in rear, and then only if there is a brake van at the Rear and a man

(g) A Driver must not, in any circumstances, foul the single line for shunting purposes unless he has received the authority of the Signalman to do so.

Regulation 17. Stop and Examine Train.—(a) Should the Signalman who receives the "Stop and Examine Train" signal be unable to ascertain from the Trainmen, after examination of the train, why the signal was sent, he must, if the next train is travelling in the opposite direction, inform the Driver of that train of the circumstances, and instruct him to proceed cautiously to the next Token Station. He must also communicate with the Signalman who sent the signal, in order that the latter may, if necessary, caution the DRIVER of the next following train.

(b) Should the "Stop and Examine Train" signal have been sent on account of a door being

open on a passenger train, the Signalman sending the signal must advise the Signalman in rear, and the first train entering the section concerned need not be detained until evidence has been obtained that the line is not obstructed, but must be stopped, the Driver informed of the circumstances, and

instructed to proceed cautiously to the next Token Station, keeping a good look out.

Regulation 20. Train Divided.—(b) If the divided train is assisted by an engine in rear, or is running on a falling gradient, or where the line is level, or between short sections, where the stoppage of the first portion would risk a collision with the second portion, the Signalman, provided permission has been obtained from the TOKEN Station in advance for the train to proceed, must not exhibit the Danger signal to stop the first portion, but must exhibit to the Driver a green hand signal waved slowly from side to side.

(c) If permission has not been obtained from the Token Station in advance for the train to proceed, or if the train is running on a rising gradient, and is not assisted by an engine in the rear and the SECTION IN THE REAR IS NOT A SHORT ONE, the Signalman must exhibit the Danger signal to stop the first portion of the train, but a green hand signal must not be exhibited. The first portion when stopped must be shunted into a siding as expeditiously as circumstances will permit, or otherwise dealt with

as may be necessary to prevent the second portion coming into collision with it. (d) In the circumstances described in clauses (b) and (c) the Signalman must, as soon as the first portion of the train has passed or otherwise been dealt with, place or maintain his Signals at Danger, and take proper measures for dealing with the second portion, placing detonators on the rail and exhibiting a red hand signal to attract the attention of the Trainmen.

(f) When the rear portion of a divided train requires to be removed from a section the provisions

of Regulation 14, Section obstructed by Accident or Disabled Train, must be observed.

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## Regulation 16.—Fouling Single Line for Shunting Purposes—page 36.

Delete clause (f) and substitute the following :-

(f) Unless special permission is given by the Operating Superintendent no train or vehicles, other than an engine or engine with one or two brake vans, must be placed outside a Home signal where the line is on a falling gradient steeper than I in 260 towards the Token Station in rear unless there is an engine at the lower end. WHERE SUCH PERMISSION IS GIVEN THERE MUST BE A BRAKE VAN AT THE LOWER END WITH A MAN IN IT. (G.A.27.0p.—1/51. L.K.1/10428/Gen.E.)

#### EXTRACTS FROM ELECTRIC TRAIN TOKEN REGULATIONS-Continued.

(9) If it is necessary for a train to proceed into a section through which the front portion only of divided train has travelled, such train must not be allowed to enter the section until the Signalman at each end is satisfied that the section is clear. The Signalman in rear must then advise the Signalman in advance of the train which is ready to enter the section and after permission to withdraw a Token has been received give the "Train entering Section" signal. When this signal has been acknowledged he must inform the Driver of what has occurred, instruct him to PASS AT DANGEE THE SIGNAL CONTROLLING THE ENTRANCE TO THE SECTION AHEAD IN ACCORDANCE WITH RULE 38 (b), and to proceed cautiously. The same course must be adopted in the event of the second portion of the divided train following the first portion through the section ahead.

Regulation 21. Shunt Train for following Train to pass.—(b) No train must shunt for another train to pass except at a Token Station or at a siding where a special Token instrument is provided and special instructions are issued authorising trains to be shunted into such siding.

Regulation 22. Train or Vehicles running away.—(c) Should the runaway train or vehicles stop in the section and assistance be required, the provisions of Regulation 14, "Section obstructed by Accident or Disabled train," must be observed. If the train or vehicles are removed from the section other than under Regulation 14, "Section obstructed by Accident or Disabled train," the Signalmen must come to a clear understanding as to what has been done, and when the next train requires to pass over the line affected it must be signalled in the usual way, but the train must be stopped, and the Signalman when handing the Token to the Driver must inform him of what has occurred and instruct him to proceed cautiously.

Regulation 23. Token Damaged or Lost.—(b) Should a Token be damaged after it has been withdrawn and before it is taken forward into the section in advance, the train for which it has been withdrawn must not be detained unless it is necessary to do so to avoid delay in establishing working by Pilotman but must be sent away with the damaged Token and the Lineman at once sent for. When a Token is damaged and cannot be passed through the instrument, it may be used by the Pilotman to take a train to the other end of the section for the purpose of establishing working by Pilotman. He must keep the Token in his possession in accordance with Regulation 25, "Failure of Token apparatus," clause (e).

(c) Should a Token be lost, working by Pilotman must be conducted in accordance with Regulation 25, "Failure of Token apparatus," until every possible inquiry and search have been made for the missing Token, and, when it has been established beyond doubt that it cannot be found, the Lineman must be sent for and the instrument adjusted by him so that the ordinary working may be resumed. In the event of the Token being afterwards found, it must be kept by the Station Master until the Lineman can arrange to return it to the instrument, but if the Token is found before the Lineman has adjusted the instrument and working by Pilotman is in operation, the Token must be handed to the Pilotman, who will then withdraw the forms, after which the Token must be restored to the instrument and ordinary working may be resumed.

Regulation 25. Failure of Token Apparatus.—(a) In the event of the failure of the Token apparatus between two Token Stations, steps must at once be taken to have the defect put right by the Lineman; if his services are not immediately available, the Station Master or other responsible person in charge at each end of the section must communicate with each other by the most expeditious means, agree as to who shall arrange for working by Pilotman, and have a clear understanding as to the arrangements to be put into operation.

If a Token is not available it will generally be found more expeditious for the Station Master or other responsible person at the opposite end of the section to that at which a train is next required to enter the section to undertake the arrangements, as the Pilotman will then only have to go through the section in one direction to issue the forms.

(b) A competent person must be appointed as Pilotman, who must wear, round his left arm above the elbow, a red armlet with the word "Pilotman" shewn thereon in white letters, thus:—



If this armlet is not immediately available the Pilotman must wear a red flag in the position indicated until the proper armlet is obtained.

(c) The person arranging working by Pilotman must fill up, sign, and address the necessary forms (see page 41 for specimen form) to :—

(i.) The Signalman at each end of the Token Section.

(ii.) The person who will act as Pilotman.

(iii.) The Station Master at each end of the section, except where the signal box at which working by Pilotman commences or finishes is not at a station and the ordinary working at the station will not be interfered with.

#### EXTRACTS FROM ELECTRIC TRAIN TOKEN REGULATIONS—Continued.

These forms must be handed to the Pilotman, who must also sign all the forms issued and deliver the necessary form to the Signalman in charge of the Token Station at which working by Pilotman commences and when satisfied that the Signalman understands that no train is to be allowed to enter the section until he returns, proceed to the other end of the section.

The Pilotman when proceeding to the other end of the section to deliver the forms must do so as quickly as possible, either by rail or road, except as provided in clause (f) of this Regulation, using the best means at his disposal for the purpose, but must not, unless a Token is in his possession, use an engine or any railway vehicle other than a trolley. See Regulation 9 (iv.), Working of Trollies.

If the failure occurs on a section of line where occupation key boxes are in use an assurance must be given by the Signalman to the Pilotman that the occupation key or keys are in the key instrument.

In the event of the telephone communication also having failed, and the Pilotman is starting from a place where no control instrument exists, as he will be unable to ascertain that the control slides are in the normal position, it will be necessary for him when walking through the section with the pilot working forms, in accordance with clause (f) of this regulation, to make certain by personal observation that the occupation keys are properly in the key boxes.

AT TOKEN STATIONS WHERE CONTROL INSTRUMENTS ARE NOT PROVIDED, THE PILOTMAN MUST, WHEN DISTRIBUTING THE PILOT WORKING FORMS, SATISFY HIMSELF BY PERSONAL OBSERVATION THAT THE OCCUPATION KEYS ARE PROPERLY IN THE KEY INSTRUMENTS.

DUPLICATE KEYS OF THE HUTS ARE HELD BY THE SIGNALMEN TO ENABLE THIS TO BE DONE.

On the arrival of the Pilotman at the other end of the section he must deliver the necessary forms to the Station Master and Signalman, and if in possession of a Token shew it to the Signalman. Each person when receiving the form must sign the Pilotman's form.

When a Station Master himself acts as Pilotman he must retain only the Pilotman's form, and unless his station comes within the exception mentioned in section (iii.) of this clause (c), he must address and give a form to the person he leaves in charge of his station.

Should there be any intermediate non-Token Stations in the section, the Pilotman must advise the persons in charge of such places that working by Pilotman is in operation.

(d) Station Masters and persons in charge issuing and receiving forms for working by Pilotman will be responsible for the Inspectors, Signalmen and others concerned at their stations being made acquainted with the circumstances immediately, and instructed in their necessary duties.

Twelve forms for working by Pilotman must be kept in a convenient place at each Token Station so as to be available at any time.

- (e) If a Token is out of the instrument at either end of the section, or if one can be withdrawn at the end of the section where a Pilotman is appointed (provided the Token is obtained with the co-operation and consent of the Signalman at the other end of the section), the Pilotman must first take possession of such Token and then, if there is a train available, and permission has been obtained for the train to proceed into the section, he may use such train for the purpose of conveying the forms to the Token Station at the other end of the section. On arriving at the other end of the section he must, when delivering the form to the Signalman there, show him the Token, and also show it to the Driver, or Drivers, of every train passing over the section during the time working by Pilotman is in operation. The Pilotman must keep the Token in his possession until it is required to be taken away by the Lineman or until the Token apparatus is again repaired and ready for use.
- (f) Should the speaking instruments as well as the Token apparatus have failed and the men at each end of the section be unable to communicate with each other the Station Master or other responsible person in charge at each end of the section must arrange for working by Pilotman and the Pilotman appointed at each end must proceed along the railway in order that they may meet, and on doing so they must go together to the nearer or more convenient end of the section or to the end from which the next train will proceed. The Pilotman who returns to the end of the section from which he started must collect the forms which had been delivered at that end of the section and return them and the other forms in his possession to the person who filled them up, and the latter must at once cancel them by writing the word "Cancelled" across them. The other Pilotman must hand one of his forms to the Station Master or person in charge and one to the Signalman and then act as the Pilotman.

When practicable the Post Office telephones should be used to arrange working by Pilotman instead of appointing two men as laid down in the preceding paragraph.

(g) After all the forms have been signed as laid down in clause (c) trains may be allowed to enter the section by the permission and under the control of the Pilotman in accordance with the following instructions:—

### REGULATION 25.—FAILURE OF TOKEN APPARATUS.—page 37.

Clause (g) (i)—The first paragraph to be amended to read:

(i) No train must be allowed to pass on to or foul the section under the Pilotman's control without the Pilotman being present. The Pilotman must inform the Driver and rear Guard of each train, also the Driver of an engine assisting in rear, if any, of the failure, and when practicable accompany every train. He must also instruct the Driver, or Drivers, TO PASS AT DANGER THE SIGNAL CONTROLLING THE ENTRANCE INTO THE SECTION AHEAD IN ACCORDANCE WITH RULE 38, CLAUSE (b) AND to proceed cautiously through the section. When an engine supplied specially for the use of the Pilotman is coupled to a train it must be attached to the front unless it is necessary for such engine to be used for ASSISTING purposes where the use of ASSISTANT engines is authorised. If the Pilotman travels on a train with two or more engines he must ride upon the rearmost engine. If the Pilotman travels on an electric train, rail motor, auto train, OR STREAMLINE RAIL CAR, he must ride with the Driver.

Clause (g) (ii)-Amended to read:

THE SIGNAL CONTROLLING THE ENTRANCE TO THE SECTION AHEAD MUST BE MAINTAINED AT DANGER IN ACCORDANCE WITH RULE 38 CLAUSE (b) AND the Pilotman must obtain the permission of the Signalman before allowing a train to enter the section.

(G.A.18. 11/47. O.M. 12248.)

## Regulation 25. Failure of Token Apparatus-page 37 .- continued.

The following to be inserted as clause (k):

(k) WHEN A TOKEN FAILURE OCCURS IN A SECTION IN WHICH AN INTERMEDIATE TOKEN INSTRUMENT IS PROVIDED AT A SIDING OR LOOP, THE PILOTMAN, WHO WILL BE APPOINTED IN ACCORDANCE WITH THIS REGULATION, WILL, IF NECESSARY, APPLY TO THE LINEMAN FOR A TOKEN TO ENABLE WORK TO BE CARRIED OUT AT THE SIDING OR LOOP. IN SUCH CIRCUMSTANCES THE PILOTMAN MUST, WHILE A TRAIN IS WORK-ING AT THE SIDING OR LOOP, REMAIN WITH SUCH TRAIN UNLESS IT IS NECESSARY FOR HIM TO PROCEED TO THE TOKEN STATION IN ADVANCE OR RETURN TO THE TOKEN STATION IN REAR TO ENABLE OTHER TRAINS TO BE PASSED OVER THE SINGLE LINE, IN WHICH CASE HE MUST SATISFY HIMSELF THAT THE TRAIN IS CLEAR OF THE SINGLE LINE, AND THAT THE POINTS ARE SECURED TO PREVENT THE SINGLE LINE BEING FOULED. HE MUST KEEP THE TOKEN IN HIS POSSESSION UNTIL THE TRAIN AT THE INTERMEDIATE SIDING OR LOOP HAS BEEN WITHDRAWN OR THE FAILURE RECTIFIED. THE TIME THE TOKEN IS WITHDRAWN AND RETURNED MUST BE ENTER-ED IN THE TRAIN REGISTER BOOK, AND THE ENTRIES INITIALLED BY THE SIGNAL-MEN AND PILOTMAN.

SHOULD, HOWEVER, A TOKEN FAILURE OCCUR WHEN A TRAIN IS AT SUCH SIDING OR LOOP THE PILOTMAN HAVING ACCOMPANIED A TRAIN TO THE POINT WHERE THE INTERMEDIATE TOKEN INSTRUMENT IS SITUATED MAY INSTRUCT THE DRIVER OF THE TRAIN ON WHICH HE HAS TRAVELLED TO PROCEED TO THE TOKEN STATION IN ADVANCE, WHILE HE REMAINS AT THE SIDING TO DEAL WITH THE TRAIN AT THE SIDING OR LOOP. IF THE PILOTMAN IS NOT IN POSSESSION OF THE TOKEN, THE SERVICES OF THE LINEMAN MUST BE OBTAINED TO RELEASE THE TOKEN FROM THE INTERMEDIATE TOKEN INSTRUMENT AND ON COMPLETION OF THE WORK AT THE SIDING THE PILOTMAN MUST RETURN THE TOKEN TO THE LINEMAN WHO WILL RESTORE IT TO THE INTERMEDIATE TOKEN INSTRUMENT UNLESS THE FAILURE HAS IN THE MEANTIME BEEN RECTIFIED, IN WHICH CASE THE TOKEN WILL BE TAKEN BY THE PILOTMAN TO THE TOKEN STATION TO WHICH HE IS PROCEEDING, IN ORDER THAT WORKING BY PILOTMAN MAY BE CANCELLED IN ACCORDANCE WITH THE REGULATIONS.

The existing clauses (k) and (l) to be re-lettered (l) and (m) respectively. (G.A.18. 11/47. O.M. 12299.)

#### EXTRACTS FROM ELECTRIC TRAIN TOKEN REGULATIONS-Continued.

to a train it must be attached to the front unless it is necessary for such engine to be used for banking purposes where the use of bank engines is authorised. If the Pilotman travels on a train with two or more engines he must ride upon the rearmost engine. If the Pilotman travels on an electric train, rail motor, auto train, or STREAMLINE RAIL CAR, he must ride with the Driver.

When a train is to be followed by one or more trains in the same direction before a train has to be started from the other end, the Pilotman must personally order each train to proceed and must ride upon the engine of the last train. The Driver of each train which is sent forward by the Pilotman but which he does not accompany, must be handed one of the Pilotman's printed tickets where in use \* (see page 42 for specimen ticket), properly filled up and signed. In the case of a train with two or more engines, or when two or more engines are coupled together, the ticket must be shewn to each Driver and issued to the Driver of the rearmost engine. The Pilotman's ticket will apply only to the single journey to the other end of the section, where it must immediately be given up to the Signalman, who must at once cancel it by writing the word "Cancelled" across the face of the ticket. After ordinary working has been resumed, the cancelled tickets must be sent to the Divisional Superintendent or District Traffic Manager.

\* PILOTMAN'S TICKETS ARE NOT IN USE ON THE GREAT WESTERN RAILWAY.

(ii.) The Pilotman must obtain the permission of the Signalman before allowing a train to enter the section.

(iii.) When the Token apparatus and bells or gongs have failed and speaking instruments are not available, the Pilotman must not allow a train to follow another train until the time usually taken by the preceding train to clear the section, after allowing for the train having been stopped, has elapsed, but in no case with a less interval than three minutes, and on those parts of the line where a longer interval of time is prescribed, until such interval has clapsed. Where a tunnel intervenes in a section, an interval of not less than ten minutes must be allowed between two trains, unless the Pilotman can satisfy himself that the tunnel is clear.

(iv.) Should a train, the Driver of which is in possession of a Pilotman's ticket, become disabled between two Token Stations, assistance must only come from the Token Station where the Pilotman has been left. The disabled train must be protected by detonators in accordance with Rule 179, the Fireman doing this on his way for assistance, and the Guard in the opposite direction. In the case of a light engine the Fireman must protect the engine as directed when going for assistance, but before doing so must protect the engine in the opposite direction by placing on the rail three detonators, 10 yards apart, not less than 300 yards from the engine.

If the Token Station ahead be the nearer, the Fireman may instead of going in the direction of the station where the Pilotman is, go to the Token Station ahead so that the Signalman there may advise the Signalman in rear that assistance is required, and the latter must in the case of a Guard protecting the train in rear instruct the Driver to pick up the Guard, who will conduct the assisting train to the disabled train.

The Fireman must take the Pilotman's ticket, WHERE IN USE, with him when he goes for assistance, and retain it until he hands it to the Pilotman, who must then cancel it.

In all cases the Pilotman must accompany the assisting train.

(h) Should it be necessary to change the Pilotman or should the Signalman be changed during working by Pilotman, the provisions of Rule 205 must be observed.

(i) When the Token apparatus is again repaired and ready for use, the Pilotman must withdraw the forms for working by Pilotman at one end of the section, then take the Token, if there be one out of the instrument, from that end of the section to the other end of the section, and after delivering it to the Signalman there, and all the forms for working by Pilotman have been collected, ordinary working may be resumed. All forms which have been issued for working by Pilotmen must be cancelled by writing the word "Cancelled" across them, afterwards being sent to the Divisional Superintendent or District Traffic Manager.

After the forms for working by Pilotman have been issued at one end of the section ordinary working must not be resumed until such forms have been withdrawn and cancelled, although the Token apparatus may have been repaired or found to be in working order before the Pilotman appointed has delivered the forms at the other end of the section. Any Token obtained by the Lineman as the result of the instruments being tested should at once be restored to the instrument by him. In the absence of the Lineman no attempt must be made to withdraw a Token from the instrument unless the Pilotman is present at that end of the section, and if as a result of the apparatus being tested a Token is then obtained, the Pilotman must take possession of such Token until it is required to be taken away by the Lineman or restored to the instrument.

(j) If there should be a failure in the token apparatus, so that a token cannot be withdrawn, the line will be worked by Pilotman according to the regulations, but if it is necessary that a train should call for traffic purposes at an intermediate siding, the ground frame of which is locked by a key fixed in the end of the token, or by the token issue, the following course must be adopted:—

THE PILOTMAN MUST APPLY TO THE LINEMAN (WHO, ACCORDING TO INSTRUCTIONS, WILL HAVE BEEN SENT FOR DIRECTLY THE FAILURE BECAME KNOWN) FOR A TOKEN FOR THE PURPOSE OF OPENING

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## EXTRACTS FROM ELECTRIC TRAIN TOKEN REGULATIONS-Continued.

THE SIDING POINTS, AND THE LINEMAN IS AUTHORISED TO EXTRACT A TOKEN FROM THE INSTRUMENT AND GIVE IT TO THE PILOTMAN FOR THE PURPOSE NAMED. THE PILOTMAN MUST HIMSELF OPEN AND CLOSE THE SIDING POINTS, AND KEEP THE TOKEN ABSOLUTELY IN HIS POSSESSION UNTIL THE TRAFFIC ARRANGEMENTS AT THE SIDING OR SIDINGS PERMIT HIM TO RESTORE THE TOKEN TO THE SAME INSTRUMENT FROM WHICH IT WAS WITHDRAWN, WHICH MUST BE DONE IN THE PRESENCE OF THE LINEMAN AND SIGNALMAN. THE PILOTMAN MUST NOT APPLY TO THE LINEMAN FOR A TOKEN UNLESS IT IS NECESSARY FOR A TRAIN TO CALL AT A SIDING DURING THE TIME PILOT WORKING WILL BE IN FORCE. THE TOKEN WHEN USED FOR SUCH PURPOSES MUST NOT BE ALLOWED TO BE IN POSSESSION OF THE ENGINEMAN, AND MUST BE REGARDED MERELY AS A KEY TO OPEN AND CLOSE THE SIDING POINTS, AND MUST IN ANY CASE BE RESTORED TO THE INSTRUMENT FROM WHICH IT WAS TAKEN BEFORE THE ORDINARY WORKING IS RESUMED. THE TIME THE TOKEN IS WITHDRAWN AND RETURNED MUST BE ENTERED IN THE TRAIN REGISTER BOOK, AND THE ENTRIES INITIALLED BY THE SIGNALMAN AND PILOTMAN.

WHEN OCCUPATION OF THE TOKEN APPARATUS IS GIVEN THE SIGNAL DEPARTMENT FOR REPAIRS OR CLEANING PURPOSES THE PILOTMAN SHOULD CARRY WITH HIM A TOKEN DURING THE TIME THE APPARATUS IS IN THE HANDS OF THE SIGNAL DEPARTMENT MEN.

When the Token apparatus fails, and there is a level crossing in the section provided with indicators, bells or gongs, but not a Token Station, the Signalman, if telephonic communication with the crossing is available, must ascertain from the person in charge of the crossing whether the indicators, bells or gongs are still in working order.

If there is no telephonic communication with the crossing, or the telephone has failed, the Signalman must request the Pilotman to instruct the Driver of each train proceeding in the direction of the crossing during the failure to approach the level crossing cautiously, sound the engine whistle and be prepared to stop short of any obstruction at such crossing. The time interval must be extended sufficiently to allow for the additional time likely to be occupied in carrying out this instruction.

WHEN IT IS KNOWN THAT THE ELECTRICAL COMMUNICATION AT A CROSSING IS NOT IN WORKING ORDER THE DISTANT SIGNALS (IF SUCH ARE PROVIDED) PROTECTING THE CROSSING MUST BE KEPT AT COUTION DANGER UNTIL INFORMATION HAS BEEN RECEIVED THAT THE COMMUNICATION IS AGAIN IN WORKING ORDER.

IF DISTANT SIGNALS ARE NOT PROVIDED, GREAT CARE MUST BE TAKEN TO ASCERTAIN, AS FAR AS POSSIBLE, THAT NO TRAIN IS APPROACHING THE CROSSING IN EITHER DIRECTION BEFORE THE GATES ARE OPENED TO THE PUBLIC.

(A) When trains are being worked in accordance with clause (g) (iii) all trains must be brought within the protection of the home signal as promptly as possible, and, to obviate a train standing with its rear portion outside the home signal, the Signalman must, if necessary, authorise the Driver to draw forward a sufficient distance to bring the rear portion within the home signal in accordance with Rule 38 (b) (iii).

If a train requires to stand outside a home signal for the purpose of attaching or detaching traffic or through any other cause, the Signalman must obtain the assistance of a Hand Signalman, provided with the necessary detonators and hand signals, who must be sent out a sufficient distance from the rear of the train to afford protection. Until this Hand Signalman has been provided, a train must not be stopped outside the home signal to attach or detach traffic.

Regulation 34. Sidings Controlled by means of Token.—(a) Points giving communication between sidings and the single line controlled by TOKEN cannot be opened without a TOKEN for that section of the line where the siding is situated, and the TOKEN cannot be removed until the points have been placed in the proper position for trains to pass upon the single line, and securely locked to prevent vehicles passing from the sidings on to the single line.

(b) On arriving at a siding, the points of which are controlled by Token, the Driver must hand the TOKEN to the Guard or man in charge of the siding to enable the points to be unlocked. When the necessary shunting has been completed, and the points have been placed in the proper position for trains to pass upon the single line, the Guard or man in charge of the siding must return the TOKEN to the Driver, and the latter must not proceed on his journey until he has obtained possession of it.

Regulation 35. Speed of Trains.—When exchanging Tokens by hand, Drivers must be careful not to exceed a speed of 10 miles an hour.

WHEN EXCHANGING TOKENS BY MEANS OF EXCHANGING APPARATUS (SEE PAGE 42), THE SPEED MUST NOT EXCEED 15 MILES PER HOUR. SPECIAL INSTRUCTIONS ARE ISSUED IN REGARD TO THE SPEED WHERE AUTOMATIC PICKING UP AND SETTING DOWN APPARATUS IS PROVIDED.

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# EXTRACTS FROM ELECTRIC TRAIN TOKEN REGULATIONS—Continued.

#### PILOTMAN'S FORM.

(Form referred to in Re Lines by Electric Token B	gulations $14C$ , $23$ and $25$ of the Regulations for $T$ flock System.)	rain Signalling on Single
	GREAT WESTERN RAILWAY.	(2988A)
	ELECTRIC TOKEN BLOCK SYSTEM.	
* \begin{cases} (a) DURING OBSTRU (b) DURING FAILURI LOST.	E OF TOKEN APPARATUS, OR WHEN TO	KEN IS DAMAGED OR
Pilo!man.	l up and used whenever it is temporarily necess	eary to work the traffic by
	***************************************	Station.
		19
(a) The single line l obstructed, the traff	between and and and	the point of obstruction
will be worked by	Pilotmanwil	l act as Pilotman and
must accompany expoint of obstruction	very train to and from	station and the
The signalling	of trains by bell or gong must be suspended.	
to pass into or foul f	Is for the section between	lost the traffic between o train must be allowed
Regulation 25, clause	g(g/ii) or $(g/iii)$ , as the case may be, observed	the provisions of Block
This order is to remain	in force until withdrawn by the Pilotman.	44
То	(Signed)	
		Time.
Noted by	at	
r(a)Noted by	at point of obstruction.	***************************************
Noted by		

#### FORM OF PILOTMAN'S TICKET.

(Ticket referred to in Regulation 25 of the Regulations for Train Signalling on Single Lines by Electric Token Block System).

#### ELECTRIC TOKEN BLOCK SYSTEM.

#### PILOTMAN'S TICKET.

To be used when it is necessary to work the traffic of a single line by Pilotman during a failure of the Token apparatus, or in the case of a Token being damaged or lost.

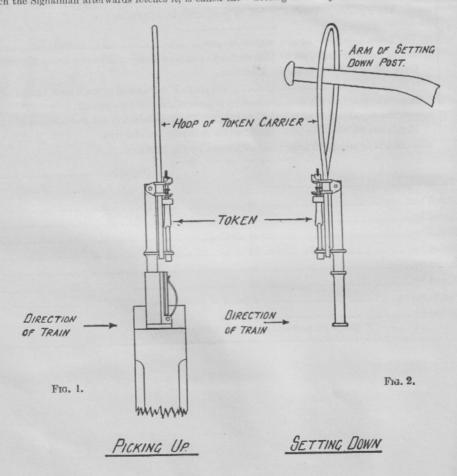
To the Driver of.....train from. You are authorised to proceed from.....to.....to.... Pilotman following. Signature of Pilotman..... Date.....

This ticket must be given up by the Driver to the person in charge of the Token working at the place to which he is authorised to proceed, immediately on arrival.

Note.—Pilotman's Tickets are not used on the Great Western Railway.

#### ELECTRIC TRAIN TOKEN EXCHANGING APPARATUS.

In this Apparatus the Token is fixed in a carrier to which is attached a hoop.
 The post on which the Signalman places the Token, and from which the Fireman takes it before he enters the Token Section, is called the "Picking up" post.
 The post on which the Fireman hangs the Token, after passing through the Section, and from which the Signalman afterwards fetches it, is called the "Setting down" post.



### INSTRUCTIONS FOR OPERATING INTERMEDIATE TOKEN INSTRUMENTS.

(1) To place token in instrument.

Press token forward into aperture in the centre of the instrument as if using an ordinary key in a lock (the key end of the token must engage on the centre pin of the instrument) then turn the token left to right as far as possible, withdraw token from centre pin and lower same into either of the columns of the magazine. Advise appropriate Signalman by means of telephone provided that token has been placed in the instrument.

When the token has been replaced in the instrument the signalmen must immediately withdraw tokens in accordance with Regulation 27 (Testing Instruments) of the Electric Train Token Regulations, and the shunter or other person responsible for operating the intermediate token instrument must remain there until the test has been made and the signalman

has informed him that everything is again in order.

(2) To obtain token from instrument.

After obtaining the authority of the appropriate Signalman by means of the telephone provided, to withdraw a token, lift it from the column of the magazine to the centre opening of the instrument, press the token forward as if using an ordinary key in a lock (the key end of the token must engage on the centre pin of the instrument), then turn the token right to left as far as possible. Wait until the needles in both the indicators are deflected (this takes place when both Signalmen hold down on their respective token ringing keys), and afterwards continue to turn the token from right to left until the token is free when it can be withdrawn from the instrument. Advise Signalman by means of the telephone provided that the token has been obtained from the instrument. (G.A. 10. 3/42. L.49736/99).

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# ELECTRIC TRAIN TOKEN EXCHANGING APPARATUS-Continued.

To deliver a Token, the Fireman must hold it at full length, with the hoop facing, fairly and squarely, to the front.

On arriving at the "Setting down" post, the hoop must be passed over the projecting arm and the Fireman should be careful to release his hold of the Token immediately it passes on to the arm.

The Token should on no account be thrown over the arm.

4. To pick up a Token, the Fireman should pass his hand and forearm fairly through the hopp and the Token Carrier will then easily draw out from the spring box of the "Picking up

5. The Signalman must be careful in placing a Token Carrier in the spring box of the "Picking up" post. up" post to see that the hoop faces, fairly and squarely, the direction of the approaching Train, and that the spring box is in proper working order.

6. The speed at which the above operations are to be performed is in no case to exceed 15 miles

per hour.

7. When new "Picking up" and "Setting down" posts are brought into use or positions are altered notices are issued by the Divisional Superintendent or District Traffic Manager, giving the positions. The positions of existing posts are shewn in the Sectional Appendices.

The Appendices to the Service Time Tables give particulars of the places where "Picking up" and "Setting down" posts are provided.

All Train Token exchanging apparatus, including "Setting down" and "Picking up" posts and platforms for same where necessary, are provided and maintained by the Signal Department, and the Local Signal Inspector should be advised of any repairs which may be required to same.

With the object of preventing Tokens being damaged by fouling engines where "Picking up" and "Setting down" posts are provided, the following instructions must be observed :-

Instructions to Signalmen. - The Token carrier must be so placed in the "Picking up" post that the Token is farthest away from the Fireman. This also applies when Tokens are exchanged by hand (Fig. 1).

Instructions to Enginemen.-The Token carrier must be placed on the "Setting down" post with the Token nearest the Fireman (Fig. 2).

# EXCHANGING TRAIN STAFFS, TOKENS OR TABLETS BY HAND.

When exchanging Train Staffs, Tablets, Tokens or Train Tickets by hand, Drivers must be careful not to exceed a speed of 10 miles per hour.

When exchanging staffs by hand the person holding he staff which is being handed over must hold it by the "key" end so that the person taking the staff may grasp it by the "butt" end.

As the Driver of a Streamlined Car is unable to exchange the Train Token whilst the Car is in motion, it must be brought to a stand at the signal box, or other convenient point, and the Signalman must go to the Car and exchange the Token with the Driver. The loop carrier must not be used.

# INSTRUCTIONS FOR WORKING AUXILIARY ELECTRIC TRAIN TOKEN INSTRUMENTS AT PLACES WHERE THE TOKEN IS WITHDRAWN BY THE FIREMAN.

The Fireman must first obtain permission from the Signalman by means of the telephone to withdraw the Token. When this permission has been given, the Fireman must lift the Token from the column to the centre opening of the instrument, press the Token forward as if using an ordinary key in a lock (the key end of the Token must engage on the centre pin of the instrument), then turn the Token right to left as far as possible. The Fireman must then wait until the bell rings and afterwards continue to turn the Token from right to left until the Token is free, when it can be withdrawn from the instrument. He must then inform the Signalman on the telephone that the Token has been withdrawn.

# EXTRACTS FROM REGULATIONS FOR TRAIN SIGNALLING ON SINGLE LINES BY THE TRAIN STAFF OR TRAIN STAFF AND TICKET BLOCK SYSTEM.

Any additions to or alterations in these Regulations differing from the agreed Railway CLEARING HOUSE STANDARD ARE SHEWN IN THIS SPECIAL TYPE.

Wording which is not an actual extract from the above Regulations is printed in italics.

Train Staff or Train Staff and Ticket Block System .- The object of the Train Staff or Train Staff and Ticket Block System of signalling is to prevent more than one train being in the block section between two signal boxes at the same time.

The signalling of trains on the Train Staff or Train Staff and Ticket Block System does not in any way dispense with the use of fixed or hand signals, or detonators, whenever and wherever such

signals or detonators may be requisite to protect obstructions on the line.

Working of Fixed Signals at Crossing Places .- (b) When a shunting movement is being made on a loop line at a crossing place the home signal applicable to a train approaching on the single line in the opposite direction must not be lowered, unless there are points which are set to prevent the sannting movement fouling the single line, until the shunting movement has come to a stand and the Driver has been instructed that no further movement towards the starting signal must be made.

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# EXTRACTS FROM BLOCK REGULATIONS—SINGLE LINES—Continued.

When a home signal at a crossing place has been lowered for an approaching train a shunting movement must not be made towards the starting signal applicable to the opposite loop until the train approaching on the single line has passed clear on to the loop line, unless there are points which are set to prevent the shunting movement fouling the single line.

Regulation 5. Section Clear but Station or Junction Blocked. (Warning Arrangement.)—The method of conveying to Drivers that they are required to proceed into the Section in accordance with the provisions of this Regulation is dealt with in Rule 41.

Regulation 5.—Section Clear but Station or Junction Blocked (Warning arrangement), Clause (d) to be amended to read :—

(f) At terminal stations where special authority is given on the footnotes for trains to be accepted under the "Warning" arrangement, while vehicles are standing against the buffer stops, the home signal must be kept at danger in such circumstances until the incoming train has been stopped, or nearly stopped, at it, after which it (or the "Calling on" Arm where provided) may be lowered.

Regulation 6. Engine Assisting in Rear of Train.—An engine must never assist a passenger train at the rear except where authorised by the Superintendent of the Line and the Chief Mechanical Engineer.

Officer's Special Train G726

Regulation 8. Ballast Train, Freight Train, or Inspection Train requiring to Stop in Section.—
(c) When a ballast train working in the section has to return to the staff station in rear, no shunting outside the home signal at that end must be allowed until a man with hand signals and detonators has been sent out to protect such shunting.

(d) Should the Guard of the ballast train require his train to return to the signal box in rear, instead of going through to the signal box in advance, he must obtain the verbal permission of the Signalman before the train enters the section.

Regulation 9. Trolley Going Into or Through Tunnel.—(a) When it is necessary for a trolley to go into or through any of the tunnels specially enumerated in the appendix to the working time table as coming within the application of this Regulation, the Ganger or man in charge of the trolley must be in possession of the train staff or ticket, and the trolley must be signalled on the block instruments. Trollies passing through all other tunnels must be protected solely by the Permanent Way Men's own signals.

When the trolley has to remain stationary in the section in which the tunnel is situated although it has been signalled on the block instruments, the train staff must be given to the Ganger or man in charge of the trolley, who must be told when receiving the staff to which end of the section it is to be taken and at what time it is to be there, in order to clear the line for the next train.

(c) Should the trolley, after passing into or through the tunnel, be removed from the rails before reaching the next signal box, the Ganger or man in charge must take the train staff or ticket to the Signalman in advance, and inform him that the trolley is clear of the line; the Signalman must then send the "Train out of Section" signal. If, however, time would be saved, the Ganger or man in charge must return with the train staff or ticket to the signal box in rear and inform the Signalman that the trolley is clear of the line; the signalman must then send the "Cancelling" signal.

TROLLIES PROPELLED BY SUCH MEANS AS ENABLE A FAST SPEED TO BE ATTAINED MUST BE SIGNALLED IN ACCORDANCE WITH THIS REGULATION, WHETHER THERE BE A TUNNEL IN THE SECTION OR NOT. SUCH TROLLIES MUST NOT BE PUT ON THE RAILS EXCEPT AT SIGNAL BOXES, AND MUST ALWAYS BE RUN THROUGH THE WHOLE BLOCK SECTION, AND CARRY THE TRAIN STAFF OR A TICKET.

Regulation 10. Train Out of Section.—(b) When the last vehicle of a train does not pass the signal box before it has been shunted into a siding, or when a train has been brought to a stand within a home signal, and it is necessary to give the "Train out of Section" signal before the train passes the signal box, the Signalman must, before giving such signal, ascertain from the Guard or Shunter in charge of the train that the whole of the train, with tail lamp attached, has arrived, and the Guard or Shunter, will be held responsible for giving this information to the Signalman, the Fireman being similarly responsible in the case of a light engine.

Regulation 12A. Animals on Line.—(a) Until it has been ascertained that the line is clear, all trains allowed to proceed must be brought to a stand and the Driver verbally informed of the circumstances and instructed to proceed cautiously.

(b) Should there be a tunnel in the obstructed section, the Signalman at either end of the section must verbally explain the circumstances to the Driver and Guard of each train entering the section and instruct them that although the train may proceed into the section it must not enter the tunnel until they have ascertained the tunnel is clear.

#### Regulation 13.-Blocking Back-page 44.

Delete clause (b) under this heading and substitute the following :-

(b) Unless special permission is given by the Operating Superintendent no train or vehicles, other than an engine or engine with one or two brake vans; must be placed outside a Home signal where the line is on a falling gradient steeper than 1 in 260 towards the signal box in rear unless there is an engine at the lower end. WHERE SUCH PERMISSION IS GIVEN THERE MUST BE A BRAKE VAN AT THE LOWER END WITH A MAN IN IT. (G.A.27.Op.—1/51. L.K.1/10428/Gen.E.)

Regulation 5.—Section Clear but Station or Junction Blocked (Warning arrangement), Clause (d) to be amended to read :-DURING FOG OR FALLING SNOW A TRAIN MUST NOT BE DRAWN TOWARDS THE

provisions of this Regulation is dealt with in Rule 41.

FROM THE SIGNAL BOX IN ADVANCE, EXCEPT WHEN A FOGSIGNALMAN IS ON DUTY AT THE SIGNAL, UNLESS THE SIGNALMAN IS ABLE TO SATISFY HIMSELF THAT THE SIGNAL IS AT DANGER, AND THE LINE TO THE REAR OF SUCH SIGNAL IS TRACK-CIRCUITED OR A TRAIN STANDING AT THE SIGNAL WILL BE WITHIN HIS VIEW. (G.A.19-10/48. L.K.1/8617/31.)

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SIGNAL CONTROLLING THE ENTRANCE TO THE SECTION AHEAD TO AWAIT ACCEPTANCE

#### EXTRACTS FROM BLOCK REGULATIONS-SINGLE LINES-Continued.

Regulation 14. Section Obstructed by Accident or by Disabled Train. (a) Should a Signalman receive information from the Fireman of a disabled train, or from the Signalman at the box in advance, that AN ASSISTING TRAIN OR ENGINE is required to enter the section to assist the disabled train, or should it be necessary for the breakdown van train to enter a section obstructed by accident or otherwise, the ASSISTING TRAIN OR ENGINE or the breakdown van train as the case may be, may, after having been brought to a stand and the Driver AND GUARD informed of the circumstances, be allowed to enter the section under the following arrangements provided the Driver is in possession of the train staff, except as shewn in Regulation 18, clause (b), paragraph (ii), of the Regulations for Working on Single Lines by Train Staff and Ticket.

- (i) The Signalman must instruct the Driver to pass at Danger the signal controlling the entrance into the section ahead as provided for in Rule 38, clause (b).
- (ii) It will not be necessary for the Signalman in rear of the obstruction to detain the assisting train or engine until the arrival of the Guard or Fireman of the disabled train at his box if information has been received from the Signalman in advance that the Guard or Fireman is coming back. On receipt of this information the Signalman in rear may allow the assisting train or engine to enter the obstructed section after the Driver has been informed that the Guard or Fireman of the disabled train is coming back, and has been instructed to keep a look out for each Chard or Fireman. The Guard or Fireman of the disabled train when

Regulation 14A. Examination of Line.—(a) When it is necessary in accordance with Regulation 17 'Stop and Examine" to ascertain if the line is clear, and speaking communication exists between the two signal boxes which will admit of a proper understanding being arrived at between the two Signalmen, an engine may be allowed to enter the section provided the "Train out of Section" signal has been sent or received, as the case may be, for the previous train. The circumstances must be explained to the Driver, who must be in possession of the train staff, and he must be instructed to pass at Danger the signal controlling the entrance into the section ahead as provided for in Rule 38, clause (b), and proceed cautiously through the section, prepared to stop short of any obstruction. Where practicable, the engine must be accompanied by a Station Master or other competent person. After sunset, during fog or falling snow, or where a tunnel intervenes, the engine must always be so accompanied.

Regulation 16. Fouling Single Lines for Shunting Purposes.—(a) Except in the case of a ballast train working in the section and which has to return to the signal box in rear (see Regulation 15 (d), Regulations for working on Single Lines by Train Staff and Ticket), permission must not be given for a train to approach on the single line when there is any obstruction outside the home signal, nor must the single line be fouled outside the home signal after permission has been given for a train to approach.

(b) Except as shown in paragraphs (i) and (ii), a train must not be allowed to foul the single line outside the home signal, unless the Driver is in possession of the train staff for the section so fouled.

(i) The Signalman may, if necessary, and provided permission has not been given for a train to approach from the opposite end of the section, allow the single line to be fouled outside the home signal, but before doing so he must send the "Blocking Back" signal in accordance with Regulation 13 and until this signal has been acknowledged the line outside the home signal must not be occupied.

The single line may, if necessary for shunting purposes, be occupied outside the home signal at both ends of the same section at the same time, provided no train is approaching in the section, and that the "Blocking Back" signal has been given to, and acknowledged by, the Signalman at the opposite end of the section.

(ii) If a train is travelling in the section away from the signal box at which shunting operations have to be performed, and it is necessary to occupy the single line outside the home signal, this may be done without first giving the "Blocking Back" Signal to the signal box at the opposite end of the section, but immediately the "Train out of Section" signal has been received for that train, the Signalman must, if the single line is still occupied outside the home signal, give the "Blocking Back" signal to the signal box at the opposite end of the section, which signal must be acknowledged.

Regulation 17. Stop and Examine Train.—(a) Should the Signalman who receives the "Stop and Examine Train" signal be unable to ascertain from the Trainmen, after examination of the train, why the signal was sent, he must, if the next train is travelling in the opposite direction, inform the Driver of that train of the circumstances, and instruct him to proceed cautiously to the next signal box. He must also communicate with the Signalman who sent the signal in order that the latter may, if necessary, caution the Driver of the next following train.

- (ii) It will not be necessary for the Signalman in rear of the obstruction to detain the assisting train or engine until the arrival of the Guard or Fireman of the disabled train at his box if information has been received from the Signalman in advance that the Guard or Fireman is coming back. On receipt of this information the Signalman in rear may allow the assisting train or engine to enter the obstructed section after the Driver has been informed that the Guard or Fireman of the disabled train is coming back, and has been instructed to keep a look-out for such Guard or Fireman. The Guard or Fireman of the disabled train, when picked up, will ride on the assisting engine of the assisting train and pilot it to the rear of the disabled train.
- (iii) If there is a tunnel in the obstructed section the Driver of the assisting train or ENGINE or breakdown van train, unless he is accompanied by the Fireman of the disabled train, must not enter such tunnel until he has met the Guard or Fireman of the disabled train or it has been ascertained that the tunnel is clear. The Driver of the assisting train or ENGINE, or breakdown van train, must be instructed accordingly by the Signalman.

When the line is clear again and permission has been obtained for another train to proceed, such train must be stopped and the Driver instructed to proceed cautiously.

(G.A.3.—12/37. O.M.11795.)

# EXTRACTS FROM BLOCK REGULATIONS—SINGLE LINES—Continued.

Should either Signalman become aware or have reason to believe that the permanent-way is fouled or damaged, he must immediately advise the Signalman at the opposite end of the section or sections affected, and no train must be allowed to proceed through such section or sections, until the line has been examined and is safe for the passage of the train; an engine may, however, be allowed to enter

the section or sections affected in the same or opposite direction in accordance with Regulation 14a,

(b) Should the "Stop and Examine Train" signal have been sent on account of a door being open on a passenger train, the Signalman sending the signal must advise the Signalman in rear, and the first train entering the section concerned need not be detained until evidence has been obtained that the line is not obstructed, but must be stopped, the Driver informed of the circumstances, and instructed to proceed cautiously to the next signal box, keeping a good look-out.

Regulation 20. Train Divided.—(b) If the divided train is assisted by an engine in rear, or is running on a falling gradient, or where the line is level, or between short sections, where the stoppage of the first portion would risk a collision with the second portion, the Signalman, provided permission has been obtained from the box in advance for the train to proceed, must not exhibit the Danger signal to stop the first portion, but must exhibit to the Driver a green hand signal waved slowly from side to side.

(c) If permission has not been obtained from the box in advance for the train to proceed, or if the train is running on a rising gradient, the Signalman must exhibit the Danger signal to stop the first portion of the train, but a green hand signal must not be exhibited. The first portion when stopped must be shunted into a siding as expeditiously as circumstances will permit, or otherwise dealt with as may be necessary to prevent the second portion coming into collision with it.

(d) In the circumstances described in clauses (b) and (c) the Signalman must, as soon as the first portion of the train has passed or otherwise been dealt with, place or maintain his signals at Danger and take proper measures for dealing with the second portion, placing detonators on the rail and exhibiting a red hand signal to attract the attention of the Trainmen.

(f) When the rear portion of a divided train requires to be removed from a section the provisions

of Regulation 14 must be observed.

(g) If it is necessary for a train to proceed into a section through which the first portion only of a divided train has travelled, such train must not be allowed to enter the section until the Signalman at each end is satisfied that the section is clear. The Signalman in rear must then advise the Signalman in advance of the train which is ready to enter the section and give the "Train entering Section signal. When this signal has been acknowledged he must inform the Driver of what has occurred, and instruct him to proceed cautiously, passing at Danger the signal controlling the entrance are the section allead in accordance with Rule 38 (b) (x.). The same course must be adopted in the event of the second portion of the divided train following the first portion through the section ahead.

Regulation 22. Train or Vehicles Running Away.—(c) If the train or vehicles are removed from the section other than under Regulation 14, "Section obstructed by accident or by disabled train," the Signalman must come to a clear understanding as to what has been done, the block indicators must be maintained at the "Train on Line" position, and when the next train requires to pass over the line affected the Signalman in rear must advise the Signalman in advance of the train which is ready to enter the section and give the "Train entering Section" signal. When this signal has been acknowledged the Signalman in rear must when handing the train staff or ticket to the Driver inform him of what has occurred and instruct him to proceed cautiously.

If, after the "Train or Vehicles running away" signal has been sent, the runaway train or vehicles are stopped and removed from the section at the end from which the signal has been sent, the Signalman in advance must be advised of the fact by speaking instrument, and both Signalmen must thereupon place the block indicators in their normal position provided the section is not occupied by another train. A note of the circumstances must be recorded in the train register books in both signal boxes, and the following train, after being signalled and accepted in the usual way, must be stopped and the Driver instructed to travel cautiously through the section.

Regulation 23. Speed of Trains.—When exchanging train staffs or tickets by hand, Drivers must be careful not to exceed a speed of 10 miles an hour. e of

25. Failure of Instruments and or Bells. In the event of any failure of the block instruments and or Bells so that the necessary signals cannot be forwarded and received in the ordinary way, the following instructions must be observed:

(a) (i) A train must not in any circumstances be allowed to pass a signal box into that section of the line where the failure exists (whether the failure has occurred to the instrument for one or both directions) without having previously been brought to a stand and the Driver and rear Guard, also the Driver of an engine assisting in the rear, if any, advised of the failure. The Signalman must, before handing the train staff or ticket to the Driver, or Drivers, INSTRUCT THEM TO PASS AT DANGER THE SIGNAL CON-TROLLING THE ENTRANCE INTO THE SECTION AHEAD IN ACCORDANCE WITH RULE 38, CLAUSE (b) AND to proceed cautiously through the section.

When a Driver has been stopped at a Signal Box and advised by the Signalman of the failure of instruments and or bells or of a track circuit controlling the block instruments, the Driver must draw his train forward and bring it again to a stand with the brake van near to the Signal Box to enable the Signalman to verbally inform the Guard in rear and Driver of an engine assisting in the rear, if any, of the failure. After the train has thus been brought to a stand for the Signalman to communicate with the Guard and Driver of the train has thus been brought to a stand for the Signalman to communicate with the Guard and Driver of an engine assisting in rear, the Driver must not start again until he receives a green hand signal from the Signalman.

(ii) The Signalman at whose Box the block instruments and/or bells have failed must advise the Signalman at the box at the other end of the section of the failure by speaking instrument. When speaking instruments are not available TRAINS MUST BE DEALT WITH IN ACCORDANCE WITH the system for Time Interval Working as shewn in clause (a) (iv) of this regulation.

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gnalstrum for The following to be inserted as the second paragraph of clause (a) (i):-

When a Driver has been stopped at a Signal Box and advised by the Signalman of the failure of instruments or bells, or of a track circuit controlling the block instruments, the Driver must draw has train forward and bring it again to a stand with the brake van near to the Signal Box to enable the Signalman to verbally inform the Guard in rear and Driver of an engine assisting in the rear, if any, the brake van Driver of an engine assisting in the rear, if any, the future. After the train has thus been brought to a stand for the Signalman to communicate the Guard and Driver of an engine assisting in rear, the Driver must not start again until he receives a green hand signal from the Signalman. O.M.11869.) (G.A. 5.-2/39.

#### Time Interval Working.

(v) When the speaking instruments are not available TIME INTERVAL WORKING MUST BE PUT INTO OPERATION. THE DRIVER OF THE FIRST TRAIN THAT IS BEING CAUTIONED MUST BE HANDED A COPY OF FORM 4981, PROPERLY FILLED UP AND SIGNED AND BE INSTRUCTED TO STOP AT THE SIGNAL BOX TO WHICH IT IS ADDRESSED AND HAND THE FORM TO THE SIGNALMAN THERE.

A train must not be allowed to follow another train until the time usually taken by the preceding train to clear the section, after allowing for the train having been stopped, has elapsed, but in no case with a less interval than three minutes. When a tunnel intervenes in a block section, an interval of not less than ten minutes must be allowed between two trains, unless the Signalman can satisfy himself that the tunnel is clear.

(v) In the event of a partial failure where some means of communication between the boxes is available for signalling purposes the Driver of the first train travelling on an adjoining line must be advised of the circumstances, and instructed to proceed cautiously through the section.

In cases where trains on one line have to be worked on the Time Interval system in consequence of the failure of all communication, all trains on the next adjoining line or lines must be stopped and the Driver of each train instructed to proceed cautiously. Trains on the next adjoining line need not, however, be stopped and cautioned on this account when the failure occurs on a line not worked in accordance with the Block Regulations.

(b) Steps must be taken immediately to have the apparatus put into working order and when the failure has been remedied and the apparatus is again in working order, OR SHOULD SPEAKING COMMUNICATION ONLY BE RESTORED, the Driver of the next train allowed to proceed through the section must be cautioned and supplied with a notice on FORM 4810 intimating that the train carrying this notice will be the last train to work on the Time Interval System through the Section and he must also be instructed to stop at the next signal box and hand this notice to the Signalman. The Signalman receiving this notice must then give the "Train out of Section" signal in accordance with Regulation 10 and the ordinary method of signalling or signalling by speaking instruments will be resumed.

When the train is worked by two engines in front or is assisted by an engine or engines in rear, the notice must be shewn to all Drivers on the train and carried by the rearmost Driver.

(d) When the block instruments and/or bells HAVE failed and there is a level crossing in the Section provided with block indicators and/or bells, but WHICH is not a block post, the Signalman if telephonic communication with the crossing is available, must inform the person in charge of the crossing that the block indicators and/or bells, as the case may be, are not in working order.

If there is no telephonic communication with the crossing, or the telephone has failed, the Signalman must instruct the Driver of every train proceeding in the direction of the crossing during the failure to approach the level crossing cautiously, sound the engine whistle and be prepared to stop short of any obstruction at such crossing. The time interval must also be extended sufficiently to allow for the additional time likely to be occupied in carrying out this instruction.

WHEN IT IS KNOWN THAT THE ELECTRICAL COMMUNICATION AT A LEVEL CROSSING IS NOT IN WORKING ORDER THE DISTANT SIGNALS (IF SUCH ARE PROVIDED) PROTECTING THE CROSSING MUST BE KEPT AT CAUTION UNTIL INFORMATION HAS BEEN RECEIVED THAT THE COMMUNICATION IS AGAIN IN WORKING ORDER.

IF DISTANT SIGNALS ARE NOT PROVIDED, GREAT CARE MUST BE TAKEN TO ASCERTAIN, AS FAR AS POSSIBLE THAT NO TRAIN IS APPROACHING THE CROSSING IN EITHER DIRECTION BEFORE THE GATES ARE OPENED TO THE PUBLIC.

(e) When trains are being worked on the time interval system, clause (a) paragraph (iv), all trains must be brought within the protection of the Home Signal as promptly as possible, and to obviate a train standing with its rear portion outside the Home Signal, the Signalman must, if necessary, authorise the Driver to draw forward a sufficient distance to bring the rear portion within the home signal AS PROVIDED FOR IN RULE 38 (b) (iii).

If a train requires to stand outside a Home Signal for the purpose of attaching or detaching traffic, or through any other cause, the Signalman must if practicable, unless he has ascertained from the Driver that he is in possession of the train staff, obtain the assistance of a Handsignalman provided with the necessary detonators and hand signals, who must be sent out a sufficient distance from the rear of the train to afford protection. Until this Handsignalman has been provided, a train must not be detained outside the Home Signal to attach or detach traffic.

(G.A.18. 11/47. O.M. 12044 and 12248.)

#### REGULATIONS FOR WORKING ON SINGLE LINES BY TRAIN STAFF AND TICKET.

1. (a) A train staff, or train ticket indicating that the staff will follow (see page 54 for specimen), must be carried with each train, and no train must be permitted to leave any staff station with a train staff ticket, unless the staff for that portion of the line over which it is to travel is then at the station.

Train staff or ticket to be carried. No train to leave unless staff is at the station.

(b) Except as provided in Regulations 17 and 20, clause (a), a Driver will render himself liable to dismissal if he leaves a staff station without the staff or ticket for the section over which he is about to run, or if he leaves with a ticket, without having first seen the proper staff.

Penalty for Driver leaving without staff or ticket.

The person in charge of the staff working will render himself liable to severe punishment should he contribute to any irregularity in the staff working.

Irregularity In staff working.

2. Each staff has engraved or marked on it the name of the staff station at each end of the section to which only it applies. The staffs, ticket boxes and tickets for adjoining sections are of different colours, and the staffs are different in shape.

Description of staffs, boxes and tickets

Place for staff when at station 3. The staff, when at the station, must not be kept in the ticket box, but in the place provided for it.

Tickets to be kept in box of which the staff is the key.

- 4. (a) The tickets must be kept in the proper ticket box fastened by an inside spring, the key to open the box being the staff, or a key attached to the staff, for the same section as the box, so that if the ticket box is kept locked, for which the person in charge of the staff working will be held strictly responsible, access to the tickets cannot be obtained unless the proper staff for the section is then at the station.
- (b) Only one ticket must be taken from the ticket box at a time except as provided for in Regulation 14. The box must be locked after a ticket is taken out (or two tickets in the case of an assisted train) and not again opened until it is necessary to obtain another ticket for a following train.

Spare tickets to be locked up. (c) All spare tickets must be kept under lock and key, and in the care of the person in charge of the station.

Custody of staff or ticket, 5. The person in charge of the staff working for the time being is the sole person authorised to receive and deliver the staff or ticket.

Fouling of single line.

6. (a) Except as provided in Regulation 15, clause (d), permission must not be given for a train to approach on the single line when there is any obstruction outside the home signal, nor must the single line be fouled outside the home signals after permission has been given for a train to approach.

(b) A Driver must not, under any circumstances, foul the single line for shunting purposes unless he has received the authority of the Signalman to do so.

Train not to shunt for another except at staff station. Shunting movements.

- 7. (a) No train must shunt for another train to pass except at a train staff station.
- (b) When a shunting movement is being made on a loop line at a crossing place the home signal applicable to a train approaching on the single line in the opposite direction must not be lowered, unless there are points which are set to prevent the shunting movement fouling the single line, until the shunting movement has come to a stand and the Driver has been instructed that no further movement towards the starting signal must be made.

When a home signal at a crossing place has been lowered for an approaching train a shunting movement must not be made towards the starting signal applicable to the opposite loop until the train approaching on the single line has passed clear on to the loop line, unless there are points which are set to prevent the shunting

movement fouling the single line.

Working of

8. (a) The normal indications of fixed signals at staff stations and signal boxes must be as under:—

# Stop signals—DANGER Distant Signals—CAUTION

and before any signal is lowered care must be taken to see that these and other relevant regulations have been complied with, and (except as otherwise provided in the Regulations for Train Signalling on Single Lines by the Train Staff or Train Staff and Ticket Block system) it has been ascertained that the line on which the train is about to run is clear.

- (b) At all other places, except where instructions are issued to the contrary, the fixed signals, where provided, must be maintained in the Clear position, unless required to be placed at Danger for the protection of trains having to stop in the section, or for the protection of any other obstruction on the running line.
- (c) When trains which have to cross each other are approaching a staff station in opposite directions the signals in both directions must be kept at Danger and when the train which has to be first admitted into the station has been brought to a stand, the home signal applicable to such train may be lowered to allow it to draw forward to the station or to the starting signal, and after it has again come to a stand and the Signalman has seen that the line on which the other train will arrive is clear, the necessary signals for that train may also be lowered.
- (d) At staff stations the signal controlling the entrance of trains into the section ahead must not be lowered until the staff is available, or a ticket has been obtained, for the train to proceed into that section, or Pilot-working is in operation and the Pilotman is present.

Points to be padlocked or securely held. 9. All points not interlocked must be padlocked or securely held by hand for the safe passage of trains in the facing direction.

10. (a) Points giving communication between sidings and the running line controlled by the train staff cannot be opened without the train staff for that section of the line where the siding is situated, and the staff cannot be removed until the points have been placed and locked in the proper position for trains to pass upon the running line, and to prevent vehicles passing from the sidings on to the running line.

- (b) When shunting has to be performed at a siding, the points of which are controlled by the train staff, the Driver must hand the staff to the Guard or man in charge of the siding to enable the points to be unlocked. When shunting has been completed, and the points have been placed and locked in the proper position for trains to pass upon the running line, the Guard or man in charge of the siding must return the staff to the Driver, and the latter must not proceed on his journey until he has obtained possession of it.
- 11. (a) No train must be permitted to leave a staff station until the Driver has received the proper staff or ticket for that section of the line over which he is about to travel, and he must not take the staff or ticket from any other than the person in charge of the staff working for the time being. After receiving the staff or ticket he must not start until the proper signals have been exhibited. On arriving at the station to which the staff or ticket extends such staff or ticket must immediately be given up to the person in charge of the staff working. All tickets so given up must be at once cancelled by the word "Cancelled" being written across them, and the tickets must afterwards be dealt with in accordance with the instructions of the Divisional Superintendent or District Traffic Manager.

(b) The Driver must be careful not to take the staff or ticket beyond the staff station at which it should be left.

Driver not to start until he has received the staff or ticket.

Staff or ticket to be given up.

Driver not to take staff or ticket beyond proper staff station. When staff is to be given to Driver.

When ticket is to be given to Driver.

12. (a) When a train is ready to start from a station, and no second train is intended to follow before the staff will be required for a train in the opposite direction, the person in charge of the staff working must give the staff to the Driver or Fireman, and the Driver must see that it is placed in a safe position on the engine.

(b) If other trains are intended to follow in succession before the staff can be returned, a ticket must be given by the person in charge of the staff working to the Driver or Fireman of the first train; the staff for the section must be shown to the Driver, and he must take charge of the ticket. The same procedure must be followed with any other train except the last, the staff itself being given to the Driver or Fireman of the last train. The person in charge of the staff working after handing the ticket to the Driver or Fireman must satisfy himself that the train has proceeded with such ticket and that the line is clear in accordance with the Regulations before he allows another train to follow. After the staff has been sent away no other train must, except as provided in Regulations 17 and 20, clause (a), leave the station to follow in the same direction until the staff for that section has been returned.

13. The person in charge of the staff working must consider it his first duty to deal with the train staff or ticket on arrival of the train, and at crossing-places must satisfy himself that the train running in the one direction has arrived complete with tail lamp attached before handing the staff or ticket to the Driver or Fireman of the train about to travel in the opposite direction.

Staff or ticket to Driver going in one direction until train from other direction has arrived complete. Assisted trains.

14. When a train is worked by more than one engine, or two or more engines are coupled together and such train or engines have to carry the staff, each engine except the rearmost must carry a ticket and the rearmost engine the staff. In cases where the train is to be followed by another train the rearmost as well as the leading engine, or engines, of the first train must carry a ticket. In such cases when a Driver or Fireman is handed a ticket the Driver must be shown the staff. When an assisting engine is allowed, by the authority of the Superintendent of the Line, to return to the station from which it started, without running through the entire section, it must always carry the staff.

15. (a) When a ballast train has to work on the line, the staff only must be given Ballast train at to the Driver in charge of it. If a ballast train is required to run over a section of single line from one staff station to the other without stopping to work on the way it may travel with staff or ticket as required. Under no circumstances must a ballast train stop to do work on the line unless the Driver is in possession of the staff.

(b) Should the Guard of a ballast train require his train to return to the staff station in rear instead of going through to the staff station in advance he must obtain the permission of the Signalman before the train enters the section.

(c) The Driver of a ballast train that has to do work on the line, must be told, when receiving the staff, to which end of the section it is to be taken, and at what time it is to be there, in order to clear the line for the next train,

(d) When a ballast train working in the section has to return to the staff station in rear, no shunting outside the home signal at that end must be allowed until a man with hand signals and detonators has been sent out to protect such shunting.

Wrong staff.

16. Should the person in charge of the staff working receive a wrong staff he must return it by the most expeditious means; but a train or trolley must not be used for the purpose unless the person returning with the wrong staff is also in possession of the proper staff.

Staff lost or damaged.

Working by Pilotman. 17. (a) Should a staff be lost, or damaged so that it will not open the ticket box, the Station Master or other responsible person in charge of the staff working at each end of the section must communicate with each other by the most expeditious means, and make arrangements to conduct the traffic over the section to which the staff belongs by Pilotman until the staff has been found or repaired, or a relief train staff and ticket boxes have been supplied; block working, where in operation, being maintained or the provisions of Block Regulation 25; "Failure of Instruments or Bells," clause (a) observed. The Lineman must be sent for immediately if necessary.

(b) The Station Master or other responsible person at each end of the section must agree as to who shall arrange for Pilot-working, and have a clear understanding

as to the arrangements to be put into operation.

If the staff is not available, it will be generally found more expeditious for the Station Master or other responsible person at the opposite end of the section to that at which a train is next required to enter the section to undertake the arrangements, as the Pilotman with Pilot-working forms will then only have to go through the section in one direction to get the necessary forms signed.

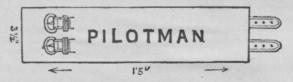
If the staff has been damaged it should be retained to institute working by Pilotman, who must take possession of it, and until required by the Lineman or replaced by relief staff and boxes the damaged staff must be shown by the pilotman

to the Driver or Drivers of each train passing over the single line.

Should, however, a train be waiting to proceed it need not be detained for Pilotworking to be established, unless it is necessary to do so to avoid delay, but must be sent away with the damaged staff.

Pilotman's armlet.

(c) A competent person must be appointed as Pilotman who must wear, round his left arm above the elbow, a red armlet with the word "Pilotman" shown thereon in white letters, thus:—



If this armlet is not immediately available the Pilotman must wear a red flag in the position indicated until the proper armlet is obtained.

Issue of forms.

- (d) The person arranging Pilot-working must fill up, sign, and address Pilot-working forms—(see page 55 for specimen) to:—
  - (i) The Signalman at each end of the staff section.
  - (ii) The Signalman at any signal box that is intermediate in the staff section provided such signal box is open or likely to be open during the time Pilotworking is in operation.
  - (iii) The person who will act as Pilotman.
  - (iv) The Station Master at each end of the staff section, except where the signal box at which Pilot-working commences or finishes is not at a station and the ordinary working at the station will not be interfered with.

These forms must be handed to the Pilotman who must also sign all the forms issued and deliver the necessary form to the Signalman in charge of the signal box at which the Pilot-working commences and then proceed to the other end of the section.

The Pilotman when proceeding to the other end of the section to deliver the forms must do so as quickly as possible, either by rail or road, using the best means at his disposal for the purpose, but must not, unless the train staff is in his possession, use an engine or any railway vehicle other than a trolley.

The Pilotman must leave the necessary form with the Signalman at any intermediate signal box and must advise the Station Master at any intermediate station that Pilot-working is in operation.

On arrival of the Pilotman at the other end of the staff section he must deliver the necessary forms to the Station Master and Signalman, and if in possession of the main staff show it to the Signalman.

Each person when receiving the form must sign the Pilotman's form.

When a Station Master himself acts as Pilotman he must retain only the Pilotman's form, and unless his station comes within the exception mentioned in section (iv) of this clause (d), he must address and give a form to the person he leaves in charge this station.

Should any intermediate signal boxes or stations be opened after Pilot-working as commenced, the Pilotman must, as soon as practicable, advise the persons in the darge of such places that Pilot-working is in operation. He must also hand forms agreed by the person who arranged the Pilot-working and himself, to the Signalman concerned, who must sign the form held by the Pilotman.

(e) Should the speaking instruments have failed and the train staff be lost or damaged the Station Master or person in charge at each end of the section must arrange for Pilot-working, and the Pilotman appointed at each end must proceed along the railway in order that they may meet, and on doing so they must go together to the nearer or more convenient end of the staff section. The Pilotman who returns to the station from which he started must collect the forms which had been delivered at that end of the section, and return them and the other forms in his possession to the person who filled them up, and the latter must at once cancel them by writing the word "Cancelled" across them. The other Pilotman must hand one of his forms to the Station Master or person in charge and one to the Signalman, and then are as Pilotman.

Failure of speaking instruments.

When practicable the Post Office telephones should be used to arrange Pilotworking instead of appointing two men as laid down in the preceding paragraph.

(f) Station Masters and persons in charge issuing and receiving Pilot-working forms will be responsible for the Inspectors, Signalmen, and others concerned at their stations being made acquainted with the circumstances immediately, and instructed in their necessary duties.

Twelve Pilot-working forms must be kept in a convenient place at each staff station, so as to be available at any time.

(g) After all the forms are signed in accordance with clause (d) of this Regulation, trains may be allowed to pass through the section by the permission and under the control of the Pilotman. Where block working or Block Regulation 25, "Failure of Instruments or Bells," clause (a/iii), is in operation the Pilotman must obtain the permission of the Signalman before allowing a train to enter the section.

Pilotman obtain ing Signalman's permission for trains to enter section.

The Pilotman must inform the Driver or Drivers and the Guard of each train of the circumstances, and when practicable must accompany every train. When a train is to be followed by one or more trains in the same direction before a train has to be started from the other end, the Pilotman must personally order each train to proceed and must ride upon the ergine of the last train. The Regulations applying to a train carrying the train staff will apply to a train accompanied by the Pilotman and the Regulations applying to a train carrying a ticket will apply to a train instructed to proceed on the Pilotman's authority but not accompanied by him.

- (h) Signalmen must not allow any train to pass into any section that is being worked by Pilotman unless he is present.
- (i) Should it be necessary to change the Pilotman or should the Signalman be changed during Pilot-working the provisions of Rule 205 must be observed.
- (j) When ordinary working can be resumed the Pilotman must withdraw the Pilot-working forms at one end of the section, then take the staff, if available, to the other end of the section, and after delivering it to the Signalman there and withdrawing the Pilot-working forms at that end of the section, the traffic will be again conducted in accordance with the Regulations. All forms which have been issued for Pilot-working must be collected and cancelled by writing the word "Cancelled" arcss them, afterwards being sent to the Divisional Superintendent or District Traffic Manager.

After the Pilot-working forms have been issued ordinary working must not be seemed until such forms have been withdrawn and cancelled, although the missing has been subsequently found or the damaged staff repaired.

Change of Pilotman or Signalmen. Resumption of ordinary working.

When missing staff is found.

(k) If the missing staff be found it must be handed to the Station Master or person in charge of the staff working at either end of the section to which it applies, who must, if the staff be undamaged, make arrangements for ordinary working to be resumed as provided for in the preceding clause. Before the regular staff is brought into use the relief staff and ticket boxes, if supplied, must be withdrawn.

Working of siding connections.

(l) In the event of failure of the apparatus in a section in which there are siding connections controlled by staff or ticket and it is necessary for trains passing through the section to stop at the sidings for traffic purposes the Pilotman must request the Lineman to accompany him with the train to the sidings and arrange for the points to be released to enable the shunting to be performed; the Pilotman must remain with the Lineman at the points until they have been restored to their normal position.

Train carrying staff disabled.

18. (a) In the event of a train which carries the staff becoming disabled between two staff stations, the Fireman must take the staff to the staff station at which assistance can be obtained, inform the person in charge there of the circumstances and show him the staff. Except as shown in the following paragraph, the Fireman must personally hand the staff to the Driver of the assisting train, and conduct the assisting train to the disabled train.

Should it be found on arrival of the Fireman at the staff station that assistance can be obtained more readily at the opposite end of the section the staff must be transferred to that end of the section by the most expeditious means. If time would be saved by the staff being so transferred by a competent person other than the Fireman, the Fireman may give up the staff for this purpose and then return to his train. The person in charge must arrange for the staff to be conveyed to the opposite end of the section and advise the Signalman there that the staff is being so transferred,

Train carrying ticket disabled.

- (b) (i) Should the train that fails be carrying a ticket instead of the staff assistance must, except as provided in paragraph (ii) or (iii) of this clause, only come from the station at which the staff has been left. If the box ahead be the nearer, the Fireman may, instead of going in the direction of the station where the staff is, go to the box ahead so that the Signalman there may advise the Signalman in rear that assistance is required, and if assistance is obtained from the rear in such circumstances the Guard must ride on the assisting train and point out to the Driver the position of the disabled train. The Fireman must take the ticket with him when he goes for assistance.
- (ii) Should a train carrying the staff or a ticket be following the disabled train and have left the staff station in rear, assistance must only be given by such following train, or the engine thereof, and the staff must not in these circumstances be transferred to the staff station in advance as described in paragraph (iii). If the engine of the following train is detached for the purpose of rendering assistance, the Driver of such engine must not, if he is in possession of a ticket, after the disabled train has been removed from the section return to his own train without the written authority of the Guard or Signalman, as the case may be, as prescribed in Rule 183 or 184.
- (iii) If assistance can be more readily obtained at the staff station towards which the train was proceeding, immediate steps must, except as shown in paragraph (ii), be taken to have the staff transferred from the rear staff station to that end of the section, and the Fireman must accompany the assisting train to the disabled train.

Disabled train not to be moved.

(c) The Driver of the disabled train must not allow it to be moved until either the Fireman has returned and the assisting train has arrived with the staff or ticket as the case may be, or, when arrangements have been made for the staff to be transferred to the opposite end of the section as provided in the second paragraph of clause (a), until the assisting train has arrived with the staff.

Station Master responsible for arrangements. (d) The Station Master at the station where assistance is provided will be responsible for carrying out all necessary arrangements during the continuance of the obstruction.

Protection of disabled train.

(e) The disabled train must be protected in accordance with the Rules applicable to trains stopped by accident, failure, obstruction or other exceptional cause, the Fireman doing this on his way for assistance, and the Guard in the opposite direction. In the case of a light engine the Fireman must protect the engine as directed when. going for assistance, but before doing so must protect the engine in the opposite direction by putting down 3 detonators on the line 10 yards apart not less than 300 yards from the engine.

In the assisting train or engine must run at reduced speed and great caution Caution to be must be observed by all concerned.

19. (a) When a train has to be left, or divided and the rear portion left, on Intentional the single line, owing to accident or inability of the engine to take the whole forward, on single line. or from any other cause, the following instructions must be observed :-

- (i.) The man who divides the train must inform the Driver how many vehicles, if any, are being taken forward, and after sunset or during fog or falling snow, or if the division is made in a tunnel, he must place ON THE RAIL 3 detonators 10 yards apart not less than 100 yards ahead of the portion left behind.
- (ii.) The Driver must not, if he is in possession of a ticket, return for the train or rear portion without the written authority of the Guard as prescribed in Rule 183, clause (f). If the Driver is in possession of the staff he may return to the train or rear portion without obtaining instructions from the Guard authorising him to do so. The Driver must, before returning to the rear portion of the train, satisfy himself that the front portion is complete.
- (iii.) The Guard must protect his train in the rear in accordance with the Rules applicable to trains stopped by accident, failure, obstruction or other exceptional cause.
- (b) When a train or portion of a train has been accidentally left on the single line, the following instructions must be observed :-

Accidental division of train on single line.

- (i.) If the train has become accidentally divided between two signal Loxes and the front portion has not arrived at the home signal for the box in advance, the front portion may be set back to the rear portion in accordance with Rule 183,
- (ii.) The Driver must (except in the case described in preceding paragraph) take the front portion forward to the nearest place where it can be disposed of.
- (iii.) The Guard must secure the train or rear portion thereof and place 3 detonators on the line 10 yards apart not less than 100 yards ahead of the portion left behind. He must then protect his train in the rear in accordance with the Rules, whether the Driver is in possession of the staff or a ticket.
- (iv.) Except as otherwise provided for in the following paragraph, the Driver must not, whether he is in possession of the staff or a ticket, return for the train or rear portion thereof, until it has been ascertained that the whole of the vehicles of the train have come to a stand, and, if there is no signal box near from which this information can be obtained, the Driver must send his Fireman on foot for the purpose. If the Driver is in possession of a ticket he must not return for the train or rear portion until he has received written authority from the Guard as prescribed in Rule 183, clause (f).

If the gradients of the line over which the train has run are such that it can be concluded that the train or rear portion is at a stand at the time when the engine requires to return, the Driver may, if in possession of the staff, return for his train or rear portion thereof, without having ascertained that the rear portion has been secured, but must proceed cautiously, and if approaching a signal box he must bring his engine to a stand at the distant signal and the

Fireman must walk in front from there to the home signal.

(v.) If the divided train has carried the staff and it is necessary to allow an engine to enter the section at the signal box in rear of the divided train for the purpose of removing the obstruction to the more convenient end of the section, the staff must be conveyed from the signal box in advance to the other end of the section by the most expeditious means, but for this purpose an engine or any railway vehicle other than a trolley must not be used. If necessary the train staff may be transferred in accordance with Regulation 18, clause (a).

(c) The Driver when returning for the portion of the train that has been left Driver returning behind must not pass any signal box without the permission of the Signalman.

(d) The Driver must retain possession of the Staff or Ticket as the case may be until the whole of the train is removed from the section, unless it is necessary for another engine to remove the rear portion. If, however, it is necessary to dispose of the front portion at a station ahead of the next Staff station, the Driver must give up the Staff or Ticket and must not enter the occupied section for the rear portion until he has again received the Staff, or Ticket, as the case may be.

Driver to retain n possession of Staff or Ticket

(G.A. 18. 11/47. R.C.H. Op. Supts. Min. 7674.)

Assistance from rear.

(e) A white light must be placed on the leading vehicle of the rear portion before that portion is propelled to the signal box in advance or drawn back to the signal box in rear.

Line obstructed.

20. (a) Should an accident occur of such a nature as to block the line, and the traffic is likely to be stopped for any considerable time, special arrangements must be made for working the trains to and from the staff station on each side of the obstruction.

The staff must be retained to work trains between the point of obstruction and the staff station on one side, no tickets being used; on the other side Pilot-working must be arranged in accordance with Regulation 17 and the Pilotman must accompany each train to and from the point of obstruction.

The person arranging Pilot-working must also issue a Pilot-working form for the man in charge at the point of obstruction and the Pilotman must deliver the form.

(b) Block working, where in operation, must be suspended and two competent men, provided with hand signals and detonators, must be appointed to protect the obstruction, one on each side.

When line again clear.

(c) When the line is again clear, no train must be allowed to pass the point where the obstruction existed without the staff. The Pilotman must accompany the first train carrying the staff to the staff station, and, after the Pilotman has withdrawn his arrangements for Pilot-working, ordinary working may be resumed.

Staff Stations shown in Appendices to Service Time Tables. 21. THE APPENDICES TO THE SERVICE TIME TABLES WILL CONTAIN A LIST OF THE STAFF STATIONS, AND OF THE SIDING POINTS CONTROLLED BY THE TRAIN STAFF.

FORM OF TRAIN STAFF TICKET.

	GREAT WESTERN RAILWAY.
	TRAIN STAFF TICKET.
	Line or Branch.
To the D	river oftrain from
You are a	authorised, after seeing the train staff for the section, to proceed
Signature of p	erson in charge
Station	

(Back of Ticket.)

This ticket must be given up by the Driver to the person in charge of the staff working at the place to which he is authorised to proceed, immediately on arrival.

### EXTRACTS FROM TRAIN STAFF AND TICKET REGULATIONS-Continued PILOTMAN'S FORM.

[Form referred to in Regulations 17 and 20 of the Regulations for working on Single Lines by Train Staff and Ticket System, and in Regulations 11 and 13 of the Regulations for working on Single Lines by one engine in steam or two

# or more engines coupled together.] GREAT WESTERN RAILWAY. TRAIN STAFF AND TICKET SYSTEM, OR ONE ENGINE IN STEAM, &c. WORKING OF SINGLE LINES BY PILOTMAN-(a) WHEN STAFF IS LOST OR DAMAGED (b) DURING OBSTRUCTION This Form must be filled up and used whenever it is temporarily necessary to work the traffic by Pilotman. (a) The train staff for the section between..... is present and personally orders the train to start. In the case of lines worked by one engine in steam, etc., he must accompany every train over the section. Block working, where in operation, must be maintained or the provisions of Block Regulation 25, clause (a), observed. station and the point of obstruction. Block working, where in operation, must be suspended. This order is to remain in force until withdrawn by the Pilotman. (Signed)..... ..... Noted by at. Noted by at. Noted by at. \* Strike out portions (a) or (b) not applicable.

FORM OF PILOTMAN'S TICKET WHERE IN USE.

#### TRAIN STAFF AND TICKET SYSTEM.

#### PILOTMAN'S TICKET.

To be used when it is necessary to work the traffic of a single line by Pilotman when the Train Staff is lost, or so damaged that it will not open the ticket bo .

To the Driver of......train from..... You are authorised to proceed from.....to..... Pilotman following.

Signature of Pilotman..... Date . . . . . . . . .

This Ticket must be given up by the Driver to the person in charge of the staff working at the place to which he is authorised to proceed, immediately on arrival.

# REGULATIONS FOR WORKING ON SINGLE LINES BY ONE ENGINE IN STEAM OR TWO OR MORE ENGINES COUPLED TOGETHER.

(Where train signalling by the block system is in operation on these lines the Regulations for Train Signalling on Single Lines by the Train Staff or Train Staff and Ticket Block System will apply.

Only one engine in steam or two or more coupled together, to be on line at a time. 1. Only one engine in steam, or two or more coupled together, which are then to be treated as one engine or train, must be allowed to be on the line at a time.

Train staff.

2. A train staff is provided, which has engraved or marked on it the name of the staff station at each end of the section to which only it applies, and an engine must not be permitted to enter upon the Single Line unless the Driver is in possession of this staff. When, however, a train is worked by two or more engines the staff must be shown to the Driver of each engine and handed to and carried by the Driver of the rearmost engine.

Staff to be given to Driver before commencing journey and given up at end of journey. 3. The staff must be shown or given to the Driver, as directed in Regulation 2, by the person in charge of the staff working immediately before commencing each journey. At the end of the journey the Driver must give up the staff to the person in charge of the staff working at that place, and be careful not to take the staff beyond the station at which it should be left.

Penalty for Driver leaving without staff. 4. Except as provided in Regulations 2, 11 and 13, a Driver will render himself liable to dismissal if he leave a staff station without the proper staff.

Place for staff.

5. The staff, when at the station, must be kept in the place provided for it; when on the engine it must be carried in a safe position.

Custody of staff.

6. The person in charge of the staff working for the time being is the sole person authorised to receive and deliver the staff.

Driver not to start until proper signals exhibited 7. A Driver after receiving the staff, must not start until the proper signals have been exhibited.

Train to arrive complete before another train enters section. 8. The person in charge of the staff working must satisfy himself when a train arrives that it is complete with tail lamp attached, before he allows another train to enter the section.

Points to be padlocked or securely held. Siding points controlled by train staff. 9. All points not interlocked must be padlocked or securely held by hand for the safe passage of trains in the facing direction.

When shunting has to be performed at a siding the points of which are controlled by the train staff, the Driver must hand the staff to the Guard or man in charge of the siding to enable the points to be unlocked. When shunting has been completed, and the points have been placed and locked in the proper position for trains to pass upon the running line, the Guard or man in charge of the siding must return the staff to the Driver, and the latter must not proceed on his journey until he has obtained possession of it.

Wrong staff.

10. Should the person in charge of the staff working receive a wrong staff he must return it by the most expeditious means; but a train or trolley must not be used for the purpose unless the person returning with the wrong staff is also in possession of the proper staff.

Staff lost

11. Should a staff be lost, the Station Master or other person responsible in charge of the staff working at each end of the section must communicate with each other by the most expeditious means, and make arrangements to conduct the traffic over the section to which the staff belongs by Pilotman in accordance with the instructions contained in Regulation 17 of the Regulations for working on Single Lines by Train Staff and Ticket so far as they apply. (For specimen Pilot-working form see page 55.)

Working by Pilotman.

The Pilotman must accompany every train over the section.

Disabled train

12. In the event of a train becoming disabled and assistance being required, the Fireman must confer with the Guard as to the best means of obtaining assistance, then take the staff to the station whence assistance can be obtained, inform the person in charge there of the circumstances and show him the staff. The latter, on receipt of such information, may allow an assisting train to enter upon the single line. The Fireman must personally hand the staff to the Driver of the assisting train, and conduct the assisting train to the disabled train.

The second sentence of clause 5 (b) to be amplified to read:

In addition all facing points not equipped with a facing point lock (with facing point lock bar or track circuit locking) must be clipped and padlocked, the keys being kept by the Officer or other person in charge of the operations.

(G.A.16. 5/46.O.M.12278)

Clause 8 (b).—As soon as a train has passed on to the loop line and inside the catch or siding points the Guard (or Fireman in the case of a light engine) must advise the Signalman by the quickest means, either by telephone or by the exhibition of a hand signal by day, or a white light by night or during fog or falling snow, to indicate that the train is in clear. The Guard's hand-signal must be given from the brake-van and the Fireman's from the footplate. The Guard (or Fireman) must continue to exhibit the hand signal until it is acknowledged by the Signalman who will, at night or during fog or falling snow, exhibit a white light held steadily. (G.A.24.—11/49.)

(c) During fog or falling snow, every train must be stopped dead before entering the section, whether occupied or not. In clear weather, if the loop line is unoccupied, a train may run direct into the loop line at a speed not exceeding 10 miles per hour.

G.A.3.—12/37. L.K.1/4513/13.)

The following to be added as Clause 9 (d):-

Trains which are admitted to a permissive line immediately following a Diesel car must be brought to a stand at the signal box in accordance with clause 9 of the Permissive Block Regulations and the Driver verbally told that a Diesel car is in the section ahead. The Driver of a train which is so admitted must proceed with caution and must take care not to buffer up to the Diesel car.

(G.A.3—12/37. O.M.11767.)

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# REGULATIONS FOR WORKING OF SINGLE LINES-Continued.

The disabled train must be protected by 3 detonators, 10 yards apart, being placed on the line not less than 100 yards from the train by the Fireman on his way

The Driver of the disabled train must not allow it to be moved until the Fireman returned and the assisting train has arrived with the staff.

Disabled train not to be moved.

The assisting train must run at reduced speed and great caution must be observed by all concerned.

Caution to be

The Guard of the disabled train will be responsible for the safe and proper working of the line until it is again clear.

13. (a) Should an accident occur of such a nature as to block the line, and the traffic is likely to be stopped for any considerable time, if necessary special arrangements must be made for working trains to and from the obstruction, where this is possible, the staff being retained to work trains between the point of obstruction and the staff station on one side; and on the other side Pilot-working being arranged as shown in Regulation 11.

Line obstructed.

The person arranging Pilot-working must also issue a Pilot-working form for the man in charge at the point of obstruction and the Pilotman must deliver this

(b) A competent person provided with hand signals and detonators, must be appointed to protect the obstruction, on one or both sides as required.

(c) When the line is again clear, no train must be allowed to pass the point where the obstruction existed without the staff. The Pilotman must accompany the first train carrying the staff to the staff station, and, after the Pilotman has withdrawn his arrangements for Pilot-working, ordinary working may be resumed.

again clear.

# EXTRACTS FROM THE REGULATIONS FOR SIGNALLING TRAINS AND ENGINES BY PERMIS-SIVE BLOCK SYSTEM OVER GOODS RUNNING LOOP LINES AND OTHER PERMISSIVE LINES.

- 1. The loop lines will be worked in accordance with the Standard Double Line Block Regulations, with the following modifications and additions:-
- 4. (a) More than one freight, mineral, or empty coach train or engine may be allowed to be in the block section at the same time.
- 5. (a) The goods loop line must not be used as a running line for passenger trains except when the usual running line is not available, either by reason of accident or failure, or because it is occupied by the Engineer by previous arrangement with the Divisional Superintendent or District Traffic Manager.
- (b) Whenever the goods running loop is used for passenger trains as set out above, absolute block working must be maintained, and passenger trains must only be accepted by the signal box in advance under the regulations applying to the main line. In addition, all facing points must be clipped and padlocked, the keys being kept by the officer in charge of the operations.

(c) A passenger train must never enter a goods loop unless the loop arm has been lowered, and the driver has received verbal instructions from the signalman or handsignalman to do so.

- 8. (b) As soon as a train has passed on to the loop line and inside the catch or siding points the Guard must exhibit a hand signal by day and a green light by night to the signalman to indicate that the train has gone in clear.
  - (a) Unless the section is clear every train before entering must be stopped at the signal controlling the entrance to the loop and the signalman must then lower that signal to allow the train to draw into the loop. In clear weather, if the loop line is unoccupied, a train may run direct into the loop line at a speed not exceeding 10 miles per hour.
  - (b) If the gradient of the loop falls more than 1 in 100 in the same direction as the train is running, every train must be stopped dead before entering the loop, whether there is another train in the loop or not.
  - (c) During fog or falling snow, every train must be stopped dead before entering the loop, whether occupied or not.
  - (d) Trains which are admitted to a permissive line immediately following a Diesel car must be brought to a stand at the Signal Box and the Driver verbally told that a Diesel car is in the section ahead. The Driver of a train which is so admitted must proceed with caution and must take care not to buffer up to the Diesel (G.A.9. 7/41. O.M. 12006.)
- 10. In all cases, irrespective of the state of the weather, Enginemen must regard the lowering of the signal to enter the loop only as an indication that the points are in a proper position, and must not expect that the road will be clear through the loop, and they will be held responsible for stopping

their trains short of any obstruction which may be in front of them.

After sumset or during fog or falling snow, in addition to the usual tail lamp, while trains are in the loop and side light must be exhibited on the side further from the Main Line and a white light on the side nearer the Main Line. If the Goods Loop runs alongcide a Relief Line and forms a third parallel line, trains on the Goods Loop must not exhibit any side lights.

(G.A.9. 7/41. O.M. 12005.)

The speed of trains passing over the loop line must never exceed 10 miles an hour, and during

fog or falling snow must not exceed 4 miles an hour.

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# EXTRACTS FROM REGULATIONS FOR SIGNALLING-GOODS LOOP LINES-Continued.

14. If it is desirable to back a train into the loop, this may be done provided there is no train travelling in the section in the right direction towards it, or that any train which has been admitted nas come to a stand.

15. If a train is already at a stand in the section, and it is necessary afterwards to shunt a train from the main line or siding to the loop line, the train on the loop line may be set back to admit the train from the main line, after the Guard of the rear train has gone back at least 300 yards or to the box in the rear or the entrance of the loop if within that distance, to protect it.

17. Trains stopped by accident, failure, obstruction or other exceptional cause must be protected in accordance with the provisions of Rules 178 to 188. If a second train arrive before the obstruction has been removed, the Guard of the second train must protect his train as directed in those Rules, and the Guard of the first train, having assured himself that the Guard of the second train has gone back with the necessary hand danger signal and detonators for the protection of the second train, may then rejoin his own train. If other trains arrive, the same arrangements must be carried out, the Guard of the last train providing the protection for the whole.

During fog or falling snow when trains are stopped at a Home Signal, the Guard of the first and each succeeding train must instantly go back with detonators and hand signals to protect it, and must proceed 100 yards, or to the box in the rear or the entrance of the loop if within 100 yards, and put down one detonator on the loop line, and at once return to his train. Should a train be assisted by an engine in the rear, the duty of so protecting the train will devolve upon the Guard in the same way as if no engine were assisting in the rear. In the case of a light engine the Fireman must, if the engine is stopped at a Home signal or in the rear of a train or another light engine, place one detonator on the rail as above directed. When there are two or more light engines coupled together, the Fireman of the rearmost engine must perform this duty. When a detonator, which has been put down as described above, is exploded by an engine, the Driver must be prepared to stop clear of the train or engine in front of him. Should the train or engine to the rear of which the detonator was placed, have moved forward, he must proceed cautiously so as to be able to stop at any moment. When a train is brought to a stand, the Driver must not start until the Guard has had time to return to his brake van.

In clear weather, if not required for shunting operations, the Guard instead of going back must be on the ground with detonators and hand signals in readiness to go back and stop any train which he may consider is approaching his own train at too great a speed. The Fireman must act similarly in the case of a light engine. In cases where train Guards are relieved by relief Guards, the latter will

be responsible for carrying out these instructions in the same manner as the train Guards.

The provisions of Rule 55 will not apply on lines operated under these Regulations. If the Inspector, or person in charge, finds it necessary to allow a train to remain on a loop line and to permit the Guard to leave the train, the Guard must, before leaving, place one detonator on the loop line 50 yards (or as far as possible up to that distance) in the rear of the train and, during darkness, see that the necessary side and tail lamps on his van are burning. The Inspector, or person in charge, must advise the Signalman controlling the entrance to the loop in order that the latter may

be aware of the circumstances and regulate the working accordingly.

20. If the loop line passes over a public crossing, the crossing must never be fouled by a train standing upon it, and if necessary Guards must divide their trains to allow vehicles or pedestrians to use the crossing. The Signalman on duty in the crossing box will be responsible for keeping the

crossing clear.

21. If any points worked by hand lead into or out of a goods loop line, such points must be locked

by a key which must be kept in the nearest signal box.

22. When the signal box in the rear is switched out of circuit, the loop may be used as a refuge siding from the signal box in advance, and when the box in advance is switched out the loop may be used as a refuge siding from the signal box in the rear.

If at the usual switching-out time there should happen to be a train standing in the loop, the signal-box (at either end) may be switched out, provided the train is not required to leave the loop at

that end.

# EXTRACTS FROM REGULATIONS FOR WORKING ON GOODS LINES WHERE THE ABSOLUTE BLOCK SYSTEM IS NOT IN OPERATION OR WHERE NO SPECIAL REGULATIONS ARE IN FORCE.

(NOT APPLICABLE TO SINGLE LINES OF RAILWAY).

Working of Signals.

1. Except where instructions are issued to the contrary, the normal indications of fixed signals must be :- Stop signals-Danger. Distant signals-Caution. and before any signal is lowered care must be taken to see that these Regulations

have been complied with. Interval between

2. UNLESS A SIGNALMAN IS SATISFIED THAT THE LINE IS CLEAR TO THE NEXT SIGNAL BOX, OR UNLESS INSTRUCTIONS ARE ISSUED TO THE CONTRARY, NO TRAIN MUST FOLLOW ANOTHER TRAIN ON THE SAME LINE WITHIN 5 MINUTES. SHOULD ARRIVE 5 MINUTES AND LESS THAN 10 MINUTES, AFTER THE PASSING OF THE PREVIOUS TRAIN, THE SIGNALMAN MAY, AFTER HAVING BROUGHT THE TRAIN TO A STAND, AND VERBALLY WARNED THE ENGINE DRIVER OF THE TIME OF THE PASSING OF THE PRECEDING TRAIN, ALLOW IT TO PROCEED, THE HOME SIGNAL BEING LOWERED AND A GREEN HAND SIGNAL SHOWN TO THE ENGINE DRIVER HELD STEADILY.

IN ANY CASE, UNLESS A SIGNALMAN IS SATISFIED THAT THE LINE IS CLEAR TO THE NEXT SIGNAL BOX, A FOLLOWING TRAIN MUST NOT BE ALLOWED TO ENTER THE SECTION UNTIL IT HAS BEEN BROUGHT TO A STAND AND THE DRIVER VERBALLY CAUTIONED. DRIVERS WILL BE HELD RESPONSIBLE FOR STOPPING THEIR TRAINS

SHORT OF ANY OBSTRUCTION.

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LEAR TO TER THE ERBALLY TRAINS The following note to be inserted on page 58 following clause 22:-

Note.—The above regulations are to be regarded as also applying to loops and goods lines controlled by one signal box insofar as they are applicable.

(G.A.12. 4/43. O.M. 12091.)

#### EXTRACTS FROM REGULATIONS FOR SIGNALLING-GOODS LINES-Continued.

When a signal is lowered for a train at a place where owing to the position of the signal box a green hand signal cannot be exhibited to the Driver, the Driver must proceed at such a speed as will enable him to stop short of any obstruction.

When a train is approaching the rear of another train, the Driver must proceed captiously and at such a distance as will enable him to avoid colliding with the train ahead, and he must not pass a signal which has been lowered for the train ahead until the signal has been replaced to Danger and lowered for his train.

When train

3. During fog or falling snow trains must travel at reduced speed and great Fog or falling caution must be observed.

4. (b) Should a train pass without a tail lamp, or if a Signalman observe or become aware of anything unusual in a train during its passage, such as goods falling off, a vehicle on fire, a hot axle box or other mishap, or should a train become divided or vehicles be running away, he must, if necessary, place and maintain his signals at Danger, and take any other measures that may be necessary and most expedient under the circumstances. He must also, if necessary, stop any following train or any train going in the opposite direction, and instruct the Driver as required so as to avoid danger in the event of the line on which he is running being obstructed. The Driver of the train so warned must caution the Driver of any train proceeding on the other line, and on arrival at the signal box in advance advise the Signalman of the circumstances. The Signalman must also, if necessary, communicate with the Station Master by the most expeditious means.

Train incomplete occurrence.

5. No engine or vehicle must be shunted or moved from one running line to the other, or from the running line into a siding or from a siding on to the running line, until the signals, where provided, protecting the movement, have been exhibited in one or both directions as may be required. The running line must not be obstructed by shunting operations when the distant signal has been lowered for a train until such train has passed or has come to a stand.

protected.

6. Trains stopped by accident, failure, obstruction or other exceptional cause must be protected in accordance with the provisions of Rules 178 to 188. If a second train arrive before the obstruction has been removed, the Guard of the second train must protect his train as directed in those Rules, and the Guard of the first train, having assured himself that the Guard of the second train has gone back with the necessary hand Danger signal and detonators for the protection of the second train, may then rejoin his own train. If other trains arrive, the same arrangements must be carried out, the Guard of the last train providing the protection for the whole.

Protection of train stopped by accident, failure, obstruction, or other exceptional

7. During fog or falling snow, when trains are stopped at a home signal the Guard to Guard of the first and each succeeding train must at once go back 100 yards, or to the Box in the rear or the entrance to the line if within 100 yards, and place one detonator on the rail to protect his train and then return to his brake. Should a train be assisted by an engine in the rear, the duty of so protecting the train will devolve upon the Guard in the same way as if no engine were assisting in the rear.

protect train during fog or falling snow.

In the case of a light engine the Fireman must, if the engine is stopped at a home signal or in the rear of a train or another light engine, place detonators on the rail as above directed. When there are two or more light engines coupled together the Fireman of the rearmost engine must perform this duty.

When a detonator, which has been put down, as described above, is exploded by an engine, the Driver must be prepared to stop clear of the train or engine in front of him. Should the train or engine to the rear of which the detonator was placed, have moved forward, he must proceed cautiously so as to be able to stop at any

When a train is brought to a stand, the Driver must not start until the Guard has had time to return to his van.

8. Trains conveying passengers must not be allowed to run on lines used for goods and mineral traffic only, except when it is necessary to divert them in case of accident or other emergency, when printed or written instructions must be issued to the Signalmen, and special arrangements made for the working of the trains. Such a train must not be allowed to leave the signal box in rear until it has been ascertained that the line is clear to the signal box in advance, and no train must be allowed to follow a train conveying passengers until it has been ascertained that the latter train has passed the signal box in advance. All facing points not equipped with facing point lock and bar must be secured by clip or scotch.

In order to obviate delay to trains, the speaking instruments may be used for the purpose of transmitting instructions for the working of passenger trains over these Goods lines, provided all such instructions before transmission are written and signed by the Station Master or person in charge of the special working arrangements, the messages to be repeated by the receiver to sender to ensure accuracy.

### REGULATIONS FOR WORKING ON SINGLE LINE BY PILOT GUARD.

Pilot Guard'sbadge. Pilot Guard to start all trains. 1. The Pilot Guard will be distinguished by a special badge; and a train must not under any circumstances be allowed to run on the line unless it is either accompanied or personally started by the Pilot Guard.

Pilot Guard, when practicable, to accompany every train. 2. The Pilot Guard must, when practicable, accompany every train, but when it is necessary to start two or more trains from one end of the section under his control before a train has to be started from the other end he must furnish the Guard in charge of each train not accompanied by himself with one of the printed Pilot Guard's tickets (see specimen on page 61) where provided, properly filled up and signed. He must also personally start such train, and himself accompany the last train. The ticket issued will apply only to the journey to the other end of the section where it must be given up immediately to the person in charge of the station. All tickets so given up must be cancelled at once by the word "Cancelled" being written across them, and the tickets must afterwards be dealt with in accordance with the instructions of the Divisional Superintendent or District Traffic Manager. The Driver must not start his train without seeing the Pilot Guard, and, when the Pilot Guard does not accompany the train, until he has received from the Guard of his train the Pilot Guard's ticket (where provided) authorising him to proceed. PLOT GUARD's TICKETS ARE NOT USED ON THE GREAT WESTERN RAILWAY. A Driver working an engine unaccompanied by a Guard must observe the same Regulations as herein laid down for a Guard with a train.

Driver working without Guard.

3. Before starting any train, the Pilot Guard must ascertain from the Guard of the train that all is right, and that he is ready to proceed.

Signalman not to allow train to proceed unless running by authority of Pilot Guard.

Starting of trains.

4. No train must be allowed to enter upon any single line section without the permission of the Signalman who must not allow it to proceed until he is perfectly satisfied that the Pilot Guard is accompanying it or has given authority for it to start.

Points to be padlocked or securely held. All points that become facing points to trains, if not interlocked, must either be padlocked or securely held by hand for the safe passage of such trains.

Disabled train.

- 6. (a) In the event of a train accompanied by the Pilot Guard becoming disabled, the Pilot Guard must make the best arrangements possible for obtaining assistance with the least delay. If it be necessary for the Pilot Guard to leave the engine on the line he must, before leaving, give the Driver a written order not to move his engine until he returns.
- (b) Should a train unaccompanied by a Pilot Guard become disabled, the Guard of the train must take the necessary steps for the protection of his train, and communicate with the Pilot Guard as soon as possible.

Portion of train left on single line,

- 7. (a) When a train or a portion of a train is left upon the single line from accident, or inability of the engine to take the whole forward, or from any other cause, the Driver must not, if he be in possession of a Pilot Guard's ticket, or if unaccompanied by the Pilot Guard, return for it, except upon written instructions from the Guard of the train, as prescribed in Rule 183, clause (f). If the Pilot Guard be with the train and accompany the engine with the first portion, the Driver may return to the rear portion of his train without obtaining instructions from the Guard of the train authorising him to do so, but the Pilot Guard must accompany the engine when it returns for the rear portion of the train.
- (b) When the train has to be divided the man who divides it must inform the Driver how many vehicles, if any, are being taken forward, and after sunset or during fog or falling snow, or if the division is made in a tunnel, he must place 3 detonators on the line 10 yards apart not less than 100 yards ahead of the portion left behind.

In the case of the train being accidentally divided the Guard must place 3 detonators on the line 10 yards apart not less than 100 yards ahead of the portion left behind.

- (c) The Guard must protect his train in the rear in accordance with the Rules applicable to trains stopped by accident, failure, obstruction or other exceptional cause whether the train is accompanied by the Pilot Guard or not.
- (d) A white light must be placed on the leading vehicle of the rear portion before that portion is propelled to the signal box in advance or drawn back to the signal box in rear.

# REGULATIONS FOR WORKING ON SINGLE LINES BY PILOT GUARD-Continued. FORM OF PILOT GUARD'S TICKET.

No
RAILWAY.
Line or Branch.
PILOT GUARD'S TICKET.
To the Guard and Driver oftrain,
You are authorised to proceed fromto
Signature of Pilot Guard
Station.
Date
This ticket must be given up by the Driver to the person in charge of the station to which he is authorised to proceed, immediately on arrival.

Note.—A Pilot Guard's Ticket is not used on the Great Western Railway.

# SECTION I. (c.)

Matters relating to the Working and Maintenance of Points and Signals,

# SECTION I. (c.)

Matters relating to the Working and Maintenance of Points and Signals.

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# INSTRUCTIONS IN CONVECTION WITH MOTOR TROLLEY SYSTEMS OF MAINTENANCE ON SINGLE LINES WORKED BY ELECTRIC TOKEN.—Page 65.

The following to be added to Clause 1 :-

Where authority is given by the Divisional Superintendent or District Traffic Manager, a motor trolley or inspection car may be stabled in a traffic siding at a time when the Ganger is not in possession of the occupation key, provided the consent of the Signalman is obtained, and the trolley is protected by a competent man with red flag and detonator. (G.A. 1. 3/37. R. & R. Min. 915.)

# INSTRUCTIONS IN CONNECTION WITH MOTOR TROLLEY SYSTEMS OF MAINTENANCE ON SINGLE LINES WORKED BY ELECTRIC TOKEN.—page 65.

Clause 3. The following to be added at the end of the second paragraph:

Where the key instruments are situated in a Signal Box it is the duty of the Signalman to replace in the instrument the key for one section before withdrawing the key for the section which the Ganger next requires to enter, but this does not relieve the Ganger of the responsibility of satisfying himself that he is in possession of the correct key for the section next to be occupied.

(G.A. 7.-3/40. LK1/5482/8.)

### INSTRUCTIONS IN CONNECTION WITH MOTOR TROLLEY SYSTEM OF MAIN-TENANCE ON SINGLE LINES—Page 65.

Section 4, Paragraph (d) to be amended to read :-

(d) Whether occupation is required for trolleying or inspection of the line and whether it is a Motor Trolley, Motor Trolley with Trailer, or a Motor Trolley with Trailer and Inspection Car travelling throughout the Section.

(G.A.19—10/48. L.K.4/26.)

Section 4.—The following to be inserted as Clause (e) :-

(e) The points to which the Motor Trolley (with or without Trailer) or Inspection Car will be run and removed from the line. (G.A.19—10/48. L.K.4/26.)

### INSTRUCTIO

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Section 6.—

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# INSTRUCTIONS IN CONNECTION WITH MOTOR TROLLEY SYSTEMS OF MAINTENANCE ON SINGLE LINES WORKED BY ELECTRIC TOKEN.

N.B.—The word "Ganger" in these instructions embraces also the Sub-Ganger, or other man in charge authorised to arrange occupation of the line in the absence of the Ganger.

1. On certain sections of single line worked by Electric Token, motor inspection cars and motor trollies have been provided for use in conjunction with occupation key instruments and telephones by means of which occupation of the line may be obtained without sending out Handsignalmen (Rules 215 and 217). Should occupation of the line be required for trolley working, or should it be necessary to carry out operations which would interfere with the running of trains, or in case of any slip or failure of the works, or if from any cause the line is unsafe, the withdrawal of the occupation key from the key instrument will afford the necessary protection.

The inspection car is provided for the use of the Ganger, but in order that structures, etc., may be

thoroughly examined he must walk over the entire length once a week.

Each Gang will be equipped with one motor-driven trolley, capable of carrying men, tools, etc. A trailer will also be available for conveyance of materials and tools.

The motor trolley will be utilised to run the gang to the site of the work and back, and from place to place, as necessary.

The motor trolley may also be used to convey Fogsignalmen to and from their posts.

The portable turntable provided must always be carried on the trolley, and by this means the motor trolley can be removed from, or replaced upon, the rails expeditiously. "Run-offs" will be provided at places where occupation key instruments exist.

2. Neither the inspection car, motor trolley nor trailer must be placed on the line, nor must the line be obstructed in any way, unless an occupation key or electric token for the section of line concerned has been withdrawn and the Ganger is in possession of it.

Note.—In the case of failure of the electric token apparatus the inspection car or motor trolley may be used for the purpose of conveying the Pilotman (delivering the Pilot Forms) through the section, or from the point where the trolley is located, in accordance with the Electric Token Block Regulation 25, clause (c), when, owing to failure of the apparatus, an electric token or occupation key cannot be withdrawn. The Signalmen at each end of the section will be responsible for coming to a clear and proper understanding before the car or trolley is placed on the rails, and must record particulars in their Train Registers.

Should an obstruction occur or the line become unsafe during any time when the Ganger may be unable to obtain an occupation key or electric token, the protection required by Rule 217 must be provided and the circumstances explained to the Signalmen by telephone.

3. Each length of line is equipped with a group of instruments and the key to work with these instruments has marked upon it the length to which it is applicable. The key may be used in any key instrument in the group to which it belongs, but a key belonging to one group must not in any circumstances be carried into any other group, as it will not afford the Ganger the necessary protection, except when used in the group to which it applies.

When a Ganger has occasion to occupy a portion of the line belonging to one token section, and then a portion of the line belonging to another token section, he must be careful to replace the key appertaining to the one token section before asking for the key of the other token section.

- 4. When a Ganger wishes to obtain occupation he must either apply verbally to the Signalman or telephone from the instrument in which the occupation key has been left, stating:—
  - (a) Place or number of instrument from which message is being sent.
  - (b) Length of time for which occupation is required.
  - (c) Points between which it is intended to do the work.
  - (d) When occupation is required for trolleying or inspection of the line, the points to which the motor trolley inspection can or motor trolley and trailer will be run and removed from the line.
- 5. The Signalmen at each end of the section must record full particulars respecting all applications for occupation in their Train Registers, and as an additional reminder, collars must be placed on the levers of the signal controlling the entrance to the section in which the occupation is given. Record must also be made of the key instrument in which the key is replaced, and the time this is done.

# Section 6.-Amended to read :-

On receiving the information set out in Clause 4, the Signalman must communicate with the Signalman at the other end of the Section, advising him whether it is a Motor Trolley, a Motor Trolley with Trailer, or a Motor Trolley with Trailer and Inspection Car which is required to occupy the Section. He must indicate whether the Motor Trolley (with or without Trailer) will be removed from the line in the section and whether the Inspection Car is travelling throughout, and must then agree whether the permission may be granted or not. If it is decided that occupation cannot be granted, the Ganger must be so informed. If the Signalmen agree to grant occupation, the Ganger must be told the exact times between which the occupation can be given. In deciding these times it must be arranged for the occupation to cease five minutes before a train is due to leave the station at either end of the section. The Ganger must in every case repeat the telephone message to show that he understands it.

(G.A.19—10/48. L.K.4/26.)

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### INSTRUCTIONS IN CONNECTION WITH MOTOR TROLLEY SYSTEMS OF MAINTENANCE ON SINGLE LINES-Continued.

7. The method of operating the Occupation Key Instruments is as follows :-

# GRANTING OCCUPATION.

### (a) Token Section in which "Control" instrument and one Occupation Key is provided.

Immediately the occupation has been agreed the Signalman to whom the Ganger has applied for occupation must withdraw the slide marked "Control" of the control instrument from No. 1 (normal) to No. 2 position and the Signalman at the other end of the section must press the bell key of his Token Instrument. This will unlock the Control slide of the occupation control instrument, which must then be withdrawn fully to No. 3 position. After the control slide has been withdrawn fully a second slide marked "Occupation Key," for releasing the Occupation Key, is free and must be drawn fully out. A press button and indicator are fixed immediately above this slide and the press button must be pushed in. This will cause the indicator needle to be deflected, and allow the Ganger (or Signalman if the key is at a Signal Box) to withdraw the Key, after which the needle will resume the normal vertical position and the press button must then be released.

# (b) Token Section in which "Control" Instrument and two or more Occupation Keys are provided.

The method is the same as (a) except that it will be necessary for the signalman who is giving the occupation to ask the Ganger concerned, or the Signalman at the other end of the section, if the occupation key is at that Signal Box, to press the plunger to enable the applicable Group Slide to be with-

It is possible for all the Occupation Keys in the Token Section to be out at the same time, but they must all be restored before Token working can be resumed.

### (c) Token Section where "Control" Instruments are not provided.

Immediately the Occupation has been agreed the Signalmen at both ends of the Token Section must press the bell keys of their respective Token Instruments.

If more than one occupation key is provided in the Token Section, only one can be out at a time.

#### (d) Withdrawal of Occupation Key.

Immediately the Ganger is informed that he can have occupation, the occupation key must be turned so that the index shows No. 2, in which position the Key can be released by the Signalman.

After turning the Key to No. 2 position, the Ganger or Signalman must wait until the word "Free"

appears on a small indicator above the Key, which must then be turned so that the index shows No. 3. The key can then be taken out of the Key Instrument. When the key has been withdrawn the Signalman who has given the occupation must be informed on the telephone.

The Ganger must keep the Occupation Key in his possession during the time of the occupation.

#### (e) Giving up Occupation.

The Ganger, when he has obtained occupation of the Line, must so arrange his work as to be able to put back the Occupation Key at the appointed time. He may put the key into any instrument in the Group to which it belongs, as it will fit all the instruments in that Group, and will restore the Token working equally well in any Key Instrument. When putting back the Key he must turn it until the Indicator shows No. 1. Having thus restored the Occupation Key, the Ganger must call up the Signalman on the Telephone and notify him that he has restored the Key (giving the number of the Key Instrument) and that the Line is safe for the passage of trains over the Section. Where "Control" instruments are provided, the Ganger, after restoring the Key, must press the plunger provided in connection with the Key Instrument until notified by the Signalman that the "Control" apparatus has been properly reset. The Signalman must then replace to No. 2 position the Slide applicable to the Ganger's length in which the Key has been restored, when, if the Occupation Key has been properly restored, the Indicator Needle will be deflected, and the Occupation Slide can be replaced in the No. 1 (normal) position. When this has been done the Control Slide must be placed to No. 1 (normal) position, restoring the Electric Train Token working. The Signalman will then notify the Signalman at the other end that the Occupation Key has been replaced.

(f) When the key has been replaced in the key instrument, the Signalman must immediately withdraw a Token in accordance with Regulation 27 (Testing instruments) of the Electric Train Token Regulations, and the Ganger must remain at the key instrument until the test has been made and the Signalman has informed him that everything is again in order. The Ganger will then replace the receiver and shut and lock the box.

Where two or more occupation keys are provided in a token section, the Signalman must make the test after the last key has been replaced.

#### Delete clause 8 and substitute the following :-

8. In the event of any obstruction arising during the time the Ganger is in possession of an occupation key, or if from some other cause the line is unsafe for the passage of trains, the Ganger must not replace the occupation key at the time arranged for the occupation to cease, but must proceed to the nearest telephone and call up the Signalman by pressing the call key on the telephone; if no call key on the telephone is provided he must take the receiver off the hook and attract the attention of the Signalman by shouting into the telephone. The Ganger must then advise the Signalman that the line is unsafe for the passage of trains, and that the key has not been restored. If, however, the obstruction is by a runaway train or vehicles the Ganger must take the key for the section which is obstructed to the nearest token station, and before restoring the occupation key to the key box or handing it to the Signalman must explain the position to the Signalman. If the line is not open at the time, the Ganger must take the first means of advising the Traffic Department man in charge or nearest available Signalman and this must be done before the next train is due to leave the signal box on either side. (G.A.27.Op.-1/51. O.M. Min. 12555.)

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Should he not receive any reply after a reasonable time the Ganger must proceed on foot to the nearest telephone or signal box and establish communication.

(G.A.12. 4/43. L.K.2/91553/S.)

When an Occupation Key Box, other than the Control Instrument, is situated in a Signal Box, the Signalman, when replacing the Occupation Key, which has been handed to him by the Ganger, must carry out the instructions applicable to the Ganger, and immediately call up the Signalman at the Box at which the Control Instrument is situated, on the telephone, and notify him that he has restored the key.

# INSTRUCTIONS IN CONNECTION WITH MOTOR TROLLEY SYSTEMS OF MAINTENANCE ON SINGLE LINES—Continued.

(a) When it is necessary for an Inspection Car, a Motor Trolley, Motor Trolley with Trailer or a Motor to Trolley with Trailer and Inspection Car to enter or proceed through a section from one token station to Trolley with Trailer and Inspection Car to enter or proceed through a section from one token station to the second must be in possession of the token or occupation beautiful to the second must be in possession of the token or occupation beautiful to the second must be in possession of the token or occupation beautiful to the second must be in possession of the token or occupation beautiful to the second must be in possession of the token or occupation beautiful to the second must be in possession of the token or occupation beautiful to the second must be in possession of the token or occupation beautiful to the second must be se

(b) When it is necessary for an Inspection Car or trolley to go into or through any of the tunnels specially enumerated in the appendices to the Service Time Tables, the ganger must be in possession of the Token or Occupation Key and Electric Train Token Regulation 9, clause (i), must be carried out.

Where a token is held, should the trolley be removed from the rails before reaching the next token station, the Ganger must take the token to the Signalman in advance and inform him that the trolley is clear of the line; the Signalman must then place the token in the instrument and send the "Train out of Section" signal. If, however, time would be saved, the Ganger must return with the token to the token station in rear and inform the Signalman that the trolley is clear of the line; the Signalman must then restore the token to the instrument and send the "Cancelling" signal.

Where an occupation key is held the trolley will not be signalled on the token instruments, but the signals must be worked. The Ganger must, after the trolley has been removed from the rails, or has passed out of the section, restore the occupation key to the nearest occupation key instrument,

informing the Signalman that the section is clear.

(c) When the signal boxes are closed the Ganger may pass the signals at Danger and the Inspection Car or trolley will pass over the same road in both directions at crossing stations. The Signalman must set the road for one direction before going off duty.

The Ganger must see that the points are properly set for the passage of the Inspection Car or

motor trolley.

9. Working single line by Pilotman.—Should it be necessary to work a section of the Single Line by Pilotman, and the latter is not in possession of the Electric Token for the section, an assurance must be given by the Signalman that the Occupation Key or keys are in the Key Instruments. This can be ascertained by the Signalman observing that neither of the Slides in the Occupation Control Instrument has been pulled out, or where the Control Instruments are fixed at the other end of the section, by the Signalman ascertaining on the telephone that the slides are properly back in the Occupation Control Instrument.

In the event of the telephone communication also having failed, and the Pilotman is starting from a place where no Control Instrument exists, as he will be unable to ascertain that the Control Slides are in the normal position, it will be necessary for him when walking through the section with the Pilot working forms, to make certain by personal observation that the Occupation Keys are properly in the

key boxes.

At Token stations where Control Instruments are not provided, the Pilotman must, when distributing the Pilot Working forms, satisfy himself by personal observation that the Occupation Keys are properly in the Key Instruments.

Duplicate keys of the huts are held by the Signalmen to enable this to be done.

- 10. If an inspection car or motor trolley be required to pass over a public level crossing provided with a disc working in conjunction with the token instruments, the Signalman must turn the pointer on the appropriate token instrument to the Up or Down "Token out" position, as the case may be, in order that the disc may properly indicate to the Crossing Keeper the occupation of the line.
- 11. When an occupation is required in the morning before a Token station is open, for example, for examination of the line, arrangements can be made for the occupation key to be withdrawn by the Signalman over night and left in a locked box outside the Token Station. The Ganger must return the occupation key to the locked box from which it was taken, or if the token stations have in the meantime opened, it may be restored in any instrument in the group in accordance with the standard instructions.
- 12. Each Guard who regularly works over the line must carry a key of the instrument boxes to enable him or the Driver, as the case may be, to obtain access to the telephone.

In the event of an engine failing and assistance being required, the telephone circuit may be used for the purpose of ascertaining from which place assistance is most likely to be obtained, and he must then act in accordance with Clause (a) of Electric Train Token Regulation 14.

If a train becomes accidentally divided the Guard in charge of the rear portion is authorised to use the telephone circuit for the purpose of communicating with the Token station, so as to expedite the arrangements for removing the obstruction from the single line.

13. If it be necessary to foul the single line outside the home signal for shunting purposes, while the Ganger is in possession of the occupation key, this may be done without first sending the Blocking Back signal to the Token station at the opposite end of the section after the Driver, when the engage is leading, or the Guard or Shunter in charge when the vehicles are propelled, has been warned to trolley may be approaching, but immediately the key is returned to the key instrument the Samust, if the single line is still fouled outside the home signal, give the "Blocking Back" station at the opposite end of the section, which must be acknowledged. When the line is to the home signal he must send the "Obstruction removed" signal, which must be acknowledged.

When it is necessary for an Inspection Car, a Motor Trolley, Motor Trolley with Trailer or a Motor Trolley with Trailer and Inspection Car to enter or proceed through a section from one token station to the next, the ganger must be in possession of the token or occupation key. If a token is held. the inspection car or trolley must be signalled in accordance with Electric Train Token Block Regulation 9, the signals being worked. If the occupation key is held, the inspection ar or trolley will not be signalled on the token instruments, but the signals must be worked (G.A.19-10/48. L.K.4/26.)

# INSTRUCTIONS IN CONNECTION WITH MOTOR TROLLEY SYSTEMS OF MAINTENANCE ON SINGLE LINES-Continued.

If the Ganger requires occupation while the single line outside the home signal is fouled for shunting purposes, this may be granted, after the Ganger has been warned that shunting outside the home signal is going on, and if running towards that point the Ganger must be told to approach with care and be prepared to stop short of any obstruction. Immediately the Occupation Key is returned to the Key Instrument the Signalman must, if the single line is still fouled outside the home signal, so advise the Signalman at the other end of the section. When the line is clear to the home signal he must send the "Obstruction removed" signal, which must be acknowledged.

14. Should the inspection car and motor trolley (with or without trailer) be required to occupy the Section at one and the same time, the inspection car must be at the rear, and every care must be taken to ensure that a suitable distance is maintained between the motor-driven vehicles, so as to avoid any risk of a collision taking place. The Ganger must ride on the inspection car and must hold the Occupation Key. He will be held responsible for seeing that both car and trolley, together with the trailer, are removed from the line before he replaces the key in the key instrument, and he must give an assurance to the Signalman accordingly. Should it be necessary for the motor trolley and trailer to be removed from the line in the section, and for the inspection car to proceed through the section, this may be done, but before the Ganger leaves the gang, he must satisfy himself that the motor trolley and trailer have been removed from the Line. He may then proceed through the section as required with the inspection car, but the key or token must not be restored until the inspection car has been removed from the rails or has passed out of the section.

Under the terms of this instruction an Inspection Car and two motor trollies (with or without trailer) may also be permitted to enter the section at the same time, and from the same end of the

section.

15. A motor trolley and trailer must not be uncoupled except at a Token Station or for the purpose of removing either or both from the line in the section.

16. Neither the inspection car nor motor trolley must be allowed to exceed a speed of 20 m.p h. Both the car and trolley must be kept in gear when running down long or steep gradients, and when running out of gear they must be kept under complete control. Where trap points exist, the inspection car and motor trolley must pass over carefully and be lifted clear of the points if necessary.

When the trolley or car is standing at a run-off, it must be secured by passing a chain through the wheels and padlocking it, or with wheels of the disc pattern, the vehicle must be secured by a chain

passed round a part of the frame to an anchor ring or staple.

Special precautions must be taken in frosty weather, as a sudden application of the brakes will cause the wheels to "pick up." When snow is on the rails a derailment may be caused if the trolley or car wheels mount the snow.

- 17. When the motor trolley is required to haul a loaded trailer over a steep gradient the Ganger must arrange for a man to travel on the trailer to apply the brake if necessary. In no circumstances may a trailer be propelled by a motor trolley. Ordinary trollies must not be attached to motor
- 18. The Inspection Car or motor trolley must approach a station with care and at a speed not exceeding 10 m.p.h. on passing the distant signal, and be prepared to stop in case of shunting going or between the distant and home signals or other station duties being performed.
- Every motor-driven vehicle must exhibit a red lamp in front and at the rear and these must be lighted during darkness, fog or falling snow, and when going into or through a tunnel. If the trailer, is attached to the motor trolley, the tail lamp must be fixed at the rear of the trailer. Each motordriven vehicle must carry a set of hand signals, not less than twelve detonators, a fire extinguisher, and be provided with a klaxon horn, or other warning device, which must be sounded frequently when passing into or through a tunnel.
- 20. The inspection car or trolley must not exceed 5 miles per hour when passing over any public level crossing, and a sharp look-out must be kept by the man in charge of the vehicle and the klaxon horn or other warning device sounded frequently.
- Men riding on a motor trolley must exercise every care to ensure that they, and any material on the motor trolley and/or trailer, are clear of platforms, signals, etc. The vehicles must not be overloaded, and the weight must be evenly distributed, to ensure that an undue weight is not placed on the sides or ends of the vehicles likely to produce a "lifting" effect on the wheels. Men must not place their legs between the trolley and the trailer in tow.
- 22. These instructions supersede those in local notices in so far as they apply. The Permanent Way Inspector must ensure that every man concerned in the operation of Motor Trollies is thoroughly conversant with them.
- 23. When a trolley is used for carrying Signal Department Staff for the purpose of testing occupation key apparatus the trolley must always be removed from the line before the occupation key is placed in the occupation key instruments, and the trolley must not be put on the line again until the key has been obtained in the authorised manner.

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The Signalman must be notified as required by Section 4 (d) and will act in accordance with (G.A.19—10/48. L.K.4/26.)

#### INSTRUCTIONS IN CONNECTION WITH MOTOR TROLLEY SYSTEM OF MAINTENANCE ON SINGLE LINES WORKED BY WOODEN TRAIN STAFF (ONE ENGINE IN STEAM, OR TWO OR MORE COUPLED TOGETHER).

N.B.—The word "Ganger" in these instructions embraces also the Sub-Ganger, or other man in charge authorised to arrange occupation of the line in the absence of the Ganger.

1. On certain sections of single line worked by wooden train staff (one engine in steam or two or more coupled together) motor inspection cars and motor trollies have been provided, and telephones have been fixed at certain points by which means the Ganger may communicate with the Stations on the Branch to obtain occupation of the line under these instructions without providing Hand signalmen in accordance with Rules 215 and 217.

The inspection car is provided for the use of the Ganger, but in order that structures, etc., may be

thoroughly examined he must walk over the entire length once a week.

Each gang will be equipped with one motor-driven trolley, capable of carrying men and tools, etc.

A trailer will also be available for conveyance of materials and tools.

The motor trolley will be utilised to run the gang to the site of the work and back, and from place

to place, as necessary.

The portable turntable provided must always be carried on the trolley, and by this means the motor trolley can be removed from, or replaced upon the rails expeditiously. "Run-offs" will be provided at places where telephones exist.

- 2. Neither the inspection car, motor trolley, nor trailer must be placed on the line, nor must the line be obstructed in any way, unless permission has first been obtained.
- 3. When it is necessary to run the inspection car and/or trolley, or trolley and trailer, along the line, or to carry out operations which would render the running of trains unsafe, the Ganger must go to the nearest station or telephone, and obtain the attention of the person in charge of the station or Signal Box in the direction from which the next train is due, and having ascertained from him that he is in possession of the train staff, and that the section is unoccupied, make his requirements perfectly clear by stating :-

(a) Point from which message is being sent.

- (b) Length of time for which occupation is required. (c) Points between which it is intended to work.
- (d) When occupation is required for trolleying or inspection of the line, the points to which the motor trolley, inspection car or trailer will be run and removed from the line.
- 4. If the person in charge of the staff working can permit the work to be done he must communicate with the person in charge of the station or signal box at the opposite end of the section, and if the occupation can be agreed to the Ganger must be informed the exact times between which the occupation may take place and each person agreeing the occupation must place three detonators, ten yards apart, and exhibit a red flag by day or a red light after sunset or during fog or falling snow, on the running line, and, in addition, a lever collar must be placed on the lever of the signal controlling the entrance to the section. It is to be understood that occupation may not be granted unless the section is clear and the train staff is in the possession of the person in charge of the staff working at one end or the other.

After permission has been given to the Ganger for occupation of the line, the detonators and red flag or light must not, in any circumstances, be removed, nor may a train be allowed to leave either station or signal box in the direction of the place where occupation of the line is authorised, until the person in charge of the station or signal box from which the Ganger obtained permission to occupy the line has received a definite message from the Ganger that the line is clear, and this message has been transmitted to the person in charge at the opposite end of the section.

If authority cannot be given for the occupation the Signalman or person in charge must make it perfectly clear to the Ganger, and the latter must acknowledge the message he receives by repeating it so that there shall be a proper understanding between the person in charge of the staff working and the Ganger.

- 5. On completion of the occupation the Ganger must immediately advise the person in charge of the station or signal box from which authority for the occupation was obtained, that the line is clear, and the latter must repeat the message to the Ganger to show that he understands it, and transmit this information to the person in charge at the opposite end of the section. Upon receiving this information the detonators and red flag or light must be removed from the line and the collar from the lever of the signal controlling the entrance to the section.
- 6. In the case of failure of the telephone instruments rendering it impossible for the Ganger to use them to report the completion of the occupation, the Ganger must proceed on the inspection car or motor trolley to the station from which the train is due to start, remove the inspection car or motor trolley from the line, and inform the Signalman or person in charge that the line is clear.
- 7. Should any permanent way work have been undertaken which it is found cannot be completed within the times of occupation agreed, the Ganger must at once communicate with the person in charge of the station or signal box in the direction from which the train is due. The person in charge of the station or signal box must repeat the message received from the Ganger and must detain the train until the Ganger advises him by telephone or otherwise that the line is clear.

#### INSTRUCTIONS IN CONNECTION WITH MOTOR TROLLEY SYSTEM OF MAINTENANCE ON SINGLE LINES WORKED BY WOODEN TRAIN STAFF—Continued.

- 8. Should the inspection car and motor trolley (with or without trailer) be required to occupy the section at one and the same time, the inspection car must be at the rear, and every care must be taken to ensure that a suitable distance is maintained between the motor-driven vehicles, so as to avoid any risk of a collision taking place. The Ganger must ride on the inspection car, and he will be held responsible for seeing that both car and trolley, together with the trailer, are removed from the line before he gives up the occupation.
- 9. Should it be necessary for the motor trolley and trailer to be removed from the line in the section and for the inspection car to proceed through the section, this may be done, but before the Ganger leaves the gang he must satisfy himself that the motor trolley and trailer have been removed from the line, and then proceed through the section as required and inform the Station Master or person in charge of the staff working when giving up the occupation that all the vehicles have been removed.
- 10. These instructions do not in any way relieve the Ganger from taking the necessary steps to protect the line in case of emergency, and when there is no time or he is unable to communicate by telephone to either of the stations in order to stop a train. In such cases the Ganger is responsible for sending out a man in each direction with hand signals and detonators, in accordance with Rule 217.
- 11. The Signalman at each end of the section must record full particulars respecting all applications for occupation in their train registers. A record must also be made of the telephone but from which the message is received that the section is clear, and the time this is received.
- 12. Each Guard who regularly works over the line must carry a key of the telephone huts or boxes to enable him or the Driver, as the case may be, to gain access to the telephone. In the event of an engine failing and assistance being required, the Driver is authorised to use the telephone circuit for the purpose of ascertaining from which place assistance is most likely to be obtained. If a train becomes accidentally divided, the Guard in charge of the rear portion is authorised to use the telephone for the purpose of making arrangements to remove the obstruction from the single line.
- 13. If it is necessary to foul the single line for the purpose of shunting operations, while the Ganger has been granted an occupation, the Driver, when the engine is leading, or the Guard or Shunter in charge when the vehicles are propelled, must be warned that a trolley may be approaching, and the detonators and danger hand signals (as described in paragraph 4) must be moved out and replaced at a sufficient distance to protect the shunting operations.
- 14. The Ganger or other person in charge of a motor driven vehicle must always approach a station with care and reduce speed to 10 m.p.h. on passing the Distant Signal, being prepared to stop if shunting operations or other station duties are being performed.
- 15. Neither the inspector car nor motor trolley must be allowed to exceed a speed of 20 m.p.h. Both the car and the trolley must be kept in gear when running down long or steep gradients, and when running out of gear they must be kept under complete control. Where trap points exist, the inspection car and motor trolley must pass over carefully and be lifted clear of the points if necessary.
- 16. When the trolley or car is standing at a run-off, it must be secured by passing a chain through the wheel and padlocking it, or, with wheel of the disc pattern, the vehicle must be secured by a chain passed round a part of the frame to an anchor ring or staple.
- 17. Special precautions must be taken in frosty weather as a sudden application of the brakes will cause the wheels to "pick up." When snow is on the rails a derailment may be caused if the trolley or car wheels mount the snow.
- 18. When a motor trolley is required to haul a loaded trailer over a steep gradient the Ganger must arrange for a man to travel on the trailer to apply the brake if necessary. In no circumstances may a trailer be propelled by a motor trolley. Ordinary trollies must not be attached to motor trollies.
- 19. Every motor-driven vehicle must exhibit a red lamp in front as a head signal, and also at the rear as a tail signal, and these must be lighted during darkness, fog or falling snow, or when going into or through a tunnel. If the trailer is attached to the motor trolley the tail lamp must be fixed at the rear of the trailer. Each motor-driven vehicle must carry a set of hand signals and a supply of not less than twelve detonators, a fire extinguisher, and be provided with a Klaxon horn or other warning device which must be sounded frequently when going into or through a tunnel.
- 20. The inspection car or trolley must not exceed 5 miles per hour when passing over any public level crossing, and a sharp look-out must be kept by the man in charge of the vehicle and the Klaxon horn or other warning device sounded frequently.
- 21. Men riding on a motor trolley must exercise every care to ensure that they and any material on the motor trolley and/or trailer are clear of platforms, signals, etc. The vehicles must not be overloaded, and the weight must be evenly distributed, to ensure that an undue weight is not placed on the sides or ends of the vehicles likely to produce a "lifting" effect on the wheels. Men must not place their legs between the trolley and the trailer in tow.
- 22. These instructions supersede those in local notices in so far as they apply. The Permanent Way Inspector must ensure that every man concerned in the operation of motor trolleys is thoroughly conversant with them.

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#### BRITISH RAILWAYS

(WESTERN OPERATING AREA)

#### Alterations and Additions to the General Appendix to the Rule Book

To come into operation on receipt

The following instructions to be inserted on page 70:-

INSTRUCTIONS FOR RUNNING AND WORKING OF THE LENNOX-LOMAX EARTH AUGER EQUIPMENT.

- 1. The machine must only be used by the staff authorised by the Signal Engineer.
- 2. The machine, which is provided with a special match truck, is capable of self-propulsion at a speed of 10 miles per hour (in forward or reverse gear) and, except when operating, must have the match truck coupled. The machine is fitted with three types of brakes, viz., Hydraulic, Transmission and hand screw wheel. The match truck has a hand brake only.
- 3. When travelling under its own power the machine or the match truck, as the case may be, must carry a white headlamp and a tail lamp, which must be lighted as necessary. A red flag must be displayed on the rear vehicle by day. The machine must be equipped with red and green handsignal flags, not less than 12 detonators, a hand lamp (lighted when necessary) and a sprag. A portable telephone or "walkietalkie" apparatus must also be available.
- 4 When boring operations are required to be carried out an absolute occupation of the line concerned must be arranged. The service on which the machine will be worked to the site of operations and the occupation required must be pre-arranged with the District Operating or District Traffic Superintendent concerned.
- 5. The machine must be worked to the agreed place near the site of work, or vice versa, by freight train carrying "F" or inferior headcodes and be marshalled next inside the Guard's brake van. The match truck may be leading or trailing. Should it be necessary to run the machine as a special train hauled by a locomotive a brake van must always be provided at the rear, in which a Guard must ride.
- 6. When the machine is proceeding under its own power to the site where it is required to work the match truck must always be attached and both the screw and chain couplings must be used for the purpose. Such movements must be confined to the shortest possible distance but if it is necessary for the machine to pass completely through one or more sections it must be signalled and dealt with as a Through Ballast Train. When running in a multiple-aspect signalling area automatic signalling must be suspended and the machine dealt with in accordance with T.C.B. Regulation 15. Where an Inner Home Signal is provided the "Is Line Clear?" signal must not be accepted until the Home signal can be lowered.
- 7. The machine must only be moved under its own power by the Driver who has been passed as competent by the Signal Engineer and he must be accompanied by a man who has been passed as competent by the Motive Power Department in (a) knowledge of the route and (b) protective duties of a Driver. A member of the crew of the machine who has been passed by the Operating Department as competent to carry out the protective duties of a Guard must always be present.
- 8. The machine must not be relied upon to actuate track circuits and Rule 55 must be observed in all cases by the man acting as Guard. When the machine is detained whilst waiting acceptance by the box in advance it must not draw forward to the signal controlling the entrance to the section ahead or to an Intermediate Block Home signal but must be held opposite the box. No train must be allowed to follow the machine towards the Intermediate Block Signal until "Train out of Section" has been received.
- 16. At the conclusion of boring operations the machine and match truck will be removed from the section under the power of the machine. If the machine is returned to the signal box in rear, the Driver must bring it to a stand before reaching the detonators protecting the work. The conductor must proceed on foot to obtain the Signalman's Wrong Line Order authorising return to the signal box. The Signal Department person in charge must give the Signalman an assurance that the section clear of obstruction.

  (G.A.32.Op—10/56. OM12832)

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Reference to the following to be made on page 71:-

#### TELEPHONES AT STOP SIGNALS.

When a train has been brought to a stand at a signal where telephone communication with a signal box is provided, the following code of instructions must, after telephonic communication has been established, be observed by the Trainman and Signalman.

The Trainman must be careful to ascertain the name of the signal box with which he has established telephonic communication, and that it is the box from which he requires to obtain instructions,

#### I. Trainman to Signalman.

brought to a stand owing to the signal being at Danger.

#### To communicate as follows :-

To intimate that train has been \*..... train waiting at †..... Signal on ....... Line.

\*-Full description to be given.

+-Name or number of signal and name of Line on which train is standing to be given.

#### 2. Signalman to Trainman.

If train to be held at signal

If signal is defective or cannot be lowered and train must not proceed.

#### To communicate as follows :-

Stop till signal clears. (If after waiting 5 minutes, or other prescribed period, signal does not clear, Trainman must again communicate with the Signalman.)

Wait at telephone for further instructions. (Trainman must communicate with Signalman at intervals of not more than 5 minutes, or other prescribed period, unless otherwise instructed.)

If signal is defective or cannot be lowered, but train may proceed.

Applicable at signal controlled from a Signal Box. Pass \*..... Signal at Danger and proceed cautiously to ..... (name point to which train can proceed). Applicable at Automatic Signal or Semi-Automatic Signal working automatically.

Pass \*..... Signal at Danger and proceed cau-

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Applicable at signal controlled from a Signal Box.

Pass \*........... Signal at Danger and proceed cautiously to ............ (name point to which train can proceed).

Applicable at Automatic Signal or Semi-Automatic Signal working automatically.

Pass \*.................. Signal at Danger and proceed cau-

\*—Name or number of signal to be given.

- 3. The Trainman and Signalman must not terminate a conversation until they are sure that a clear understanding has been reached.
- 4. If a Trainman cannot establish communication with the Signalman, the Driver must, unless instructions to the contrary are exhibited in the telephone box, send his Fireman to the signal box from which the signal is controlled in order to receive the Signalman's instructions. (This clause is not applicable at Automatic or Semi-Automatic Stop signals.)
- 5. At intermediate Block Signals the Trainman must communicate with the Signalman at intervals of 3 minutes. See instructions headed "Intermediate Block Signals."

  (C.A.25.—1/50. R.E. Stand. L.K.1/9986/Gen.)

#### INTERMEDIATE BLOCK SIGNALS.

Object.—The object of intermediate block signals is to allow a train to leave the signal box in rear before the preceding train has passed the signal box in advance but to prevent more than one train being in the section between the signal box in the rear and an intermediate block Home Signal or between an intermediate block Home Signal and the signal box in advance.

Distant Signals working in connection with the Intermediate Block Home Signals are provided the usual distance in rear of the Intermediate Block Home Signal.

General Instructions.—(a) After a train has been brought to a stand at an intermediate block home signal, controlling entrance to the section ahead, the provisions of Rule 41, clause (b), will apply when the signal is lowered.

(b) Where intermediate block signals controlled from the Signal Box in rear are provided and a train is stopped by accident, failure, obstruction or other exceptional cause between the signal controlling the entrance to the intermediate block section and the intermediate block home signal or between the intermediate block home signal and the home signal of the box ahead, Rules 179, 180 and 181 must be carried out. The telephone provided at the intermediate block home signal, must, if possible, be used by the Trainmen where by this means the Signalman can be more quickly advised of the failure.

Where intermediate block signals controlled from the signal box in rear are provided, should it be necessary for a train to return in the wrong direction to such signal box, although the train may not have passed the intermediate block home signal, the provisions of Rules 183 (i) or 184 must be complied with and a "Wrong Line" order form "C" or "D," as the case may be, issued to the Driver before the movement is made.

(c) Should a train be brought to a stand at an intermediate block home signal controlled from the Signal Box in rear and that signal remain at Danger, the Driver must, if the signal be not lowered immediately, instruct the Fireman to use the telephone provided at the signal to advise the Signalman of such detention and act upon instructions received from him.

The Signalman, on receipt of this advice, will inform the Fireman why the signal cannot be lowered and, if it is due to a failure of the track circuit or signalling apparatus, the Signalman must, when he is in a position to do so, instruct the Fireman that the signal may be passed at Danger and the train proceed cautiously into the forward section, and until the Fireman has received this instruction he must remain at the telephone and the train must not proceed. In the meantime, the Fireman must, at intervals of three minutes, remind the Signalman that the train is still waiting at the intermediate block home signal.

(d) Where intermediate block signals controlled from the signal box in rear are provided, should any failure of these signals or of the track circuits or telephones in connection with the signals occur, or should the light of an intermediate block signal be out when it should be burning, no train must be allowed to proceed towards the intermediate block home signal until the "Is Line Clear" signal has been acknowledged by the box in advance. Where necessary the Driver must be instructed by the Signalman in accordance with the G.W. Co.'s amendment to clause (l) of Rule 81 to pass the intermediate block home signal at Danger, and to be prepared to stop short of any obstruction; at the same time the form shewn below must be properly filled up and signed and handed to the Driver, and the circumstances explained to him and the Guard or Guards and also to the Driver of any assisting engine.

#### Order to proceed against intermediate block signals.

	Signal Box.
	, 19
To the Driver of train from	1 to
The signals are out of or	der and you must proceed with caution to the Home
Signal at	
	Signed

(e) Should, however, a train have proceeded towards the intermediate block home signal before such failure is observed and that signal remains at Danger, the Driver if unable to communicate with the Signalman must wait there three minutes or ten minutes where a tunnel exists between that signal and the box in advance, give one long whistle and proceed cautiously at a speed not exceeding 10 miles an hour as far as the line is clear or to the next stop signal in advance, and to be prepared to stop short of any obstruction. The Driver must inform the Signalman at the next box in advance of the circumstances.

(f) In the event of single line working by Pilotman being in operation, the block telegraph must be maintained, and only one train must be allowed between the signal boxes at one time, and the intermediate block signals must be maintained at Danger and passed on the instructions of the Pilotman.

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#### INTERMEDIATE BLOCK SIGNALS-Continued.

(g) All delays at an intermediate block home signal must be noted by Drivers and Guards on their journals.

If any defect hindering, or likely to hinder, the proper working of the intermediate block signals is noticed by any employee on the line, steps should at once be taken to communicate with the Signal-

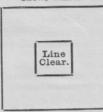
man on either side, so that the defect may be remedied without delay. No employee other than a properly authorised employee of the Signal Engineer's Department is permitted to make any wire connection with the rails or do any work whatsoever in connection with the special signalling appliances.

#### INSTRUCTIONS TO SIGNALMEN AND GATEMEN WHOSE POSTS ARE NOT BLOCK POSTS, BUT WHO ARE SUPPLIED WITH INDICATORS, AND ALSO IN SOME CASES BELLS, TO INFORM THEM WHEN A TRAIN IS APPROACHING.

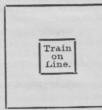
1. On double lines an electrical indicator is provided having two discs—one for the up line and the other for the down line. The diagram given below shows the positions of the disc when the line is free and also when a train is signalled.

When the Line is free both indicators must show half white and half red.





When a Train has left next Signal Box the Indicator shews red.



2. On single lines worked by disc block, only one disc is provided, which shows the same three indications as above, but does not show from which direction the train is approaching.

3. On Single Lines worked by the Electric Train Token an indicator is provided having three positions, as follows :-

When the Line is free.



When permission has been given for an Up Train to leave next Signal Box.

> UP TRAIN ON LINE.

When permission has been given for a Down Train to leave next Signal Box.

> DOWN TRAIN ON LINE.

A bell or telephone is in some cases provided in addition to the disc, and all the bell signals passing between the signal boxes on either side sound on this bell also.

Traction or other heavy engines, heavy vehicles, or heavily loaded vehicles, or droves of cattle, must not be allowed to pass over the level crossing when the indicator shows that permission has been given for a train to approach.

UNLESS SPECIAL INSTRUCTIONS ARE ISSUED TO THE CONTRARY, FOOT PASSENGERS, HORSEMEN AND THE LIGHTER KINDS OF VEHICULAR TRAFFIC MAY, HOWEVER, BE ALLOWED TO PASS OVER THE CROSSING WHEN THE INDICATOR IS SHOWING TRAIN ON LINE UNTIL SUCH TIME AS IT MAY BE NECESSARY

TO CLOSE THE CROSSING FOR THE PASSAGE OF THE TRAINS. If the Bell or Indicator should get out of working order, the Station Master must at once be advised. When it is known that the electrical communication at a level crossing is not in working order the distant signals (if such are provided) protecting the crossing must be kept at caution until information has been received that communication has been restored.

If distant signals are not provided, great care must be taken to ascertain, as far as possible, that no train is approaching the crossing in either direction before the gates are opened to the public.

During Fog or Falling Snow, unless a fogsignalman is stationed at the distant signal, the gates must not be used for road traffic after a train has left the next signal box in the rear until it has passed the crossing.

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(h) Trolley Wo

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The Signal Block Signals trolley has eit Home Signal o

#### ERMEDIATE BLOCK SIGNALS-Pages 71 and 72.

The following to be inserted as Clause (h) paragraph on page 72:-

#### (h) Trolley Working.

Where Intermediate Block Signals are provided, before a trolley is placed upon or run over a track circuit, the permission of the Signalman to whose box the track circuit is connected must first be obtained. In such cases a telephone is provided which may be used by the Ganger to communicate with the Signalman.

Where the overlap track circuit of the Intermediate Block Home Signal does not extend to the Home Signal of the signal box in advance and a trolley is required to be placed on the line in advance of the overlap track circuit of the Intermediate Block Home Signal, the Handsignalman, when going back to protect such trolley must, if he should reach the Intermediate Block Home Signal, telephone to the Signalman and, after acquainting him with the position, continue to go back protecting the trolley until reaching the  $\frac{1}{4}$  mile point as required by the Rule. He must then act in accordance with Rule 215, Clause (d).

When the trolley proceeds the Handsignalman upon following must, when he comes to the telephone at the Intermediate Block Home Signal, advise the Signalman that the trolley has proceeded. The Signalman must be similarly advised if the trolley is removed from the line before the Handsignalman has passed in advance of the Intermediate Block Home Signal.

The Signalman, upon being advised by the Handsignalman, must keep the Intermediate Block Signals in the "On" position until such time as he receives intimation that the trolley has either gone forward to a point ½ mile in advance of the Intermediate Block Home Signal or has been removed from the line.

(G.A.31-7/56. O.M.12789)

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#### TRAINS NOT COMPLETELY WITHIN FIXED SIGNALS.

(a) In cases where-

(i) an outlet signal is provided to control movements from a siding to a running line, and

(ii) a shunting signal is provided to control set-back movements

Drivers must regard such signals as controlling the movement, although the engine may be standing on the wrong side of the signal, and must not move their engines until they are satisfied the signal is off. Where, however, the Driver cannot see the signal and the movement is accompanied by a Shunter, the latter must advise the Driver when the signal is lowered.

If for any reason the signal cannot be lowered, the Shunter or person in charge will be responsible for ascertaining from the Signalman that it is in order for the movement to be made and for satisfying himself that the points are in the proper position. In the case of a light engine un-

accompanied by a Shunter, this duty must be carried out by the Fireman.

(b) On platform lines or other running lines when an engine is ahead of the signal controlling the starting of trains owing to the length of the train, a Driver must regard that signal as controlling his movement. When the Driver cannot see such signal, or back indication where provided, or the signal cannot be lowered owing to the engine occupying a track circuit or other apparatus which prevents the signal being lowered, the Driver must not proceed until he receives a green handsignal from the Signalman or verbal intimation to do so from the person acting under the instructions of the Signalman. The Driver must not be authorised to proceed until it has been ascertained that any points concerned have been correctly set.

In cases, however, where Absolute Block Working is not in operation and the signal has been lowered to enable the train to draw forward for station duties the signal must be placed at Danger in accordance with Rule 68a (ii) and (iii). In such circumstances the signal must not be regarded as controlling the further movement of the train. If it is necessary for the Signalman to allow a conflicting movement to take place ahead of the standing train he must not do so until the Driver of such train has been advised of what is about to be done. After the conflicting movement has been completed and when the train is ready to continue its journey, the Driver must not proceed until he has received a verbal intimation to do so from the Signalman or the person acting under the Signalman's instructions, in addition to the Guard's "right away" signal.

When the fixed signals referred to in clauses (a) and (b) lead to more than one running line, the Driver should satisfy himself by observation which line he is travelling over, but the person in charge of the movement mentioned in clause (a) or the Signalman or person acting under his instructions as mentioned in clause (b) must, whenever practicable, also inform the Driver over

which line he is about to travel. (G.A.30 Op.-9/54 476/E)

#### WORKING OF FIXED SIGNALS AT DIVERGING POINTS-page 73.

The words "or a Class "C" or a Class "D" train" to be substituted for the words "or a fully fitted or a partially fitted freight train" in the second and third lines. (G.A.26 Op.-5/50. R.E. Stand.)

The following to be inserted as the last item on page 73:-

#### WORKING OF FIXED SIGNALS AT DIVERGING POINTS.

At those places where Automatic Train Control is not in operation, in the event of it being necessary for a passenger train, or other train composed of coaching stock, one fully fitted on a partially fitted freight train to be run from one line to another through a junction over which it is necessary that speed should be reduced and such route is not the regular booked route for the train to travel, the Junction Stop Signal must not be lowered until the train is close to such signal and the Signalman must, where practicable, satisfy himself that the speed of the train has been suitably reduced.

Where there is a stop signal worked from the same Signal Box on diverging line ahead of the junction, such signal should not be lowered until the Junction Stop Signal is lowered and then only (G.A.23-7/49. Op. Com. Min. 35. 14/10/48.) in accordance with the Block Regulation.

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PUBLIC LEVEL CROSSINGS LOCKED BY PADLOCK.

Public Crossing gates supplied with padlocks must always be properly locked when trains are signalled from the box on either side of the crossing

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#### ELECTRIC REPEATERS AND LAMP INDICATORS.

These instruments record in the signal boxes in which they are fixed the position of signal arms, slots, facing point switches and bolt locks, and also show whether signal lamps are "In" or "Out."

Signal Repeater (Normal position "On").—This repeater shows whether the arm is "On," "Off," or "Wrong" (an intermediate position); the instrument maintaining a visual signal, which can only be altered by the working of the arm itself.

When the Arm Indicator is showing "Wrong," the necessary steps must be taken at once to have the arm put in proper working order, and to carry out Rule 81.

Slot Repeater (Normal position "On").—This repeater shows whether the slots are "On," "Off," or "Wrong," the instrument maintaining a visual signal which can only be altered by the working of the slot itself.

Lamp Indicator.—When the lamp is not burning, or the light is insufficient, the Indicator in the box will show "Lamp Out."

After a lamp has been lighted a very short time the indicator should show "Lamp In," when the switch, where provided at the bottom of the instrument, must be turned from "Bell Battery Off" to "Bell On."

By the reversal of the switch where provided, the bell is brought into action, and in the event of the lamp going out, the indication "Lamp Out" shews in the indicating instrument, and the bell immediately commences to ring.

As soon as the Signalman has been warned, as above, that the lamp is out or not burning properly, he must turn the switch where provided to "Bell Off," in which position it must remain until the lamp has been attended to, and the indication "Lamp In" is again showing.

When the Lamp Indicator Switch where provided is turned only to the upright or "Lamp Battery On" position, the bell will not ring, and when the lamps are lighted the switch must be completely turned to the "Bell Battery On" position.

Arm and lamp indicators must be closely watched every time the levers concerned are operated, and any difficulty or irregularity in their working immediately reported.

Switching off Signal and Slot Repeaters and Lamp Indicator Batteries.—The following rules must be strictly complied with:—

When a signal box is closed or switched out, the switches where provided must be turned to "Signal Battery Off" and "Lamp Battery Off" respectively, as well as to "Slot Battery Off" where slot repeaters are in use. The switch for the signal arm and for the slot must always be kept to "Signal Battery On" and "Slot Battery On" except as above provided for, but the switch for the lamp must only be kept to "Lamp Battery On" when it is necessary that it should be seen whether the lamp is "In" or "Out."

At signal boxes which are open during darkness the switches where provided are to be kept to "Signal Battery On" as well as to "Lamp Battery On" and "Slot Battery On," to give an indication in regard to the signal arm lamp and slot where slots exist.

Electrically repeated Long Burning Lamps.—In all cases where electrically repeated long burning lamps are in use, the repeater switches must be continually maintained in the "Bell Battery On" position, in order that immediate attention may be called to the failure of a lamp whenever it may occur.

Facing Point and Bolt Lock, &c., Repeaters.—At boxes where electric repeaters are provided in connection with facing point switches or bolt locks, or electric light, pointer or other type of repeaters are provided in connection with electrically-operated points, the Signalmen must pay proper attention to them and, should they get out of order or indicate that the points or bolts, &c., which they repeat are not working correctly, the necessary precautions must be immediately taken for ensuring the safe working of traffic until matters have been put right.

Rule 81.—A signal which is electrically repeated in the signal box must be regarded as defective if the repeater is out of order and the Signalman is unable to satisfy himself that the signal arm is working properly or the lamp burning satisfactorily.

#### SIGNALMEN TO REPORT IRREGULARITIES.

Any infringement or disregard of instructions, or any occurrence coming under the notice of Signalmen affecting the safe and proper working of the line, must be reported to their superior Officer in accordance with Rule 1(b) of the Rule Book and noted in the Train Register Book.

#### PROPER WORKING OF SIGNALS.

The special attention of Signalmen is directed to the importance of strictly carrying out the provisions of Rule 65 of the Rule Book, Regulation 4A, Clause (f), of the Regulations for Signalling Trains by the absolute Block System in regard to the working of signals, and the instructions on this page in regard to "Electric Repeaters and Lamp Indicators."

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#### ADJUSTMENT OF SIGNAL WIRES.

The breaking of signal wires frequently arises from want of proper adjustment when the temperature suddenly becomes lower, and the special attention of Signalmen is directed to Rule 65 of the Rule Book with regard to the adjustment of Signal Wires and the proper working of Signals.

A Signal may be working properly in the day-time, but at night, should the temperature become much lower, a great strain is put upon the Signal Wire if it is not let out, and Signalmen are instructed to see that the wires are adjusted whenever necessary owing to a sudden change of temperature.

#### DEFECTIVE SIGNALS AND POINTS.

Where it is found that any Signal or Point will not answer to the lever in the Locking Frame, or when reversing a Facing Point the road cannot be properly set, steps must at once be taken to endeavour to trace the cause of the defect.

It may be found in connection with cases of defective Faring Points that it is in consequence of the Detector Lock not clearing properly; this may be due to the Home Signal Wire requiring adjustment.

In other cases of Signals or Points not answering to the lever, not only may it be from the wire or rod requiring adjustment, but from a stone getting in the wheel, or the chain or wire getting off the wheel, in the case of Points it may be from a stone becoming wedged in the runners over which the rods travel, or having got between the tongue of point and the stock rail. Such cases as these can be detected generally by the sudden stoppage of the lever in the Frame and, whenever anything of the kind occurs, steps must at once be taken to have the wire or rod traced to the Signal or Points commenting from the connection with the lever underneath the Signal Box.

If a Signalman be unable to leave his Box for the purpose of tracing, he must obtain assistance from the Station or Yard Staff. After such steps have been taken, if the defect cannot be discovered, the Lineman must be immediately sent for. When sending for the Lineman full information as to the nature of the defect should be stated.

#### TESTING ELECTRICALLY LOCKED SIGNALS.

To ensure the correct working of electric locks on signals, Signalmen working such signals must test the electric locks by pulling the levers in order to make sure that the lock is effective.

So that the test may be made regularly, the Signalman who is on duty at 12.0 noon will be held responsible for trying the locks each day in the manner described, and these tests must be recorded in the Train Register.

Any failure must be reported at once to the Lineman and also to the Station Master.

#### CLEANING OF SIGNAL BOXES.

The attention of Signalmen is drawn to the necessity for keeping their Boxes clean and tidy, and removing all cancelled notices.

Station Masters and District Inspectors will be held responsible for seeing that this is done.

#### PRIVACY OF SIGNAL BOXES.

Linemen and other men of the Signal Department must not take their meals in signal boxes nor remain in them longer than is necessary for the purpose of the work they have to do. The lower part of signal boxes may be used for shelter or taking meals in the absence of any other suitable place.

#### UNDER PORTION OF SIGNAL BOXES.

The under portion of Signal-boxes must not be used as storehouses for Coal, Straw, Firewood, Rubbish, etc., but must be kept clear, in order that nothing may interfere with the working of the locking gear and signal wires, and also that the Signal Linemen may have easy access to the machinery and have the means of repairing any defects without difficulty.

The keys of the under portions of Signal Boxes must be kept in the custody of the Signalmen in order to prevent the entrance of unauthorised persons thereto.

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Reference to the following to be made on page 74:-

#### FAILURE OF ELECTRICAL APPARATUS IN SIGNAL BOXES WHERE OVERLOAD PROTECTION SWITCHES ARE PROVIDED ON THE OPERATING FLOOR.

Should a failure occur, ascertain that all switches are in the "ON" position. If any switch has dropped to the "OFF" position, reset to "ON".

Send for lineman if switch will not remain in "ON" position.

(G.A.29.Op.-5/52. SE.-G.40907/Clg./Fm.)

Reference to the following to be made on page 74:-

#### LIMITED CLEARANCE—WARNING TO STAFF.

The attention of all concerned is directed towards the need for exercising care when working at places where there is a restricted clearance between the running line or siding and adjacent structures, or between running lines and/or sidings. This applies to Footplate Staff and Guards as well as to staff working on the ground.

At certain places the limited clearance may be indicated by a Red and White chequered board bearing the words "Warning—Limited Clearance."

(G.A.30 Op .-- 9/54 L.K.1/10483/364)

#### DEFECTIVE SIGNALS AND POINTS.—Page 74.

Delete the instructions under the above heading and substitute the following:-

#### Defective Signals and Points.

If it is found that any signals or points do not respond to the working of the controlling lever or that it is not possible to correctly set up a route, the Signalman must first replace the levers and operate them again, provided it is safe for him to do so; if this is not successful it is probable that the cause is due to some hold up in the mechanism, such as a stone in the points or other obstruction in the wire or rodding. The Signalman should then endeavour to locate the fault and, if possible, remove the obstruction.

If the Signalman is unable to leave his box for the purpose of tracing the fault he should obtain assistance from a member of the station or yard staff.

Should the Signalman be unable to trace or remove the obstruction he must send for the Lineman.

No attempt should be made by the Signalman to interfere in any way with electrical signalling apparatus. (G.A.30 Op.-9/54 476/E)

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#### EMERGENCY OPERATION OF ELECTRIC POINT MACHINES.—Page 75.

The following to be inserted as the fourth and fifth paragraphs:-

The crank handle must not be restored to the circuit controller if the Signalman has given permission for a train to pass over the points, until such train has cleared the points.

When the failure has been rectified, and the points set in a position corresponding to the lever in the frame, i.e. normal or reverse, the crank handle must be placed in the circuit controller and the co a test made to ensure that the points are working correctly. (G.A.30 Op.-9/54 O.M.12594)

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#### APPENDIX TO No. 14 SERVICE TIME TABLE

Page 150.—Siding off loop line between South Fork and East Box. Delete entry.

(K2/336-3/56)

#### APPENDIX TO No. 16 SECTION OF THE SERVICE TIME TABLES

Page 21.—Ground Frames and Intermediate Sidings.

Buttington Brack Yard

Amend to read:

Name of Station or Siding	Situated between	By whom attended	How locked	Remarks	
Buttington	Buttington Jct. and Buttington Gates.	Guard, Shunter or person-in- charge.	Keys released electrically from Buttington Jct. Signal Box.	See pages 72 and 73	

#### Pages 73 and 74

#### BUTTINGTON

#### Ground Frame.

The instructions to be amended as under:

The connections from the Down Main Line to and from the Cattle Pen Siding, Down Siding and Up Siding are worked by means of a three lever Ground Frame. Each lever is released by a separate key, locked in the appropriate key release instrument and released electrically from Buttington Junction Signal Box. A telephone is provided at the Ground Frame connected to the Signal Box.

When it is necessary to work the Ground Frame the Guard, Shunter or person in charge must request the Signalman by telephone to release the appropriate key. The Signalman, if permission can be given, will operate the appropriate interlocking lever which will enable the key to be withdrawn. The key must then be inserted in the Annett's lock for the point lever concerned which will then be released for the shunting to be performed.

As soon as the shunting has been completed and the Guard, Shunter, or person in charge has satisfied himself no loose vehicles have been left on the running line, the lever must be restored to normal, the key withdrawn from the Ground Frame lock and replaced in the key instrument and the Signalman advised,

The instructions shown on page 77 of the General Appendix (GA 19) headed "Instructions for working Ground Frames operated by inter-locking lever at Signal Box and key release instrument at Ground Frame" apply.

(K2/335-2/56)

#### INSTRUCTIONS FOR SIGNALLING TRAINS DURING FOG OR FALLING SNOW DATED OCTOBER 1951

Distant Signals at which Fogsignalmen are not provided.

Page 17-Reading and Cogload via Westbury.

Add: Curry Rivel Junction-Up Line (Colour Light).

(K2/335-2/56-NW13053)

#### INSTRUCTIONS FOR SIGNALLING TRAINS DURING FOG OR FALLING SNOW, DATED OCTOBER 1951

Distant Signals at which Fogsignalmen are not provided

Page 35

The following to be deleted from the instructions under this heading in the section "Ponty-pridd and Merthyr Brandy Bridge".

Signal Box	Distant Signals NOT Fogged		
Signal Box	Up Line	Down Line	
Brandy Bridge	*Main.		

(K2/335-2.56-W9929)

#### .. INSTRUCTIONS FOR SIGNALLING TRAINS DURING FOG OR FALLING SNOW, DATED OCTOBER 1951

Particulars of permanent speed restriction indicators shewing whether Fogsignalmen provided

Page 68-The following to be included:

10000	Milana	For Up or Down trains	Restricted Speed m.p.h.	Whether Fogsignalmen provided		
Location of Speed Indicator	m. ch.			Independent of Distant Signal	At Distant Signal to include Speed Indicator	
Penpergwm	25 251	Down.	50	No."	Yes.	
Penpergwm	26 221	Up.	50	No.	Yes.	

K2/332-2/56-W.10450



Emergency Operation of Electric Point Machines-Pages 75, 76 and 77 The seventh paragraph of these instructions, shown on page 75, to be amended as follows: "In every case where there is a failure of power-operated points, whether operated by hand crank or not, both facing and trailing ends must be clipped and padlocked in the required position. A handsignalman must be appointed.

(K2/332-2/56-O.M.12814)

#### \*\* GENERAL APPENDIX TO THE RULE BOOK

Automatic Train Control System in use on the Western Operating Area-Pages 80, 81 and 82

The following to be added at the end of the paragraph shown on page 82 under the heading "Automatic Train Control Ramps under Repairs": "Similar action must be taken if a ramp is removed for other purposes,"

(K2/332-2/56-O.M.12815)

#### \*.\* GENERAL APPENDIX TO THE RULE BOOK

Instructions to Guards and Ticket Collectors on Corridor Trains-Page 115

The following to be added as a fifth paragraph of Clause 7:

Lavatory door locks.

In the latest B.R. Standard carriage the locking device is incorporated in a separate Indicator unit in which case it is necessary to withdraw the four screws securing the indicator frame, remove frame and pull enamelled indicator plate and operating spindle away from the unit. This will expose a square hole and by inserting a carriage key the inside catch can be released.

(K2/332-2/56-LK2/13532/275)

#### GENERAL APPENDIX TO THE RULE BOOK

Extracts from the Regulations for Signalling Trains and Engines by Permissive Block System over Goods Running Loop Lines and over other Permissive Lines—pages 57 and 58.

The following to be added as paragraph (e) to Clause 9:
"In multiple-aspect signalled areas where a delayed miniature yellow signal is provided at the entrance to Goods Lines or Loops, paragraphs (a), (b) and (c) of this clause will not apply, but the speed of trains entering a Goods Line or Loop must not exceed 10 m.p.h.

(LK2/336-3/56-OM.12816)

#### INSTRUCTIONS TO BE OBSERVED FOR WORKING TIME RELEASE INSTRUMENTS, Etc., IN CONNECTION WITH APPROACH LOCKING OF SIGNALS AND DISCS.

Approach locking of signals and discs controlling movements over electrically operated points has been provided at certain places and Time Release instruments have been installed to enable the approach locking to be released when required.

The Time Release instruments are set as follows :-

For running movements governed by signals—2 minutes. For movements governed by discs and backing signals— $\frac{1}{2}$  minute.

(Any variation of the time setting of the Release Instruments as set out above will be notified (G.A.19-10/48. O.M.12453.) by special instruction in the signal box concerned.)

Should it be necessary :-

- 1. To divert an approaching train from one line to another after the signals or discs have been lowered, or
- 2. To replace a signal to Danger after it has been lowered and before the approaching train has passed the signal and occupied the track circuit immediately in advance of that signal, the following instructions must be observed :-
- (a) The signal lever concerned must be put back to the normal check-lock position in order to restore the signal to Danger.
- (b) The handle of the Time Release instrument must then be given a twisting movement to the left and the pointer will slowly move away from the normal setting (up to two minutes or less, as the case may be), to zero (0) position.
- (c) When the pointer has reached the zero position the signal lever concerned can be restored to normal position. Provided the train has been brought to a stand at the signal concerned the points may then be altered. The handle of the Time Release instrument must then immediately be returned to the normal position, otherwise the signal levers will be locked.

In the case of independent discs and backing signals, the Signalman should wait for the track circuit section next in advance of the disc or backing signal concerned to show "Occupied," and the lever should then be replaced to the normal position before that track circuit again shows clear.

If this is not done it will be necessary to operate the Time Release in accordance with the previous

The provision of Time Release instruments does not cancel the instructions contained in Double Line Block Signalling Regulation 4, Clause (c).

The instructions in Rule 70, as far as they apply to the signals or discs affected, must also be

A record must be made in the Train Register of each occasion when the Time Release instrument is used.

#### EMERGENCY OPERATION OF ELECTRIC POINT MACHINES.

Where hand cranks are provided for emergency purposes they are kept in a locked box in the Signal Box, the key being kept in a glass-fronted case. The glass must be broken to obtain the key.

In the event of the failure of points to respond to the inovement of the lever the latter must be placed in its original position either normal or reverse as the case may be. The Station Master must then be advised immediately, also the Lineman, and arrangements made for the points to be worked by means of the hand crank by the Signalman until some other competent person can be appointed. In the majority of cases the electric power circuit has to be broken down before the points can be operated by hand. There are different methods of accomplishing this, varying with the types of machines.

When the hand crank has been inserted in the machine it may be rotated to move the points as required, under the direction of the Signalman.

In the event of the operator leaving the points before rectification of the defect he must extract the hand crank and return it to the Signalman who will be responsible for its safe custody until it is replaced in its proper position.

In every case where points are operated by hand crank they must be clipped and padlock the required position and a handsignalman must be appointed.

Entries must be made in the Train Register Book of the time the hand crank is extracted, each entry being signed by the Signalman on duty and the man using the hand crank. When ordinary working is restored and the hand crank is replaced an entry must be made in the Train Register Book, to be signed by the Signalman and Lineman.

Iman has given nts. ing to the lever

controller and O.M.12594)

#### EMERGENCY OPERATION OF ELECTRIC POINT MACHINES Continued.

When points have to be moved during failure and a hand crank is not provided, the motor can be disconnected from the points by removing the pins and uncoupling the driving rods. When so disconnected, the points must be clipped and padlocked in the required position.

When points have been worked on the ground whether by hand crank or other means, no attempt must be made to resume normal working from the Signal Box until permission is given by the Signal

In all cases of failure at electrically operated points the signals concerned must be maintained at Danger unless mechanical detection is provided and this is in working order, when the applicable signals may be lowered. The key of the padlock securing the points must be in the custody of the Signalman before the Stop signals are lowered.

The provisions of Rules 77, 81 and the instructions on page 15 of the General Appendix to the Rule Book must be observed while the points are out of order.

The key of the small glass-fronted case must be kept by the Lineman.

See following instructions for details respecting the method of using the particular type of hand crank provided:—

#### General Railway Signal Company's Type Machine.

- 1. In no circumstances must the hand crank be inserted in the machine while the motor is running.
- 2. To use the hand crank the hinged cover over the hole in the point machine must be lifted and the crank inserted, moved slightly sideways and then pushed into the machine as far as it will go. The handle must then be rotated to move the points into either the normal or reverse positions as required.

In moving facing points either way the rotation of the hand crank must not be stopped when it is seen that the points have been moved, but the rotation must be continued to its fullest extent to ensure the points being properly held and bolted.

- 3. The insertion of the hand crank in the point machine cuts off the electric power from the points so that while the hand crank is inserted into the opening the machine cannot be operated electrically.
  - 4. When the points are again in order the hand crank must be returned to the Signal Box.

#### Siemen's type machine.

The metal plug on the point machine cover must be removed by utilising the projection on the hand crank for the purpose. The end of the crank must then be fitted over the circular spindle having a slot in the top. The crank may then be rotated to move the points into either the normal or reverse positions, as required. In moving facing points either way the rotation of the hand crank must not be stopped when it is seen that the points have been moved, but the rotation must be continued to its fullest extent to ensure the points being properly bolted.

- 2. No circuit breaker is provided in connection with the hand crank, that is to say, the electric power cannot be cut off from the points before the hand crank is used. The point lever must not, therefore, be moved without the consent of the man in possession of the hand crank.
- 3. When the points are in working order again the hand crank must be returned to the Signal Box.

#### Westinghouse Brake and Saxby Signal Company's type Machine, Style "C."

- 1. The hand crank is inserted in an electric circuit controller fixed in the locked box in the Signal Box.
- 2. To withdraw the hand crank it must be turned slightly which will enable it to be withdrawn from the circuit controller. The electric power will then be cut off from the points.
- 3. One end of the crank is fitted to hold the square end of plug in the point machine casing which must be unscrewed and removed. The hand crank can then be inserted in the plug hole of the point machine and placed on a similar squared head inside the machine, when the crank may be rotated to move the points into either the normal or reverse positions as required. In moving facing points either way, the rotation of the hand crank must not be stopped when it is seen that the points have been moved, but the rotation must be continued to its fullest extent to ensure the points being properly bolted.
- 4. The hand crank must be restored to the circuit controller after use to complete the power circuit.
- 5. In some cases where a number of point machines are installed, separate and differently shaped hand cranks inserted in circuit breakers are provided. The cranks are so made that they only fit the point machines to which they apply. The key for each hand crank is to be locked up in a separate glass fronted box.

#### INSTRUCTIONS FOR WORKING GROUND FRAMES OPERATED BY INTER-LOCKING LEVER AT SIGNAL BOX AND KEY RELEASE INSTRUMENT AT GROUND FRAME.

When the train arrives at the ground frame, the guard or person in charge must call up the signalman on the telephone for permission to use the ground frame. If he is in a position to grant the permission, the signalman, provided the applicable signal is at "Danger," must pull over the ground frame interlocking lever and inform the guard or person in charge on the telephone that the ground frame key can be released.

The key in the key release instrument at ground frame must then be turned "anti-clockwise" from No. 1 to No. 2 position and when the indicator shows "Free" further turned to No. 3 and then withdrawn. The key when inserted in Annett's lock releases the ground frame.

After the work is completed the key must be withdrawn from the Annett's lock and re-inserted in the key release instrument and turned "clockwise" to No. I position and the signalman informed on the telephone. Where a plunger is provided this must be pressed.

Signalman must press plunger provided in connection with the electric lock on the interlocking lever and place the lever to normal. He must then inform the guard, or person in charge of the ground frame on the telephone that apparatus is restored. Until this message is received the guard must not rejoin or allow his train to proceed. Neither must he do so until he has given an assurance to the signalman that no vehicle has been left foul of the main line.

Undue force must not be used in manipulating the release instrument key.

#### INSTRUCTIONS FOR WORKING GROUND FRAMES OPERATED BY KEY CONTROL INSTRUMENT AT SIGNAL BOX AND KEY RELEASE INSTRUMENT AT GROUND FRAME.

An electrically-locked key control instrument is provided at the signal box for effecting locking with the applicable signal, and an electric key release instrument at the ground frame.

When the train arrives at the ground frame, the guard or person in charge must telephone to the signalman for permission to use the ground frame. If he is in a position to grant the permission, the signalman, provided the applicable signal is at "Danger," must turn handle of the key control instrument from No. I to No. 2 position and then to No. 4 position and inform the guard or person in charge on telephone that ground frame key can be released.

The key in the key release instrument at ground frame must then be turned "anti-clockwise" from No. 1 to No. 2 position and when indicator shows "Free" further turned to No. 3 position and key withdrawn. The key when inserted in Annett's lock must be turned to release the ground frame.

After the work is completed the key must be withdrawn from the Annett's lock and re-inserted in key release instrument which must then be turned "clockwise" to No. I position and signalman informed on the telephone.

Signalman must turn handle of key control instrument from No. 4 to No. 3 position and request guard or person in charge to press plunger at ground frame. The signalman must turn handle of key control instrument from No. 3 to No. 1 position. The guard or person in charge of ground frame should then be informed on telephone that apparatus is restored and until this message is received the guard must not rejoin nor allow his train to proceed. Neither must he do so until he has given an assurance to the signalman that no vehicle has been left foul of the main line.

Undue force must not be used in manipulating the key release or the key control instruments.

(G.A.19—10/48. S.E.—G.38,480/D.s/H.)

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#### EMERGENCY OPERATION OF ELECTRIC POINT MACHINES-Continued.

#### Westinghouse Brake and Saxby Signal Company's Type Machine, Style M.3.

- 1. One end of a metal strap or hasp is hinged to the centre of the top of the point machine case and the other end of the strap is secured to the side of the case by means of a padlock. This strap when secured to the side of the case covers a plug in the top of the case. In no circumstances must the strap be unfastened while the motor is running.
- 2. To work the points by hand in the case of failure the key of the padlock and the hand crank must be taken from the Signal Box by the handsignalman. He must unlock the strap or hasp on the point machine and lift it back on to the top of the case. This will release a spring plug which will cut off the electric power from the points. The hand crank must then be placed over the plug in the top of the case and rotated to move the points as required.

If the points are fitted with a facing point lock the hand crank must be turned until they are properly bolted.

3. When the points are again in order the hasp or strap must be replaced in its normal position and padlocked. The key and the hand crank must be taken back to the Signal Box.

#### USE OF SIGNALMEN'S LEVER COLLARS.

A lever collar must be placed on the home signal lever, or any other lever affected in the box, when a train or engine is standing on the running lines or the signal in question is protecting an obstruction. Lever collars when not in use must be placed on the hook provided, and not left on the levers except when used as a reminder of a train, engine or vehicle standing on the running lines.

#### BADGES FOR PLACING OVER LEVERS IN LOCKING FRAMES.

Reversible metal badges with the words "Disconnected" and "Workman" respectively are provided in Signal-boxes for attaching to any lever when disconnected to act as a reminder to the Signalman and thus prevent possible injury to himself by his pulling it over in the usual way, or to indicate to the Signalman that workmen are engaged at the points or signal operated by that lever. Before moving this lever the Signalman must inform the workmen, when in close proximity to the box, that he is about to do so, but when they are engaged beyond speaking distance the Signalman must, before moving the lever right over, give two or three small movements to indicate to the workmen that the lever is about to be reversed. The Signal Department man will be responsible for placing the badges on the levers.

#### SIGNAL DEPARTMENT WORKMEN SIGNING ON DUTY.

Where the timekeeping arrangements for Linemen and Assisting Linemen of the Signal Department provide for their booking on and off duty in the Signal Box in close proximity to their depot, they must do so on lose leaf form No. 506a or 506b, the men signing the form in the order in which they come on duty. The Signalman will initial the entries, and make a note of any absentees and of the reason for absence, if known.

Where convenient the Linemen may sign on duty on form 506 A or 506 B at the Telegraph Office at their home station.

Signal Department men returning to their Depots after the usual closing hour must book off duty at the home station, the Traffic Department Officer on duty at the time initialling the necessary entries on form 506a or 506a.

In all such cases forms 506a or 506B should be passed to the responsible Signal and Telegraph Inspector each Friday night.

#### MAINTENANCE OF GROUND FRAMES LOCKED BY ELECTRIC TOKENS.

To facilitate the carrying out of cleaning, oiling and repairs by the Signal Department at ground frames locked by the electric token, the following arrangements must be observed:—

- (a) For examination, oiling or small repairs, which can be done between trains, the Signal Inspector must first arrange with the person in charge of the station as to when the work can be carried out. The person in charge will then arrange for a responsible person to meet the Signal Lineman at the nearest token station and accompany him with the token, in the interval between two trains, to the ground frame, remaining with the Lineman while the work is being carried out and, upon its completion, returning to the station with the token.
- (b) Where the ground frame is too far from the station, or the intervals between trains are too short to permit of this arrangement being carried out, arrangements may be made for a passing goods train or engine to stop for a short time at the ground frame.

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#### MAINTENANCE OF GROUND FRAMES LOCKED BY ELECTRIC TOKENS-Continued.

(c) For heavy repairs or renewals necessitating such a lengthened stay at the ground frame as cannot be obtained between trains, arrangements must be made for the points in the main line, and those protecting the main line, to be disconnected from the ground frame and properly secured in their normal position with clips and padlocks by a competent Traffic Department man, who must have the token for the section in his custody while the points are being disconnected, and who must retain the padlock keys in his possession during the whole of the time the work is in progress.

On completion of the work, arrangements must be made for a train or light engine to call at the siding with the token for the necessary test, the points being unlocked and unclipped for this purpose by the Traffic Department man.

(d) When it is necessary for the Signal Department to examine, clean or carry out small repairs to ground frames not situated near a signal box, advantage should be taken of the use of occupation keys where in operation. The Signal Department will arrange for the provision of the necessary duplicate Annett's key to enable this to be carried out. The occupation key should be replaced in the key-box on completion of the work.

#### REPAIRS, &c., TO SIGNALS AND POINTS AT INTERMEDIATE SIDINGS.

Whenever it is necessary for Signal Department Staff to oil or repair the points or signals at intermediate sidings at which no Signalman is stationed, information must be given to the person in charge of the adjoining station where the keys of the siding points and signals are kept, and, if the repairs are of a slight nature, the latter must arrange for a competent member of his staff to accompany the Fitters and remain with them at the siding until the work is finished, when he must see that everything is left safe and the points and signals locked in their proper position.

If the repairs are of an extensive nature the Signal Engineer must give proper advice to the Divisional Superintendent or District Traffic Manager, so that special arrangements can be made in accordance with Rules 77 and 79 of the Rule Book.

#### CARRYING OUT WORK ENTAILING DISCONNECTION OF POINTS AT SMALL STATIONS OR OUTLYING SIGNAL BOXES.

When it is necessary to disconnect points at small stations or outlying Signal Boxes in connection with work to be performed by the Engineering Department on a Sunday, the Divisional Superintendent or District Traffic Manager, will, if possible, arrange with the Signal Department for the points to be disconnected on the previous Saturday and clipped and padlocked, the key being handed to the Traffic Department Inspector or other responsible person in whose custody it must remain until the points are reconnected by the Signal Department on the following Monday.

#### ENGINEERING OCCUPATIONS ON SUNDAYS ON LINES CLOSED FOR TRAFFIC PURPOSES.

Where, in the opinion of the Divisional Superintendent or District Traffic Manager, economy would be effected, arrangements may be made for a Relief Signalman to travel with the ballast train to operate points and signals where necessary, instead of manning signal boxes especially for the purpose, both on double lines and on single lines not provided for in the following paragraph.

#### ENGINEERING OCCUPATIONS ON SUNDAYS ON ELECTRIC TOKEN LINES WHEN THE LINE IS CLOSED.

When it is necessary to run a ballast train at a time when the line would otherwise be closed a token may be withdrawn by the Signalmen for the section or sections concerned before leaving duty. The token so withdrawn to be left on the "Train Register" or other specially appointed place.

The ballast train when travelling through two or more sections must in all cases be accompanied by a Relief Signalman, who must be acquainted with the working of the Token Station, and he must change the token at each Token Station and work the necessary points and signals.

The certificate as to the fitness of the line, which is required to be furnished by the Permanent Way Department, should be left with the token in the last Token Station of the particular section in which the work has been done, whilst in those cases where a special box is provided in which to place the token, the certificate should be placed with the token in the box.

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#### WORKING OF CRANES IN CONNECTION WITH MISHAPS OR ENGINEERING OPERATIONS .- PROTECTION OF TRAINS ON ADJOIN-ING LINES-page 78.

Clause ii amended to read:-

No train must be allowed to pass the site where the crane is working without the permission of the Operating Department District Inspector, who must not give his permission until (a) the person in charge of the crane has ensured that it is clear of the line on which the train will run and no further movement of the crane will be made, and (b) the hook and lifting beam (where used) is secured to prevent movement.

The following to be added at the end of Clause IV, paragraph iii:-If, however, the Handsignalman when going out to protect an obstruction should arrive at an Intermediate Block Home Signal before he has reached the distance of 1 mile, he must make use of the telephone provided, and request the Signalman to maintain the Intermediate Block Home signal at Danger until the Handsignalman has informed him that the obstruction has been removed, and that the line is clear and safe for the passage of trains. Under these circumstances the Handsignalman must remain at the Intermediate Block Home signal, place on the rail 3 detonators, 10 yards apart, and exhibit a hand danger signal, until the Operating Department District Inspector authorises the train to proceed. Should the telephone at the Intermediate Block Home signal have failed, the Handsignalman must proceed for the prescribed (G.A.29.Op.-5/52. LK1/9665/Gen.E.) distance in accordance with Rule 217.

#### WORKING OF CRANES IN CONNECTION WITH MISHAPS OR ENGINEERING OPERATIONS—PROTECTION OF TRAINS ON ADJOINING LINES.

Where a crane is being used in connection with mishaps or engineering operations and it is necessary for trains to travel over any line which may be fouled by the movement of the crane the following precautions must be taken:—

- (i) A District Inspector (or other responsible member of the Operating Department staff) must be in attendance and no line must be fouled by the operation of the crane until his permission has been given. He must keep in touch with the Signalman or Signalmen concerned so as to obtain accurate information as to the running of trains. Where necessary a portable telephone in communication with the signal box or boxes concerned must be provided.
- (ii) No train must be allowed to pass the site where the crane is working without the permission of the Operating Department District Inspector, who must not give his permission until (a) the person in charge of the crane has ensured that it is clear of the line on which the train will run and no further movement of the crane will be made, and (b) the hook is secured to prevent movement and the boom (where used) is removed from the hook.
- (iii) After a train has passed the site of the work the crane may re-commence operations as soon as the Operating Department District Inspector has ascertained that there is a suitable interval for work to proceed and after the protective arrangements shewn in clause (iv) have been carried out.
- (iv) When the site at which the crane is working is not within the protection of the Fixed signals of the lines on which trains require to run Handsignalmen must be appointed in accordance with Rule 217. When the Handsignalmen have taken up their positions a train may be allowed to enter the section, but the Handsignalman at the site of the work must continue to exhibit a danger signal until the Operating Department District Inspector authorises the train to proceed.

If the site at which the crane is working is within the protection of the Home signal of the line on which trains require to run, such line must not be fouled within the authorised clearing point by the crane antil the "Blocking Back Inside Home Signal" signal has been sent to the signal box in the rear and acknowledged. Where block instruments are not provided the "Blocking Black" (2—4) rear and acknowledged. Where block instruments are not provided the "Blocking Black" (2—4) rear and acknowledged on the lever of the signal controlling the entrance to the section and must not acknowledge the "Blocking Black" signal until this has been done.

In the case of an Intermediate Block Home signal controlled from the signal box in the rear, if the site where the crane is working is within the clearing point of such signal the Operating Department District Inspector must request the Signalman at the box in rear to place a lever collar on the lever controlling the Intermediate Block Home signal and also on the lever of the signal controlling the entrance to the Intermediate Block section until the conditions in clause (ii) are carried out. Where the site at which the crane is working is ahead of the clearing point of the Intermediate Block Home signal Handsignalmen must be appointed in accordance with Rule 217.

At places where automatic signalling is in operation a Handsignalman must be stationed at the automatic Stop signal in rear of the site of the crane working and wherever possible this signal must be placed and maintained at Danger in which case a distant Handsignalman will not be required. If it is not possible for the signal to be kept at Danger a distant Handsignalman must be appointed in accordance with Rule 217. On the arrival of a train at such signal the Handsignalman must advise the Operating Department District Inspector who, after ensuring that the line is clear in accordance with paragraph (ii), may instruct the Handsignalman to authorise the train to proceed.

(G.A.24.—11/49. R.E. Stand:—L.K.1/9665/Gen.)

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#### ENGINEERING OCCUPATIONS ON SUNDAYS-Continued.

When Sunday Engineering occupations are in force a token may be withdrawn at each Token Station affected and the Signalman must place the token in a locked box provided outside the cabin before leaving duty, making a suitable entry in the Train Register. In addition to the Signalman's key, the Ganger or other appointed Engineering Department man, will be provided with a key of the box to enable him to obtain the token on Sunday morning, and will be held responsible for seeing that it is kept locked up in the box provided, on completion of the work, so that the Signalman will be able to obtain it when coming on duty. The Signalman before replacing the token in the instrument must make an entry in the Train Register.

#### EXAMINATION OF LOCKING GEAR.

Signalmen must notice the interlocking of the Levers, and test it as frequently as possible. If in any case they find that a Lever can be moved, when in their judgment it should be locked, they must immediately report the occurrence to their superior officer, who will transmit the Report to the Signal Engineer and to the Divisional Superintendent or District Traffic Manager.

This does not relieve the Signal Department from the responsibility of regularly testing the Locking and keeping it in proper working order.

#### LAYING IN NEW SWITCHES.

- 1. When new switches are to be put into Main Passenger Lines the work must be done at the nearest convenient date to that appointed for connecting up to the Signal Box, and the Engineering and Signal Departments must, as far as possible, arrange for the fittings to be framed and fitted up, so that on the day appointed for cutting them in the work may be done with the utmost despatch.
- 2. The Permanent Way Inspector and Ganger must see that the switches (whether facing or trailing) which have been cut in, but not connected up to the signal box and brought into use, are secured in the following manner:—
  - (a) The closed tongue to be fastened by a screw clip, padlocked, and clipped by a fish-plate bolted down to the sleeper by two fang-bolts.
  - (b) The open tongue to be secured in place by the front switch rod being properly fixed complete with all its bolts and also by the insertion of an oak chock wedge between the switch and the stock rail, the wedge being bolted down to the sleeper by two fang-bolts.
- 3. The Traffic Department will supply the rail clips and padlocks, and the keys must be held by the Divisional Superintendent or District Traffic Manager, who will supply a competent man to lock and unlock the switches and supervise any work which it may be necessary to carry out between the time the switches are cut in and when they are finally rodded up and brought into use.

#### PROTECTION OF SIDINGS, GOODS SHEDS AND OTHER BUILDINGS TEMPORARILY IN THE OCCUPATION OF THE ENGINEERING OR LOCOMOTIVE DEPARTMENTS.

Whenever the whole or any portion of a siding, goods shed or other building into which vehicles are moved is in the exclusive occupation of the Engineering Department, the Siding must, if possible, be closed for traffic in the following manner:—

- (a) Controlling of Points Worked by Hand.—The points must be either spiked or fastened over with clip and padlock, or a suitable piece of timber placed across the rails and securely fastened. If timber is used a red hand signal shewing in the direction from which vehicles might approach must be provided.
- (b) Controlling of Points Worked from a Signal Box.—The Engineering Department must advise the Traffic Department by notice giving particulars of the time that occupation will be required, and the Signalman must be instructed by the Inspector, Foreman or Ganger in charge of the work not to permit any vehicle to pass the points until further advised; also where practicable the points should be secured by clip and padlock, or spiked to prevent the points being moved, or a piece of timber placed across the rails, securely fastened and a red hand signal shewing towards the direction from which vehicles might approach must be provided. Where practicable a lever collar or workman's badge must also be placed on the levers affected.

Should it not be possible to close the siding the instructions contained in clause (a) of Rule 112 must be strictly observed and, in addition, a Handsignalman must be appointed who must place detonators on the rails at a sufficient distance from the men at work in the siding or goods shed, and also give effective warning of the approach of a vehicle.

Organizating anca AUTOMATIC TRAIN CONTROL SYSTEM IN USE ON THE GREAT WESTERN RAHWAY. General Description. Objects achieved by the system. (1) The primary object of this system is to give audible warning on the engine when the train is approaching a distant signal, or passing a lower distant signal fixed below a "Stop" signal, and the distant signal being in the "On" (proceed with caution) position, also, in the event of this warning being disregarded, to apply the brakes automatically, so as to ensure the train being pulled up before it reaches the Another and distinctive audible indication is also given on the engine when the distant signal is "Off" (Proceed). The value of this latter indication is that it facilitates the running of the train when the semaphore signals cannot be seen during fogs and snowstorms. Audible signals. (2) The audible signals given are the sounding of a siren indicating "Proceed with caution," and the ringing of a bell, indicating "Proceed." Location at which (3) The point at which the audible signals are given is usually about 440 yards Audible Signals on before the distant signal is reached. Where, however, the distant signal is a lower Engine are arm on a "Stop" signal, the audible signals are given just as the "Stop" signal is operated. passed. Apparatus on Permanent Way. (4) The apparatus fixed on the permanent way for operating the audible signals on the engine is a ramp about 40 feet long, which is fixed between the running rails. and is made up of a steel \( \) bar mounted on a baulk of timber. The ramp at its highest point is  $3\frac{1}{2}$  inches above rail level. Apparatus in (5) A telegraph wire connects the ramp with a switch in the Signal Box through Signal Box. a contact attached to the distant signal arm. This switch is attached to the lever controlling the distant signal, so that when the lever is operated to place the distant signal to the "Off" (Proceed) position, an electric battery is connected to the ramp, provided the signal has correctly responded to the movement of the lever. When the lever is replaced to restore the signal to the "On" (Proceed with caution) position, the battery is disconnected from the ramp. The ramp is, therefore, electrified when the distant signal is "Off." When the distant signal is "On," the ramp is electrically "Dead," as is also the case in the event of the battery failing, or the arm not responding correctly to the lever, or the telegraph wire breaking. Apparatus on the Engine. (6) The apparatus on the engine comprises a contact shoe with switch, and an electrically-controlled combined brake valve and siren, and an electric bell, in the The contact shoe is fixed in the centre line of the engine and projects to within 21 inches above rail level, in which position it is held by gravity assisted by a powerful spring. It is capable of being raised vertically, and being in line with the ramp it is lifted one inch whenever a ramp is passed over. This lift of one inch is utilised for effectively opening a switch attached to the contact shoe. The switch is connected with the electrically-controlled brake valve and siren in such a way that whenever it is opened it results, except as hereafter described, in air being admitted through the siren and the brake valve to the train pipe, sounding the siren and applying the automatic brake on the train. This happens when an engine passes over an unelectrified ramp. The Driver, by acknowledging the warning given by the siren can stop the siren sounding and stop the application of the brakes. This he does by raising a handle provided for the purpose,

When the ramp is electrified by the distant signal being placed in the "Off" (Proceed) position, the brake valve is not released by the engine passing over the ramp, but the bell on the Engine rings instead. The contact shoe is lifted as before, but the current is picked up from the electrified ramp, the effect of which is to cut out, or render inoperative, the switch attached to the contact shoe; so that, although the switch is opened it does not release the valve admitting air through the siren to the train pine.

When an engine is at a stand and remains thus for more than half an hour, the automatic battery switch operates and cuts the battery off from the cab apparatus, thus economising battery power. This battery switch is operated by the vacuum maintained in the engine reservoir. When the vacuum is restored by the Engineman the automatic switch pulls up and closes the battery circuit and energises the cab

apparatus.

Failures

Single lines.

(7) In the event of a failure to pick up the electric current when a ramp is passed over, the effect on the engine apparatus is the same as though the ramp was not electrified, that is, the valve admitting air through the siren to the train pipe is opened, and the automatic brake is applied on the train, thus ensuring that any failure of the electrical apparatus shall produce the warning indications irrespective of the position of the signals.

(8) On engines fitted for working over single lines, the apparatus is so arranged that the indications in the cab are only given when passing over the ramps applicable to the direction in which the train is travelling.

(9) The special attention of Enginemen is directed to Rule 82 of the Rule Book, (10) If from any cause the position of the distant arm should not accord with the audible signal received in the cab of the engine, it must in all cases be treated as a "Caution" Signal, unless a handsignalman is present and exhibits a green hand signal.

# AUTOMATIC TRAIN CONTROL.

## NORMAL.

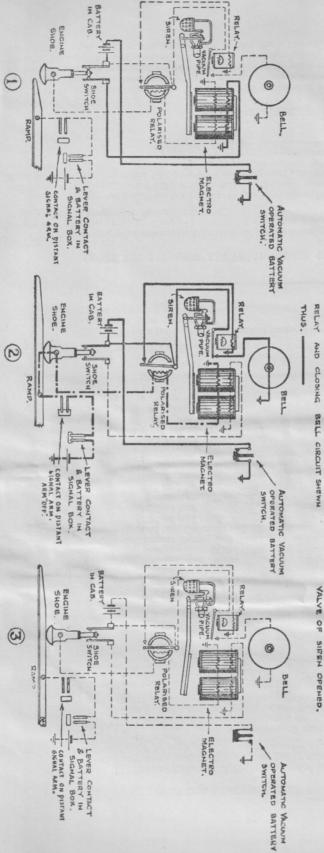
CLOSED CIRCUIT HOLDING UP ARMATURE

## PROCEED (Distant Signal "Off.")

# PROCEED WITH CAUTION (Distant Signal "On.")

SHOS SWITCH, NO CURRENT FROM SICHAL BOX.

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#### AUTOMATIC TRAIN CONTROL SYSTEM IN USE ON THE G.W.R .- Continued.

(11) Printed forms (No. 4,074) are provided for the purpose of enabling Drivers to record any irregularity in the working of the automatic train control apparatus, and these should be properly filled up as occasion requires and handed in when booking off duty at the end of the trip.

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(12) The existing distant signals will continue in use (except on the Fairford Single Line Branch), and the audible signals will therefore at present only supplement, and not take the place of, the semaphore distant signals on double lines.

The automatic train control system is in operation on the undermentioned sections of the line (double line sections except where otherwise indicated).

#### AUTOMATIC TRAIN CONTROL SYSTEM IN USE ON THE WESTERN OPER-ATING AREA.

The second paragraph of Clause 12 to be amended to read:-

The automatic train control system is in operation on the undermentioned sections of the line (double line sections except where otherwise indicated). Paddington and Didcot (Four lines.)

West Ealing and Greenford.

Henley Branch.

Reading and Penzance via Westbury.

Didcot and Newbury.
Thingley Junction and Bradford Junction.
Bathampton and Westbury.

Castle Cary and Weymouth. Newton Abbot and Paignton.

Didcot and Taunton via Bath.

Swindon and Severn Tunnel Junction via Gloucester.

Wootton Bassett and Bristol and Severn Tunnel Junction via Badminton.

Severn Tunnel Junction and Fishguard.

Skewen and Llandilo Junction. (Swansea District Line.)

Old Oak Common and Saltney Dee Junction via Birmingham. Didcot and Aynho Junction.
Oxford and Wolverhampton via Worcester.

Worcester and Newport.

Fairford Branch. (Single Line.)

Handsworth Junction and Stourbridge Junction.

Tyseley and Gloucester. Shrewsbury and Hereford.

(G.A.30 Op.-9/54 LKI/11041/232)

#### Automatic Train Control Ramps under Repairs.

While an automatic train control ramp is under repair and out of use, a Handsignalman must be provided at the distant signal to repeat the distant as is ordinarily done during fog or falling snow.

#### COUPLINGS STRIKING AUTOMATIC TRAIN CONTROL RAMPS.

Cases have occurred of automatic train control ramps being struck by the ball on lever of screw connection of other railway Companies' vehicles, when the screw connection is not in use and the "D" shackle is hanging on the carrying hook.

Chains are provided on the headstocks of dertain other Companies' covered goods wagons, refrigerator vans, etc., to hold the screw connection levers above the regulation clearance of 4 ins. from rail level, and care must be taken when coupling such vehicles to see that the levers are properly secured by the chains.

#### TRACK CIRCUITS.

#### General Description.

A track circuit indicates in the Signal Box the presence of a train on any particular section of line so equipped. This is accomplished by passing an electric current through the rails so that the connection from the battery at one end of the section to the relay or magnet at the other is established directly through the actual running rails. The rails through which the current passes are bonded together by wire, and, at the extremities of the track circuit, insulated joints are provided by

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#### Action to be taken if bell indication received when the Distant Signal is at 'Caution.'

Should a Driver receive the bell indication "Proceed" at a ramp, but find that the Distant signal controlling the ramp is at "Caution" he must stop at the Signal Box to which the Distant Signal applies and inform the Signalman who must advise the Signalman at the Signal Box in rear in order that subsequent trains passing through the section may be stopped and the Drivers advised that the A.T.C. ramp is defective. The Signalman first advised of the failure must promptly call the Lineman and the District Operating Superintendent or District Traffic Superintendent must be notified immediately.

The Distant Signal concerned must be maintained at "Caution" until the Signalman to whom the failure was first reported is advised by the Lineman that the ramp is in order when he must notify the Signalman at the Signal Box in rear that normal working may be resumed.

An entry must be made by each Signalman in his Train Register Book shewing the time the defective A.T.C. ramp is reported. The Signalman at the Signal Box to which the Distant Signal applies must upon being advised that the failure has been rectified notify the Signalman at the Signal Box in the rear. Each Signalman must then enter the time in the Train Register Book, and the entry in the Train Register Book at the Signal Box to which the Distant Signal applies must be countersigned by the Lineman.

(G.A.30 Op.—9/54 O.M./12652).

#### TRACK CIRCUITS.—Pages 82-83.

The following to be added at the end of these instructions:-

Sand-use of by Enginemen.

Enginemen must, as far as practicable, avoid the use of sand when standing on or passing over track circuits, lock bars, points or crossings. If, however, it is essential that sand be used in these circumstances the minimum quantity necessary should be used.

(G.A.30 Op .- 9/54.)

The "General Instructions" at the end of these regulations amended to read:-General Instructions.

1. Signalmen must watch the action of track circuit indicators each time a train or engine passes on to or off that portion of the line to which each indicator applies, and if it fails to give a correct indication, immediately advise the Lineman.

If an indicator fails to show when a train or vehicle is on the track circuit the lineman must be advised by the most expeditious means, and arrangements made for the vehicles to be examined as soon as possible and, except as shown below, they must be taken out of traffic and held until such examination has been made.

Should the vehicle or vehicles be loaded and undue delay would be caused to the contents, or in the case of an empty vehicle specially required for traffic, a wire must be sent to the destination or exchange station requesting that an examination be made there.

In every case of failure of a vehicle to operate a track circuit a detailed report must be sent to the District Operating Superintendent or District Traffic Superintendent from the Station at which the failure occurred.

An entry must be made in the Train Register Book of all irregularities in the working of track Facing circuits.

In case of emergency during the time a track circuit is out of order, a lock may be released by the Lineman after an understanding with the Signalman, to allow a signal to be restored to normal, or points moved to either normal or reverse, on the written authority of the Station Master, Assistant Station Master or District Inspector.

An entry must be made in the Train Register Book and signed by the Signalman and Lineman.

- 3. In no circumstances must a lock be released to allow a signal to be lowered or a block instrument be released to allow "LINE CLEAR" to be given when the track circuit controlling either is out of order.
- Referring to Rule 83; if, in consequence of a derailment, permanent way operations, signalling alterations or other exceptional causes, one or more track circuits are occupied for an extended period, certain movements cannot be signalled even though they would be clear of the obstruction the Lineman at the request, IN WRITING, of the Station Master, Assistant Station Master or District Inspector may, after satisfying himself that the obstruction is clear of the movement to be made, give the necessary release. The Signalman must make use of the lever collars or other similar devices where provided and points both facing and trailing which are locked by the track circuit concerned must be secured by clip or scotch until the movement is completed.

A separate release must be given for each Movement.

After each movement has been completed the Lineman must restore the full track circuit controls. The time and details of the release and the restoration of the controls must be entered in the Train Register Book on each occasion and signed by the Signalman and Lineman and also the Station Master, Assistant Station Master or District Inspector as the case may be.

- 5. Enginemen must not throw cinders on track circuited portions of the lines.
- An Engineer's trolley, motor trolley or velocipede must not be relied upon to operate t circuits and protection must be provided in accordance with the Rules or Instructions applica
- Before a trolley is placed upon, or run over, any portion of a line that is track circuited, permission of the Signalman in whose box the track circuit in Indicated must be obtained.

(G.A.30 Op .-- 9/54 LKI

#### METHOD OF WORKING TO BE ADOPTED DURING FAILURE OF TRACK CIRCUITS OR RELAYING OF TRACK CIRCUITED LINES.—Page 83.

The following to be added to the last paragraph :-

In addition the Locomotive Foreman or Inspector must be immediately advised of any failure which is attributed to the presence of sand on the rail.

(G.A.1. 3/37. LK.1/4332.)

#### METHOD OF WORKING TO BE ADOPTED DURING FAILURE OF TRACK CIRCUITS OR RELAYING OF TRACK-CIRCUITED LINES.-page 83.

The following additional instruction to be added as the third paragraph:

In the case of failure of a track circuit controlling facing-point lock or motor-worked points in advance of a Home Signal where the track circuit does not continue up to the rear of the Starting Signal, in consequence of which the Home Signal is not locked at danger by the failure, the Distant Signal must be maintained at "Caution".

(G.A. 18. 11/47. L.K. 1/8706/Gen.)

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--- The second of the page of ---Facing Points controlled by Track Circuits.-Emergency Release. When track circuits are provided in lieu of Facing Point Lock Bars, in certain cases emergency

releases will be provided and the following Instructions must be carried out in the event of a failure of the track circuit:-When the track circuit falls and the facing point lock lever cannot be restored to normal, the

Emergency Release provided in the signal box must be used and the following instructions must be carried out by the Signalman in the order shown:-(a) Satisfy himself by personal observation, or by assurance from a responsible person, that

no track circuit controlling the Facing Point Lock lever is fouled by any vehicle and that it is in order for the facing points to be reversed or restored to normal as the case may be. (b) Break the glass on the front of the box containing the release plunger.

(c) Operate plunger and restore F.P.L. lever to normal, thus releasing the points. (d)

The facing points can then be operated normally and when the F.P.L. lever is reversed the appropriate signal, if not locked by the track circuit which has failed, can be lowered, provided the detection is intact. An entry must be made in the Train Register Book with the time whenever the glass is (6)

broken. The entry must be signed by the Signalman, who must promptly report the circumstances to the Station Master and Lineman. While the track circuit lock is out of use it will be necessary to press the emergency (f)

plunger on each occasion when the F.P.L. lever is to be restored to normal.

(g) While the track circuit remains out of use a Handsignalman or competent man must be appointed as required by Rule 77(e). (h)

When the track circuit is out of use the Signalman must, before restoring the F.P.L. lever to normal, satisfy himself by personal observation, or by assurance from the Handsignalman or competent man, that any train or engine signalled to pass over the facing points has passed clear of same. The Station Master must specially report to the District Operating Superintendent or District

Traffic Superintendent every occasion on which the glass front has been broken and must see that

it is renewed immediately normal working is resumed, and that the paper label fixed to the new glass bears the Lineman's signature and the date replaced. An entry must be made in the Train Register Book showing the time the track circuit is again in order and normal working resumed, and this must be signed by the Lineman.

The Emergency Release Plunger is for use in connection with the failure of the track circuit only.

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#### TRACK CIRCUITS-Continued.

means of some non-conducting material, in order to confine the current to that particular section.

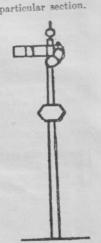
The normal position of the indicator in the signal box is "Track Clear," and when the Track Circuited Section is occupied the indication shews "Track Occupied," the next stop signal in rear being then locked at danger. In some cases an the next stop signal in rear being then locked at danger. illuminated diagram is provided and where necessary, the occupation of the track circuit to the rear of a Home signal, or within the clearing point, prevents a following train being accepted on the block instruments.

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Usually to the rear of home starting, or advanced starting signals fixed a long distance from the signal box, or where there is not a good view from the box. Platform lines which cannot be easily seen from the signal box have also been

At a number of important junctions and stations, home signals a quarter of a mile from inner home signals have been provided to enable Signalmen to accept a mue from more nome signals have been provided to enable Signalmen to accept trains on each converging line under "Line Clear." Track circuiting has been installed to the rear of such signals to advise Signalmen of arrival of trains and

avoid necessity of Trainmen going to box. Where track circuiting is provided white enamelled notice plates, as shewn in the margin hereof, are fixed on Signal Posts or adjacent thereto as a reminder to the Trainmen that it is unnecessary for them to proceed to the signal box to carry out Rule 55, except that the Engine whistle or Klaxon horn, where no whistle is provided, must be sounded in accordance with the Rule.



#### "VEHICLE ON LINE" SWITCHES IN CONNECTION WITH TRACK CIRCUITS.

Where platform lines are track circuited, "Vehicle on Line" Switches are sometimes provided. The switch must be placed to the "Vehicle on Line" position whenever the platform is occupied by a vehicle or vehicles without engine, and must be replaced to the "Line clear of vehicle" position immediately the line is clear. The door of the switch box must not be left open.

When the switch is operated it will lock the stop signal in the rear, and also operate the indicator in the signal box in the same manner as the presence of a train or engine on the track circuited portion

Whenever a vehicle or vehicles are left or placed on the track-circuited portion of line, and there of line would do. is no engine attached, the Shunter or person conducting the shunting must personally operate the proper switch in order to protect such vehicles, and the switch must not be put back to its normal position until the vehicle or vehicles have been removed, and the line is free from obstruction.

Track circuits must not be relied upon to protect vehicles to which an engine is not attached, and the person in charge of the station concerned must see that these instructions are properly carried out.

#### METHOD OF WORKING TO BE ADOPTED DURING FAILURE OF TRACK CIRCUITS OR RELAYING OF TRACK CIRCUITED LINES.

Addition to Standard Rule 81 (b) to be observed during Failure of Track Circuits, Track Circuit Indicators, or Relaying of Track Circuited Lines.

When a track circuit to the rear of a home signal or Indicator applying thereto is out of order, and a Handsignalman has not been appointed, a train must not be accepted from the box in rear until the line is clear to the starting signal, or to the standard clearing point if beyond the starting signal. The Signalman in rear must be advised of the failure and he must carry out the provisions of Rule 81 (b)

When a track circuit to the rear of a starting or advanced starting signal or Indicator applying thereto is out of order, and a Handsignalman has not been appointed, a train must not be allowed to proceed towards that signal, except for shunting purposes, until the block indicator has been placed at "Line Clear" and the signal has been lowered for the train to proceed into the forward section.

Where a Handsignalman is not employed and a platform switch in connection with a track circuited line is provided, during the time that such track circuit or platform switch is out of order, a man must be deputed by the person in charge to advise verbally the Signalman of any vehicles detached on the

track circuited line concerned, also when it is clear.

The Station Master or person in charge must advise the Divisional Superintendent or District Traffic Manager immediately any failure occurs, and forward full particulars of the temporary arrangements made.

#### TRACK CIRCUITED BLOCK SECTIONS.

Where a track circuit extends through a short block section from the most advanced stop signal of one signal box to the outermost home signal of the box in advance, the locking is as under

The block instruments are free of any locking with the track circuit, but the starting signal for home signal where no starting signal is provided) governing the entrance to the track circuited section, is controlled by the "Line Clear" indication of block instrument and by the track circuit.

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#### SIGNAL DEPARTMENT MEN ASSISTING IN SNOW STORMS.

When a heavy snow storm comes on and looks like continuing for some time, and men who are engaged at out stations are compelled to cease work owing to the storm, they should report themselves to the Permanent Way Inspector or Ganger, and ask if their services are required. If such are accepted they must obey any instructions given to them.

Men employed at Depot stations must hold themselves in readiness to render assistance, and the Inspector in charge of the Depot should ask the Permanent Way Inspector or Ganger if any assistance

is required, and instruct his men accordingly

If the Permanent Way Inspector at Reading requires any further assistance than can be furnished him by the Signal and Telegraph Inspector, he may apply to the Signal Engineer at the Reading Signal Works, who will arrange for assistance to be given.

Linemen will be exempt from assisting other men if their services are required in connection with their own duties, it being understood that they should devote their attention to seeing that the locking bars, facing points, etc., are in working order.

If assistance is required from the Signal Department after working hours, the Permanent Way Inspector will apply to the Signal and Telegraph Inspectors at the various Depots and the Signal Engineer at Reading for the necessary men.

The Permanent Way Inspectors have a supply of brooms, shovels, and salt, but men of the Signal

Department should take with them whatever brooms and shovels are available.

The time occupied in clearing snow must be appropriated to the Engineering Department.

#### SNOW-PLOUGHS.

1. Snow-ploughs, which must be kept ready for immediate use, are stationed in charge of the The comparison of the following Depots:-

Aberdare. Neath. Reading. Tondu. Banbury. 2. Croes Newydd (2). Newport (Ebbw Jct.). Shrewsbury. Tyselev. Newton Abbot. Westbury Dowlais. Southall. Stourbridge Jct. Wolverhampton. Bristol. Exeter. Oxford. Gloucester. Plymouth (Laira). Cardiff (Canton). Swindon. Worcester. Cardiff (Cathays). Hereford. Pontypool Road. Taunton. Yeovil. GA23. Dowestry. Machynlleth. 2. Carmarthen.

2. Whenever there is any likelihood of the line becoming blocked by snow, the Station Master, must communicate by the quickest means available with the Locomotive Department Foreman at

the nearest of the above-mentioned Depots.

A Snow-Plough, attached to a locomotive of suitable type, will be sent to the place where its services are needed. If a suitable engine is not in readiness, a goods train may be stopped and shunted, and the engine employed to work with the plough. Whenever possible not less than six labourers with shovels should accompany the engine and plough to render assistance, and a suitable vehicle must be provided for the men to ride in if this can be done without causing undue delay.

3. Immediate information as to a possible blockage of the Line by snow must also be given by the Station Master to the Divisional Engineer, the Permanent Way Inspector, and the Permanent Way Ganger, whose duty it is to make instant and vigorous efforts to keep the Line clear, and to furnish every assistance that may be at their command.

4. When a Plough cannot be obtained promptly, and it appears imminent that the Line will become blocked, arrangements should be made for an Engine to work over the threatened Section, in order to keep the rails as clear as practicable, and to prevent an accumulation of snow. This precaution is especially needed on Lines on which comparatively few Trains are run; and on Branch Lines on which no Trains usually run by night or on Sundays.

5. The Station Master must report to the Divisional Superintendent or District Traffic Manager, and to the Superintendent of the Line at Paddington, the circumstances connected with each case, stating the locality and duration of the block, with the length and depth of the drift that had to be cut through.

#### DAMAGE TO ELECTRIC CABLES BY RATS AND MICE. THROWING AWAY SCRAPS OF FOOD.

Damage has been caused to electric cables, etc., by rats and mice, which are enticed to certain places owing to scraps of food being thrown about by men working in the vicinity. All concerned are requested to avoid doing this, in order to prevent such damage.

#### DETONATORS. PERIODS KEPT IN STOCK. PAINTING OF DETONATORS.

The attention of the staff is directed to the importance of carrying out the instructions in regard he use and storage of detonators in Rules 35 58. to the use and storage of detonators in Rules 58.

A sufficient supply of detonators must be kept ready for use at each station, depot and signal box. Except where instructions are issued to the contrary, detouators must be returned to the Stores Department at the expiration of three years from the date stamped upon them for redistribution. Stations receiving redistributed detonators from Stores Department must use such detonators before continuing to use their normal stocks. At places where authority is given to use detonators up to five years old, they must be returned to the Stores Department at the expiration of that period. Detonators must not, under any circumstances, be used after they are five years old.

#### Vear ending-Iune 30th.

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1943/4 1944/5		1946/7 1947/8 1948/9 1949/50	•••	

Reference to the CLEARING POINT

In order to facilita places on the system.

- (a) Steam lar (b) Armoured
- (c) Brass con
- (d) "Y" conr from one engine.
  - (e) One pair

The equipment is where the operation of Signal Box selected by one of the Signal Box

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  - 3. The flexible hos
- 4. The Driver is himself that the two-wa position to direct steam or Engineering Departs with the Driver as to th can be closed down to a pressure built up in the
- 5. Salt should be a melting snow turning t
- 6. After the points Signal Box from which
- 7. The Station Ma-tion of such equipment available and that there

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Reference to the following to be made on page 84.

#### CLEARING POINTS OF SNOW-USE OF STEAM LANCES.

ond X. Van Second

In order to facilitate the clearance of snow from points, special equipment has been supplied to certain places on the system. The equipment comprises the following:

- (a) Steam lances to be attached to the steam cock on an engine by means of the flexible hosepipe.
- (b) Armoured flexible hose—issued in 20ft. lengths.
- (c) Brass connection—used for coupling two 20ft. lengths of hose.
- (d) "Y" connection—provided at certain specified places to enable two steam lances to be operated from one engine. This enables two lengths of hose to be coupled to the steam cock.
  - (e) One pair of leather gloves for each steam lance set.

The equipment is intended for use at any place in the vicinity of the Signal Box to which it is allocated, where the operation of points is impeded by snow. When not in use the equipment will be stored in the Signal Box selected by the Divisional Superintendent or District Traffic Manager; the gloves to be kept in one of the Signal Box Lockers.

The following instructions must be observed by all concerned:-

- 1. When the equipment is required, the Station Master or Signalman must apply to the Station Master who supervises the nearest Signal Box where the apparatus is stored, or in his absence to the Signalman on duty in such Box, and must also advise the Locomotive Department Foreman. The latter to be informed whether an engine is available in the vicinity of the place concerned.
- 2. If an engine is not available at the place concerned, the Station Master must advise the Locomotive Foreman concerned, the approximate time at which the steam lance or lances will be ready for use.
  - 3. The flexible hosepipe should be taken under the rail to avoid the possibility of the hose being damaged.
- 4. The Driver is responsible for coupling up the apparatus to the steam cock, and he must satisfy himself that the two-way cock on the smokebox, where fitted on certain classes of engines, is in the correct position to direct steam to the lance cock. The steam jet must be directed on to the switches by any Traffic, or Engineering Department staff available, who will be responsible for coming to a proper understanding with the Driver as to the amount of steam to be admitted to the hose. The steam cock on any locomotive can be closed down to any extent to reduce pressure of steam in the hose and there should be very little steam pressure built up in the rubber hose if throttled down as necessary.
- 5. Salt should be applied immediately after the use of the lances to prevent the water formed by the melting snow turning to ice. Supplies of salt for this purpose are kept by the Permanent Way staff.
- After the points have been cleared of snow, the equipment must be returned immediately to the Signal Box from which it was obtained so that it may be available if required at some other point.
- 7. The Station Master who supervises the Signal Box where the equipment is stored must make inspection of such equipment at monthly intervals in order to satisfy himself that the whole of the equipment is available and that there is no sign of deterioration.

  (G.A.16. 5/46. L.K.1/6172/5)

#### DETONATORS. PERIODS KEPT IN STOCK. PAINTING OF DETONATORS.-

The following information regarding the painting of detonators to be inserted on page 85:—
Year ending—June 30th.

1950/51		Blue.	1954/55	Red.
1951/52		White.	1955/56	Green.
1952/53		Black.	1956/57	Grev.
1953/54	****	Brown.	1957/58	Yellow.
			(G.A.26 Op —	5/50 1 K 1/69

40 Gen.)

DETONATOR "PLACER" MACHINES-page 85.

Delete the third and fourth paragraphs and substitute the following:-

The special detonators provided for use with the machines are obtainable from the Stores Department. They must not be used for any other purpose.

When placing detonators in the machines care must be taken to see that the metal strips are, where necessary, bent in such a manner as to secure the detonator in the machine. The clearance between the bottom of the detonators and the top of the rail, when the detonators are in position for exploding, must not exceed one-eighth inch and Signalmen must adjust the detonator concerned if the clearance is more than this.

The detonators fixed in the machines must be replaced on the first Monday in each month and a record made in the Train Register Book when the change is made, and the Station Master will be responsible for seeing this is done. Those taken from the machines must be returned to the Stores Department.

Station Masters must frequently examine each detonator placer under their control, in order to satisfy themselves that the detonators are in good condition, and should it be found on making such examination that one (or both) of the detonators are damaged in the slightest degree, fresh detonators must be at once substituted, and the matter reported to the District Operating Superintendent or District Traffic Superintendent.

The Ganger or other authorised person, when walking his length, must immediately advise the Signalman of any detonators which have been exploded, damaged, or displaced in any detonator placer on his length, and replace a detonator where necessary. For this purpose the Ganger must have a few detonators of these types in his possession.

Each Signalman must, on commencing duty each day, test the operating lever stirrup and, as far as is practicable, see that the apparatus places the detonators properly on the line. The apparatus must also be tested when the Ganger or other authorised person passes the signal box whilst he is examining the line.

If at any time the detonators are exploded, the Signalmen must take steps to have fresh detonators immediately placed in the apparatus and will be held responsible for so doing. The Signalman must record in the Train Register Book the time when and the train by which they are exploded and the time when they are replaced. He must also report the circumstances to the Station Master, a report afterwards being sent to the District Operating Superintendent or District Traffic Superintendent.

A supply of not less than 12, but not more than 24, detonators must always be kept on hand. Any defect in the apparatus must be immediately reported to the Signal Lineman.

(G.A.29.Op.—5/52. LK1/10730/31.E.)

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#### No. 15.

2.55 p.m. Paddington to Pembroke Dock. Van Second X, Second X, Compo X, Bk. Compo X, Paddington to Pembroke Dock. Compo X, Second X, Van Second X, Paddington to

Milford Haven.

Kitchen Buffet, Compo Dining Saloon, Compo X, Van Second X, Paddington to Swansea.

#### No. 16.

3.30 p.m. Paddington to Plymouth. Van Second X, Second X, Second X, First (7) X, Second X, Van Second X, Refreshment Car, Bk. Compo X, Paddington to Plymouth.

Bk. Compo X, Compo X, Second X, Van Second X, Paddington to Paignton.

#### No. 17 (M).

b3.33 p.m. Paddington to Bristol (T.M.).

9†25 p.m. Bristol (T.M.) to Malago Vale. b Bk. Van 98 or 185 (RR), Old Oak Common to Neyland, proceeding from Bristol (T.M.) 1.5 a.m. Monday. Note.—This vehicle will pass by this service only when the 2.2 p.m. (Fish Empties) Old Oak Common to Neyland the previous day has been cancelled. (Set No. 400 MO.)

#### No. 18.

4.15 p.m. Paddington to Plymouth.

Van Second X, Second X, Second X, Compo X, Compo X, Second X, Second X, Van Second X, Paddington to Plymouth.

#### No. 19.

5.30 p.m. Paddington to Exeter. Van Second X, Second X, Second X, Second X, Compo X, Compo X, Second X, Second X, Second X, Bk. Compo X. Paddington to Exeter.

#### No. 21.

5.55 p.m. Paddington to Swansea. Bk. Compo X, Second X, Compo X, Kitchen Buffet, Compo Dining Saloon, Second X, Van Second X, Paddington to Swansea. Second X, Second X, Compo X, Second X, Van Second X, Paddington to Cardiff.

#### No. 22.

6. 5 p.m. Paddington to Trowbridge. Bk. Compo X, Second X, Paddington to Trowbridge unbalanced. Van Second X, First X, Second X, Van Second X, Paddington to Trowbridge, balanced 7.15 a.m. Trow-Van, Paddington to Trowbridge, unbalanced. (Forms No. 402 next day.)

#### No. 23.

6.35 p.m. Paddington to Cheltenham.
Van Second X, Second X, Second X, Second X, First
(7) X, Refreshment Car, Second X, Van Second X, Paddington to Cheltenham. Second X, Compo X, Van Second X, Paddington to Swindon, balanced 9.0 a.m. Cheltenham.

#### No. 24.

7.15 p.m. Paddington to Bristol (T.M.). 10†0 p.m. Bristol (T.M.) to Malago Vale. Van Second X, Second X, aCompo X, aCompo X, Second X, Van Second X, Paddington to Bristol (T.M.), balanced to Paddington MO 6.40 a.m. Bristol (T.M.) to Swindon (Week-day Set No. 468). Van Second X, Compo X, Second X, Van Second X, dirigton to Bristol (T.M.). (Set No. 378 MO.)

#### DETONATORS-Continued.

In order to ensure that detonators shall not be kept on hand for more than five years, arrangements are made for detonators to be withdrawn as may be necessary from stations with low consumption and re-issued to those stations using more within the effective life of five years.

Detonators will be painted externally a different colour for the period, July 1st to June 30th, each vear.

Year ending December 3	31st,	1931		 Red.
,, ,, ,,	,,	1932		 Green.
	99	1933		
January to June 30th, 1				 Yellow
Year end	ling	June 3	Oth.	Pad

	7	Year ending	g June 30th	1.	D 1
1934/5	 	Blue.	1938/9		 Red.
1935/6	 	White.	1939/40		 Green.
1936/7	 	Black.	1940/1		 Grey.
1937/8	 	Brown.	1941/2		 Yellow.

Each detonator will be clearly stamped on the domeplate so as to show the month and year (in figures) of manufacture, and the type (e.g. single, duplex, etc.). All detonators used on this Company's system are of the single chamber type.

The month and year (in figures) of manufacture will also be shown on the label outside each packet

It must be clearly understood by all concerned that until the existing stock of "green" detonators of detonators. is exhausted none of the "grey" (1933) detonators are to be used. Similarly none of the "blue (1934-5) detonators must be used until the stock of "green" detonators has been exhausted, and in like manner with each succeeding issue. These arrangements must be properly observed to ensure that detonators over three years old are not kept in stock on any portion of the system, except those between 3 and 5 years old, re-distributed by the Stores Department for use to authorised stations.

The stock (including loose detonators and those supplied to Enginemen, Signalmen, Guards, Gangers, Undermen, also those kept in all Guards' Brake Vans), must be examined by the person in charge of the station or depot, on whose paybill the Staff is included during the first week of March and September of each year, and any that are more than three years old, or that bear any signs of rust, must be returned to the Stores Department. Any detonators withdrawn must, where necessary, be replaced, a special requisition on Form 224 being sent to the Stores Department, Swindon.

When a packet of detonators is opened the whole of them must be examined, and should any show signs of rust on the outside of the case, none of them must be used, but the whole of the packet must

be returned at once to the Stores Department.

Should any detonator fail to explode at any time when a train or vehicle passes over it, the circumstances must be promptly reported to the Divisional Superintendent or District Traffic Manager, and the defective detonator forwarded to him for examination.

Station and Yard Masters on whose paybill the Staff is included are responsible for seeing that

these instructions are properly carried out by the Staff under their supervision.

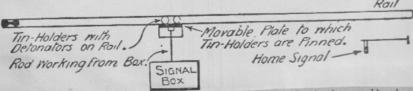
District Inspectors must also visit and examine the stock and storage arrangements in all signal boxes, Guards' and Porters rooms and Shunters' cabins once a year.

#### DETONATOR "PLACER" MACHINES.

Where detonator "Placer" machines have been provided, the Signalman is able by means of a lever in the Locking Frame to place two detonators on the rail in advance of the home signal. At places where these machines are fixed it will not be necessary for the Signalman to place also two other detonators on the line in front of his box, in order to comply with Clause 4 (e) of the Double Line Block Telegraph Regulations.

THESE MACHINES MUST ALSO BE MADE USE OF FOR THE PURPOSE OF WARNING THE DRIVER OF ANY TRAIN WHICH IS FOUND TO BE APPROACHING AT TOO FAST A SPEED TO BE ABLE TO STOP AT THE HOME SIGNAL IF THAT SIGNAL IS AT DANGER, OR IN ANY OTHER CIRCUMSTANCE WHEN IT MAY BE NECESSARY

TO GIVE WARNING TO TRAINMEN.



In order that the machines may always be ready in case of emergency, the signalmen working them must frequently examine them to satisfy themselves that they are in proper working order and that the deconators

wo fresh detonators must be placed in the machines every month by the Signal Lineman when going rounds, who must inform the Signalman that he has changed the detonators in order that a note of the fact may be made in the Train Register book by the Signalman and the entry must be counterpagned by the Signal are intact G.A.15-12/44 O.M.12180)

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#### DETONATOR "PLACER" MACHINES-Continued.

Whenever the detonators are exploded, the Signalman on duty at the time must report the facts to the Station Master, who must communicate them to the Divisional Superintendent or District Traffic

Manager. The Signalman must also record the circumstance in the train register book.

Arrangements have been made for special tin fixings to be supplied for these machines, which will prevent the second detonator being blown out of the clip by the explosion of the first, and when a fresh supply of tin clips is required the requisition should clearly state that they are for use in the detonator "Placer" machines and must be supplied with the special fixings.

The Signal Lineman must be immediately advised should any defect be found in the apparatus.

#### "THREE-SHOT" DETONATOR MACHINES WORKED IN CONJUNCTION WITH TRAILING POINTS.

These machines are provided at certain junctions and work in conjunction with the points. - In some cases machines are provided on each converging line at a junction, one set of detonators being normally "on" and the other "off."

In order that the machines may always be ready in case of emergency the Signalman working them must frequently examine them to see that the detonators are intact and work properly on to

Three resh detonators will be placed in the machines every month by the Signal Lineman when going his round. The latter should inform the Signalman that he has changed the detonators in order that a rote of the last may be made in the Train Register Book by the Signalman. The old detonators taken out of the machine in this way must be returned by the Station Master to the Stores Department on the st June and 1st December each year with the name of the sensing station.

Station Masters and District Inspectors must examine the machines at frequent intervals and

satisfy themselves that they are in proper working order.

The Signal Lineman must immediately be advised should any defect be found in the apparatus. Whenever the detonators are exploded, the Signalman on duty at the time must report the facts to the Station Master and District Inspector, who will communicate them to the Divisional Superintendent or District Traffic Manager.

The Signalman must also record the circumstances in the Train Register Book and see that the

machine is immediately recharged with detonators.

When vehicles are shunted from one line to another, Signalmen should be careful not to turn the points until they are certain the vehicles have passed beyond the machine; and Shunters must be careful to place any vehicles they may be shunting clear of the machine to avoid damage to it.

#### WHISTLE BOARD.

Whistle boards, as illustrated in the diagram below, are provided:-



The letters "SW" signify "Sound Whistle," Drivers must sound the engine whistle when passing these boards.

Instructions for Signalling Trains during FOGS and FALLING SNOW and in Frosty Weather. See General Manager's Circular.

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## "THREE SHOT" DETONATOR MACHINE WORKED IN CONJUNCTION WITH TRAILING POINTS.—Page 86.

The third paragraph of the above instructions to be deleted and the following substituted:—

The Station Master must see that three fresh detonators are placed in the machine every month and a note to the effect that the detonators have been changed, must be made in the Train Register Book. Where the change is not made by the Signalman the person appointed to change the detonators must countersign the entry. The old detonators taken out of the machine in this way must be returned by the Station Master to the Stores Department on the 1st June and 1st December each year with the name of the sending station.

(G.A.30 Op.-9/54 LKI/10730/363E)

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# SECTION II.

### CENERAL INSTRUCTIONS AFFECTING THE WORKING OF TRAINS.

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	Passenger and freight trains			139 to 166
(c)	Freight trains	 	 	 168 to 186

## SECTION II. (a).

### INSTRUCTIONS CONCERNING PASSENCER TRAINS.

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Slip Carriage Working									 • •	 89
Vacuum Brake Regulatio	ns								 	 98
Chain communication on									 	 101
Formation of Passenger '	Trains in	ncludir	ng Prov	ision o	f Guard	ls and	Hand !	Brakes	 	 109
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WATER TROUGHS, G.W. AND JOINT LINES.-Page 89.

The following to be added :-

Water Troughs, Clearance of Ice.

- 1. The Length Ganger will be responsible for keeping ice broken and cleared from the troughs.
- 2. The Length Ganger must use his discretion when the ice thickens as to when the troughs should be closed, but must not allow the ice to become more than  $\frac{1}{8}$  in. thick before arranging for the troughs to be closed. If the temperature is so low that the water freezes as quickly as it is cleared the troughs must be closed.
- 3. If ice collects in the four foot to a depth of 1 in. due to spillage the troughs must be closed.
- 4. When the Length Ganger finds it necessary to close the troughs as set out in Clause 2, he must advise the nearest Signalman who will notify the District Controller. In turn the District Controller will immediately notify the following:—

Chief Mechanical and Electrical Engineer's Outdoor Machinery District Mechanical Foreman or Local Mechanical Chargeman and responsible Water Fitter whichever is most convenient.

Motive Power Depots.
District Motive Power Superintendent.

District Motive Power Superintenden

Headquarter's Control.

Chief Mechanical and Electrical Engineer's Works Manager.

District Engineer.

The Length Ganger must also advise his Permanent Way Inspector.

- 5. When a thaw sets in after prolonged frost the Chief Mechanical and Electrical Engineer's Outdoor Machinery District Mechanical Foreman or Local Mechanical Chargeman or responsible Water Fitter, whichever is most convenient, after consulting with the Ganger, will say when normal taking of water can be resumed, and will advise the nearest Signalman and arrange for the water to be turned on. The Signalman must, in turn, advise the District Controller, and he will notify the Departments listed in Clause 4 that normal working of the trough can be resumed.
- 6. The District Operating Superintendent to notify promptly the Chief Operating Superintendent, who will issue the necessary instructions in regard to the taking of water while the water trough is out of use.

#### TRAIN INSTRUCTIONS. PASSENGER

### WATER TROUGHS, G.W.R. AND JOINT LINES.

Water Troughs are situated as under :-

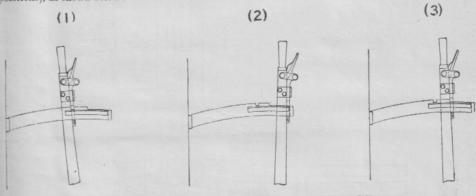
Situated between	Miles.	Chains.	Miles.	Chains.	Lines served	Length— yards.	
Pangbourne and Goring and Streatley	45 45 111 111 159 199 114 103 150 240 2 81 114	32 34 <sup>3</sup> 1 27 52 13 78 45 <sup>1</sup> 4 70 8 <sup>1</sup> / <sub>2</sub> 44 <sup>1</sup> / <sub>3</sub> 18 <sup>1</sup> / <sub>4</sub> 28 27	43 45 111 111 159 200 114 104 150 240 2 81	$\begin{array}{c} 60 \ \left\{ \\ 59^{\frac{7}{4}} \\ 52 \\ 29^{\frac{1}{2}} \\ 37^{\frac{7}{2}} \\ 23 \\ 70^{\frac{1}{2}} \\ 14 \\ 34 \\ 72 \\ 43^{\frac{1}{4}} \\ 53 \\ 52 \\ \end{array} \right.$	Both Main. Both Relief. Both. Down. Up. Both.	620 620 560 553 495 560 560 620 524 560 620 560 560 560	SAI. BOTH MAIN BOTH RELIEF
Charlbury and Ascott-under- Wychwood	77	70 22½	78 26	15 48	Both.	560 613	

### SLIP CARRIAGE WORKING.

Description of Apparatus and Instructions for Working.

MAIN TRAIN \_ (RUNNING POSITION)

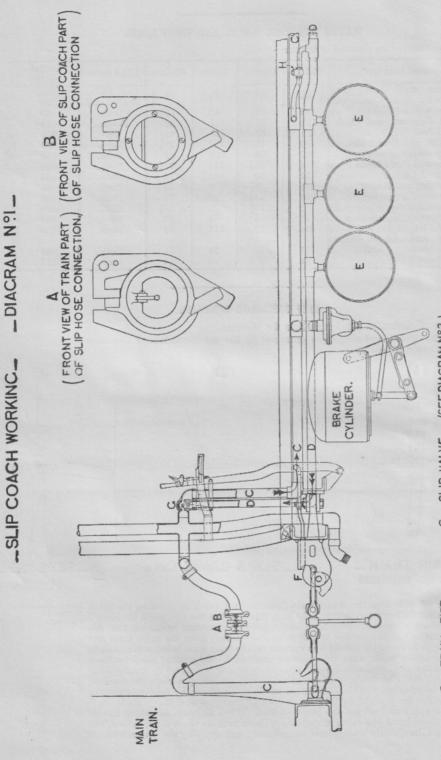
1. The operation of slipping is performed by the use of one lever (with three positions), as shewn below :-



\_SLIP & BRAKE ON\_

2. In order to slip, the Guard must first raise the catch by pressing the catch Description and handle against the lever and then pull the lever right back to the "Slip and Brake on" working. position, which releases the slip hook and applies the vacuum brake on the slip portion, causing the slip portion to fall away immediately from the main train and the hose pipes to be pulled apart; the vacuum on the main train being automatically the hose pipes to be pulled apart; the vacuum on the main train being automatically sealed by a spring valve when the pipes are separated. By putting the lever back to the midway or "Brake Released" position, where it is stopped by means of the catch, which must be down, coming against a stop on the quadrant, the vacuum brake can be released, and the Slip Guard has it in his power, by manipulating the lever between the upright or "Brake Released" position and the "Brake on" position to apply and release the brake at will. When slipping, the catch handle must be released and the catch allowed to drop as soon as the lever is pulled, the lever being subsequently manipulated for releasing and applying the brake, without

\_RELEASE\_



- DIACRAM NºI-

σi TRAIN PIPE. RESERVOIR. SLIP HOOK. COWI

SLIP VALVE. (SEEDIAGRAM N°2.)
VALVE CONNECTING TRAIN PIPE WITH RESERVOIR PIPE.
THIS VALVE IS SO CONSTRUCTED THAT AIR MAY BE DRAWN FROM.
BUT CANNOT RETURN TO,THE RESERVOIRS.

#### SLIP CARRIAGE WORKING .- Page 91.

The following additional paragraphs to be added to Clause 4.

- (d) The rear vehicle of the main train to which the slip coach is to be attached and from which it is to be slipped must always be a Western Region or B.R. Standard stock type vehicle. Other Region's vehicles are prohibited from working as the rear vehicle of the Main Train and if it is necessary for such vehicle to be conveyed on the train it must be marshalled inside the rear Western Region or B.R. standard vehicle of the Main Train or arrangements must be made for the train to stop at the Slipping Station.
  - (e) When a B.R. Standard coach is the last vehicle on the Main Train, a special gangway door special steam pipe safety chain bracket, and coupling safety clip for use on the buckeye draw hook, must be used for the attachment of the slip portion.

(G.A.30 Op.—9/54 LKI/8247/2/Gen.)

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#### SLIP CARRIAGE WORKING-Continued.

touching the catch handle, and the catch not raised again until it is required to replace the lever in the running position. The number of times the brake can be released will depend upon the number of vehicles attached to the slip coach. Should there be the maximum number of vehicles, probably only three releases could be made after which the brake could not be released by means of the lever; but with the Slip Carriage only there might be as many as six or seven separate applications and satisfactory releases.

3. The Slip Guard must, before starting, see that the slipping lever is firmly slipping Lever to secured in the "Main Train" or "running" position by the cotter being placed in the each rod, and in the case of double-ended slips, that the levers at both ends are so placed and secured.

A small padlock is provided in every Slip Guard's compartment, attached by Provision of Padmeans of a chain to the slipping apparatus. Holes are also drilled in the ends of the cotters which hold the catches down, and, by means of padlocks, the cotters can be locked in position.

Each Slip Guard will be supplied with a key fitting the padlocks, which he must retain in his possession and always have with him when on duty.

(a) EXCEPT IN CERTAIN SPECIAL CASES AUTHORISED BY THE SUPERINTENDENT Coupling of Slip OF THE LINE, slip carriages must be coupled to the train in the ordinary way by the screw coupling of the slip coach until the train arrives at the last station at which it is BOOKED REGULARLY TO STOP prior to passing the station where the slip portion has to be detached. At this point, the coupling must be adjusted and placed in the slip hook.

(b) Great care must be exercised when screwing up the coupling to the slip Adjustment of HOOK IN ORDER TO AVOID THE SCREW COMING SO NEAR TO THE HOOK AS TO PREVENT IT Screw Coupling. TURNING OVER EASILY WHEN THE COACH IS SLIPPED. BEFORE COUPLING UP, THE SCREWS AT BOTH ENDS OF THE COUPLING SHOULD BE EXAMINED AND ADJUSTED, SO THAT THE SAME LENGTH OF SCREW SHALL BE INSIDE EACH OF THE SHACKLES. IF THIS IS NOT DONE, IN SOME CASES THE SCREW ON THE SLIP CARRIAGE SIDE WILL PROJECT SO FAR INTO THE SHACKLE BEFORE IT IS TIGHTENED THAT WHEN THE TIGHTENING PROCESS HAS BEEN COMPLETED THE SCREW WILL PRESS AGAINST THE SLIP HOOK AND PREVENT IT FROM TURNING OVER. The coupling must be screwed up in the same manner as when connecting ordinary coaches together.

(c) At the point where the Slip apparatus is adjusted, the Slip Guard must be Adjustment of present and make the necessary movements with the slipping lever to enable the Slip Couplings connector to couple the vehicle. The Slip Guard must then, by means of the cotter, secure the lever in the correct running position and lock the cotter by means of the special padlock provided for the purpose, and must not unlock the padlock until the train has started from the station at which it last stops before reaching the slipping point. In those cases where the slip couplings are adjusted at the starting point of the train, the padlock must not be unlocked until the train has proceeded some distance on its journey. The Slip Guard must also be careful to see, before giving the signal to start, that the cotter is in its proper place and padlocked.

On arrival at the slipping station the padlock must be left hanging on its chain until it is again required to be used on a slipping journey.

5. (a) Before leaving the last stopping place prior to slipping, the Slip Guard's duties before must see:

:(1) That the couplings and the hose pipe adaptors are properly connected, last stopping and that the flexible pipes are securely looped up by means of the chains and place. brackets provided.

(2) That the slipping lever is secured and padlocked in the "Main Train" or "Running" position.

(3) That 23 inches at least is indicated by the vacuum gauge in the slip compartment.

(4) That the proper Slip signals are carried, including a white head light after dusk.

(b) If 23 inches is not registered on the vacuum gauge the slip portion Not to Slip if less MUST NOT BE SLIPPED, BUT THE TRAIN MUST STOP AT THE SLIPPING STATION.

than 23 inches of Vacuum before leaving last leaving last stopping place, of Slipping

18.

#### PASSENGER TRAIN INSTRUCTIONS.

Slip Carriage Working-page 91. Delete paragraph (a) of clause 6 and substitute the following :-

(a) The Guard of each Slip portion will be responsible for seeing by examination and testing before the train starts or at the station authorised for the adjustment of the Slip apparatus, where he joins the train otherwise than at the starting point, that the brake, slipping and other apparatus are in good order. He must also see that the prescribed Slip Carriage signals are attached at the point of adjustment. (G.A.27.Op.-1/51. O.M. Min. 12556.)

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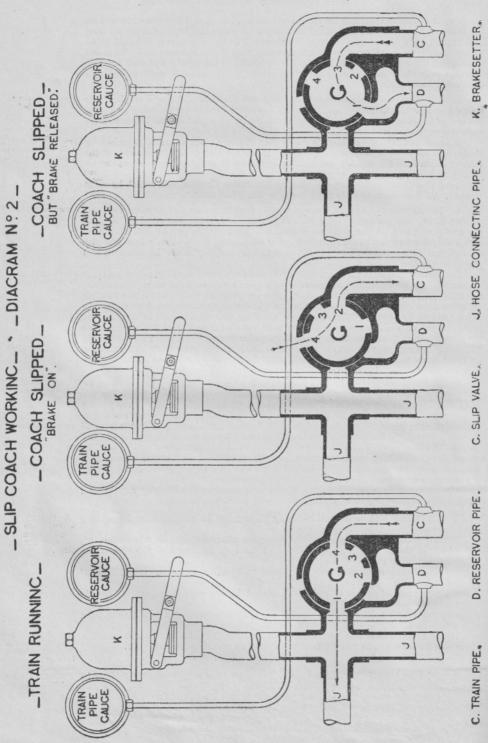
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### PASSENGER TRAIN INSTRUCTIONS. SLIP CARRIAGE WORKING—Continued.



### SLIP CARRIAGE WORKING-Continued.

(b) Should the slip hook or other apparatus, INCLUDING THE VACUUM BRAKE, be defective, the slip portion must be attached by the screw coupling of the slip coach to the main train, and the train must stop at the slipping station.

Slip Guards and the staff responsible for starting trains conveying coaches to be slipped should clearly understand that although no absolute defect may be apparent in the apparatus, if there is the slightest reason to suppose that the slipping apparatus is not in perfect order, and that difficulty may possibly arise in slipping, an order must be given for the train to stop at the Slipping Station.

(a) THE POINT AT WHICH A SLIP CARRIAGE IS REGULARLY SLIPPED WILL BE FIXED BY THE DIVISIONAL TRAFFIC AND LOCOMOTIVE SUPERINTENDENTS, WHO WILL ISSUE THE NECESSARY PRINTED OR WRITTEN INSTRUCTIONS TO ALL CONCERNED. The Slip Guard must exercise great care as to the point at which the slip carriages are detached, having regard to the number of carriages in the slip portion, the gradient of the line, the state of the rails and other circumstances.

(b) The Slip Guard must be careful to maintain a sufficient interval between the Slip portion and the train, so as to enable him to stop the slip portion clear of the train in the event of the latter slackening speed or coming to a stand from any cause before reaching the platform.

(c) The Driver or Fireman and the Guards must look back where the Slip portion is usually detached, and on the side on which it can best be seen, in order to satisfy themselves whether or not it is properly detached, but no attempt must be made to stop the train unless the Slip Guard has opened the cock provided below the brake

8. The Slipping Lever must on no account be touched until just before reaching the slipping point, or if speed has been reduced, such point nearer the station as the Slip Guard may consider suitable. On approaching the station at which the slip carriages have to be left, the Slip Guard must look out for the distant signal, and if it is at "All right" and the train is running at the usual speed, and not less than 23 inches of vacuum are registered on the gauge, he must remove the cotter, and pull the lever back to release the Slip. He must then so manipulate the lever as to bring the Slip portion to rest at the proper point, but must, immediately after the slip is detached, apply the brakes with sufficient force to enable the main train to get well away from the Slip. The Guard, after slipping, must keep his hand on the Slipping Lever and look ahead until the Slip portion is at rest. In order to avoid stopping the slip portion with a sudden jerk, it will be found advisable to put the lever back, if possible, to the "Brake Released" position just before the slip portion comes to a dead stand.

In order that Slip Guards may be able to give audible warning of the approach of Slip Carriages after they have been slipped, bells have been provided on the Slip Carriages worked by a foot lever placed in such a position that it can be readily operated while at the same time leaving the Slip Guard's hands quite free to manipulate the slipping lever.

Whenever, after slipping, Slip Guards find it necessary to give warning of the approach of slip vehicles, they must sound the bell vigorously several times. This should, in any case, always be done before passing Level Crossings or entering stations.

10. If, when he pulls the slip lever, a Slip Guard finds he is unable to detach the slip portion, he must not attempt to replace the slip lever but must open the cock provided below the brake setter.

If 23 inches of vacuum are not registered on the gauge, no attempt must be made to

slip, but the Slip Guard must open the brake cock.

The Driver, on noticing that the vacuum gauge needle is falling, will acknowledge this indication by giving three short sharp whistles. On hearing this acknowledgment the Slip Guard must close the cock and allow the Driver to proceed, and the train must be brought to a stand well in advance of the platform at the slipping station, provided the necessary stop signals have been lowered.

The same action must be taken by the Slip Guard and the Driver in the event of the Distant Signal at a slipping station worked by one signal box, or the lower distant arm underneath the stop signal immediately to the rear of the platform at a station where there are two or more signal boxes, being passed by the Slip Guard at Danger, in consequence of which the slip portion cannot be slipped.

Should the slip portion of a train be conveyed beyond the station at which it should have been slipped, the Slip Guard must not take any further action in regard to the slipping apparatus, but the Signalman must advise the box in advance by speaking instrument, where, unless the section is short (in which case the advice must be sent on immediately to the next Signal Box, by speaking instrument) the

When the Slip Coupling or other apparatus is defective.

Slipping of

Bells on Slip

When unable to

#### SLIP CARRIAGE WORKING-Continued.

train must be stopped and the Driver and Main Train Guard advised of the circumstances. The slip lever must be secured and padlocked in the Main Train (running) position and the key handed to the Main Train Guard. At the same time the Tail lamps must be altered to agree with the proper tail signals normally carried by the train over that section of the line, and the slip portion taken on to the most convenient station, the Driver and Guards being instructed where the vehicles are to be detached.

11. On bringing the Slip Carriage to a stand after slipping, the Slip Guard must, before leaving the Slip compartment, serew the hand brake on tightly.

12. Immediately after the slip portion has come to a stand the tongue of the slip hook must be replaced in its normal position. The Guard must then secure the Slip Lever in "Main Train" or "running" position by means of the cotter provided for the purpose.

Mode of dealing with adaptors after slipping.

stopped.

Guard to secure Slipping Lever after Slip has

13. Two vacuum hose pipe adaptors are required for each Slip, and they will be fixed by a Carriage Department man at stations where that Department has a staff. The adaptor and chains left on the main portion of the train must be removed by a Traffic Department man at the first stopping place after slipping. The box for carrying the adaptor and chains will be found in the van, and this, with the adaptor and chains, must be promptly returned to the station to which it belongs, by first available train, booked as a parcel. The adaptor, or adaptors, and chains left on the hose pipes of Slip Carriages after slipping must be removed by a Traffic Department man after arrival of the vehicles at the platform or appointed place and placed in the boxes provided for the purpose, which will be found in the slip ends of the coaches, and returned promptly to the station to which they belong by the first available train, booked as parcels.

Hose pipe adaptors are also provided for use in connection with the steam heating pipes, when the latter are in use, and these must be dealt with in a similar manner to the vacuum pipe adaptors.

Great care must be exercised in handling the adaptors so as to avoid damage, and every effort must be made to guard against loss and ensure that the articles are properly dealt with.

Two vacuum gauges on Slip Carriages. 14. A second vacuum gauge lettered in red "Reservoir," is fitted in the Guard's compartment of slip coaches, which indicates the amount of vacuum created in the additional vacuum reservoirs.

Slip Guards should observe this gauge, as if from leakage or other cause it records a lower amount of vacuum than the main train gauge the vacuum brake will not be as readily released as usual on application after slipping.

Slip Carriage to be accompanied by a Guard. Slip Carriage indicators. 15. (a) Every slip carriage or set of slip carriages to be slipped must be accompanied by a Guard, who must ride in the Slip brake compartment.

(b) When Slip carriages are run on a train, Slip carriage indicators, as prescribed in Rule 125, must be carried.

Special Tail Lamp for use on Trains to which Slips are attached. 16. The last vehicle of the main portion of a train conveying slip coaches will carry a special tail signal consisting of a double white tail lamp showing red lights by night placed vertically as shewn below between the station at which the slip coupling is adjusted and the station at which the train is first booked to stop after slipping:—



Guards in charge of such trains must, before the trains start from the station at which the coupling is adjusted, see that the special double white tail lamp with red lens is placed on the rear vehicle of the main portion of the train instead of the ordinary tail lamp, and on arrival at the first station at which the trains are booked to stop after slipping, the special tail lamp must be removed and the ordinary tail lamp substituted for it.

The ordinary tail lamp must be carried in the rear Guard's van of the main portion of the train from the station at which the slip coupling is adjusted, in order that no delay may arise in promptly replacing it on the rear of the main portion of the train at the first booked stopping station after slipping. The main train Guard will be responsible for lighting the ordinary tail lamp in sufficient time before reaching the first booked stopping station after slipping to ensure the lamp burning properly when placed on the rear of the main or through portion of the train.

THE DOUBLE WHITE TAIL LAMP WITH RED LENS WHEN REMOVED FROM THE MAIN PORTION OF THE TRAIN MUST BE RETURNED TO THE STATION TO WHICH IT RELONCS AS INDICATED AN THE MAIN PORTION OF THE TRAIN MUST BE RETURNED TO THE STATION TO WHICH IT RELONCS AS INDICATED AN THE MAIN PORTION OF THE TRAIN MUST BE RETURNED TO THE STATION TO WHICH IT RELONCS AS INDICATED AN THE MAIN PORTION OF THE TRAIN MUST BE RETURNED TO THE STATION TO WHICH IT RELONCES AS INDICATED AN THE MAIN PORTION OF THE TRAIN MUST BE RETURNED TO THE STATION TO WHICH IT RELONCES AS INDICATED AN THE MAIN PORTION OF THE TRAIN MUST BE RETURNED TO THE STATION TO WHICH IT RELONCES AS INDICATED AND THE MAIN PORTION OF THE TRAIN MUST BE RETURNED TO THE STATION TO WHICH IT RELONCES AS INDICATED AND THE MAIN PORTION OF THE TRAIN MUST BE RETURNED TO THE STATION TO WHICH IT RELONCES AS INDICATED AND THE MAIN PORTION OF THE TRAIN MUST BE RETURNED.

#### SLIP CARRIAGE WORKING-Continued.

THE REAR OF THE MAIN PORTION OF THE TRAIN AT THE FIRST BOOKED STOPPING STATION AFTER SLIPPING. THE MAIN TRAIN GUARD WILL BE DESPONSIBLE FOR MIGHTING THE ORDINARY TAIL LAMP IN SUFFICIENT TIME DEFORE BEACHING THE FIRST BOOKED STOPPING STATION AFTER SLEPPING TO ENSURE THE LAMP BURNING PROPERLY WHEN PLACED ON THE REAR OF THE MAIN OR THROUGH PORTION OF THE TRAIN

THE DOUBLE BED TAIL LAMP WHEN REMOVED FROM THE MAIN PORTION OF THE TRAIN MUST BE RETURNED TO THE STATION TO WHICH IT BELONGS, AS INDICATED ON THE BRAIN LABEL AFFIXED TO IT, BY THE FIRST AVAILABLE TRAIN

THE DIVISIONAL SUPERINTENDENTS AND DISTRICT TRAFFIC MANAGERS MUST ABRANCE FOR SPECIFIC PERSONS AT THE VARIOUS STATIONS TO BE APPOINTED TO SEE THAT THE ARRANGEMENTS MADE ARE CARRIED OUT.

A LIST OF THE SERVICES ON WHICH THE SPECIAL LAMPS WORK, AND THE TRAINS BY WHICH THEY ARE RETURNED TO THE FORWARDING STATION, IS SHEWN IN THE THROUGH COACH PROGRAMME.

17. EXCEPT FOR THE PURPOSE OF SLIPPING, THE COUPLING MUST ONLY BE USED Use of Slip Hook. IN THE SLIP HOOK IN CASES OF NECESSITY. GREAT CARE MUST BE EXERCISED WHEN SHUNTING SLIP VEHICLES HAVING SLIP CONNECTIONS IN ORDER TO AVOID DAMAGE TO THE SLIP HOOKS.

18. Slip hooks on slip carriages are liable to be strained and damaged through Damage to Slip vehicles being shunted with the vacuum brake on. Before any attempt is made. Hooks, etc. therefore, to move such vehicles the hand brake must be properly taken off and the vacuum brake released by means of the cord or wire when the tongue of the slip hook is in its normal position and the slip lever is in the "Main Train" or "Running" position.

19. When it is necessary to shunt two or more Slip Carriages coupled together, Shunting two it is of the utmost importance that the vacuum brake be released by means of the coupled together. cord or wire after the operation of coupling up has been performed as, when there is a vacuum in the reservoirs, the necessary movement of the Slipping Lever during the process of coupling applies the brake.

### Clause 20 to be amended to read:

(a) Unless instructions to the contrary are issued by the Superintendent of the Line, when a train, which conveys slip carriages, has to be run in two portions from its STARTING POINT, OR IS DIVIDED AT ANY STATION AFTER IT HAS COMMENCED ITS JOURNEY, the first portion must not carry the usual slip carriage lamps, but will carry the ordinary single tail lamp. The second train must convey the slip portion and carry the prescribed slip carriage lamps. In addition, the slipping station and the intermediate stations and signal boxes must be advised by notice when practicable, or when this cannot be done then by speaking instrument. done, then by speaking instrument.

(b) WHEN A TRAIN WHICH CONVEYS A SLIP CARRIAGE OR CARRIAGES IS RUNNING LATE, AND AN EXTRA TRAIN HAS TO BE DESPATCHED IN ADVANCE OF IT FROM ANY INTERMEDIATE STATION, THE FIRST PART MUST BE CONSIDERED THE SPECIAL, AND CARRY AN ORDINARY SINGLE TAIL LAMP, THE SLIPPING STATION AND THE INTERMEDIATE STATIONS AND SIGNAL BOXES MUST BE ADVISED BY SPEAKING INSTRUMENT. THE SPECIAL, IF NOT MORE THAN TWENTY MINUTES IN ADVANCE OF THE REGULAR TRAIN AT THE STATION WHERE THE SLIP IS DETACHED, MUST RUN THROUGH THE SLIP STATION OR STATIONS INSTEAD OF STOPPING THERE. IF THE REGULAR OR SECOND PORTION OF THE TRAIN WILL BE MORE THAN TWENTY MINUTES BEHIND THE SPECIAL OR FIRST PORTION OF THE TRAIN AT THE SLIP-DETACHING STATION, THE LATTER MUST STOP AT THE SLIP-DETACHING STATION IF THERE ARE ANY PASSENGERS TO PUT DOWN. THE SECOND OR LATE PORTION OF THE TRAIN IN EITHER CASE MUST CONVEY THE SLIP CARRIAGE OR CARRIAGES (FURNISHED WITH THE SLIP CARRIAGE TAIL LAMPS), TO BE SLIPPED AT THE PROPER PLACES IN THE USUAL WAY.

SUPERMEMENT OF THE WHEN IT IS NECESSAI (G.A. 18. 11/47, R.C.H. Op. Sup. Min. 7946.)

SUPERMEMENT OF THE MEDICAL STATION OF THE TRAIN IN EITHER CASE OF THE SLIP CARRIAGE OR CARRIAGES (FURNISHED WITH THE SLIP CARRIAGE TAIL LAMPS), TO BE SLIPPED AT THE PROPER PLACES IN THE USUAL WAY.

SUPERMEMENT OF THE WHEN IT IS NECESSAI (G.A. 18. 11/47, R.C.H. Op. Sup. Min. 7946.)

slipped at a station where one is not usually slipped, an advice must be issued by the Divisional Superintendent to all Stations and Signal Boxes between the preceding stopping station and the station where the carriage has to be slipped. The Station Masters, on receipt of such advice, must immediately advise all persons concerned, including the Permanent Way Gangers working in the neighbourhood of the station at which the carriage is intended to be slipped, but in no case should it be arranged to slip a carriage under this Regulation without ample time being afforded for complying with the instructions contained therein,

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#### SLIP CARRIAGE WORKING-Continued.

When Permanent Way operations are in progress at Slipping Stations, etc. 22. (a) If any engineering work should be in progress of a character to render it undesirable to slip carriages, or if, owing to fog, falling snow or other cause, the Station Master, or person in charge at a place where a carriage is usually slipped considers it desirable that the train should stop instead of the carriages being slipped, he must give instructions that the signals applicable to the slipping train are not to be lowered until the train has come to a stand at the first stop signal after passing the point at which the slip carriages are usually slipped, when that signal may be lowered and the train admitted to the platform. In such cases slip carriages must not be detached until the

TRAIN HAS COME TO A STAND AT THE PLATFORM.

22 (b) When practicable, the station at which the train last stops must be advised by telegram or telephone of the circumstances. The Station Master, or other person in charge receiving this message, must immediately acknowledge it to the sending station, issue form 2079, and specially instruct the Driver, the Slip Guard, and the other Guards of the train that the train must stop at the slipping station instead of slipping the carriages, and he must see that the Slip Guard locks the slipping lever in the running position and hands the padlock key to the Main Train Guard before the train is allowed to proceed. On arrival at the slipping station the Main Train Guard must return the key to the Slip Guard to allow the latter to unlock the padlock and release the slipping lever. The lever must remain in the "brake on" position until the main portion of the train has started. The slip hook must then be replaced in the normal position, and the slip lever in the running position. The flexible pipes and adaptors will be dealt with at the usual stations.

If it is not practicable to wire the station at which the train last stops in sufficient time to advise the trainmen there, the advice must be sent to the station next in rear of the slipping station. This must be immediately acknowledged, and the train must be stopped there and the trainmen advised by the Station Master or other person in charge and form 2079 issued. The slipping lever must also be padlocked in the running position and the padlock key handed to the main train guard, who must retain it in his possession until the train arrives at the slipping station.

If train has to stop instead of slipping.

(c) When no slip carriage is available, and no notice has been issued that the slip carriage will not be run, the Station Master or other person in charge at the station where the slip carriage should be attached, must instruct the Driver and guards that the train must stop at the slipping station, and the Station Master or person in charge, at any intermediate station at which the train is booked to call, must satisfy himself that the trainmen understand they are required to stop at the slipping station. If from any cause arising at the last stopping station a train is required to stop at a station where it ordinarily slips, the Station Master or person in charge at such last stopping station must instruct the Driver and Guards that the train must stop at the slipping station, and he must, if a slip carriage or slip carriages are on the train, see that the Slip Guard locks the slipping lever in the running position and Hands the paddock key to the Main Train Guard before the slipping station. (See paragraph 31).

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In all such circumstances as those referred to above, or whenever vacuum adaptors have been fixed, the Slip Guard must ride in the brake compartment of

the slip carriage until the vehicle is detached.

Maximum number of carriages to be slipped. 23. When the slip portion consists of more than six 4- or 6-wheel vehicles, or four 8-wheel vehicles, the ordinary screw couplings must be used (the screw coupling on the main train vehicle must not in such circumstances be placed in the slip-hook), and the train must be stopped at the station at which the slip portion is usually detached, that station being also advised by speaking instrument that the train will stop, unless it is decided to run the train in two parts, for instructions as to which see Paragraph 20 (a).

For the purpose of this regulation, four 70-feet vehicles may be considered as equivalent to four 8-wheel vehicles, and not on the basis of 10 wheels per 70-feet

coach.

Slipping at stations where there is more than one signal box. 24. (a) In those cases where there are two or more signal boxes at the slipping station, the Slip Guard may sometimes find the distant signal, at or near which he is instructed to slip, at "Caution." He must, if this is so, refrain from slipping unless and until he can plainly see that the stop signal ahead of him, immediately in the rear of the platform, and the distant arm beneath it have been lowered for the train to run past the platform. When slipping in such circumstances, Slip Guards must make due allowance for the reduced distance they will have to run before reaching the platform.

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The last sentence of the first paragraph of Clause 22 (c) to be amended to read:

The Station Master must also issue Form 2079 and advise by speaking instrument the slipping station. (See paragraph 31.)

(G.A. 18. 11/47. L.K. 1/8326/Gen. 4.)

SLIP CARRIAGE WORKING-Pages 89 to 98.

Clause 31.—Delete second sentence in the first paragraph and substitute the following:— THE SLIP CARRIAGE TAIL SIGNALS MUST BE REMOVED AT THE USUAL SLIPPING STATION IN THOSE CASES WHERE THE TRAIN STOPS INSTEAD OF SLIPPING AND THE DOUBLE WHITE TAIL LAMP SHOWING RED LIGHTS PUT ON. THE DOUBLE WHITE TAIL LAMP MUST BE CARRIED AS FAR AS THE STATION AT WHICH THE TRAIN IS FIRST BOOKED TO STOP AFTER LEAVING THE SLIPPING STATION. (G.A.23—7/49. L.K.1/9379/37.)

(b) BLOCK ARMS. STATIO OF THE TRAIN ' WORKE TO SLII 25 THE SL BE STO REVERS HIMSEL 26

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#### SLIP CARRIAGE WORKING-Continued.

(b) CLAUSE (A) OF DOUBLE LINE BLOCK REGULATION 4A OF THE DOUBLE LINE Block BLOCK INSTRUCTIONS WITH REFERENCE TO THE WORKING OF LOWER DISTANT SIGNAL Regulation 4A. ARMS, MUST NOT BE APPLIED TO THE WORKING OF TRAINS SLIPPING CARRIAGES AT STATIONS. UNLESS "LINE CLEAR" HAS BEEN OBTAINED SUFFICIENTLY FAR AHEAD OF THE SLIPPING STATION TO ENABLE ALL THE SIGNALS APPLICABLE TO THE SLIPPING TRAIN TO BE LOWERED, "LINE CLEAR" MUST NOT BE RETURNED TO THE SIGNAL BOX WORKING THE DISTANT-SIGNAL WHICH SERVES AS AN INDICATION THAT ALL IS RIGHT TO SLIP.

25. AT STATIONS WHERE THE TRAIN RUNS THROUGH THE MIDDLE ROAD, AND Where the Slip-THE SLIP PORTION IS DIVERTED TO THE PLATFORM ROAD, THE SLIP FORTION MUST ping train does be stopped at the facing point home signal, and the signalman must not the platform road. REVERSE THE POINTS OR LOWER THE PLATFORM HOME SIGNAL UNTIL HE HAS ASSURED HIMSELF THAT THE SLIP PORTION HAS COME TO A STAND TO THE REAR OF IT.

26. If, after the distant signal has been passed at "All Right" and the slip When Home portion has been detached, the home signal at a Slipping Station should be put to Danger or train "Danger" before the train has passed, or should any other necessity arise for the driver to reduce speed or stop the train after the slip portion has been detached, after Slip portion he must immediately whistle for the brakes of THE SLIP PORTION TO BE APPLIED after Slip portion BY SOUNDING THE BRAKE WHISTLE, and the Slip Guard must apply the vacuum brake to prevent the slip portion running into the train. The Slip Guard must not, however, rely upon the sounding of the whistle in such an emergency, but must keep a good look-out, after having detached the slip portion, so that in the event of any danger signals being exhitibed unexpectedly, he may be prepared to prevent the

slip portion from colliding with the train. 27. At certain places where the Distant signals cannot be easily seen, Repeating thing Distant repeating Distant signals working with the ordinary Distant signals, or separate Distant Signals.

auxiliary Distant signals, are provided in such positions that the Trainmen can more readily observe them. These additional signals are provided primarily for the Slip Guards, but the Enginemen are not relieved of their responsibility of observing them.

observing them.

at the slipping station to take water, or for any other essential purpose, he must increase the slipping station to take water, or for any other essential purpose, he must necessary bring his train to a stand at the station next in the rear of the slipping point or at to stop. a signal box intermediate between such station and the slipping point if the train has already passed such station and inform the Station Master, person in charge or Signalman, of his requirements. The person so informed must advise the slipping station accordingly, in order that the slip signal may be maintained at Caution, form 2079 must be issued, and the provisions of the first paragraph

If the Driver finds it necessary to stop his train otherwise than in obedience to a Danger signal after he has passed the station or signal box next in the rear of the slipping point, he must take care that he does not stop until the train is well clear of the platform at which the slip portion will have to stop, in order to avoid the slip portion coming into collision with the train.

of Clause 22 (b) must also be complied with.

(b) Where there is a Public Level Crossing between the place where the slip portion.

Where Public portion is detached and the station, the line must not be fouled between the train Level Crossing and the slip portion.

30. Should there be any irregularity in the working of the slip portion, the Irregularities to Guard must immediately report full particulars to the Superintendent, and also be reported. record the facts in his journal, AND CALL THE ATTENTION OF THE CARRIAGE EXAMINER AT THE FIRST EXAMINING STATION.

31. Except in the circumstances shewn in Clause 32, the slip carriage slip Carriage be indicators, whether the train is ordered to stop at the slipping station or indicators to be always contain. NOT, MUST ALWAYS BE CARRIED BY THE TRAINS ON WHICH SLIP CARRIAGES ARE trains on which ARRANGED TO RUN. THE SELP CARRIAGE TAIL SIGNALS MUST BE REMOVED AT THE they are arranged USUAL SLIPPING STATION IN THOSE CASES WHERE THE TRAIN STOPS INSTEAD OF SLIP, to run,

PING, AND THE ORDINARY TAIL LAMP PUT ON. WHEN SLIP COACHES ARE NOT ATTACHED TO A TRAIN WHICH IS SCHEDULED TO CONVEY SLIP PORTIONS, THE REAR COACH MUST CARRY THE USUAL SLIP LAMPS AND THE LAMPS FOR THE OTHER SLIP PORTIONS MUST BE PLACED IN THE VAN, SO THAT THEY MAY BE ATTACHED AT THE VARIOUS STOPPING PLACES, A WIRE TO BE SENT TO THE SLIPPING STATIONS ACCORDINGLY.

32. IN THOSE CASES, HOWEVER, WHERE THE SLIP CARRIAGES WHICH ARE USUALLY When train has to RUN ON A TRAIN ARE RUN AS A SPECIAL INSTEAD, AND NOT SLIPPED FROM THE call at ORDINARY TRAIN, THE TRAIN WHICH USUALLY SLIPS THE CARRIAGES MUST NOT CARRY Slipping Station. THE SLIP INDICATORS OF THE PORTION RUN AS A SPECIAL. IN SUCH CIRCUMSTANCES,

ollowing :-ING STATION THE DOUBLE L LAMP MUST CED TO STOP .K.1/9379/37.)

#### SLIP CARRIAGE WORKING-Continued.

THE TRAIN MUST CARRY ORDINARY TAIL LAMPS IF THERE ARE NO CARRIAGES TO BE SLIPPED, AND IF THERE IS ONE SET OF SLIP CARRIAGES THEN THE TRAIN MUST CARRY THE PRESCRIBED LAMPS FOR TRAINS SLIPPING AT ONE STATION, AND THE DIVISIONAL SUPERINTENDENTS WILL ISSUE A NOTICE TO THE STATIONS CONCERNED TO THAT EFFECT.

Station Master and 33. Station Masters and Inspectors must satisfy themselves that Guards work-Inspectors to see ing slip carriages are thoroughly competent, accustomed to the working of the slip that Guards are competent to work apparatus, and well acquainted with the duty and the places where they have to slip carriages.

Return of Discs or Lamps. 34. The Special Lamps must in every case be returned with the Slip Carriage unless otherwise specially ordered, and the Vacuum Brake and Steam Heating Hose Pipe Adaptors must be booked to the forwarding station from the station at which the train first stops after slipping a coach. Any irregularity in working them must be immediately reported to the Superintendent.

#### REGULATIONS FOR WORKING THE VACUUM BRAKE.

1. Description.—(a) The operation of the vacuum brake depends upon the creation and maintenance of the required vacuum.

The normal vacuum on the G.W.R. is 25 inches. It should in no case be more than 26 inches. On passenger trains it should be not less than 23 inches and, on freight trains, not less than 20 inches.

(b) The vacuum brake is continuous throughout the fitted and piped portion of the train, the amount of vacuum available in the train pipe for operating the brake being shewn on the gauges on the engine and in the brake vans. On many engines and in certain vans belonging to other Companies the gauge has two pointers, one indicating the amount of vacuum in the train pipe and the other the amount in the reservoir.

A "fitted" vehicle must be understood to mean a vehicle which carries its own brake apparatus connected by a branch pipe or pipes to the train pipe, and on which the brake blocks are operated

by the vacuum brake.

A "piped" vehicle must be understood to mean a vehicle which is equipped with a continuous pipe only, i.e., without brake blocks operated by the vacuum brake.

(c) The vacuum train pipes at the ends of "fitted" vehicles are painted black; the vacuum train pipes at the ends of "piped" rehicles are painted red. 4930

2. Operation of Brake.—(a) The brake is applied by admitting air into the train pipe and is taken off by closing the air valve and restoring the vacuum to the required amount.

(b) The brake is ordinarily applied by the Driver, but it can also be applied by the Guard. A partial application of the brake can also be made, in case of emergency, by pulling the passenger communication chain.

3. Before Starting.—(a) When the engine is attached to the train it is the duty of the person attaching to connect the hose pipe of the engine to that of the train, and the Driver must satisfy himself that these pipes have been properly connected. The hose pipes between the vehicles must also be properly connected, and the hose pipes at the leading end of the engine, and at the rear end of the last fitted or piped vehicle connected through to the engine, must be placed on the stop plugs. When an additional engine is attached in front of the train engine the Driver of the leading engine must satisfy himself by personal observation that the hose pipes between the two engines are properly connected.

When the hose pipes have been connected the Driver must create the required vacuum. He must then place the ejector in the running position and satisfy himself by observing the gauge that there is no undue leakage. Should he not be able to create the necessary vacuum, or should he find such undue leakage as will prevent him from being able to maintain the necessary vacuum, he must, after satisfying himself that the brake apparatus on his engine is in proper order, at once inform the Guard or Station Staff, who must take steps to have the train examined. Should the brake on any vehicle be defective the brake on such vehicle must be rendered inoperative, or the vehicle detached, or any other steps taken as may be necessary.

(b) When the engine has been attached to the train, or an engine is changed, or an additional engine or vehicle is attached or detached, the Guard, or the Rear Guard where there is more than one Guard, must see that the necessary vacuum is registered on the guage in the rear van, and then open the brake setter or cock in (his brake van. It there is an inrush of air he will know that the hose pipes are properly coupled up between this van and the engine. If no inrush takes place, he must inform the Driver and also take steps to ascertain the defect, and have it remedied.

If there are vehicles behind the rear brake van the Guard must himself see that the brake pipes are properly coupled up and the rear hose pipe of the last fitted or piped vehicle is on the stop plug.

The Guard must see that the necessary vacuum is registered on the gauge in his van before giving the Driver the signal to start.

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## REGULATIONS FOR WORKING THE VACUUM BRAKE.—Page 98.

Clause I (c) of these instructions to be amended to read:—
The vacuum train pipes at the ends of "fitted" vehicles are painted red, the vacuum train pipes at the ends of "piped" vehicles are painted white.

(G.A.30 Op.—9/54 RI/5662)

The following to be added after the third paragraph of Clause 3 (b):-

Guards in charge of Auto trains must open the driver's Automatic Train Control Cut-out Switch before testing the vacuum brake and satisfying themselves that the required vacuum is registered on the gauge. When this has been done the Cut-out Switch must be closed immediately, showing "Not in use."

This instruction will also apply in the case of trailers formed in trains to act as a brake.

(G.A. 8.-5/41. LK1/5512/6.)

#### VACUUM BRAKE REGULATIONS-Continued.

Guards must serew the hand brakes off before starting, as laid down in Rule 129/iv (c), and affix the chain or strap where provided.

(c) The Driver must, before starting, satisfy himself that the gauge on the engine indicates the necessary vacuum.

The Driver must accept the signal to start given by the Guard, not only as an indication that the train is ready to proceed, but also as an assurance that the hose pipes are properly connected throughout the train and that the gauge in the rear van indicates the necessary vacuum.

(d) The Guard must, before starting, and at places where the engine is changed or any vehicle is attached or detached, inform the Driver the number of vehicles there are on the train, and IN THE CASE OF PASSENGER TRAINS, the weight of the train in tons. In the event of the brake not being in operation on the whole of the vehicles, the Guard must also inform the Driver upon how many it cannot be applied. At certain places, where authorised, the Station Master or other appointed person will give the Driver this information. In each case the Driver must have this information before he PROCEEDS ON HIS JOURNEY.

The instructions contained in the preceding paragraph do not apply to trains running daily with the formation unaltered, and the brake complete; if, however, the formation is altered or any vehicle is attached or detached, the Guard must advise the Driver.

(e) Care must be taken that the proportion of vehicles fitted with pipes only and not with brake apparatus does not exceed one in four in any passenger train running a distance of ten miles or under without a stop, nor one in six in any passenger train running more than ten miles without a stop.

For the purposes of this instruction, the number of vehicles forming a passenger train must be

counted as follows .-

Horse box, carriage truck, fish van, or	other	4-whee	eled vel	hicles r	ot car	rying		
passengers							As	vehicle,
Coaching vehicles 4- or 6-wheeled							,, 1	"
Coaching vehicles, 8- or 12-wheeled							,, 2	vehicles.
Articulated coaching vehicles, each bogi	е							vehicle.
Tank engine, 4- or 6-wheeled coupled								vehicles.
Tender engine, 4-coupled							,, 3	11
Tender engine, 6- or 8-coupled		.,					,, 4	91

Tables shewing the proportion in which piped vehicles may be run in Passenger trains IN ACCORDANCE WITH REGULATION 3, CLAUSE (e) OF THE REGULATIONS FOR WORKING THE VACUUM

Table shewing what proportion of piped vehicles may be attached to a Passenger train

Table shewing what proportion of piped vehicles may be attached to a Passenger train running more than 10 miles without a

running no	ot more than 10	miles w	a stop.	stop.						
To a trai	in (including e	ngine) o	: :	Unbraked vehicles equal to—	To a train (including engine) con- Unbrak sisting of braked vehicles equal to: vehicles equal to					
3	may be adde	d		1	3	may be add	ded	-	quar to—	
31				î	31				1	
4	"	.,		1	1	"			1	
41/2	"			11	41	"			2	
5	"			11/2	$\frac{4\frac{1}{2}}{5}$	**			12	
	"			$\begin{bmatrix} 1\frac{1}{2} \\ 1\frac{1}{2} \\ 2 \end{bmatrix}$	9	"	**		1	
51/2	"	**	**	12	$5\frac{1}{2}$	"			1	
6	"			2	6	"			1	
$6\frac{1}{2}$	,,	.,	**	2 2	$6\frac{1}{2}$	"			1	
7	"			2	7	, ,,			1	
71/2	"			21/2	$7\frac{1}{2}$	,,			11/2	
8	**			21/2	8	,,			11	
81/2	,,			$\begin{bmatrix} 2\frac{7}{2} \\ 3 \\ 3 \end{bmatrix}$	81	,,			1 1 1 1 1 1 1 1 1 1 1 1 1 1 1 1 1 1 1	
9	"			3	9	"			11	
91	"			3	$9\frac{1}{2}$	,,			11	
10	"			3	10					
101				$3\frac{1}{2}$	101	",			2 2 2 2 2	
11	"			31	11	"			2	
111	,,			31	114	,,		.,	9	
12	"	•••			12	***			9	
121	"			4		"			2 2 <del>1</del> / <sub>2</sub>	
13	***			4	121	***			25	
	"			4	13	"			21	
$-13\frac{1}{2}$	"			41	$13\frac{1}{2}$	"			21/2	
14	"			41/2	14	,,	19		21	
141	.,			41/2	141	,,			$-2\frac{1}{2}$	
15	**	.,		5 1	15	",			3	

#### VACUUM BRAKE REGULATIONS-Continued.

- (f) Should the brake not be in operation on any vehicle conveying passengers formed behind the rear brake van, such passengers (unless in charge of live stock) must be transferred to the vehicles in front of it unless another fitted vehicle on which the brake can be applied by the Driver is attached in rear.
- 4. During Journey.—(a) The required vacuum should be maintained throughout the journey except when it is necessary to apply the brake.

Should the Guard find during the journey that the gauge in the van shews less than the required vacuum (unless he is satisfied that this is caused by the Driver applying the brake) he must be prepared to apply his hand brake as may be required.

- (b) The Driver and Guard must report any irregularity in connection with the working of the brake or defect in its action, or other special circumstances, and call the attention of a Carriage Examiner to the matter at the first opportunity. The Guard must also note the particulars on his journal.
- (c) Where vehicles have to be attached or detached, the vacuum in the train pipe must be destroyed by opening the air valve on the engine or in the nearest brake van.
- (d) Should the Driver find that his train is being retarded owing to the brake blocks on all the vehicles not being off, he must stop, under the protection of fixed signals if practicable, and have the brakes properly released.
- (e) When two engines are attached to a train, the Driver of the leading engine must work the vacuum brake, as laid down in Rule 135 (a).
- (f) When it is necessary for the Guard to apply the brake to attract the Driver's attention in an emergency, or on becoming aware that the Driver is not going to make a booked stop, or from any other cause, he must apply the brake by opening the cock in his van and keep it fully open until he is satisfied that the Driver has become aware that the train is required to stop. He should then leave the Driver to bring the train to a stand at the most convenient situation.
- 5. Stopping. -(a) The vacuum brake must be used for the ordinary stopping of the train by the Driver. The brake should be so operated during the stop that it is not necessary to make a powerful application of the brake when the train is travelling at a low speed, but, in case of emergency, the vacuum should be destroyed as rapidly as possible.
- (b) Before finally coming to a stand, the vacuum should be partially recreated so as to prevent a rebound of the vehicles, or undue strain on the couplings.
- (c) After the brake has been applied, either slightly or fully, steam must not be applied to move the train until the brake has been released throughout the train.
- (d) The working of the vacuum brake by the Driver will not relieve the Guards from the responsibility of observing Rule 148.
- (e) In the case of a train becoming divided the Guard in the rear van must put his hand brake hard on and secure it with the chain or strap, where provided, before leaving his van. He must also take any other measures that may be necessary to prevent the rear portion moving.
- 6. Testing Brake when Running.—In addition to tests laid down in Regulation 3, Drivers must also test the brake in good time before reaching their full braking distance when approaching:-
  - (i) Steep falling gradients.
  - (ii) A terminus.
  - (iii) A principal station at which the train has to stop.

  - (iv) A crossing place on a single line at which the train has to stop.
    (v) AFTER RECEIVING A "WARNING" SIGNAL, THE DISTANT SIGNAL APPLICABLE TO THE HOME SIGNAL AT WHICH THEY WILL HAVE TO STOP IF IT IS AT DANGER.

The speed of the train must be reduced by the test and Drivers must enter such stations, or a dead end bay at any station, at a speed which will enable them to stop the train at the proper place.

Unless the vacuum brake is working properly when thus tested, the Driver must whistle for the Guard's hand brake, stop the train, and inform the Guard that the vacuum brake is out of order, and that the hand brakes must be relied upon for controlling the train. Special care must be taken to regulate the speed of the train.

7. Release of Brake by Hand.—To release the brake on any vehicle, the cord or wire (the position of which is indicated by star, arrow, or the letter "A" or "V" on the solebar of each vehicle) must be pulled and the release valve held open until the brake is sufficiently released. The cord or wire must never be fastened. On vehicles where two brake cylinders are provided the cords or wires of both must be pulled and the release valves held open.

WHERE OTHER COMPANIES' VEHICLES DETACHED FROM THE TRAIN ARE REQUIRED TO HAVE THE BRAKES FULLY RELEASED, IT IS IMPORTANT THAT THIS SHOULD BE DONE BEFORE THE HOSE PIPES ARE PUT ON THE STOP PLUGS AS, IF SUCH VEHICLES ARE RELEASED WITHOUT ONE END AT LEAST OF THE CONTINUOUS PIPE BEING OPEN TO THE ATMOSPHERE, THERE IS A RISK OF THE BRAKE SUBSEQUENTLY RE-APPLYING ITSELF.

Clause 8.

The third paragraph to be amended to read:

If the Driver, is from any cause, unable to work the vacuum brake, it must be cut off and released throughout the train. The train should then be worked by hand brakes only, the speed being so regulated as to enable the Driver to have full control of the train by the hand brakes; a Guard must travel in the rearmost brake van on the train, be on the alert, and assist in stopping the train with the hand brake.

The following to be added as the final paragraph:

Unless the train is being assisted in the rear, passengers must not be conveyed in vehicles on which the continuous brake is inoperative if such vehicles are marshalled behind the brake van in which a Guard rides. If the defect cannot be quickly remedied, any passengers in these vehicles must be detrained or transferred to the vehicles in front of the rear brake van, unless the vehicles on which the brake is inoperative can with safety and without causing serious delay be remarshalled inside the rear brake van or an additional brake van can be attached to the rear of the train. If it is not practicable to remarshal this passenger stock or to attach an additional brake van to the rear of the train, the vehicle or vehicles must be detached at the nearest convenient point. Should, however, the failure occur between stations, the train may be worked forward to the next station where the foregoing procedure must be observed.

MAY, 1945.

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#### VACUUM BRAKE REGULATIONS-Continued.

WHEN PORTERS AND OTHERS ARE DEALING WITH HORSE BOXES, CARRIAGE TRUCKS, FRUIT TRUCKS, ETC., DETACHED FROM PASSENGER TRAINS AT STATIONS SITUATED ON GRADIENTS THEY SHOULD BE VERY CAREFUL NOT TO RELEASE THE VACUUM BRAKE UNTIL THEY HAVE FIRST ASCERTAINED THAT THE VEHICLE IS PROVIDED WITH A HAND-BRAKE, AS SOME OF THESE VEHICLES, BELONGING TO OTHER COMPANIES, ARE NOT PROVIDED WITH HAND BRAKES, AND THERE IS CONSIDERABLE RISK OF SUCH VEHICLES RUNNING AWAY IF THE VACUUM BRAKE IS RELEASED BEFORE THEY ARE SAFELY SCOTCHED.

8. Failure of Brake. In case of failure of the brake, provided the defect is not in the train pipe, the defective cylinder or cylinders should, where practicable, be put out of action and the vehicle worked as one with through pipes only; the brake will then be effective on the remainder of the train.

If the defect is in the train pipe, or in the absence of facilities for putting the cylinder or cylinders out of action, the hose pipe at the leading end of the defective vehicle must be disconnected and the hose pipe at the rear end of the vehicle next in front placed on the stop plug. The brake on the front vehicles can then be worked as usual. The brake on the defective vehicle, and those in rear of it, must be released and the train worked forward under the control of the hand brakes with the assistance the Driver can give with the vacuum brake on the vehicles still connected to the engine, to the next station where the defective vehicle can be conveniently detached or the defect remedied.

If the Driver is, from any cause, unable to work the vacuum brake, it must be cut off and released throughout the train. The train should then be worked by hand brakes only, the speed being so regulated as to enable the Driver to have full control of the train by the hand brakes; Guards must be on the alert, and assist in stopping the train with their hand brakes.

 Detaching of Engines or Vehicles.—(a) When an engine is detached, it is the duty of the person detaching to place the hose pipe of the engine and that of the front vehicle on the stop plugs.

(b) When vehicles fitted with the vacuum brake are placed in sidings, the vacuum brake must be released, and when it is necessary that brakes should be applied for the purpose of keeping the vehicles from moving, the hand brakes, where provided, must be used, supplemented by scotches if necessary.

- 10. Frosty Weather. -In frosty weather special attention must be paid to the vacuum BRAKE. ANY EJECTORS WITH LEAKY STEAM VALVES MUST BE CHANGED OR REPAIRED, ALL DRIP TRAPS MUST BE EXAMINED DAILY AND ASCERTAINED TO BE FREE FROM WATER OR ICE, AND THE DRIP VALVES CLEAR AND WORKING PROPERLY, AND PIPES BETWEEN ENGINE AND TENDER (IF NOT FITTED WITH SCREWED CONNECTIONS) MUST BE PARTED AND ASCERTAINED TO BE FREE FROM WATER OR ICE. DRIVERS MUST ALSO USE SPECIAL CARE IN APPROACHING TERMINAL STATIONS AND OTHER PLACES SPECIFIED IN SECTION 6, PLACES WHERE A SPEED RESTRICTION IS IN FORCE, AND AFTER RECEIVING A "WARNING" SIGNAL.
- 11. Working of Brake. -As a result of experiments made to ascertain the cause of the ROUGH STOPS THAT ARE SOMETIMES MADE WITH PASSENGER TRAINS, IT HAS BEEN FOUND THAT THESE INVARIABLY OCCUR WHENEVER A SUDDEN AND HEAVY REDUCTON OF VACUUM IS MADE JUST BEFORE THE TRAIN COMES TO REST.

ATTENTION IS CALLED TO THE INSTRUCTIONS IN SECTION 5 (b) THAT "BEFORE FINALLY COMING TO A STAND THE VACUUM SHOULD BE PARTIALLY RECREATED SO AS TO PREVENT A REBOUND OF THE VEHICLES, OR UNDUE STRAIN ON THE COUPLINGS." WHEN IT IS NOT POSSIBLE TO DO THIS, THE YACUUM SHOULD BE STEADILY REDUCED AS MUCH AS POSSIBLE BEFORE THE FINAL APPLICATION IS MADE, AND THIS APPLICATION SHOULD NOT BE MADE BY SUDDENLY THROWING THE HANDLE RIGHT OVER.

A TRAIN SHOULD ALWAYS BE STOPPED WITHOUT CLOSING UP THE BUFFERS BETWEEN THE CARRIAGES, AS WHEN THIS TAKES PLACE, ALTHOUGH NOTHING MAY BE NOTICED ON THE ENGINE, A SEVERE JAR IS ALWAYS FELT IN THE TRAIN.

12. Improper Use of Vacuum Brake.—Guards and others must not make use of the BRAKE SETTERS IN THE GUARD'S VANS TO BRING TRAINS TO A STAND FOR SUCH PURPOSES AS THE MORE CONVENIENT LOADING OR UNLOADING OF LUGGAGE, OR BECAUSE SOME PERSONS MAY HAVE FAILED TO ALIGHT FROM OR ENTER THE TRAIN BEFORE IT BEGAN TO MOVE. SUCH APPLICATIONS OF THE VACUUM BRAKE ARE STRICTLY PROHIBITED, UNLESS THE BRAKE SETTERS ARE REQUIRED TO BE USED IN CASE OF EMERGENCY IN ORDER TO AVOID AN ACCIDENT.

It is to be understood that the cock should in ordinary circumstances be used by guards WHEN IT IS REQUIRED TO ATTRACT THE DRIVER'S ATTENTION FOR THE PURPOSE OF STOPPING THE TRAIN. IF NO COCK IS PROVIDED THE CHAIN COMMUNICATION SHOULD BE USED.

- 13. Regulations for Working Chain Communication between Passengers and Guard and Driver by means of the Vacuum Brake.

  (a) ALL G.W. CARRIAGES AND HORSE BOXES HAVE BEEN FITTED WITH THE CHAIN COMMUNICATION,
- WHICH IS CONNECTED WITH THE VACUUM BRAKE.
- (b) (i) A PASSENGER REQUIRING TO COMMUNICATE WITH THE GUARD AND DRIVER WILL PULL DOWN THE CHAIN PROVIDED FOR THE PURPOSE. THIS WILL CAUSE THE PARTIAL APPLICATION OF THE brake, which will be indicated by the gauges on the engine and in the Guard's van.
  - (ii) A small disc which is normally in a horizont I position on each side of that Carriage from which the communication has been made will be turned and exhibited in a vertical position.
- [111] THE GUARD CAN ASCERTAIN FROM WHICH COMPARTMENT OF THE CARRIAGE THE ALARM HAS BEEN GIVEN BY THE SLACKNESS OF THE COMMUNICATION CHAIN.

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#### VACUUM BRAKE REGULATIONS-Continued.

- (c) (i) When the Driver observes that the brake is being applied, he must infer that the communication has been used and must stop his train with as little delay as possible, having due regard to Rule 187 in the Rule Book. He must, however, exercise discretion in stopping, it being undesirable to bring the train to a stand on an overbridge, or a viaduct, on catch points, or in a tunnel, or other places of a similar character.
- (ii) When the Guard observes that the communication has been applied he must act as circumstances require.
- (d) When the train has been brought to a stand it must be protected as prescribed in Rule 181 (f).
- (e) THE GUARD MUST TAKE STEPS TO ASCERTAIN WHY, AND BY WHOM, THE COMMUNICATION HAS BEEN APPLIED, AND, SHOULD THE ALARM HAVE BEEN MISCHIEVOUSLY GIVEN, OR FOR AN INSUFFICIENT CAUSE, THE NAMES AND ADDRESSES OF ALL THE PASSENGERS IN THE COMPARTMENT MUST BE TAKEN IN ORDER THAT THE OFFENDING PASSENGER MAY BE PROPERLY DEALT WITH.
- (f) The Guard must, in addition to the notes in his journal, specially report any use that may have been made of the communication on the journey, or any failure in its action.
- (g) Before the train commences its journey, the Guard must satisfy himself that the discs are in their normal position.
- 14.—Engines fitted with Vacuum Reservoirs to Automatic Steam Brake Attachment.—Auto Engines and Engines having trip cocks or automatic Train Control Apparatus, when steam-braked, are fitted with vacuum reservoirs to the Automatic Steam Brake Attachment. The automatic action is thereby made considerably more sensitive than usual, and the steam brake will be applied by ordinary leakage of the vacuum in the train pipe.

WHEN RUNNING, THE EJECTOR MUST ALWAYS BE WORKED AS ON A VACUUM BRAKE ENGINE, i.e., THE BRAKE MUST BE BLOWN OFF UNTIL THE ENGINE IS MOVING FAST ENOUGH TO ENABLE THE PUMP TO MAINTAIN THE VACUUM.

SHOULD THE BRAKE FAIL TO COME OFF ON RECREATING THE VACUUM AFTER AN APPLICATION, A MOVEMENT IN RITHER DIRECTION OF THE HANDLE OF THE RELEASE VALVE ON TOP OF THE RESERVOIR WILL REMEDY THIS.

WHEN THERE IS A VACUUM IN THE RESERVOIR, AN APPLICATION OF THE BRAKES EITHER BY MEANS OF THE EJECTOR HANDLE OR OTHERWISE, DOES NOT FULLY APPLY THE STEAM BRAKE. TO APPLY THE STEAM BRAKE WITH FULL FORCE THE ENGINEMAN SHOULD DESTROY THE RESERVOIR VACUUM BY MEANS OF THE RELEASE VALVE IN ADDITION TO PUTTING THE EJECTOR HANDLE INTO THE FULL "BRAKES ON" POSITION

WHEN SHUNTING, OR WORKING NON-VACUUM FITTED FREIGHT TRAINS, IN ORDER TO AVOID THE SUDDEN APPLICATION OF THE STEAM BRAKE, IT HAS BEEN THE PRACTICE TO REMOVE THE FLEXIBLE VACUUM TRAIN PIPE FROM THE PLUG AND SO PREVENT THE APPLICATION OF THE STEAM BRAKE BY THE VACUUM ATTACHMENT, AND ALLOW OF IT BEING APPLIED EITHER FULLY OR GRADUALLY BY MOVEMENT OF THE EJECTOR HANDLE.

THIS PRACTICE OF REMOVING THE FLEXIBLE PIPE MUST NOT BE ADOPTED ON ENGINES FITTED WITH THE VACUUM RESERVOIRS REFERRED TO IN THE FIRST PARAGRAPH WHEN RUNNING ON MAIN LINES OR ON THE METROPOLITAN OR HAMMERSMITH AND CITY RAILWAYS, AS TO DO SO WOULD INTERFERE WITH THE OPERATION OF THE AUTOMATIC TRAIN CONTROL APPARATUS. A FULL OR GRADUATED APPLICATION OF THE STEAM BRAKE MUST BE MADE BY OPERATING THE RELEASE VALVE ON THE RESERVOIR AND BY MOVING THE EJECTOR HANDLE TO THE "BRAKES ON" POSITION.

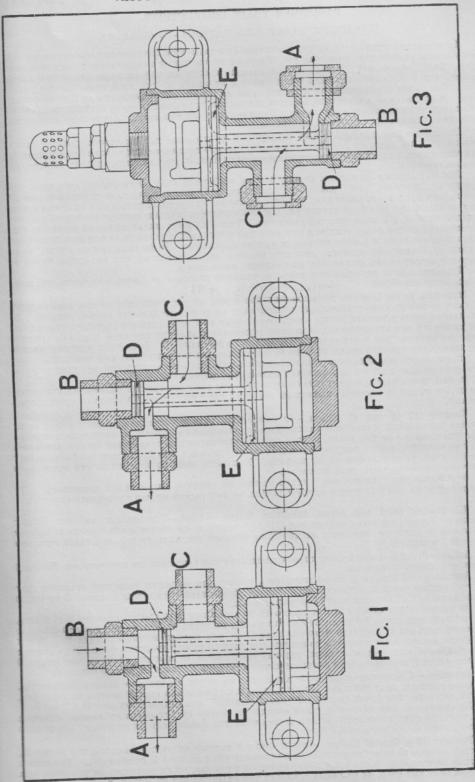
Engines fitted with Vacuum Reservoirs must on no account be moved on the Main Lines or the Metropolitan and Hammersmith and City Lines until a vacuum has first been created in both the reservoir and the train pipe.

IT IS IMPORTANT THAT A FULL VACUUM SHOULD BE MAINTAINED IN THE RESERVOIR, AND DRIVERS SHOULD IMMEDIATELY REPORT ANY LEARAGE.

15.—Engines fitted with Vacuum Retaining Valves.—Object.—The object of this valve is to secure a more efficient action of the engine brake, at the same time reducing the cost of maintenance.

Action.—Figure 1 shows the latest type of valve in running position. It is connected at A to the pump, at B to the train pipe and at C to the reservoirs. There is thus always train pipe vacuum in the spaces above the small piston D and below the large piston E (these two spaces being connected by means of the hole through the piston rod), and reservoir vacuum in the space between the pistons. When running, air is extracted from the train pipe by the pump in the usual way as shewn by the arrows.

When the brake is applied, air from the train pipe flows underneath the large piston E and raises the pistons to the position shewn in Figure 2. The piston D then seals the passage B and, being raised above the passage A, opens communication between the latter and the passage C, and air is then sucked by the pump from the reservoir, thus maintaining the vacuum therein. A "pepper-box" valve on the connecting pipe prevents the reservoir vacuum from rising to an excessive amount and so preventing the pistons returning to their normal position when the brake is blown off.



#### VACUUM BRAKE REGULATIONS-Continued.

In the older type of valve the reservoir and train pipe connections are reversed and the valve is placed the other way up. Figure 3 shows one of these valves in running position. The "pepper-box" valve, in this case, is placed on the chamber above the large piston, which communicates with the reservoir by means of the hole through the piston rod. The valve is connected at A to the pump, at B to the reservoir and at C to the train pipe. When the brake is applied, air from the train pipe enters the space between the pistons and raises them so that the small piston D comes above the passage A. Air is then extracted from the reservoir through the passage B.

Inspection when running.—When everything is in order the beservoir vacuum gauge needle will be seen, on applying the brake, first to fall slightly (owing to the motion of the piston in the brake cylinder compressing the residual air), and then remain stationary or rise slightly.

THE INITIAL DROP IN VACUUM SHOULD NOT BE MORE THAN ABOUT FIVE INCHES AND, WITH THE HANDLE IN "RUNNING" OR "BRAKES ON" POSITION, THE NEEDLE SHOULD, IF IT FALLS BELOW 22 INCHES, RISE AGAIN TO AT LEAST THAT FIGURE. AN EXCESSIVE INITIAL DROP IS A SIGN THAT THE BRAKE GEAR IS SLACK AND REQUIRES TAKING UP. IF THE RESERVOIR NEEDLE SHOULD CONTINUE FALLING THERE IS PROBABLY A LEAK INTO THE RESERVOIR. THIS WILL MOST LIKELY BE FOUND EITHER IN THE CONNECTIONS OF THE SMALL PLEXIBLE PIPE BETWEEN ENGINE AND TENDER OR IN THE PIPE TO THE RELEASE COCK OR VACUUM GAUGE, THOUGH IT MAY BE ELSEWHERE. IF NO LEAK CAN BE FOUND, OR IF A LEAK IS FOUND AND STOPPED, AND THE TROUBLE STILL CONTINUES, THE RETAINING VALVE IS PROBABLY OUT OF ORDER AND SHOULD BE EXAMINED OR CHANGED.

Enginemen must see that the brake is working properly as above described, and must

at once report any defect. Twelve Monthly. 4A13

Inspection in the Shed—A NINE MONTHLY INSPECTION OF THE CONDITION OF THE BRAKES MUST BE MADE IN THE SHED AS FOLLOWS: A VACUUM MUST BE BLOWN UP AND THE BRAKES APPLIED. IF THE NEEDLE OF THE VACUUM GAUGE SHOULD SHEW THAT AN UNDUE LEAKAGE IS TAKING PLACE INTO THE RESERVOIR THE RETAINING VALVE MUST BE TEMPORARILY BLANKED OFF AT A, B AND C. IF THIS STOPS THE LEAKAGE THE VALVE IS DEFECTIVE AND MUST BE CHANGED. IF IT DOES NOT THE DEFECT LIES ELSEWHERE AND THE BRAKE MUST BE EXAMINED AND THE DEFECT FOUND AND REMEDIED.

IN ALL CASES THE RETAINING VALVE SHOULD BE CLEANED AND OILED WHEN THE TEST IS MADE

(THIS SHOULD ALSO BE DONE OCCASIONALLY BETWEEN INSPECTIONS).

If one of the new pattern valves should fail and only old pattern valves are available to replace it, one of these can be used by removing the "pepper-box" valve and substituting a blank.

Blanking Off in case of Failure.—If a retaining valve should fail in such a way as to prevent the engineman from being able to release his brake properly, or otherwise interfere with the working of the train, on the first opportunity the connection to the reservoir (i.e. the connection C in the new type of valves, Figures 1 and 2, and the connection B in the old type, Figure 3) should be blanked. The cover must be unscrewed and the pistons removed, the cover being then replaced.

IF POSSIBLE THE "PEPPER-BOX" VALVE SHOULD BE REMOVED AND A BLANK SUBSTITUTED. IF THIS IS NOT DONE, ONLY 22 INCHES OF VACUUM WILL BE MAINTAINED BY THE PUMP WHILE RUNNING.

16.—Coaches fitted with Direct Admission Valves.—Object.—The object of the Direct Admission Valve (commonly called the "D.A." valve) is to obtain a more rapid action of the brake in cases of emergency by reducing the time required to fill the train PIPE and Cylinders with air.

Action.—Figures 1 and 2 shew diagrammatically the valve and its connections. Figure 3 shews the valve in section.

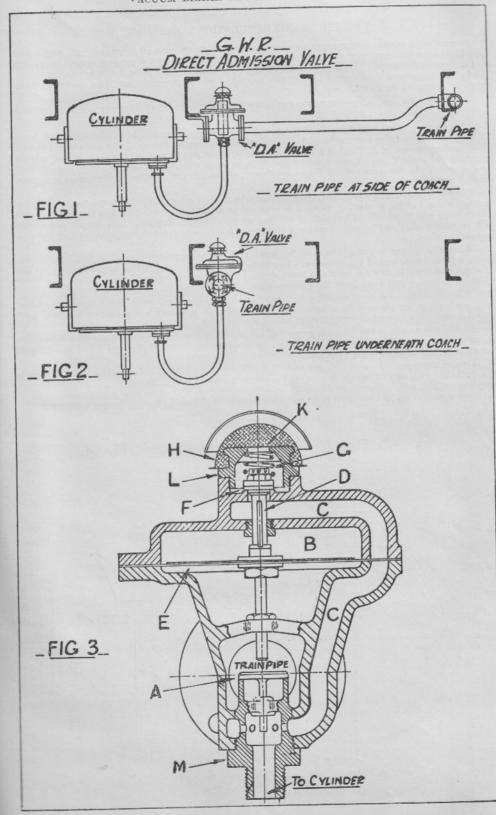
REFERRING TO FIGURE 3, ON BLOWING UP A VACUUM AIR IS DRAWN FROM THE CYLINDER PAST THE NON-RETURN VALVE A. A VACUUM BEING FORMED AT THE SAME TIME IN THE CHAMBER B BY MEANS OF THE PASSAGE C.C. AND THE FLAT D ON THE SPINDLE.

ON REDUCING THE VACUUM IN THE TRAIN PIPE TO APPLY THE BRAKE THE DIAPHRAGM E IS RAISED, THUS OPENING THE VALUE F AND ADMITTING AIR TO THE CYLINDER BY WAY OF THE PASSAGE C.C. AT THE SAME TIME AIR IS ADMITTED PAST THE FLAT D ON THE SPINDLE TO THE CHAMBER B. WHEN SUFFICIENT AIR HAS BEEN ADMITTED TO REDUCE THE VACUUM IN THE CYLINDER TO THE SAME AMOUNT AS IN THE TRAIN PIPE, THE DIAPHRAGM, BEING IN EQUILIBRIUM, IS BROUGHT DOWN BY THE SPRING GAND THE VALUE F IS CLOSED, THUS PREVENTING THE ADMISSION OF MORE AIR TO THE CYLINDER.

Inspection.—Examiners must give attention to these valves and satisfy themselves, as far as possible, that they are working properly. The brake blocks should be felt, and, if they are cold, the coach should be "green carded" for examination of "D.A." valve.

On arrival at the yard the valve should be tested by attaching an engine, creating a vacuum and then destroying it and seeing that the brake goes on properly. If it does not do so the valve is probably defective.

Blanking Off in Case of Failure.—If the valve F should be found leaking, the shield H, if fitted should be removed and a cork inserted in the  $\frac{3}{4}$  in. hole K in the cap L. The shield can then be replaced if the cork does not project too far. The insertion of the cork will stop the leak but will, at the same time, cut the cylinder out of action.



#### VACUUM BRAKE REGULATIONS-Continued.

IF TIME WILL ALLOW, THE FLEXIBLE PIPE TO THE CYLINDER SHOULD BE DISCONNECTED, THE VALVE CAGE M UNSCREWED AND THE VALVE A REMOVED. ON REPLACING THE CAGE AND COUPLING UP THE CYLINDER CONNECTION THE BRAKE WILL ACT AS ON A COACH NOT FITTED WITH A "D.A." VALVE.

IN ALL CASES THE COACH MUST BE "GREEN CARDED" TO DESTINATION, AND THE VALVE A, WHEN

REMOVED, SENT TO SWINDON.

DEFECTIVE VALVES.—DEFECTIVE VALVES MUST BE SENT TO SWINDON AND REPLACED BY OTHERS.

17. Engines fitted with Four Cone Ejectors.—Modern four cone ejectors, often referred to as "Three-one" ejectors, are arranged with a small extra steam valve on top of the main steam and air valve casting (shewn at A, Figures 1 and 2), which operates one cone only, the main steam valve B operating the other three. These ejectors can thus be used either as single cone, three cone, or four cone ejectors. In the older type of four cone ejector, steam valve B operates all four cones and steam valve A is not fitted.

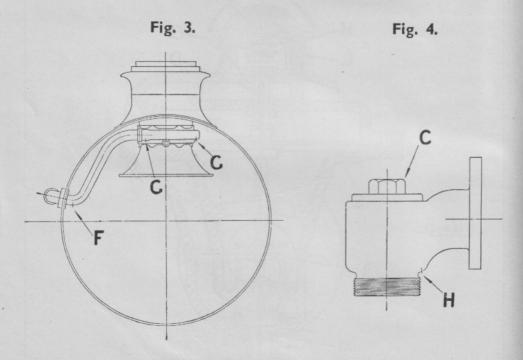
The small valve, when provided, should always be used in place of the large one to maintain the vacuum while waiting to start or when it is required to assist the pump. By this means less steam will be used and there will be less risk of blowing up an excessive vacuum. The large valve should only be used when it is required to create a vacuum quickly, or to clear the exhaust pipe of water as explained below.

When the ejector has not been used for some time, it may occasionally, when first blowing up, throw a little water out of the chimney. Enginemen should, therefore, when approaching a station at which the train is to stop, open the large steam valve for a few seconds, thus ejecting any water while the engine is still away from the platforms.

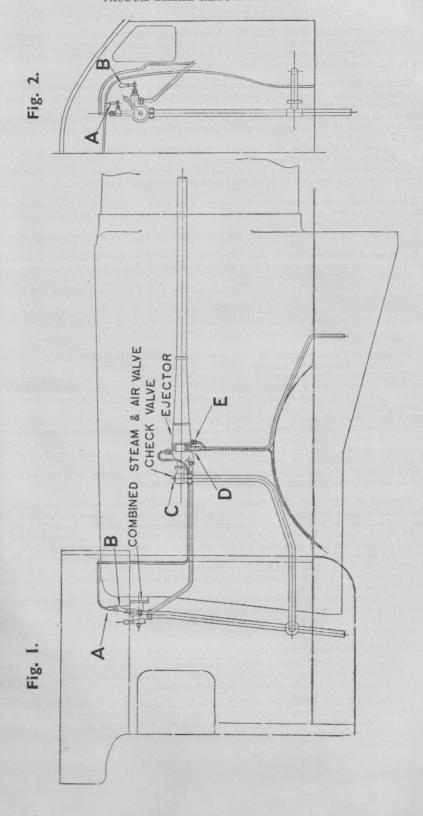
When an engine has been standing for some time in a station, the steam valve should be opened very gradually when blowing up a vacuum before moving, so that any water ejected may fall back into the smokebox instead of being thrown out of the chimney.

Should the needle of the vacuum gauge fall rapidly after the steam valves have been closed, the check valve should be examined. This is done by unscrewing the cap C (Figures 1 and 4) on top of the valve box, the valve being then accessible.

Drains are provided on the ejector D and E (Figure 1) and drain holes on the exhaust pipe in the smokebox at F and round the bottom of the chimney ring at G G (Figure 3). In the older form of check valve box, which slopes away from the ejector (see Figure 4), a drain hole is also provided at H. All these drains must be kept clear and the drip valves at D (Figure 1) kept in proper working order.



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PASSENGER TRAIN INSTRUCTIONS.
VACUUM BRAKE REGULATIONS—Continued.



#### VACUUM BRAKE REGULATIONS-Continued.

18. Securing of Vacuum Brake Hose Pipe Connections.—The vacuum brake hose-pipe connections are fitted with a chain and split pin. The pins must be inserted in the holes of the connections when the pipes of adjoining vehicles are connected, and when a pipe is placed on a stop plug, the pin must be used to secure the pipe into the plug.

19. General.—(a) Vacuum hose pipes when not in use must not be left loose, but must be properly secured on the stop plugs. When safety pins are provided together with holes in the stop plugs, the pins must be inserted in the holes.

(b) To couple the hose pipes, they must be taken one in each hand and lifted sufficiently high to hook the bottom horns of the couplings together first and then, when lowered, the top horns of the

couplings will fall in the slots.

(c) To uncouple the hose pipes, they must be lifted straight up, when the horns at the top will

come out of the slots, and the couplings will separate.

Safety pins, where provided, must be removed before the hose pipes are lifted. Hose pipes on goods wagons must be disconnected by hand and must not be uncoupled by lifting them with a shunting pole.

(d) When connecting or disconnecting hose pipes, care must be taken that the washers are not damaged, displaced or lost, and men must not have in their hands waste or any other material that might be drawn into the pipes by suction.

(e) Employees passing between vehicles must not step on, or interfere with, or damage, hose pipes

or couplings.

(f) Any snow or ice with which the hose pipe couplings or plugs may have become coated must be removed before attempting to connect the hose pipes or place a hose pipe on the stop plug, in order that the necessary connections may be air tight.

(g) Guards and others concerned must see that articles placed in brake vans cannot come into

contact with the handle of the Brake Setter.

(h) Station Masters, Inspectors, and others concerned, should satisfy themselves by personal observation, whenever possible, that the foregoing Regulations are being properly observed.

Note.—The term "Brake Van" includes any vehicle fitted with a brake compartment.

20. Operation of Vacuum Brake on Trains conveying other Companies' Stock.—The brake cylinders on other Companies' vehicles are more sensitive than those on G.W. vehicles, and, in consequence, a slight variation in vacuum is liable to cause the brakes to drag.

CARE MUST BE TAKEN WHEN WORKING TRAINS CONVEYING THESE VEHICLES THAT NOT MORE THAN 25 INCHES OF VACUUM IS CREATED BY THE EJECTOR, AND THAT THE SAME AMOUNT OF VACUUM IS MAIN-

TAINED WHEN RUNNING.

SHOULD MORE THAN 25 INCHES OF VACUUM BE CREATED WHEN WORKING OTHER COMPANIES' STOCK, THE DRIVER MUST INFORM THE EXAMINER AT THE STATION WHERE THE ENGINE IS DETACHED, AND THE BRAKES MUST BE PARTIALLY RELEASED ON THE FOREIGN COACHES BEFORE THEY ARE TAKEN ON BY ANOTHER ENGINE. THE MATTER MUST ALSO BE REPORTED AT THE LOCOMOTIVE SHED, WHEN THE PEPPER-BOX VALVE SHOULD BE CHANGED.

To avoid risk of the vehicles standing without any brake power, the brakes must not

BE RELEASED UNTIL AFTER THE SECOND ENGINE HAS BEEN ATTACHED.

21. Coupling of G.W. and other Companies' Engines.—G.W. engines must not be coupled to other Companies' engines for long distances owing to the difference in the vacuum created by the respective engines. When, however, it is necessary in consequence of a failure for a G.W. engine to be attached to the engine of another Company, care must be taken by the trainmen to destroy the vacuum in the engine and train reservoirs. If the train is to be worked forward by the G.W. engine, the driver must be careful not to create more than 20 inches of vacuum, and, in any case, when the brake is applied, the G.W. driver must watch his vacuum gauge and use the release cocks if necessary to prevent the brake dragging and damaging the tyres of his engine. The disabled engine must be detached at the first opportunity.

G.W. light engines may be coupled to other Companies' light engines for short distances, such as between Shed, Station, etc., and in such cases the vacuum pipes must be connected in accordance with Rule 133, Clause (e). The G.W. driver must carefully watch his vacuum gauge and use the release

cocks as necessary to prevent the brake dragging.

EXCEPTION.—Southern Railway engines which are not fitted with vacuum pumps may be coupled to G.W. engines without restriction, but, when the Southern Railway engine is leading, the G.W. Driver must be prepared to assist with his ejector, when required, to obtain 25 inches of vacuum.

22. Vehicles fitted with Westinghouse Automatic Brake attached to Trains fitted with Vacuum Automatic Brake.—The handle of the cock at the end of each vehicle fitted with the Westinghouse brake must be turned across or away from the pipe which is painted black; the release cord or wire under the vehicle must then be pulled to allow all the air to escape from the cylinder.

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Vacuum E WEST-CK; THE ESCAPE (h) (i) If a vehicle has to be detached and the vacuum pipes cannot be uncoupled, the Guard or other competent person must verbally instruct the Driver to destroy the vacuum or must open the air valve in the nearest brake van.

(ii) When it is necessary to intimate to the Driver that the vacuum requires to be created so that the Guard may comply with the provisions of Regulation 3 (b), the Guard, Shunter, or other person in charge, must either so inform the Driver verbally or exhibit a hand signal to him as follows:—

By day .. Arm moved vertically up and down above shoulder level.

By night .. Red light moved vertically up and down above shoulder level

Divisional Superintendent's Office, Bristol. G. W. 7.

Ref:- Al/-

2nd June, 1942.

Dear Sir,

FORMATION OF PASSE GER TRAINS. Guards riding in rear Brake Vehicle.

Referring to the instruction on Page 109 of the General Appendix, Clause 4 regarding The Formation of Passenger Trains including provision of Guards and handbrakes", there appears to be some misunderstanding in regard to the interpretation of the instruction.

This was framed to cover the attachment of additional vehicles at intermediate points en route, and was so worded to cover cases in which such vehicles did not include a brake.

It is desirable that the Head Guard shall ride in the

rear Brake Vehicle on all possible occasions.

Will you please note and instruct your staff accordingly.

Yours truly

19

#### FORMATION OF PASSENGER TRAINS, INCLUDING PROVISION OF GUARDS AND HAND BRAKES.

1. The brakes referred to below are :-

(a) Continuous Automatic brake, viz., either Vacuum or Westinghouse. (See pages 98 and 108).

(b) Guard's hand brake, viz., the screw brake in the Guard's compartment.

(c) Outside hand brake on certain vehicles and applied from the ground,

2. The "piped" vehicles referred to are vehicles without continuous automatic brakes, with or without outside hand brakes, but with pipes through which the automatic brake on vehicles each side of them can be operated.

3. The Continuous Automatic Brake-pipe connections between vehicles, including the engine and tender, must all be coupled up and the Continuous Automatic must be Brake must be capable of being applied by the Driver and Guard to every vehicle in applicable. the train, except as follows :-

Vehicles to

(1) A proportion of "piped" vehicles may be formed in the train as shewn

in Clause 3 (e) and (f) of Vacuum Brake Instructions.

(2) THE LAST VEHICLE OF EVERY PASSENGER TRAIN MUST BE FITTED WITH THE CONTINUOUS BRAKE COMPLETE, except that, where necessary to avoid delay, one vehicle, not being a passenger-carrying vehicle, may be attached in the rear of A BRANCH OR LOCAL PASSENGER STOPPING TRAIN without being fitted with the continuous brake or pipe, PROVIDED SUCH VEHICLE IS ATTACHED BY MEANS OF THE SCREW COUPLING OF THE LAST PASSENGER TRAIN VEHICLE, BUT IN THIS RESPECT AN EMERGENCY SCREW COUPLING SHOULD BE USED WHEREVER AVAIL-ABLE. GROOMS OR ATTENDANTS TRAVELLING IN HORSE BOXES OR OTHER SUCH VEHICLES NOT ORDINARILY USED FOR THE CONVEYANCE OF PASSENGERS ARE NOT, FOR THE PURPOSE OF THIS INSTRUCTION, REGARDED AS PASSENGERS.

4. Unless special instructions are given to the contrary by the Superintendent of the Line, the maximum number of wheels allowed behind the brake in which the rear Guard rides to be as follows :-

Ruling Gradient.	Number of wheels of vehicles of all descriptions.	Number of wheels of vehicles containing passengers.*
Rising not steeper than 1 in 100 or falling or level	40	40
Rising steeper than 1 in 100, but not steeper than 1 in 40	40 16	24 1 vehicle.

\* Provided the vehicles containing passengers are in all cases fitted with Continuous Brake complete.

WHERE PRACTICABLE, THE VEHICLES CONTAINING PASSENGERS SHOULD BE FORMED IMMEDIATELY BEHIND THE VAN.

Note.—When the train is travelling up gradients steeper than 1 in 200 the Guard or Guards must be prepared to apply instantly the hand brake.

(a) Passenger and Empty Coaching Stock Trains should be worked by one Guard only, except where the work on the train or at stations, or other exceptional circumstances, necessitates the employment of an additional man or men. two Guards are employed the assisting Guard must ride in the front of the train, and the Guard in charge of the train at the rear.

Number of trains.

(b) Loaded passenger trains must not exceed 136 wheels.

The load of empty passenger stock trains must not exceed the equivalent of 20

eight-wheel coaches Parcels, Perishable, Fish and Milk trains must not exceed a gross weight of 550 tons (excluding engine or engines but inclusive of the brake van), whether assisted or

[a] Loaded passenger trains having more than of wheels (G.A. 8.-5/41. T.10.M.)

two vans or brake compartment vehicles.

Note.—Either one or two horse boxes, carriage trucks, or such like vehicles with two vans or brake compartment vehicles. which do not carry the public, may be attached to a train of 96 wheels without a second brake being attached.

page 109.

Delete paragraph (e) of clause 5 and the additional sentence shewn in Circular G.A.6 and substitute the following :-

(e) A brake van or vehicle with brake compartment leading should, as far as practicable, be marshalled next the engine of all passenger trains, except where the formation is otherwise specified in the Carriage Working Instructions or delay will be caused at starting points; similarly a brake van or vehicle with brake compartment trailing should, as far as practicable, be marshalled at the rear of passenger trains.

When passenger-carrying vehicles are attached en route to either the front or rear of the train, they should be marshalled within the brake van where this can be done (G.A.27.Op.-1/51. L.K.1/9823/Gen.E.) without causing delay to the working.

### PROVISION OF GUARDS AND HAND BRAKES-Continued.

Persons in charge of stations to see that trains have sufficient brake power. 6. Persons responsible for starting trains from stations where the journey is commenced, and the Guards, must see that brake power is provided in accordance with the instructions, and such persons are similarly responsible at stations where vehicles are added.

Guards to see that hand-brake is in good order. 7. Guards must see that hand brakes are in working order and secured in the "off" position before starting.

Certain vehicles not to be formed in front of mail coaches. 8. Vehicles containing passengers, or stock 9'-3" to 9'-7" wide over body and brake vans with guards' projections, must not be formed in front of Mail vehicles which have to use the picking up and setting down Mail apparatus on the journey.

Coupling of Passenger vehicles. **9.** (a) To obtain good and steady running, it is most important that a train should move as one unit, and there should be no slackness between any of the coaches.

(b) In forming trains it is necessary for the vehicles to be brought together until the buffers just touch, two more turns being then given to the coupling, which results in the springs being compressed about half an inch. If more than two turns are given, the friction between the buffers is too great and prevents the comfortable sliding motion which steadies the vehicles without jar. When coupling on a curve the screw coupling should be so adjusted as to bring about the above-mentioned conditions as nearly as possible.

(c) Guards, Train Ticket Collectors, Restaurant Car Conductors and Sleeping Car Attendants must promptly report all cases of rough riding and excessive oscillation and give particulars of the vehicle and the locality. Any slackness between coaches must also be reported, and steps taken at the first opportunity to have the couplings tightened and attended to. The supervisory staff concerned must also give particular attention to this matter, and take steps to have the instructions rigidly carried out.

Duties of Train Ticket Collectors or men assisting on trains. 10. Train Ticket Collectors or other men assisting on Passenger trains worked by one Guard, must, in addition to their special duties, assist the Guard of the train as far as possible, both on the journey and while at stations. Men so working, with the exception of Travelling Parcel Porters, must carry with them a set of flags, not less than 12 detonators, and a trimmed hand lamp.

(d) When coupling an engine to a passenger train the engine coupling should be used unless a slip coach is formed as the leading vehicle in the train.

In the event of failure of the engine coupling, the coupling of the leading vehicle must be used.

1. Four-wheeled Non-passenger carrying Coursely Sund (G.A. 5.—2/39. T.28419.G./2.)
Passenger Trains.

Four-wheeled vehicles, either coaching, or braked freight stock, requiring to be run in Passenger trains, must comply with the following requirements:—

(f) Vehicles to be fitted with:
Oil axleboxes, automatic brakes or through pipes, serew couplings and long buffers.

(ii) Minimum tare to be 6 tons.

(iii) Vehicles with a wheel base of less than 9 ft. to be restricted from running in express.

(iv) All four-wheeled vehicles to be marshalled at the back of the train in rear of any bogie vehicles carrying passengers, but where this is impracticable in traffic working they may be placed next to the engine, or mixed with six-wheeled Non-Passenger carrying vehicles provided all such vehicles are attached in front of bogie vehicles, but vehicles not fitted with through steam pipes, having to be conveyed on trains which are steam-heated, must be formed at rear of steam-heated vehicles. Four-wheeled vehicles must not be conveyed between bogie stock except in the following circumstances, when they may, if absolutely necessary, be marshalled in the positions indicated—

(a) Between bogic fish or other non-passenger carrying vehicles when such are marshalled behind the last vehicle conveying passengers on the train

(b) Between empty bogie passenger vehicles and the real brake.

(a) Vahicles containing Theatrical, Naval, Military or Air Force Traffic may be marshalled according to destination when FORMED IN TRAINS COMPOSED ENTIRELY OF SUCH

Sub-Section (iv) does not apply to four-wheeled vehicles having a wheelbase of 15 th or over, the position of which on Passenger trains need not be restricted if the vehicles comply with Sub-Sections (i) and (ii).

For the use of the Co

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NOTE.—All under 15 feet wh "XP" on the righ shewn underneath

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October 1946

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Station and Depeter, under their supe

## GREAT WESTERN RAILWAY.

## Alterations and Additions to the General Appendix to the Rule Book

To come into operation forthwith.

## REGULATIONS GOVERNING THE RUNNING OF FOUR-WHEELED VEHICLES IN PASSENGER TRAINS—Page 110.

The NOTE at the end of Clause 1 (v) of these Instructions to be amended to read:

NOTE.—All four-wheeled non-passenger carrying coaching stock and braked freight stock of 10 feet and under 15 feet wheelbase suitable for running in passenger trains carrying "A" headlamps will be marked "XP" on the right hand corner of each vehicle or the right hand corner of the solebar. The wheelbase will be shewn underneath the letters "XP".

Any passenger train and any other train composed of coaching stock (i.e., empty stock train, parcels train, newspaper train, milk train, horsebox train or pigeon train, or any fish, meat, fruit, or perishable train if not signalled as a fitted freight train) conveying one or more four-wheeled vehicles of less than 15 feet wheelbase must not exceed 60 m.p.h. at any point.

In every case when four-wheeled vehicles of less than 15 feet wheelbase are marshalled on the train the Guard must advise the Driver before starting, so that the speed of 60 m.p.h. may not be exceeded at any point whilst such vehicle is, or vehicles are attached to the train.

The load of any four-wheeled vehicle marked "XP" and with less than 15 feet wheelbase, must be restricted to 6 tons if it is to be conveyed on any passenger train and any other train composed of coaching stock (i.e., empty stock train, parcels train, newspaper train, milk train, horsebox train or pigeon train, or any fish, meat, fruit or perishable train if not signalled as a fitted freight train). This loading restriction will apply to such vehicles loaded on this Company's system only.

(G.A.17. 10/46.LK1/7217/10.)

October 1946 5442C-45,000

JAMES MILNE, General Manager.

Each member of the Staff receiving a copy of this Circular is required to read carefully and note the contents, and, if supplied with a copy of the General Appendix, to alter, or cancel in ink the present instructions on the subject appearing therein, afterwards pasting the amendments in their proper places in the General Appendix.

Station and Depot Masters are responsible for seeing that copies of the General Appendix supplied to Signal Boxes, etc., under their supervision, are corrected in accordance with this Circular.

to the Formation of Passenger Trains,

## PRIVATE AND NOT BRITISH RAILWAYS NOTICE NO. 928 (WESTERN OPERATING AREA)

District Operating Superintendent's Office, BRISTOL (IM) W. R. 7 27th May 1953

## FRIDAY, 29th MAY

5-55pm PADDINGTON TO CARMARTHEN PAGE 43 PRINTED NOTICE NO. 950. The FIRST PART will now run to Swansea.

## SATURDAY, 30th MAY

12-20am (SLEEPING CARS) PADDINGTON TO PENZANCE RELIEF Will run at the following times :-

arr. dep. arr. dep. a.m. a.m. 2/21 a.m. Clink Road Jen. Bedwyn Patney & C. Heywood Rd. Jcn. Fairwood Jcn. 2/24 Blatchbridge Jcn. Castle Cary \_\_\_

7-50am TAUNTON TO PADDINGTON

PAGE 46. PRINTED NOTICE NO. 950 Now to be strengthened with three Thirds at Bristol (IM) Load 13 from Swindon.

8-15am BRISTOL (TM) TO PONTYPOOL ROAD. PAGE 47 PRINTED NOTICE NO. 950. Will run to Manchester (Mayf) eld). To be labelled "Manchester".

10-30am PENZANCE TO WOLVERHAME TON PAGE 48 PRINTED NOTICE NO. 950. A Restaurant Car will be conveyed on the first and second parts.

12-35pm CARDIFF TO PENZANCE PAGE 49 PRINTED NOTICE NO. 950. The FIRST PART will also wall at Teignmouth and Kingskerswell. Will carry TRAIN NO. 761/ The SECOND PART will carry RRAIN NO. 760.

## SATURDAY, 30th MAY (contd)

3-45pm (PBLE.) PENZANCE TO PADDINGTON
Will run and be signalled as a passenger train ('B' Headcode)
from Taunton to Reading.

4-25pm (PanCELS) PLYMOUTH TO PadDINGTON
Will convey Medical Examination Goach No. 1159 from Exeter to
Old Cak Common.

### PASSENGER TRAIN ADVISING -RUNNING OF 4-WHEEL VEHICLES ON EXPRESS PASSENGER TRAINS

A case arose recently where two 4-wheeled vans with a wheel base of 10ft. or over, but less than 15ft. were attached next to the engine but en route it was also necessary to attach a bogie van on the front. It was not until the train arrived that the staff became aware of the wheel base of the four-wheel vehicles with the result that the bogie vehicle had then to be formed inside the four-wheel vans.

In order to avoid a similar case arisin in the future, the train advices must include the wheel base of all four-wheel vehicles when attached to the front of passenger trains.

RECEIPT OF THIS NOTICE TO BE ACKNOWLEDGED BY WIRE IMMEDIATELY TO HISTROP P. BELSTOL 'ARNO 928'.

ARLANGS AND ADVISE ALL CONCERNED

THIS NOTICE TO BE RETAINED FOR REFERENCE PURPOSES.

FOR L. ELWARDS



PRIVATE AND NOT

# BRITISH RAILWAYS (WESTERN REGION)

CIRCULAR NO. 0

District Operating Superintendent's Office, BRISTOL (TEMPLE MEADS)

29th January 1951.

GX. 3718.

## MISSING COACHES. S.R. STOVE VANS SERIES NOS. 370 TO 419.

Any S.R. Stove Vans with painted numbers in the series 370 to 419 must be withdrawn from working and sent home via the nearest junction. Control R.S. Bristol must be advised the details of despatch in all cases and also the painted numbers of vehicles forwarded home.

GX, 4440.

# COACHING STOCK

With reference to my Circular letter of July 21st 1950, the full information concerning the user of unrostered coaching stock is still not being received and immediate action must be taken to ensure that the instructions are carried out.

GX. 4437.

# NEW W.R. SIX WHEEL INSULATED FISH VANS

With reference to my circular letter of August 29th 1950, when numbers 3505 and 5537 are released from Swinden Shops, all Western Region six wheel Insulated Fish vans, which are numbered in the series 3501 to 5550 will be suitable to run in express passenger trains which exceed 60 m.p.h. in running.

OP2/47299 .

# TRAFFIO DEADT WITH AT STATIONS -- FORM 481.

I have not yet received all the completed 481 forms from stations. Will all Station Masters concerned please make the necessary arrangements.

A1/72051.

# BRISTOL (T.M.) TELEPHONE EXCHANGE

The following additional line has been connected to the Bristol Telephone Exchange.

Extension 555 - District Refreshment Room Manager's Office.

84/71014

# NATIONAL SERVICE ACT 1948 STAFF CALLED UP FOR MILITARY SERVICE.

Men born between January 1st 1933 and March 31st 1933 are required to register for National Service on Saturday, February 3rd 1951.

A circular letter has already been despatched to stations and depots where men in this category are employed and any Station or Yard Master at whose station or depot staff are required to register in accordance with the foregoing, who has not received this letter, should communicate with this Office immediately.

RESIDENTIAL FREE PASS NO. 28359. AVAILABLE BETWEEN PARSON STRUET AND FILTON JUNCTION. ISSUED IN FAVOUR OF MR. O.J.TREMLETT.

# S. 60500 .V. MOON-WEBS SCHOLARSHIP, LIVERIOOL UNIVERSITY

Details of the goove scholarenip aregiven below: -

The Moon-Webb Scholarship shall be of the minimum titular value of £150 per annum, tenable for three years and awarded triennially.

- The Scholarship shall be open to persons employed by the British Railways, who are not less than 18 and not more than 23 years of age on the 1st October in the year in which the examination is held, and who have been so employed for not less than twelve months.
- 5. The Scholarship shall be awarded upon the results of a special entrance scholarship examination to be held by the University. To making the award consideration shall be paid by the examiners to the ushwol records of the candidates.

# Subjects of Examination

Pure Mathematics.

(ii) Heat Engines.
(iii) Engineering Trawing.
(iv) Theoretical and Appl

Theoretical and Applied Mechanics,

- (v) Either Physics (Heat, Magnetism and Electricity) or Chomistry,
- 4. The Scholar shall be required to read for the degree of Bachelor of Engineering or for the Certificate in Engineering,
- Competitors must send in their applications to the Registrar of the University on or before 31st March through the Regional Staff Officer of the Region in which they are employed.

Station Masters and others concerned to please bring these details to the notice of all eligible staff and send me the names of any applicants as quickly as possible when the necessary application forms will be supplied.

s2/60601/2

PERIODICAL PASS NO. 1672 ISSUED IN FAVOUR OF "ONE SIGNAL LAMPMAN" AVAILABLE BETWEEN EASYON - WEYMOUTH - MAIDEN NEWTON - BRIDPORT.

The above periodical free ticket has been lost and I shall bo glad if all concerned will please take the necessary steps to prevent misuse. .

np2/48803.

QUARTERLY STORES REQUISITION BOOKS

All Station Masters on the "G" Group Stores Distribution list are reminded that the requisition books are due in this office on Thursday next, February 1st 1957.

Please arrange accordingly.

ATRANGE AND ADVISE ALL CONCERNED.

FOR L. EDWARDS

REGULATIONS GOVERNING THE RUNNING OF FOUR-WHEELED VEHICLES IN PASSENGER TRAINS-page 110.

Delete the whole of the instructions under this heading and substitute the following :-

#### CONVEYANCE OF FOUR-WHEELED NON-PASSENGER-CARRYING COACHING STOCK AND BRAKED FREIGHT STOCK IN PASSENGER TRAINS.

Four-wheeled vehicles, either non-passenger-carrying Coaching Stock or Braked Freight Stock, requiring to run in passenger trains must comply with the following requirements :-

(a) Oil axle boxes.

\*(b) Automatic brake or through pipes. (c) Screw couplings and long buffers.

(d) A minimum tare weight of 6 tons. (The minimum tare weight of 6 tons does not apply to Container wagons when such wagons are carrying containers either loaded or empty and the total load, i.e. tare weight of wagon plus weight of container, is 6 tons or over.

Four-wheeled vehicles conforming to the above requirements and having a wheelbase of 10 feet or over are marked "X.P." together with the wheelbase.

The term "non-passenger-carrying Coaching Stock" refers to stock not constructed for con-

veying passengers but includes Horse Boxes and Cattle Boxes.

\*NOTE.—This requirement does not modify the authority (where given) to attach at the extreme rear one vehicle not conveying passengers and not fitted with the continuous brake or through pipe. (See instructions headed "Formation of Passenger Trains, Clause 3, Sub-section (2)".)

When a four-wheeled vehicle is attached to a passenger train next to a bogie vehicle the screw coupling of the four-wheeled vehicle must be used and must be screwed up tightly. Instanter couplings must not be used for attaching vehicles to a passenger train and the emergency screw toupling or screw coupling of the adjacent vehicle must be used.

Four-wheeled vehicles marked "X.P." and having a wheelbase of 15 feet or over may be attached to passenger trains without restriction, unless otherwise specially prohibited.

2. (a) Passenger trains conveying vehicles with a wheelbase of less than 15 feet must not exceed a speed of 60 m.p.h. at any point. In every case where four-wheeled vehicles of less than 15 feet wheelbase are marshalled in the train the Guard must advise the Driver before starting so that the speed of 60 m.p.h. may not be exceeded at any point when such vehicle is, or vehicles are, attached to the trains.

(b) Four-wheeled vehicles with a wheelbase of less than 10 feet must not be conveyed on Express

Passenger Trains

3. Four-wheeled vehicles with a wheelbase of under 15 feet should as a general rule be marshalled at the back of passenger trains and at the rear of all bogie vehicles. Where this is impracticable in the interests of traffic working, they may be marshalled as shewn below :-

(ii) Between bogie non-passenger-carrying vehicles and/or bogie passenger coaches not convey-

ing passengers when these are placed behind the last vehicle conveying passengers.

(iii) According to destination when conveying Theatrical, Naval, Military or Air Force traffic when formed in trains composed entirely of such traffic.

4. Four-wheeled vehicles should not be placed next to the engine during the period when steam heating is in operation, unless the vehicle or vehicles are fitted with steam pipes.

5. Four- and six-wheeled vehicles may be intermixed provided they are all marshalled at the front of all bogie vehicles or behind all bogie vehicles. (G.A.26 Op.—5/50. R.E. Op. Com. Min. 228.) underneath the letters "XP.

(vi) Except in the special circumstances enumerated in sub-section (vii) below all four-wheeled vehicles specified above (including also those of 9 ft. and under 10 ft. wheelbase), conveyed by passanger trains other than those carrying "A" headlamps must be marshalled in the rearrof any bogie vehicles, but where this is imprecticable in traffic working they must be placed next to the engine, or mixed with six-wheeled non-passenge carrying vehicles provided all such vehicles are attached in front of boare vehicles, but vehicles are fitted with through steam pages, having to be conveyed on trains which are steam heated, must be formed at the rear of steam heated vehicles.

(vii) Four-wheeled vehicles with a wheelbase of less than 15 ft. must not be conveyed between bogie stock except in the following cheumstances, when they may if absolutely necessary, be may shalled in the positions indicated, but in all cases when such vehicles are marshalled between bogic stock the Driver of the train must be informed by the Guard and the speed of the train must not exceed 60 miles per hour at any point:—

(a) between bogie fish or other non-passenger carrying vehicles when such are marshalled behind the last vehicle conveying passengers on the train.

(b) between empty bogie passenger vehicles and the rear brake.

(c) Vehicles conveying Theatrical, Naval, Military, Air Force or Race Horse Traffic may be marshalled according to destination WHEN FORMED IN THAINS COMPOSED ENTIRELY OF SUCH TRAFFIC. Four-wheeled vehicles having a wheelbase of 15 ft. of over, may be marshalled without restriction provided the vehicles comply with sub-sections (i) and (ii).

Four-wheeled non-passenger carrying Coaching Stock (other than Rail Milk Tanks and Jour-wheeled under carriages conveying Mobile Milk Tanks) and Braked Freight Stock, conveyed at the rear of Rassenger trains, may be long or short buffered, provided the vehicles otherwise conform with Clause 1, sections (i) (ii) and (iii)

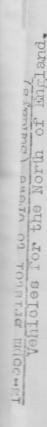
(a) the first vehicle (if not fitted with screw couplings) is attached to the train by means of the screw coupling of the last Passenger train vehicle, or by an emergency screw coupling. The latter should be used whenever

(b) in instances where more than one four-wheeled, chicle is attached at the rear of a Passenger train, the second vehicle and subsequent vehicles, if not fitted with suitable serew couplings, must be attached by means of emergency screw couplings.

The provision of Clause 3, sub-section (2) of the instructions in regard to the Formation of Passenger Trains, must be observed.

Except where instructions exist to the contrary, instanter couplings must not be used for attaching vehicles the rear of a Passenger train, but emergency screw couplings must be used. (G.A.4-9/38, G.M.Min. 3001)

CONTINUED ON PAGE III



# A.F.V. TRAINS-PROTECTIVE ARRANGEMENTS IN THE EVENT OF A BREAKAWAY.

Spackann

Whenever a loaded A.F.V. train is brought to a stand where the line is not level or where it is important the train should not move back and it is necessary for the guard to leave his van, he must, before doing so, apply his hand brake.

If, for any reason, the engine is to be detached from the train, or the train is divided, either by intention or accidentally, on gradients of 1 in 125 or steeper, the guard must take immediate steps to apply the hand brakes on ramp wagons, warflats or other brake vehicles, working forward from the brake vehicle in which he was riding.

(G.A.15—12/44, L.K,1/7429/6A)

Divisional Superintendent's Office,

Temple Meads Station,

BRISTOL

P2/

4th November, 1938

Dear Sir,

Regulations Governing the running of Four-Wheeled vehicles in Passenger Trains.

With reference to the instructions contained in Clause (iv) of Circular G.A.4 dated September 1938, vehicles with a wheel base of 10 feet and under 12 feet may be conveyed next engine of the following trains between Bristol Temple Meads and Newport as under:-

5- Opm Penzance to Crewe Vehicles for the North of England.

12-35pm Bristol to Crewe (Sundays)
Vehicles for the North of England.

5-50pm Bristol to Grewe (Sundays) Vehicles for South Wales.

11-50pm Bristol to Crewe (Sundays)
Vehicles for the North of England.

Authority has also been issued by the Divisional Superintendent at Newport for such vehicles to be formed next engine between Newport and Bristol (Temple Meads) on the under-mentioned trains:-

8-17pm Crewe to Bristol (TM) 1-25am Crewe to Bristol (TM) (M.O.) 11-55am Crewe to Bristol (TM) (Sundays) 7- 2pm Hereford to Bristol (TM) (Sundays).

These instructions will not apply to vehicles which are not fitted with through steam pipes. These vehicles must be formed at the rear of steam-heated vehicles as shewn in the printed Circular.

Yours truly,

RUNNING OF FOUR-WHEELED VEHICLES IN PASSENGER TRAINS-Continued.

# CONVEYANCE OF FOUR-WHEELED NON-PASSENGER-CARRYING COACHING STOCK AND BRAKED FREIGHT STOCK IN EMPTY COACHING STOCK AND PARCELS, ETC., TRAINS.

Any train (other than Passenger) composed of coaching stock, i.e. empty coaching stock train, parcels train, newspaper train, milk train, horse-box train, pigeon train, fish, meat, fruit, or perishable train conveying one or more four-wheeled vehicles of less than 15 feet wheelbase must not exceed 60 m.p.h. at any point. In every case where four-wheeled vehicles of less than 15 feet wheelbase are marshalled on the train, the guard must advise the driver before starting, so that the speed of 60 m.p.h. may not be exceeded at any point while such vehicle is, or vehicles are, attached to the train. (G.A.26 Op.—5/50. R.E. Op. Com. Min. 228.)

The provision of Clause 3, sub-section (2) of the instructions in regard to the Formation of Passenger Trains, etc., must be observed.

Except where instructions exist to the contrary, instanter couplings must not be used for attaching vehicles to the rear of a Passenger train, but emergency screw couplings must be used.

Reference to the following to be made on page III :-

CONVEYANCE OF RAIL MILK TANKS.

"Six-wheeled milk tanks, loaded or empty, may be formed in any position in express passenger trains, except between coaches conveying passengers. (G.A.24.-11/49. L.K.1/8533/Gen. 4.)

The couplings to be screwed up tightly in accordance with the instruction in Clause 9 (b),

Milk tanks must not be formed in Freight trains except by the special authority of the Operating Superintendent. (G.A.23-7/49. L.K.1/8533/Gen.4.)

Flat Container Wagons.

The Regulations governing the running of four-wheeled vehicles in Passenger trains will not apply to Flat Container Wagons numbered 39000, etc., so far as the tare of 6 tons is concerned. These wagons, although alone less than 6 tons tare, considerably exceed that weight with the container and may be conveyed by Passenger trains when loaded with a container which is either empty or loaded,

Vacuum Fitted Goods stock on Passenger Trains.—Cattle wagons having a wheelbase of 11'-0"s 11'-3", or 11'-6", and fitted with the vacuum brake and screw couplings, may be run in Passenger trains on which they are authorised to be conveyed in accordance with clause 1 of the Regulations governing the running of four-wheeled vehicles in Passenger trains, except that when such cattle wagons have to be conveyed on trains which are steam-heated they must be formed at the rear of the steam-heated vehicles.

Any vacuum fitted grease box goods vehicles which may be required to work by Passenger train must be first specifically examined and passed by the Locomotive Department examiners and attached to trains which do not run more than 10 miles without stopping.

#### DETACHING OF ENGINES FROM, AND DIVISION OF, PASSENGER TRAINS ON GRADIENTS.

In the event of a passenger train coming to a stand on a gradient which is steeper than 1 in 100, owing to the inability of the engine to work the train, the engine may be detached, or the train may be divided and the first portion taken forward, provided that, after the continuous brake has been applied, the hand brakes on all brake vans are screwed hard on and the weight of the train left standing does not exceed that shewn in the following table:-

GRADIEN	г. ]	LOAD (I	NCLUD	ING WEI	GHT OF	BRAK	E VAN OR VANS
1 in 30	1	Twice	weigh	t of brak	e van o	r vans	S.
1 in 40				weight o			
1 in 50		Four	,,	,,	,,	,,	***
1 in 60		Five	,,	,,	"	,,	,,
1 in 70		Six	12	77	"	77	,,
1 in 80		Seven	,,	,,	"	"	,,
1 in 90		Eight	"	,,	,,	,,	,,
1 in 100		Nine	,,	,,	,,	,,	,,

#### COMPUTATION OF LOADS OF PASSENGER, PARCELS, MIXED AND FISH TRAINS.

The loads of all Passenger, Parcels, and Fish trains are calculated in tons.

To enable Guards and others to calculate the loads in tons, the tare weight of the vehicle, in  $2\frac{1}{2}$ -inch metal figures, has been painted at both ends, at alternate corners, of all stock formed in trains coming within the category of those mentioned above, and the method of computing the loads is as follows:-.. By adding together the tare weights, irrespective of whether the vehicles, PASSENGER TRAINS

passenger or otherwise, are loaded or empty.

PARCELS TRAINS ... .. By adding together the tare weights of the vehicles plus one ton for each loaded vehicle.

.. By adding together the tare weights of the vehicles plus three tons for FISH TRAINS each loaded vehicle.

Where trains are authorised to be run as "Mixed" trains, the total MIXED TRAINS

weight of the train is to be obtained as follows :-Tonnage of Passenger stock.

Tare weight of Goods Brake Van (where provided).

Tonnage of freight vehicles to be calculated as under :-

Class 1 traffic = 16 tons per wagon,

,, = 13 22 = 10

Empties (4-wheel stock) = 6 tons per wagon,

Standard allowances to be made for vehicles exceeding 10 tons

In the case of another Company's vehicle not marked with the tare weight, being formed in a train, the weight of the vehicle must be counted as ten tons for a horse box, carriage truck or other such small vehicle; twenty tons for a four or six-wheeled passenger-carrying vehicle or brake van; thirty tons for an eight-wheeled passenger-carrying vehicle or brake van other than 70 feet stock; and forty tons for a dining car, sleeping car, or 70 feet passenger-carrying vehicle or brake van.

The loads of trains calculated as above must be ascertained by Guards and the information given to Drivers at the starting points and at any subsequent points at which vehicles are attached or detached.

Guards must show on their journals the number of vehicles and the actual tonnage of the trains at starting points and on leaving subsequent points at which vehicles are attached or detached.

Stations when wiring particulars of trains must give the weight and number of vehicles for each

portion, viz.:-

"1,30 p.m. Paddington, 144 tons, 4 vehicles Penzance, 72 tons, 2 vehicles Kingswear, etc." THE TONNAGE LOADS OF PASSENGER, PARCELS AND FISH TRAINS FOR ENGINE WORKING PURPOSES ARE SHEWN IN THE SERVICE TIME TABLES.

THE LOADS REPRESENT THE CAPACITY OF THE ENGINE IF THE STANDARD POINT TO POINT TIMING IS TO BE MAINTAINED. ON SECTIONS WHERE THE GRADIENTS WILL PERMIT THE LOADS MAY BE EXCEEDED WITH A SUITABLE INCREASE IN THE POINT TO POINT TIMING, BUT ON SECTIONS WHERE THERE ARE STEEP RISING GRADIENTS IT WILL BE NECESSARY TO PROVIDE AN ASSISTANT ENGINE.

Note. - These instructions for calculating the loads do not affect those contained in this Appendix respecting the formation of passenger and mixed trains.

#### PASSENGER TRAIN VEHICLES BRANDED "NOT TO RUN IN THROUGH FAST TRAINS."

A number of passenger train vehicles, principally stock formerly belonging to the Absorbed Companies, is still in traffic, the vehicles being painted on the sole bars "Not to run in through fast trains." The vehicles are constructed with wooden under-frames, and it is undesirable they should be formed with heavy metal frame vehicles and subjected to severe stresses on stopping.

These vehicles must not be allowed to work on trains which run more than 30 miles without stopping, nor on any passenger trains through the Severn Tunnel, but should, as far as possible, be confined to local services in the section from which they originally came. In the event of any of the vehicles referred to getting out of course, an advice must be sent to the Rolling Stock Department, Office of Superintendent of the Line, in order that they may be traced and restored to their regular services.

#### CARE OF PASSENGERS' LUGGAGE IN COMPARTMENTS.

Care must be exercised in dealing with luggage conveyed in compartments with passengers. Guards, Train Ticket Collectors and the staff generally should exercise all possible vigilance over the property of passengers who are temporarily absent from their compartments. Very careful search of compartments at stations where coach working terminates must be made.

Luggage should not be placed in compartments except in the presence of the owner, and if it is known that passengers purpose leaving the compartment after the luggage is placed therein it should be pointed out that the Company cannot accept any responsibility in the event of loss during their

When it is known that passengers intend travelling in a Restaurant Car for a considerable portion of the journey it should be suggested that suit-cases be labelled and placed in the luggage van.

#### RUNNING OF SPECIAL PASSENGER, EXCURSION AND DIVIDED PASSENGER TRAINS.

1. Distribution of Working Notices.—Station Masters, or persons in charge, must personally distribute copies of the Special Train Notice to their staff, including Signalmen, and every person supplied with a copy is held responsible for reading it carefully through, noting the general information it contains and carrying out the instructions particularly applicable to himself. No excuse of want of knowledge of Special Arrangements can be admitted for any failure or neglect of duty.

If anything in a Special Train Notice is not thoroughly understood by those concerned, immediate application must be made to the Station Master for explanation and instructions, and, if necessary, the Station Master should at once apply to the Divisional Superintendent or District Traffic Manager if he is in any doubt upon any point. Should there be any defect or omission in the Notice, the person discovering it must at once draw the attention of his superior officer, so that it may be set right without

delay.

The Station Master, District Inspector, or other person supplying Notices to the staff must, in all

cases, take the men's receipts at the time the Notices are handed to them.

Station Masters, or persons in charge, must carefully examine all printed and written Notices, and take steps to ensure the attendance of Signalmen at the signal boxes, which are required to be kept open until all trains have been disposed of.

It is important that the working notices should be supplied to the Locomotive and Carriage Department in good time, so that proper arrangements can be made for providing the necessary

engine power.

- 2. Booking Offices. Station Masters are responsible for making proper and sufficient arrangements for the tickets to be issued in good time to avoid confusion or crowding of the passengers. Wherever necessary, direction notices or suitable men, or both, should be conspicuously placed to direct passengers to the Booking Offices or trains. As far as practicable, and at those stations where platform tickets are not issued, persons who are not travelling should be excluded from the platforms. With a view to securing punctuality in the despatch of trains, and also to avoid the Booking Offices becoming overcrowded, Station Masters and Chief Clerks are particularly requested to arrange for the office windows at the large stations or elsewhere, where necessary, to be kept open, except when absolutely necessary to be closed for office purposes. Under any circumstances the windows must be opened at holiday periods at least 30 minutes before the time each train is due to leave.
- 3. Brake power.—Station Masters, Inspectors, or any other person entrusted with the duty of starting trains or adding to them during the journey, will be responsible for seeing that all trains made up at, or starting from, their stations are provided with sufficient brake power according to the gradients over which they will travel. (For list of gradients, see appendices to Service Time Tables.)
- 4. Carriage accommodation .- Station Masters, Inspectors, Guards and others concerned, must report all cases in which there is insufficient carriage accommodation on the trains. Where it is anticipated that trains may require strengthening, an advice must be sent to the station where the work has to be done. An advice must also be sent where it is considered the number of coaches in the train may be reduced so that arrangements can be made for the work to be done without delay.

5. Relief trains.—Relief trains, when run at pre-arranged times, must not leave any station more than ten minutes before the advertised time, unless otherwise ordered, and must not be kept for connecting trains; but if the ordinary trains, which the Specials are appointed to relieve, be late, the Special

trains may be kept back, where necessary, to RUN AT THE ADVERTISED TIMES.

The first portions of divided trains must not be detained at Junctions waiting for Branch trains, except where the through portion of the train is the first part, but the second portion must call at Junction stations to maintain the connections. The second portion of a divided train need not call at the appointed stations, except for the purpose of setting down passengers, but the Superintendents must lay down definite instructions in their respective divisions as to the mode of carrying out this Regulation. The Guard in charge of the Second portion of a divided train is held responsible for seeing that there are no passengers in the train for the stations at which he instructs the Driver not to stop.

6. Equipment of trains.—All vehicles must be properly cleaned and lavatory compartments equipped before leaving the starting station, and the Guards must examine the lavatories, gas indicators

and electric lighting before starting, en route, and before commencing the return journey.

Upon arrival at destination on the forward journey, the Guards must at once see the person in charge and arrange for any necessary replenishing to be done before the return journey is commenced, and if it is not possible to supply Gas at the station from which the return excursion starts, arrangements must be made for the work to be done at the first suitable point on the homeward journey—an advice being sent to such place in sufficient time to ensure the necessary attention.

Luggage.—Care must be taken in labelling and stowing luggage at the various stations, and especially with through trains which are divided. All persons concerned must be very careful to see

that passengers and luggage are loaded in the proper portions of the divided trains.

If, for any special reason, luggage or other traffic is incorrectly loaded, i.e. in the wrong part of the train, a sufficiently early advice as to the exact position in which the traffic is stowed must be sent to the station ahead, where the traffic has to be unloaded or transferred.

#### RUNNING OF SPECIAL PASSENGER, EXCURSION, ETC., TRAINS-Continued.

Luggage must be sorted on barrows on the platform before the arrival of the trains and placed in charge of Porters opposite the van into which it has to be loaded.

Where special kinds of labels are provided for luggage to be conveyed by Excursion trains, such

labels must be used.

8. Telegraphing trains.—Particular attention must be paid to telegraphing the trains forward to the appointed stations; and the departure from a station of every train, the running of which is telegraphed forward, must be immediately reported to the Telegraph Office at that station. See Telegraph Appendix and train telegraphing circulars for full instructions respecting train telegraphing.

The stations starting an additional Special must in all cases immediately advise the principal stations

ahead by wire, in addition to the usual Box-to-Box message.

9. Guards.—Guards working special trains must assist at the terminal station on the forward journey as required, and on no account leave the station without first obtaining permission from the Station Master or person in charge, who must give the Guards clear instructions as to the time they are to return to duty. They must also assist the Ticket Collectors by opening and closing the carriage doors, and when specially called upon to do so by the person in charge, assist in the collection and examination of tickets.

The Station Masters or persons in charge of the stations where trains are started from, will be held responsible for providing Guards for special and excursion trains. They must carefully summarise the trains starting from their stations at least three days before the Guards are required, and must make special application to the Divisional Superintendent or District Traffic Manager for the Guards

required beyond those who can be furnished out of their own staff.

The Station Masters or persons in charge must prepare a list of each Guard's work, which must be handed to the Guard by the Station Master or Inspector where he is located, when he is summoned for the special duty. When men are taken away from their homes to work excursions starting from and terminating at other stations, their duties must, as far as possible, be arranged so as to admit of their getting to their homes in reasonable time after completing their work as Guards, and their orders must show how they are expected to return to their home station. Station Masters or persons in charge will also be responsible for supplying each of the Guards starting from their stations with a copy of the Special Train Notice, and all other Special Notices affecting the working of the traffic and the line over which they have to work; at the same time satisfying themselves that the Guards whom they may appoint to work special trips and special sets of trains are thoroughly acquainted with the road and the circumstances of the working of such trains.

Guards of Special trains must take sufficient rest between the time of arrival at destination of forward trains and the time of starting on the home journey. Station Masters or persons in charge of Stations at which forward excursion trains terminate, must see the Guards of such trains upon arrival, and where the total time from start to finish necessitates such a course, require them to take adequate rest, and they must not allow such Guards to work the homeward excursion without first satisfying themselves that they have had sufficient rest. Station Masters must keep a register of the names of

the Guards and others employed as above, for reference.

Guards must see that windows of unoccupied compartments are kept closed.

10. Severn Tunnel.—All trains must be made up in accordance with the special standard

instructions for working through the Severn Tunnel.

Station Masters providing Guards to work through the Severn Tunnel will be responsible for seeing that such Guards are in possession of a copy of No. 4 Appendix containing the instructions for working through the Tunnel.

No passenger train must be allowed to run through the Tunnel without lighted roof and tail lamps.

See Severn Tunnel Instructions.

- 11. Up trains to Paddington.—The Station Master at Paddington must be advised of the number of vehicles, and weight of special or excursion trains working into Paddington by the stations making up such trains, also by stations attaching additional vehicles *en route*, when the load of such train exceeds 16 8-wheel vehicles.
- 12. Bank Holiday traffic.—Station Masters at the principal stations must report to their Superintendents briefly on the general working, and state whether the traffic has been more or less heavy than usual, and whether any particular inconvenience has been experienced from want of coaches or other causes.

#### LADIES' COMPARTMENTS.

One or more compartments of each class, as required, must be provided for "ladies only" on semi-fast trains, other than business services, which are formed with non-corridor stock; also on the express night trains.

The compartments must be labelled before the commencement of the journey, and Guards and Ticket Collectors should be instructed that they must inform lady passengers who are unaccompanied that there are Ladies' compartments in the train.

Intermediate doors between First and Third Class Compartments in corridor composite vehicles with end doors only must be left unlocked in all cases.

(G.A. 10. 3/42. LK.1/6625/5).

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#### PASSENGER TRAINS NOT TO STOP ON BRIDGES.

Wherever the Railway passes over a bridge or viaduct near a stop signal, and the bridge or viaduct is not provided with a high parapet, no passenger train should be brought to a stand on the bridge or viaduct, but should, if possible, stop clear of it.

#### FASTENING CARRIAGE DOORS.

Station Masters or persons in charge of stations or platforms must see that all doors of vehicles

on passenger trains are closed and fastened.

To assist in the working of such trains, relief trainmen and other members of the staff travelling on the Company's business should make a point of closing doors of compartments upon alighting. If compartments are then empty, the windows should also be closed and during the months when the steam heating pipes are connected up the handle of the regulator in the compartment should be placed to the "On" position.

At all stations Guards must co-operate in closing and fastening doors of vehicles on their trains, and must see that all fastenings on horse boxes, carriage trucks or similar vehicles which are formed in their

trains or attached on the journey are secured.

#### "SLAM" LOCKS ON G.W. PASSENGER COACHES.

The doors of passenger coaches fitted with slam locks will automatically catch when pushed to the closed position, and the handles turn to the horizontal position. Unless the door is properly closed the handle will not turn to the horizontal position, but remain at an angle of about 45 degrees.

The handles of doors fitted with slam locks are of a different pattern from the standard handle, and members of the staff concerned must see that the doors of all vehicles are properly closed and

completely fastened.

Where trains are made up of vehicles fitted with both types of locks, care must be taken to see that doors provided with handles which are not automatically operated are secured by the handles being turned to the horizontal position.

#### CLOSING WINDOWS IN BRAKE AND LUGGAGE COMPARTMENTS OF COACHES IN WHICH GUARDS ARE NOT RIDING.

To prevent risk of fire from engine sparks, the windows of brake or luggage compartments, in which Guards are not riding, must be closed at the starting point and kept closed throughout the journey.

#### INSTRUCTIONS TO GUARDS AND TICKET COLLECTORS ON CORRIDOR TRAINS.

#### INSTRUCTIONS TO GUARDS AND TICKET COLLECTORS ON CORRIDOR TRAINS .- Page 115.

The existing paragraph I of Clause I to be deleted and the following substituted:-Corridor and gangway doors should be left unlocked so as to provide free access Locking of through the train, except as shown below:-

(I) Gangway doors at the extreme ends of the trains. Care must be taken to ensure that when vehicles are detached from a train en route that the gangway doors at the point of detachment are locked.

(2) Brake Vans or Luggage Vans at the extreme ends of the train. Where, however, a guard is riding in the van or it is empty, the doors should be unlocked. Should the guard have occasion to leave his van whilst the train is in motion he must lock the door.

(3) Brake Vans or Luggage Vans intermediate in the train. Where, however, there is a restaurant or buffet car on the train or a guard is riding in the van or the latter is empty, the doors should be unlocked.

(4) Where the gangway connections cannot be made.

The gangway doors at both ends of sleeping car accommodation on trains. Where, however, it is necessary to admit passengers to their berths or to give access to a restaurant car during the time the restaurant service operates, the the doors should be unlocked.

In laying down the marshalling of trains, arrangements should be made, if possible, to avoid a passenger carrying vehicle being isolated from the remainder of the train by being marshalled between the brakevan in which a guard is not riding and the sleeping car accommodation. In cases where, in the interests of the working, this is not desirable the door leading to the sleeping car must be

The Guard will be responsible for carrying out these instructions but Travelling Ticket Staff, where provided, should assist. In the case of Sleeping Cars, the Sleeping Car Attendant will be responsible.

corridor and gangway doors

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#### TO GUARDS AND TICKET COLLECTORS ON CORRIDOR TRAINS-Continued,

Passengers passing through Luggage Vans.

4. When it is necessary for passengers to pass through vans and luggage compartments which contain luggage, the Guard in charge must see that the luggage is securely stowed and that sufficient space is given to enable passengers to pass through readily and safely.

Train Lavatory Equipment. 5. Lavatories must be kept clean and properly equipped, and the Guard must report on his journal any irregularity he may detect.

Train Ticket Collectors or Guards, where no Train Ticket Collector is provided, must see that the lavatories are fully equipped at the commencement of the journey and hand a report to the Guard for attachment to his journal in all cases where the equipment is not complete.

6. Lavatories must, subject to the modifications shewn below, be equipped, with soap, sanitary paper and 3 towels at starting point, immediately before departure, where practicable, by the Chief Mechanical Engineer's Department.

Lavatories of Ocean Specials, Dining Car units and slip coaches must be equipped with 6 towels.

Lavatories of third class sleeping cars to be equipped with soap, sanitary paper and three towels at the starting point, immediately before departure, and in addition the Sleeping Car Attendant to be supplied with sufficient towels to enable him to hand one to each third class sleeping car passenger.

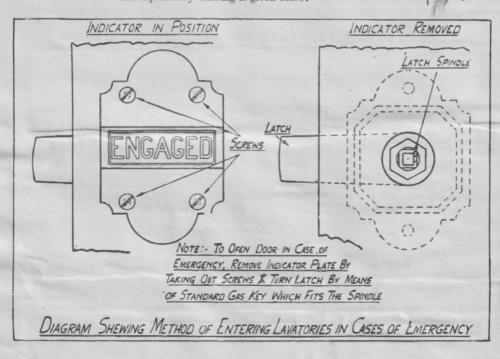
Lavatories of special trains or carriages reserved for the conveyance of Government forces, ships' crews or other similar parties, must not be provided with towels, except the first class lavatory compartments, which must be equipped with 3 towels.

A supply of clean, spare towels must be provided at starting point by the Chief Mechanical Engineer's Department for replenishment purposes en route, as shewn in the current supplement to these Instructions. These towels must be placed in the custody of the Train Ticket Collector, or Guard, if a Collector is not provided. If the Train Ticket Collector or Guard is relieved en route, the spare towels must be duly transferred to the relieving men.

Lavatory Door Locks.

7. In the event of it becoming necessary to obtain access to the locked levatory in a coach in the case of an emergency, this can be done by removing the indicator plate, which is secured by four screws, when the end of the spindle becomes exposed. This spindle will take the standard gas key, and, by using the same, the door can be readily opened.

An explanatory drawing is given below.



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The following additional paragraphs to be added to Clause 5:

On arrival at the terminating point of a journey Guards must satisfy themselves that all lavatories, other than those labelled "Lavatory Out of Use", are unoccupied. In any instances where it has not been possible for Carriage Examining Staff to give attention to the securing of the door of a defective lavatory en route, it is the responsibility of the Guard to specially inspect such lavatories at completion of the journey to ensure that they are unoccupied.

Guards, when in charge of passenger trains, must have with them a supply of labels reading "Lavatory out of Use" and lavatories which are defective and in need of repair must be labelled with one of these labels. Should a Travelling Ticket Collector or Guard have his attention drawn to a defect in a train lavatory such as to make it desirable for the lavatory to be put out of use, one of the labels reading "Lavatory out of use" must be affixed to the door. In such circumstances steps must be taken to see that the gangway or corridor door between the defective lavatory and the next usable one is left unlocked.

In cases where it is necessary to give effect to this instruction, a record must be made on the Guard's journal and the incident reported at the first opportunity to a Carriage & Wagon Examiner or other responsible person.

The following to be added to Clause 7:

When it is necessary to put a lavatory out of use through defect or other causes, in addition to affixing a gummed label as required by Clause 5, the following instructions must be carried out:

- (a) The inside handle and square spindle should be removed from the segment lock, leaving the outside handle only in position.
- (b) The door should be locked by reversing the methods shewn in the above paragraph for unlocking.
- (c) The coach itself should be labelled with a green "for repairs" label, giving the station, date and defects.

(G.A.16. 5/46. L.K.2/9657/F.)

The first paragraph of Clause 7 amended to read:—
In the event of it becoming necessary to obtain access to the locked lavatory in a coach of the former G.W.R. design in the case of an emergency, this can be done by removing the indicator plate, which is secured by four screws, when the end of the spindle becomes exposed. This spindle will take the standard gas key and, by using the same, the door can be readily opened.

The following to be added as the third paragraph of Clause 7:—
When it is necessary to release a lavatory door lock (with indicator set at "Engaged") belonging to a British Standard Carriage, the three screws which secure the indicator plate should be withdrawn, the plate removed, and the indicator driving pin will then become exposed. Upon sliding this pin towards the lock handle it will be possible to gain access by turning the lock handle.

(G.A.30 Op.—9/54 LKI/6624/Gen. E.)

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Paragraph 2 of the instructions under this heading to be deleted and substituted by the following:—

2. The Guard in charge must see that the inside sliding doors of luggage compartments, where such are provided, and the steel grilles of luggage compartments fitted in British Railways Standard Stock, are kept closed and locked whilst the train is travelling.

(G.A.31-7/56-T.50, 639G/7 L.K.1/13138/420)

DAMAGE TO CARRIAGE WINDOWS, ETC .- page 117. The following to be added to the list of amounts to be collected from persons responsible for the breaking of carriage windows: d. New type coaches. d. Window under ventilator, compart-Fixed window, corridor side, Erst 2 32 ment side Class ... Window side of ventilator, compart-Fixed window, corridor side, Third 3 43 ment side Class Fixed window, corridor partition,
First and Third Class
Door window, sliding door, corri-Drop window, corridor side First and Third Class 11 Door drop window dor partition /.. Lavatory drop window Window, sliding ventilator, compt. side (G.A.3.-12/37. C.M.E.-33/1

NOTE. The foregoing dates to be regarded as a general guide but franks and Storm Stoff must use heir descretion in the soons event of unusual climate dandinous.

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#### PASSENGER TRAIN INSTRUCTIONS.

#### ROBBERIES FROM LUGGAGE VANS.

In order to prevent robberies from luggage vans, the attention of Guards, Train Ticket Collectors, and Restaurant and Sleeping Car Attendants is directed to the importance of seeing that the following instructions are strictly observed:—

- (1) Unauthorised persons are not permitted to enter vans.
- (2) The Chard in charge must see that the inside sliding doors of luggage compartments, where such are provided, are kept closed and locked whilst the train is travelling.
- (3) Guards are warned against the practice occasionally adopted of an accomplice decoying them temporarily away from their vans or detaining them in conversation on some pretext, to afford the thieves time to operate.

#### SECURITY OF MAILS AND PARCEL POST RECEPTACLES.

The exercising every

ATTACHING COMPANY'S LADDERS, POLES, &c., TO STEPS OR ROOFS OF CARRIAGES.

Ladders, poles, etc., must not be conveyed on the footboards of vehicles on passenger trains, and under no circumstances must these articles be placed upon the roofs of carriages.

STEAM HEATING OF PASSENGER TRAINS-Page 117.

Clause I of the existing instructions to be cancelled and the following substituted:-

- The following dates are those which normally must be followed in the application and discontinuance of steam heat for passenger trains:
   Commencement.
- (f) All steam heater pipes to be fitted and coupled for use by 25th August.
- (ii) Heat to be applied:
  Sleeping Car trains

  (iii) Ist September

Other Express trains while running after 5.0 p.m. and before 10.0 a.m.

and before 10.0 a.m.

All other passenger trains

Ist October

Discontinuance.

- (i) Heat to be discontinued:
  All trains except as shown below
  Sleeping Car trains
  Other express trains while running after 5.0 p.m. } 15th June
- Other express trains while running after 5.0 p.m. and before 10.0 a.m.

  (ii) Pipes to be removed as soon as possible after 15th June

The foregoing dates for the application and discontinuance of steam heating are to be regarded as a general guide, but guards and others concerned must use their discretion in the event of unusual climatic conditions.

(G.A.30 Op.—9/54 T33320 G/2)

climatic conditions. (G.A.23—7/49. R.E. Stand:—T.33,320, G.2.)

#### DAMAGE TO CARRIAGE WINDOWS, ETC .- Page 117.

The following to be substituted for the list of amounts that must be collected from persons responsible for the breakage of windows or carriage fittings, if possible at the time of the incident, otherwise the identity and address of the person(s) should be established and reported.

			WIN	DOWS		
Old Type Coaches		s.	d.		s.	d.
Top side light		6	0	Large light in corridor of saloon		
Ordinary door light		16	0	coaches under 34 inches wide	35	0
Corridor door light		14	0	Large light in corridor of saloon		
Ordinary quarter light		22	Ö	coaches, 34 inches wide and not		
Corridor quarter light		17	Ö	exceeding 50 inches wide	52	6
Lavatory drop light		8	6	Large light in corridor of saloon		
Lavatory fixed light		10	6	coaches over 50 inches wide	56	6
Quarter light "Smoking"		22	0	Ventilators, large in Auto cars	86	0
	lights	69	6	Ventilators, small in Auto cars	52	6
Door light frame including the	lights	07	0	Venciacors, smail in Auto cars	32	
New Type Coaches		s.	d.		s.	d.
Door light frame (with glass)		71	6	Window under ventilator, com-		
Fixed window, corridor side				partment side	52	6
First Class		72	6	Window side of ventilator, com-		
Fixed window, corridor side,				partment side	10	0
Third Class		62	6	Fixed window, corridor partition,		
Drop window, corridor side,				First and Third Class	16	6
First and Third Class		17	6	Door window, sliding door,		
Door drop window		19	0	corridor partition	19	6
				Window, sliding ventilator, com-		
Blinds, First Class				partment side	10	0
Door 19½ inches		12	0			
Quarter light		9	0	Rack netting, First Class	28	0
Sliding door		10	6	Rack netting, Third Class	27	0
				Wash basin, old type	75	0
Blinds, Third Class				Wash basin, N.P. without		
Door 19½ inches		12	0	pedestal	78	6
Quarter light		10	6	Pedestal N.P. without wash basin	72	0
Sliding door		10	0	Pedestal and wash basin N.P		
				complete	144	0
Carriage Fittings				Cushions, Third Class corridor	138	0
Curtains, Third Class		26	6	Cushions, Third Class, non corr.	220	0
Curtains, First Class		25	6	Cushions, First Class corridor		
Electric light bulbs		2	0	6 per compartment	74	6
Window straps, First Class		12	6	Cushions, First Class corridor		
Window straps, Third Class		5	6	4 per compartment	128	0
Mirrors, Lavatory O.P. (inc. fran		33	0			
Mirrors, Lavatory O.P. (Glass on		8	0			
Mirrors, N.P	***	17	6			
Mirrors, Compartment		19	0			
	so be	charge	ed in th	e event of breakage of windows and carria	ge fitti	ngs

The same amount should also be charged in the event of breakage of windows and carriage fittings in other Region's stock working on the Western Region. (G.A.30 Op.—9/54 C.S.O.—C.B/D)

#### STEAM HEATING OF PASSENGER TRAINS-Continued.

flexible pipes must be connected throughout—the stop-cocks being opened by bringing the handles in line with the pipes. The stop-cock at the rear of the last vehicle must be closed by bringing the handle in line with the end of the vehicle.

The flexible pipe at the rear must be properly secured with the hook and chain provided for the purpose, the hook to be attached to the clip at the lower end of

the pipe and not in the mouth of the connection.

3. Steam must be passed through for a sufficient time to warm the whole train, and the driver must regulate the pressure of steam in the train according to the weather and length of the train, i.e., for trains not exceeding 5 coaches, 40 lbs. pressure must be maintained; for trains consisting of 6 to 8 coaches, 60 lbs. pressure must be maintained; and for trains consisting of more than 8 coaches, 70 to 80 lbs. pressure must be maintained.

In severe weather the full pressure of 80 lbs. must be maintained in all trains formed of more than eight coaches.

4. The engine should, whenever possible, be attached to the train at least ten minutes before starting, and the pipes coupled. The Driver must, unless otherwise instructed by the Guard, turn on steam at once.

At terminal or other stations, where the empty trains are drawn or backed into the station, the pilot engine must be used to warm the train before the train engine

attached.

- 5. Defects or irregularities in the working of the apparatus must be reported by the Guard on his train journal and an Examiner's attention called to the matter at the first opportunity. The Junior Guard or Train Ticket Collector will report to the Head Guard if necessary.
- 6. To enable the Examiners at terminal stations, or at stations where engines are changed, to locate any defects, the Driver must not shut off steam until the engine is about to be uncoupled.
- 7. All concerned should see that the handles of the regulating valves in compartments are placed in the "On" position. If already in that position, they should be tried to see that they work freely; if not, the matter should be reported to the nearest Carriage Examiner.
- 8. At stations where Carriage Cleaners are employed, they must place the handle of the regulating valve in each compartment to the "On" position when cleaning the carriages.
- 9. Prior to the commencement of the journey the Guard in charge of the train must, as far as possible, see that the regulating valve in each compartment is turned to the "On" position, and in the case of corridor trains, Guards and Train Ticket Collectors should be on the alert when passing through their trains to see that the handles of the regulating valves in empty compartments are placed in the "On" position.
- 10. When uncoupling steam pipes, the stop cocks at the ends of the vehicles (or engine and vehicle when the engine is being detached) must be closed by placing the handles in line with the ends of the vehicles, or vehicle and engine as the case may be, and away from the draw bar hook. If there is any steam in the pipes it will escape through a small port in the cock.
- 11. Drivers of shunting engines must see that their steam-pipes are coupled and steam turned on. Particular attention must be paid to the warming of the night trains.
- 12. Special arrangements have been made locally for preventing the steampipes of coaches running on important trains from freezing, and these must be put into force by the Locomotive and Carriage Department when they consider it necessary to do so.
- 13. In the case of coaches not covered by Clause 12, the following arrangements must be carried out:—
  - (i) Steam must be kept continuously on the train whilst in traffic.
  - (ii) As far as possible the engine must remain coupled to the train whilst in traffic, and if the engine has to be detached for any reason, the Traffic Department staff must see that it is allowed to be recoupled to the train as quickly as possible.
  - (iii) When putting away trains or coaches, the men responsible for uncoupling the steam-pipes must open the cock at the end of the train and satisfy themselves

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Special Precautions in time of Frost. ringing le must

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coupling emselves 5. Defects or irregularities in the working of the apparatus must be reported specially by the Guard on a memorandum, in duplicate, with particulars of the pressures recorded on the front and rear gauges, one copy to be handed to an Examiner, calling his attention to the matter at the first opportunity, the other copy to be attached to the train journal. The Junior Guard or Train Ticket Collector will report to the Head Guard if necessary.

#### STEAM HEATING OF PASSENGER TRAINS-Continued.

that steam is passing through the rear vehicle before the engine is released. If steam does not issue from the cock, the nearest Locomotive and Carriage Department Examiner or responsible man must be informed at once, in order that he may make arrangements for putting the train in order before its next trip.

- (iv) After uncoupling the engine, the stop-cocks must be opened and the flexible pipes between each coach must be uncoupled and left hanging down.
- (v) When necessary, arrangements must be made for engines to join their trains half an hour earlier for the first trip in order to warm their carriages. The men who couple the pipes should leave the cock at the rear end of the train open until dry steam blows from it.
- 14. Slip Coaches.—To admit of slip coaches being steam heated, an adaptor is provided whereby steam-heating pipes can be separated automatically when the coaches are slipped. The adaptors must be fixed by the Carriage Examiners and removed by the Traffic Department staff. Boxes are provided for conveying adaptors back to their home stations, the name of the station being stamped on a brass plate affixed to the lid.

When there is only one slip portion on the train, one set of adaptors will be used,

being fixed between the Main train and slip.

When two or more slip portions are attached, additional sets of adaptors will be

required, and fixed between each slip portion.

The box for the adaptor on the end of the Main train will be handed to the rear Guard, who must hand it to the staff at the station at which the adaptor is removed. The boxes for the adaptors on the slip coaches will be handed to the Slip Guards. The adaptors should be placed in the boxes on arrival at the slipping stations and booked back to their home stations by the first available train. The adaptors and boxes must be kept in a specially appointed place at the home station, so that the Locomotive and Carriage Department may have ready access to them.

#### REGULATION OF STEAM-HEATING APPARATUS ON BANANA VANS.

All banana vans are fitted with the steam-heating apparatus, and, when loaded, must be formed next to the engine and steam heated when required.

#### LIGHTING OF TRAINS.

1. Station Masters, Inspectors and others at starting stations must see that Gas. trains timed to arrive at their destination after dark, or where required, or which are likely to work back after dark, are properly supplied with gas at the starting stations.

Coaches fitted with lamps to burn gas must have the globes and reflectors properly

cleaned and the reservoirs charged with gas.

The gas must be lighted on the bye-pass before leaving the starting station if the lights will be required before the train arrives at its destination, or for Tunnel purposes. The lights must be turned full on at the last stopping-place, unless otherwise specially provided for, before entering the tunnels named below. They must be e\_tinguished in each corridor coach after passing through the tunnel for which the Train has been lighted or at the first stopping place, provided the Train will reach its destination before dark. Train Ticket Collectors, when going through the trains, must assist Guards in extinguishing lights.

If, owing to a bye-pass defect, the gas will not light on the bye-pass it must burn

full on.

- 2. When there is an interval of more than half an hour at stations at either end of, or at intermediate points on Branch lines before trains are again required for use, the lights in the compartments should be placed on the bye-passes. When the trains are again required the lights should be turned up only in as many coaches as will comfortably accommodate the passengers, further vehicles being lighted up as the necessity arises.
- 3. Guards and others concerned must see that the cocks on all vehicles, other than passenger-carrying, fitted with incandescent gas, are turned down when the lights are not required for use, and between long-distance points, the lights should always be on the bye-pass.

#### LIGHTING OF TRAINS-Continued.

4. Fish, Fruit and Milk trucks fitted with gas lighting are provided with a byepass cock beneath the bodies of the vehicles to enable the gas to be turned up or down as may be necessary.

Electricity.

5. Every effort must be made to economise the electricity stored in the batteries of electrically-lighted coaches, and Station Masters, Inspectors and others must see that the lights are burning only when required. The lamps in electrically-lighted coaches must not be switched on more than 30 minutes before the train leaves the

Electric light Failures.

6. In cases where the electric light fails, or any electrical defect is developed in a coach in the train they are working, particulars giving number of coach and compartment, with brief details of failure must be given by Guards at the next stopping-place at which there is an Examiner, and similar particulars must be shewn on their journals.

Electric Through Control Couplers.-When not in use, electric through control couplers must be placed in the pockets provided, and not allowed to hang loose.

Electric Lighting of trains through Tunnels.

8 H N Electrically lighted trains must be lighted through all tunnels irrespective of length where the Guard has facilities for switching the lights on and off, while running.

(b) All passenger trains must be lighted through the following tunnels during daylight.

Ardley. Ballingham (between Holme Lacy & Fawley). Bincombe (between Dorchester and Weymouth). Birkenhead. Box. Brimscombe. Bryn (P.T. Section). Caerphilly.

Campden. Chipping Sodbury.

Clifton Down. Cockett. Colwall. Cymmer.

Dainton. Dinmore. Dudley. Evershet.

Fox's Wood. Grenofen (Launceston Branch).

Grimstone. Halton. Ledbury.

Llangyfelach. Lonlas.

Marley. Merthyr.

Mitcheldean. Newport.

Old Hill. Patchway. Pencader.

Pencaedrain (Vale of Neath).

Penllergaer.

Perran (Falmouth Branch). Perridge (between Longdown &

Christow).

Pinnock (Fowey Branch). Poundbury (near Dorchester).

Pontypridd. Quaker's Yard. Rhondda. Sapperton. Severn.

Shaugh (Launceston Branch).

Somerton.

Sparnick (Falmouth Branch).

Tidenham. Wenvoe. Whiteball.

Yelverton (Launceston Branch).

The electric lights must be switched on during daylight immediately before entering the tunnel where the Guard has the means of doing this while running, otherwise they must be switched on at the last stopping place before entering the

The lights must be switched off immediately after passing the tunnel where the Guard has the means of doing this while running, otherwise they must be switched off at the first stopping station after passing through the tunnel.

The Guard of the train (Head Guard when there is more than one Guard) will be responsible for seeing that the instructions are duly observed.

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#### 7. ELECTRIC THROUGH CONTROL COUPLERS.

Through control cables must be coupled on all coaches before the train starts and when any alteration in the formation of the train is made care must be taken to see that the whole of the lights on the train can be switched on from the Brake Van in which the Guard is to ride. The Guard of the train must satisfy himself that this instruction is complied with. When not in use, electric through control couplers must be placed in the pockets provided, and not allowed to hang loose.

Clause 7 of the existing instructions to be renumbered 8 and the first paragraph lettered (a).

(G.A. 18. 11/47. L. K.1/8235/Gen. 4.)

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Reference to the following to be made on page 121:-

FIRES IN PASSENGER TRAINS.

If any member of the staff, either in the employ of the British Transport Commission or the British Transport Hotels and Catering Services, becomes aware of a fire on the train on which he is travelling he should take appropriate steps to extinguish it. If, however, he is unable to do so promptly he must arrange for the train to be stopped immediately so that suitable action can be taken.

After the train has been stopped the services of a Carriage and Wagon Examiner should be obtained, if possible. Attention is also drawn to the fact that if the fire occurred in a roof with a ceiling, it may be necessary to remove part of the ceiling to ensure that the fire is properly extinguished. When the train proceeds on its journey a member of the staff should, if possible, travel in the affected compartment or carriage for the purpose of observation and he should be perpared to deal with any subsequent outbreak of fire. Should a member of the staff not be available to ride in the compartment or carriage, the Guard should examine the affected compartment or carriage as frequently as possible.

Passengers should not be permitted to travel in the vehicle concerned until it has been passed fit for service by a Carriage and Wagon Examiner. (G.A.30 Op.—9/54 LKI/II343/365E)

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#### WATER CANS FOR USE IN LAVATORY COMPARTMENTS IN CASE OF EMERGENCY.

During exceptionally severe weather in winter months, the water tanks in lavatory compartments not fitted with warming apparatus, should, where possible, be emptied at night and refilled in the morning.

To avoid the risk of damage to lavatory fittings by frost in the case of coaches sent to various points to stable for long periods during the winter months, it is very important that the tanks, pipes, etc., should be completely emptied of water, and the following arrangements should operate:

In coaches not fitted with hot water apparatus, tanks to be emptied by means of the flushing valve and valve over basin, but with coaches fitted with hot water apparatus, the drain cock below the boiler must also be opened. This to be done by the Department concerned before vehicles leave a station or depot for stabling.

When the vehicles have finally come to rest at the stabling point, the valves over basins and pedestals must be operated to clear the pipes of any residual water in the tank which may have entered the pipes through oscillation when running. This work to be performed by the Traffic Department staff of the station nearest to the stabling point, or as may be otherwise specially arranged.

A sufficient number of water-cans will be supplied each winter to all stations from which carriages equipped with lavatories commence their journey. These cans are only to be used in case of emergency, such as burst or frozen pipes.

At the close of the winter season all the cans must be returned to the Stores Department, Swindon.

As a guide to the men responsible for filling the tanks on coaches which have heating apparatus under the lavatory tanks, the letter "H" is fixed on the tank filling cover on the roof of the vehicle. This arrangement will gradually supersede the old method of painting "H.W." on the end of the vehicle.

#### LAVATORY COMPARTMENTS.

Station Masters at terminal stations, and members of the Travelling Staff, should take steps to satisfy themselves, by frequent personal inspection, that all the fittings and equipment of the train lavatories are clean, and in thorough working order, also that they are provided with a good supply of water, sanitary paper, soap and clean towels.

#### EMERGENCY APPLIANCES. &c., IN PASSENGER TRAINS.

Emergency appliances and "First Aid" requisites are provided on passenger trains, as shewn below, for use in the event of an accident.

Description.	Where Provided.
HAND FIRE EXTINGUISHER	As far as possible two No. 1 "Pressure" extinguishers are provided on all Passenger trains (one at each end of the train, in Guards' brake compartment), two in each sleeping
	car (one at each end of corridor) and two in each Restaurant Car (one in latchen and one in vestibule).  One No. 4 "Hand Cup" extinguisher will be provided as far as possible in all corridor coaches (at one end of corridor).
SET OF TOOLS, &c	One case in the Guard's compartment of each passenger brake vehicle.
"FIRST AID REQUISITES	One case or outfit in the Guard's compartment of each pas- senger brake vehicle.

NOTE .- The FIRE EXTINGUISHERS AND TOOLS must only be used in the case of fire, or in the event of a train accident.

1. In the event of an accident to a train conveying passengers, it is of the first importance that immediate steps should be taken to relieve any who may have sustained injury, as well as to adopt all precautionary measures, which the circum- Inspectors and others. stances may render desirable, in the direction of limiting the effects of the occurrence. The following important points are therefore set out as supplementing the instructions of the Rule Book and those detailed above, and as being necessary to be borne

Special Instructions to Station Masters.

(a) Examine train to see if there is any sign of fire from live engine goal or broken gas cylinders or connections, and if so take prompt steps to extinguish it by means of the Fire Extinguishers provided on the train, or any other available means.

Immediately call for assistance from the nearest Fire Drigades, who

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EMERGENCY APPLIANCES, ETC.-Continued.

should, if possible, be informed the exact situation of the trains and the means of access.

(b) Ascertain where the greatest damage has occurred, and take instant steps to release my passengers who may be entangled in the wreckage. Tools and other appliances provided in the Brake Compartments to

be handed out to the most experienced men available.

(c) Insure prompt telegraph or telephone communication with the nearest likely places for Doctors, Nusses, Ambulances, Refreshments, &c., cording to circumstances.

The assistance of all available "First Aid" men to be obtained, and the best possible use made of the "First Aid" Outfits provided in the

Brake Compartments.

At stations where Ambulance Sections are formed, in the event of a train accident in the vicinity, the Station Master must arrange immediately to despatch one or more of the organised Ambulance Sections, with their equipment, to the scene of the accident on receipt of a telegraphic or telephonic request

- (d) Enlist the services of any uninjured and willing passengers or other persons to convey messages, and to assist generally, under the direction of the Railway Staff.
- (e) Use cushions taken from the coaches in such a manner as to form comfortable couches for any injured persons, until such persons can be
- (f) Advise the nearest Station Master and other officials, and also the Permanent Way men, at the earliest possible moment.

Broken Gas Pipes Should gas be escaping from a broken pipe, the pipe should be bent away from timber or inflammable wreckage clear of any source of ignition and, if practicable, in the same direction as the wind. The pipe can be easily bent or hammered flat to

prevent the escape of gas.

Should the gas be ignited at a broken pipe-end it can be extinguished by directing a stream of liquid from the No. 1 Pressure Extinguisher on to the end of the pipe

# EMERGENCY APPLIANCES, ETC., IN PASSENGER AND PARCELS TRAINS .- Page 121.

The following to be inserted after "Diesel Cars" in the table in the first paragraph:-

New Type Diesel Cars.

Power Cars.—One water/CO.2 pressure extinguisher in the brake compartment and two CO.2 gas extinguishers in the driving cab.

Note.—A chlorobromomethane (C.B.) extinguishing system is fitted to each diesel engine, with detectors to give the driver warning of fire and indicate which motor is affected.

Trailer Cars.—One water/CO.2 pressure extinguisher at one end of the corridor.

(G.A.31-7/56 A.1/72034/102)

Instructions as to use.

TO OPERATE:-

1. Screw down wheel to right.

2. Turn valve to control jet.

(On the turn valve the "open" and "shut" positions are shown by raised letters on the body of the

Screw down wheel to right

Press valve to control jet. (On the press valve the word" press" is stamped.)

Ox the top of the wheel are the words "Screw right down," the direction being shown by an arrow.

NOTE.—The wheel on the top of the No. 1 Pressure Extinguisher must not be screwed down nor the appliance made use of except in the case of fire.

EMERGE

parcel

HAND FIR

SET OF TO

FIRST AID

Note-The

EMERGENCY APPLIANCES, &c., IN PASSENGER TRAINS .- Page 121.

The instructions under this heading to be cancelled and the following substituted:-

EMERGENCY APPLIANCES, &c., IN PASSENGER & PARCELS TRAINS.

Emergency appliances and first aid requisites are provided in vehicles on passenger and parcels trains for use in the event of fire or accident in accordance with the following:-

Description	Where provided					
HAND FIRE EXTINGUISHERS	Sleeping Cars	Two water/CO.2 pressure extinguishers, one at each end of corridor, or alternatively the two extinguishers in a recess at one end of the corridor.				
	Restaurant Cars	wie corridor.				
	Twin Units	One foam and one CO.2 gas extinguisher in kitchen. One water/CO.2 pressure extinguisher in corridor at end of kitchen and one water/CO.2 pressure extinguisher in corridor at end of dining car.				
	Single Units	One foam and one CO.2 gas extinguisher in kitchen, and one water/CO.2 pressure extinguisher in corridor at end of car.				
	Corridor Brake Vans	Standard Stock—One water/CO.2 pressure extinguisher and two buckets in brake compartment. One water/CO.2 pressure extinguisher in corridor.  Non-Standard Stock—One water/CO.2 pressure extinguisher in brake compartment.				
	Other Corridor Vehicles	One water/CO.2 pressure extinguisher or W.R. No. 4 "Hand Cup" extinguisher at end of corridor.				
	Non-Corridor Brake Vans	Standard Stock—One water/CO.2 pressure extinguisher and two buckets in each brake compartment.  Non-Standard Stock—One water /CO.2 pressure extinguisher in each brake compartment.				
-	Diesel Cars	One water/CO.2 pressure extinguisher and one W.R. No. 5 "Vessel and Syringe" C.T.C. extinguisher.				
	Post Office Vans	Two W.R. No. 4 "Hand Cup" extinguishers and two buckets of sand.				
SET OF TOOLS &c.	One case in the gu vehicle.	ard's compartment of each passenger brake				
FIRST AID REQUISITES	One cabinet in the g	guard's compartment of each passenger brake				

accident.

and others.

- 1. In the event of an accident to a train conveying passengers, it is of the first import-Instructions to Station Masters, ance, after protection as required by the Rules, that immediate steps be taken to relieve any who may have sustained injury, and to adopt all precautionary measures which the circumstances may render desirable in the direction of limiting the effects of the occurrence. The following further important points which are supplementary to the instructions of the Rule Book must be borne in mind:-
  - (a) Examine train to see if there is any sign of fire from live engine coal or broken gas cylinders or connections, and if so take prompt steps to extinguish it by means of the fire extinguishers provided on the train, or any other available means.

Immediately call for assistance from the nearest Fire Brigade, who should, if possible, be informed the exact situation of the train and the means of access. The correct method for calling the brigade by telephone is included in the Fire Notice exhibited at every station, yard and signal box.

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(b) Ascertain where the greatest damage has occurred and take instant steps to release any passengers who may be entangled in the wreckage.

Hand out tools and other appliances provided in the brake compartments to the most experienced men available.

(c) Ensure prompt telegraph or telephone communication with the local police officer and with the nearest likely places for doctors, nurses, ambulances, refreshments, etc., according to circumstances.

Obtain the assistance of all available first aid men and make the best possible use of the first aid cabinets provided in the brake compartments.

If a serious train accident occurs in the vicinity of a station, the station master or person in charge must despatch immediately as many staff qualified to render first aid as possible, with equipment.

- (d) Enlist the services of any uninjured and willing passengers or other persons to convey messages, and to assist generally, under the direction of the railway staff.
- (e) Use cushions taken from the coaches in such a manner as to form comfortable couches for any injured persons until such persons can be removed.
- (f) Advise the nearest station master and other officials, also the permanent way men, at the earliest possible moment.
- (g) Should gas be escaping from a broken pipe, bend the pipe away from timber or inflammable wreckage clear of any source of ignition and, if practicable, in the direction to which the wind is blowing. If possible, the pipe should be hammered flat to prevent the escape of gas.

Should the gas be ignited at a broken pipe end and it is not possible to flatten it, remove all inflammable material from the vicinity of the flame. If the end of the pipe is not accessible the adjacent woodwork should be wetted. Provided persons are not trapped in the wreckage, the flame may be extinguished by directing a stream of liquid from a water /CO.2 pressure extinguisher to the end of the pipe in the SAME DIRECTION as the issuing flame. Naked lights should not be allowed in the vicinity until the gas has been dispersed.

#### 2. Fire Extinguishers

The W.R. No. I pressure extinguisher is fitted with a quick-acting press valve, which is controlled by thumb pressure and closes automatically to reserve the liquid.

Entinguishers.

The following proprietary extinguishers are provided in passenger vehicles for use as shown:—

Name	Туре	Use
Conquest 55	Water/CO.2 pressure	All fires except electrical, petrol and oil
Firesnow	do.	do. do.
Waterloo	do.	do, do.
Phomene	Foam	Petrol, oil and fat fires.
Kidde (Lux)	CO.2 gas	Electrical, petrol, oil and fat fires
Pyrene CO.2	do.	do. do.

Instructional Booklet. In addition to the instructions shown on each appliance, booklet B.R.7006—"Portable Fire Extinguishers on Trains" illustrates the various types of extinguishers and their methods of operation.

All train staff must be in possession of this booklet and be fully acquainted with its contents.

Seals on Extinguishers.

Armangements

Lead seals marked "B.R.(W)" and "S.F.B." are provided on W.R. No. 1 water/CO.2 pressure, W.R. No. 4 "Hand Cup," C.T.C. and CO.2 gas extinguishers.

Guards, ticket collectors, sleeping car attendants, restaurant car attendants and travelling carriage cleaners will be instructed in the use of each type of extinguisher, as may be appropriate according to the types they are likely to encounter. Where possible they will have actual experience in handling the appliances.

The periodical examination of trainmen in the rules and regulations must include the use of fire appliances.

Train staff must not on any account test the appliances by operating them and drawing off small quantities of liquid. When seals are provided they must not be broken unless the extinguishers are required for a fire or when permission for doing so is given by an authorised instructor.

3, Tool Cases.

The tool cases are painted red with the words "FOR EMERGENCY USE ONLY" in gilt lettering, black edged, on the front of the cases and a rectangular piece of reinforced glass is inset in the lid to enable the contents to be seen. The lid is taped and sealed at the top and bottom on one side, and in the event of an accident it is only necessary to cut or break the tape in order to open the lid and obtain the tools and appliances.

Each tool case in Western Region stock contains the following articles:-

Saw, hand, 26 inches, rough tooth. Saw, hand, 22 inches, rough tooth.

Felling Axe.

Axe, hand, small.

2 Crowbars, 3ft. 5in. and 2ft. 5in. long respectively.

Extension Tube.

Chain, 12ft. Oin. long, with hook at one end and ring at the other.

Spade.

Set of Splints (4 to set).

Metal Sleeve (for extension of splints)

Tool cases in B.R. standard stock contain the following articles:-

Saw, hand, large. Saw, hand, small. Felling Axe.

Axe, hand, small

Crowbar, 3ft. 0in. long. Crowbar, 4ft. 6in. long (in clips on left-hand side of tool case).

Crowbar Extension Tube.

Sledge Hammer. 2 Steel Wedges.

Coil of Rope, approx. 13 yards long and lin. diameter.

Coil of Rope, approx. 12 yards long and ½in. diameter. Inspection Lamp, electric, fitted with 200ft. flexible lead and plug.

2 Lamps, hand, paraffin.

### 4. Examination and Maintenance of Fire Extinguishers and Tool Cases.

The Chargeman Carriage & Wagon Examiner or other authorised member of the Carriage & Wagon Engineer's staff at the various depots will be held responsible for seeing the fire extinguishers and tool cases are in their proper places and that seals (where provided) are intact.

Should an extinguisher be damaged or missing or be found with a broken or missing lead seal when it should have one, such extinguisher must at once be replaced. A supply of spare extinguishers and tool case replenishments will be kept on hand for renewal purposes at the following depots:-

per places

Fire Extinguishers. Aberdare H.L. Oxford Aberystwyth Paddington Barry Penzance Birmingham, S.H. Plymouth Bridgend Pontypool Road Bristol (Dr. Days)

Caerphilly (To draw on Cardiff Queen St.)

Cardiff General Cardiff Queen Street Carmarthen

Cheltenham St. James-

Exeter Goodwick Helston Hereford Leamington Liskeard Marylebone Neasden Shops

Newport (Ebbw) Carriage Shed

Newquay Neyland

Old Oak Common

Oswestry

Oswestry Shops

Pontypridd (To draw on Cardiff Q. St.)

Pwllheli Reading Ruabon St. Blazey St. Erth

Severn Tunnel Junction

Slough Southall

Stratford-upon-Avon Swansea High Street

Swindon Taunton Trowbridge Truro Tyseley Westbury West London

Wolverhampton Cannock Road

Wolverhampton L.L.

Worcester Wrexham

Tool Cases.
Aberystwyth
Barry
Bridgend
Bristol
Cardiff General
Carmarthen
Cheltenham St. James
Fishguard & Goodwick
Hereford
Marylebone
Neasden Shops
Neath
Newport (Ebbw) Carriage Shed

Neyland
Old Oak Common
Oswestry,
Penzance
Plymouth
Pwilheli
Swansea High Street
Taunton
Tyseley
West London
Wolverhampton Cannock Road
Wolverhampton L.L.
Worcester

Duties of Guards, Sleeping Car Actendants and Restaurant Car Actendants.

Guards in charge of trains must satisfy themselves that extinguishers are installed in accordance with the introductory paragraph to these instructions and that the seals on extinguishers (where provided) and tool cases are intact. In the case of restaurant and sleeping cars the attendants will be responsible for examining the extinguishers and reporting defects, etc., to the guard.

Duties of Carriage Cleaning Staff.

Carriage cleaning staff observing deficiencies, damage or broken seals when carrying out interior cleaning, must advise the Garriage & Wagon Engineer's staff, so that replacements can be installed with a minimum of delay.

Fire Appliances damaged or missing— Guards to report. The guard will be responsible for reporting to the Carriage & Wagon Examiner on duty at the station where the train terminates its journey any cases of extinguishers being damaged, missing or having broken seals. 'Where restaurant or sleeping cars are formed in the train the guard must confer with the attendants.

In cases where the guard does not proceed with the train to its destination he must report any defects, etc., to the guard who relieves him, and the latter will be responsible for advising the Carriage & Wagon Examiner at the terminating station.

Any such report made by the guard must be recorded on his journal and the District Operating or District Traffic Superintendent in whose office the journal is filed must immediately pass an extract of the report to the District Outdoor Carriage & Wagon Engineer. The latter must then report any special matters or defects in connection with fire appliances to the Carriage & Wagon Engineer, Swindon.

Replacing Extinguishers. If an extinguisher is damaged or the seal provided is broken or missing it must at once be exchanged and the defective appliance returned in one of the crates provided to the Fire Station, M. & E. Engineer's Department, Swindon; the Chargeman, Carriage & Wagon Examiner or other authorised member of the Carriage & Wagon Engineer's staff at the depots mentioned being responsible for this duty.

#### 5. First Aid Cabinets.

Contents of Cabinets. The First Aid Cabinet is marked "BR(W)—FIRST AID No. 3" and contains the following requisites:—

Sterilised Dressing (small) .				2
				2
		•••		2
	•	~ .		6
		***	***	3
Sterilised Cotton Wool, ½oz. p.	acket			3
Antiseptic No. 5—2oz. bottle		•••		1
Sal Volatile-2oz. bottle with s	crew cap			
(also to be used as smell	ling salts)			1
			***	1
Triangular bandages (individuall		1)		9
Roller bandages 2ins. x 2 yards				6
Safety pins—set of 6 .				1
Splints, with junctions—set .				1
		***		1
Lint—white, ½oz. packet		***		3
Eye drop No. IA (castor oil)-	Joz. bottle	е		1
Card of Instructions .		***		1
Report Form				1

The First Aid Cabinet is primarily intended for use in "Train Accidents."

The lid of the cabinet is sealed but the seal may be broken in event of emergency as indicated above, when the contents will be obtainable.

Cabinet. How sealed.

#### 6. Examination and Maintenance of Train First Aid Cabinet.

Responsibility.

Cabinets requiring umention. Station and Yard Masters will be held responsible for ensuring that the examination, replenishment and sealing of First Aid Cabinets is carried out by the Carriage Cleaning Staff.

Should a First Aid Cabinet be damaged or missing, or found with a broken or missing seal, it must be replaced or replenished without delay and a supply of spare First Aid requisites will be maintained for this purpose at certain Stations and Depots. When a First Aid Cabinet has been replenished and sealed a small coloured adhesive label must be affixed to the lid to indicate when the contents were last inspected. The labels will be forwarded annually, without requisition, to District Officers by the Stationery Department and will be used in the following sequence:—

Colour

For use during 3 months
commencing

Pale Blue
Brick
Apple Green
White

For use during 3 months
commencing
January Ist
April Ist
April Ist
October Ist

#### 7. Train First Aid Cabinets-Replenishment Cupboards.

Stations at which Replenishment Cupboards are provided.

		~			
Operating District	Station or Depot	Grade	Operating District	Station or Depot	Grade
LONDON	Didcot Henley-on-Thames Old Oak Common Oxford Paddington Reading Slough Southall West London	C C A C B B C C A	CARDIFF 3 4A31	Barry Bridgend Caerphilly Cardiff (Bute Road) Cardiff (Carriage Sdgs) Cardiff (General) Cardiff (Queen St.) Dowlais (Caeharris) Llantrisant Maerdy	OBOUADOUD
BRISTOL	Bristol (Dr. Days) Chippenham Frome Swindon Trowbridge Westbury Weston-Super-Mare	ACCBACBB	SWANSEA	Penarth Town Pontypridd Porthcawl Rhymney Tondu Treherbert	0000000
EXETER	Weymouth Yeovil (Pen Mill)  Exeter (St. Davids) Kingswear Paignton Newton Abbot Taunton	C C B C B B	SYVANSEA	Aberavon Town Carmarthen Fishguard Harbour Llanelly Neath (Carriage Sidgs) Neath (General) Neyland Pembroke Dock Swansea (High St.)	BOOBBOOA
PLYMOUTH	Bodmin Helston Kingsbridge Laira Liskeard Newquay	000000	WORCESTER	Swansea (Victoria) Whitland  Evesham Kidderminster Worcester (Shrub Hill)	ВСССВ
SLOUCESTER	Penzance Plymouth (Millbay) St. Blazey St. Ives Truro	BACCB	BIRMINGHAM	Banbury Birmingham (Snow Hill) Leamington Spa Stourbridge Jct. Stratford-upon-Avon Tyseley (Carriage Sdgs)	CBCACA
- SOCESTER	Cheltenham (St. James) Gloucester (Central) Lydney Ross-on-Wye	0000		Wolverhampton (Cannock Road) Wolverhampton (L.L.)	BA

Operating District	Station or Depot	Grade	Operating District	Station or Depot	Grade
NEWPORT	Aberbeeg Aberdare (High Level) Abergavenny Junction Ebbw Jct. (Carriage Shed) Merthyr Newport (High Street) Pontypool Road Severn Tunnel Junction Tredegar	00040000	CHESTER  CENTRAL WALES	Hereford Ruabon Shrewsbury Wellington (Salop) Wrexham Aberystwyth Brecon Oswestry Pwllheli	BCBCC CCCC

### Replenishment Cupboards-Details of Stock to be held.

Details of the stock to be held should be pasted on the inner side of the replenishment cupboard door and the stock must be maintained as near as possible to the quantities stated.

Excessive stocks must not be held.

Spare empty cabinets when required should be obtained from the nearest Carriage Repair Depot. Replenishments to be ordered on form "BR.9016."

Standard Item Number	Description of Material	Stock Unit	Stock permitted at Stations				
		No.			Grade	of St	ation
					"A"	"B"	"C'
02-204 02-207 02-211	Bandages Roller 2in. x 2 yards Bandages Triangular—Wrapped Blocks Wood			doz. each	4 72 1	2 36 1	18
02-222 02-223 02-224 02-225 02-226 02-228 02 102-237 102-238 102-245 102-250 102-250 102-253 102-255 102-259	Dressings Sterilised Compressed, Mines type, large Compressed, Mines type, medium Ordinary, Finger Ordinary, Large, wound Ordinary, Medium, wound Drops Eye, No. 1A (castor oil) Forms 5042 (Guard's Report) Lint, White ½oz. packets Lotion, Antiseptic, 2oz. bottles Pins, safety (sets of six) Sal Volatile—2oz. bottles Seals—Lead ½in. Splints—with joints Tape, Sealing Wool, Absorbent Cards of Instructions			" " " " " " " " " " " " " " " " " " "	48 24 16 16 16 8 24 2 8 8 8 150 3 4 24 8	24 12 8 8 8 4 12 1 4 4 4 100 2 3 12 4	12 6 4 4 4 2 6 1 2 2 2 2 2 50 1 2

Duties of Guards.

8. Guards in charge of trains must satisfy themselves that the seals of the First Aid Cabinets are intact and if any of the equipment is damaged or missing, or the seals broken or missing, must report the matter to the person in charge at the station at which the train terminates its journey, who must advise the Carriage Cleaning Staff that a First Aid Cabinet requires attention.

In cases where a Guard does not proceed with the train to its destination he should report any defects to the Guard who relieves him and the latter will be responsible for advising the person in charge as stated above.

Entry Guard's

Any such report must be recorded on the Guard's journal and the District Operating Superintendent or District Traffic Superintendent in whose office the journal is filed must ensure that the necessary replenishment and re-sealing is carried out.

Record of

In the event of a First Aid Cabinet having been used the Guard must make an appropriate entry on the form provided and leave the form inside the Cabinet for the information of the Carriage Cleaning Staff at the terminating station or depot.

# Pilferage from First Aid Cabinets.

Numerous losses from First Aid Cabinets in trains have been brought to notice and there is reason to believe that these occur during the night or whilst the vans are stand-

Guards when taking charge of trains should satisfy themselves that the Cabinets are in order and call attention of the Station or Yard Master to any irregularity, which should

Station and Yard Masters should report all irregularities to the District Operating Superintendent or District Traffic Superintendent and, if necessary, the B.T.C. Police.

The co-operation of the staff is desired to prevent pilferage from First Aid Cabinets.

(G.A.30 Op.-9/54 A1/7 1163/102 S2/AMB/7)

EMERGENCY APPLIANCES ETC., IN PASSENGER TRAINS-pages 121 to 127. The solution to be added on page 122 at the end of Clause 2—Fire Extinguishers. Sushers of a few design are now being supplied for use on passenger trains.

"Waterloo," which are for use in corridor stock, are of the poperate them it is necessary to remove the plunger cover, carry the strike the knob and direct the jet at the base of the fire. type is for use in kitchen cars in connection with burning liquids. To consider it should be carried to the fire, the lever at the head of the cylinder the container inverted. The foam should be allowed to fall gently on the Waterloo " and " Phomen extinguishers do not bear wire and (G.A.29.Op.-5/52. A1/71163/3.) to the sealing of fire appliances to their brackets or supports, as (G.A.29.Op. 5/52. Al/71163/3.)

123 and 127 to "green tape and lead seal" to be amended to read (G.A.29.Op.—5/52. Al/7163/3.) see 124 to be deleted and the following substituted:

BR(W) "BR(W)" September 100 (G.A.29.Op.—5/57. A1/71163/3.)

page 125 to be amended to read "wire and lead seal." (G.A.29.Op.-5/52. A1/71163/3.) To be deleted and the following substituted:depots will be supplied with a sufficient number of SOUTE . exemples and crates to meet requirements. (G.A.29.Op.-5/52. A1/71163/3.)

#### PASSENGER TRAIN INSTRUCTIONS. EMERGENCY APPLIANCES, ETC .- Continued.

TURN VALVE.

PRESS VALVE.



# No. 4. "Hand Cup" Extinguisher.

The No. 4 Hand Cup Extinguisher consists of a vessel containing two gallons of Description.

fire extinguishing liquid and a hand cup for applying it.

The outlet of the cassel is closed with a bung to which a handle is fitted for the purpose of withdrawing it.

The hand cup is inverted over the outlet of the vessel. TO OPERATE:

Tak small quantities of liquid in hand cup and DASH FORCIBLY ON SEAT OF FIRE as to user. Should the fire extinguishing liquid be exhausted before the fire is extinguished, supplies of water should be obtained, by mans of the "Hand Cup" appliance, from a tender, stream, ditch, or other available source.

Additional supplies of water.

Instructions



Hand cup for applying liquid, inverted on outlet of vessel.

Bung handle. Twist and draw up to emove. esset closed with bung.

Extinguisher sealed to support here with brown tape and lead seal marked L.C.D.

Vessel containing 2 galls, extinguishing liquid. fire



Pouring small quad-titles of liquid into hand cup regu-lating the amount by tipping the vessel as required with the thumb.



hand cup.

Dash liquid forcibly on seat of fire.

#### EMERGENCY APPLIANCES, ETC .- Continued

Seals on Extinguishers. The appliances are sealed with green tape and lead seal marked "G.W.R." "F.B.S.," and are sealed in position with brown tape and lead seal marked "G.W.R." "L.C.D."

Arrangements for Instructing Guards.

Guards will be instructed in the use of each type of extinguisher, and will have actual experience in handling the appliances.

Note.—Guards must not on any account test the appliances by operating them and drawing off small quantities of liquid from time to time, and the seals of the extinguishers should not be broken unless required for extinguishing a fire.

#### Tool Cases.

Description.

3. The Tool Cases are painted red, with the words:

#### "TOOLS.

#### FOR EMERGENCY USE ONLY."

in white lettering on the front of the case and a glass square is inset in the lid to enable the contents to be seen. The lid is taped and sealed at the top and bottom on one side, but in the event of an accident it is only necessary to cut or break the tape in order to open the lid and obtain the tools and appliances.

Contents of Case

Each Tool Case contains the following articles :-

- 1 Felling Axe.
- 1 Small Bench Axe (with hammer end).
- 2 Single-handed rough-toothed Saws.
- 2 Crowbars 3 ft. 5 in. and 2 ft. 5 in. long respectively, with one extension.
- 1 Chain 12 feet long (with hook at one end and ring at the other).
- 1 Short-handled Spade.

The First Aid Outfit is marked "G.W.B. First Aid Outfit" and contains the following requisites:—

Card of Instructions.

"First Aid in a Few Words."

Bi-Carbonate of Soda (1 tin).

Swansdown Bandage (1). Petroleum Jelly (1 tube).

Splints (1 Set).

Bandages (3 Roller, 12 in. wide).

(2 Roller, 2 ins. wide). (1 Roller, 3 ins. wide).

(3 Triangular).

Boric Lint (6 packets).

Cotton Wool (6 packets).

Cotton-wool Mops (6).

Form 5042 Guard's Report Forms (3).

Plaster (1 spool).

Safety Pins (6).

Tourniquet in Box (2).

Antiseptic Tablets (12).

Eye Loop.

Scissors.

Washing Bowl.

Tincture of Iodine (2 ozs.)

Measuring Glass (1).

In wood

block

Smelling Salts (1 bottle).

Castor Oil (1/2 oz. Bottle).

Sal Volatile (4 tubes).

Burn Dressings, No. 8 (2)

Use of Outfit.

The "First Aid" outfit is primarily intended for use in "Train Accidents," but may be utilised when occasion requires for any personal injury when Station or Depôt appliances are not available.

How Sealed

The lid of the Outfit is taped and sealed, but the tape may be broken in event of emergency as indicated above, when the contents will be obtainable.

#### Examination and Maintenance of Fire Extinguishers, Tool Cases and First Aid Outfits.

Appliances to be in proper places with seals intact.

5. The Chargeman Carriage Cleaners at the various depôts will be held responsible for seeing that the Extinguishers, Tool Cases, and "First Aid" Outfits are in their proper places with the seals intact.

## EMERGENCY APPLIANCES, ETC .- Continued.

Should an extinguisher or "First Aid" Outfit be damaged or missing, or be Replacing found with a broken or missing tape seal, it must be at once replaced, and a supply Extinguishers of spare Extinguishers and replenishments will be kept on hand for renewal purposes outfits.

#### First Aid Cabinets-Replenishment Cupboards.

Stations at which Replenishment Cupboards are provided :-

Locomotive Division.	Station.	Grade.	Locomotive Division.	Station.	Grade
opdon	Didcot Old Oak Common Oxford Paddington Station Reading Slough Soorhall West London	C A C B B C C A	Newport	Aberdare (H.L.) Aberbeeg Bridgend Cardiff Carr. Shed Cardiff Gen. Station Ebbw Jct. Carr. Shed Llantrisant Merthyr Newport (High St.)	C C B A C C C C
Bristol	Bridgwater Bristol (Dr. Day's) Chippenham Frome Trowbridge Swindon Westbury Weston-super-Mare Weymouth Yeovil	C C C C B C B B C	Neath	Pontypool Road Severn Tunnel Jet.  Carmarthen Danygraig Duffryn Yard Fisbguard Lianelly Neath Neyland Pembroke Dock Whitland Swansea (High St.)	C C C C B C C C A
ewton Abbot	Bodmin Exeter Helston Kingsbridge Kingswear Laira Liskeard Newquay Newton Abbot Penzance Plymouth (Mill B.) St. Blazey St. Ives Taunton Truro	C C C B C C C B B A C C C B B	Wolverhampt'n	Banbury Birkenhead Birmingham (Snow H.) Chester Leamington Spa Ruabon Shrewsbury Stratford-on-Avon Tyseley Wellington (Salop) Wolverhampton (L.L.) Wrexham	C B B C C C B C A C C
orcester	Cheltenham (St. J.) Gloucester Hereford Kidderminster Kington Lydney	C C B C C C	Cardiff Valleys	Barry Cardiff (Bute Road) Capdiff (Queen St.) Capdiff (Queen St.) Capdiff (Capdiff Capdiff Cap	00000
/	Ross Worcester	C B		Brecon	0 0

#### EMERGENCY APPLIANCES, ETC .- Continued.

## Replenishment Cupboards-Details of Stock to be held.

This form to be posted on the inner side of replenishment cupboard door.

Stock kept in this cupboard to be maintained as near as possible to the quantities stated below according to grade.

Excessive stocks must not be held.

Replenishments to be ordered on form " 5097."

Antiseptic Tablets, tins of 12 each	Denomination	Stock per Grade "A"	Grade "B"	Stations.
Antiseptic Tablets, tins of 12 each		Grade "A"		
Antiseptic Tablets, tins of 12 each	D /	/		C"
Antiseptic Tablets, tins of 12 each	p/		-	
	Boxes /	6	3/	2
Bandages, Roller, 1 in. by 1 yd.	Doz.	6 doz.	3 doz.	3 doz.
,, ,, 2 in. by yds	,,/	4 ,,	10	0
,, 3 in. by 6 yds	/	2 "	1 "	1 "
" Triangular	each	12 "/	6 "	6 .
" Swansdown	/ "	4/	2	1 /
Bi-carbonate of Soda, in tin	1 lb. tins	6	3	2/
Block, Wood	each	/1	1	X
Boric Lint 2 oz. packets	Doz.	2 doz.	I doz.	1 doz.
" 1/8 oz. packets	,, /	2 ,,	1 /	î
" Sintment, 1 oz. tubes	each /	12	6 "/	3 "
Boyls, Washing Kidney	,,1	2	1/	1
Boxes, Tourniquet	/	4	9	î
Card of Instructions	/	4	/2	9
Castor Oil, ½ oz. bottles	/ "	6	3	2
Corks, I.R	,,	6	3	2
Cotton Wool, ½ oz. packets	,,	24 /	12	6
Dressings, Burn, No. 8	Doz.	2/doz.	1 doz.	½ doz.
Eye loops (in envelopes)	each	12	6	3
First Aid in a few words	,,	4	2	2
Form 5042	"	24	12	6
Glasses, Measuring, graduated	,, /	4	2	1/
Iodine, 2 oz. bottles	,, /	6	3	6
Mops, Cotton Wool, box of 6	Boxes	. 6	4	2
Petroleum Jelly, in 1 oz. tubes	each .	6	3	2
Plaster, in. wide, 5 vds. op reel	Keel	6	3/	2
Safety Pins, sets of 6	Sets	12 sets	8 sets	3 sets
Sal Volatile, tea-spoon phials	each	36	51 8018	12
cissors	Pairs	4	2	1
Smelling Salts, 1 oz. bottles	each	6	4	2
Splints, sets of 5 each	Set	2 sets	1 set	1 set
l'ourniquets /	each	4	1 set	1 set
Гаре	Yds.	26	36	36
Seals, Lead	each	100	50	50
/ " "	/	100	00	00/

Duties of Guards and Conductors. 6. Guards in charge of trains must satisfy themselves that the seals of the Extinguishers, Tool Cases and "First Aid" Outlits are intact. In the case of Restaurant and Sleeping Cars the Conductors will be responsible for examining the Extinguishers.

If appliances damaged or missing, Guards to report.

A Guard or Conductor on discovering any of the appliances, &c., damaged or missing, or the seals broken or missing, must report the matter to the Carriage Examiner on duty at the station of which the train concludes its journey, and the Carriage Examiner in charge must at once advise the Chargeman Carriage Cleaner.

In cases where a Guard does not proceed with the train to its destination he should report any defects, etc., to the Guard who reviews him, and the latter will be responsible for advising the Carriage Examiner as shewn above.

(G.A.3.—12/27. L.K. 1/4712/8.)

Any such report made by a Guard or Conductor mist be recorded on his journal and the Divisional Superintendent or District Traffic Manager in whose office the journal is filed must pass an extract of the report to the Divisional Locomotive Superintendent. The latter must then report any special matters or defects in connection with fire appliances to the Chief Mechanical Engineer, Swindon

#### EMERGENCY APPLIANCES, ETC .- Continued.

In the event of the contents of a "First Aid" Outfit having been used the Use of First Aid Guard must fill up one of the printed forms provided in each Case, and leave the Outfit. form inside the Case for the information of the Chargeman Carriage Cleaner at the depôt at which the train terminates.

If an Extinguisher is damaged or the green tape seal broken or missing, it must be at once exchanged and the defective appliance returned in a crate provided to Fire Station, Locomotive Department, Swindon, the Chargeman Carriage Cleaner of the depôts mentioned to be held responsible for this duty.

Extinguishers, Tape, Seals, etc.

Replacing

Extinguishers.

Each of these depôts will be supplied with a sufficient number of Extinguishers and crates to meet the requirements, also sealing pliers, lead seals and brown tape

for scaling Extinguishers in position when an exchange is made.

On being advised that Tools are missing or damaged, the Locomotive and Carriage Superintendent will arrange for replacements to be sent from Swindon. Replacing Tools.

#### First Aid Cabinets in Trains.

Numerous losses from Ambulance Cabinets in trains have been brought to notice, and there is reason to believe that these occur during the night or whilst the vans are standing in isolated sidings. Guards should satisfy themselves that the sabinets are in order when taking charge of trains, and call the attention of the Station Master to, and report on their journals any irregularity. When material is used for a legitimate purpose an intimation of this should be placed in the Cabinet on the form pro-When material vided in each case.

Station Masters should report all irregularities to the Divisional Superintendent or District Traffic Manager and Chief of Police.

The co-operation of the staff is desired to prevent pilferages from the Cabinets.

#### GAS RINGS AND ELECTRIC HEATERS IN PASSENGER BRAKE VANS.

Guards and others concerned must see that waste paper, or other inflammable material, is not left on or near gas rings or electric heaters in passenger brake vans.

#### ELECTRICAL COMMUNICATION ON RAIL MOTOR CARS, DINING CARS, Etc.

The Chief Mechanical Engineer's Department is responsible for the maintenance of the electrical communication on all rolling stock fitted with electrical communication.

#### FAMILY, SALOON, AND INVALID CARRIAGES.—Pages 127 and 128.

These instructions are amended as follows :-

- 1. These vehicles are under the control of the Superintendent of the Line. Applications for their use are to be made through the Divisional Superintendent or District Traffic Manager and it must be stated for whose use the coach is required, the number of passengers, by what train, date of journey, and where it is to work from and to. Family carriages must not be supplied for composite parties of first and third class passengers attending race meetings, horse shows, and similar events.
- 2. When one of these vehicles is ordered to be sent away from a Depot for use at another station, it must be gassed, the tanks filled with water, and otherwise fully equipped ready for use; it must also be labelled to destination with the special label provided for the purpose, and the Guard instructed as to where it is required. Should a vehicle arrive at a station not properly equipped, the circumstance must be reported to the Divisional Superintendent or District Traffic Manager,
- 3. As soon as the journey is completed the vehicle must be sent by Passenger or Parcel train, properly labelled, as ordered; until this can be done it must be kept under cover wherever possible.
- 4. In order that the best possible use may be made of such vehicles and to prevent their being detained when returning empty, a telegram must be sent to the Divisional Superintendent or District Traffic Manager, stating the number and train by which any of these vehicles are being forwarded.
- 5. When it is necessary to send one of them to the shops for repairs, the Superintendent in whose Division the vehicle is stationed must advise the Superintendent of the Line. All concerned are requested to see that care is taken of these vehicles.
- All saloons on hand in Division or worked away to be shewn on Daily Coaching Reports (Form 6329) sent by Divisional Superintendent or District Traffic Manager to Superintendent of the Line. ken of these vehicles.

## FAMILY, SALOON AND INVALID CARRIAGES-Continued,

7. Securing Invalid Couches.

The invalid couch must be suspended from the roof with the chains provided, the spring ends of which should be attached nearest the roof. The loose adjustable springs underneath must be coupled up to the couch and floor afterwards and adjusted to suit the weight of the patient. Particular care must be taken to see that the tension springs are correctly and evenly adjusted.

There are three of these adjusting springs at each end of the couch, one to take up the longitudianal movement,

the other two to be crossed to prevent undue lateral sway.

When it is necessary to carry the couch the brass-knobbed handles immediately underneath the suspension rings may be pulled out.

The bedding of the invalid couches, when not in use, is to be carefully stored in a dry place, and kept aired.

(G.A. 1. 3/37, R6/1618.A.)

#### HORSE AND CARRIAGE TRAFFIC.

A Circular relating to the conveyance of Horse and Carriage Traffic, and the services which are available for it, is issued as necessary from time to time by the Superintendent of the Line, and the instructions contained therein must be carefully observed.

This Circular is not issued concurrently with each change of the public time tables, and copies

of each issue must be retained until the next issue is circulated,

## GENERAL INSTRUCTIONS TO BE OBSERVED IN CONNECTION WITH AUTO-CAR SERVICES.

Passengers alighting from and entering car.

Fastening Doors and Gates.

Passengers not to ride on Car Platforms, etc.

Cars with Centre Vestibules.

Signal to start to be given on Bell. 1. Guards must use every care in getting passengers into and out of the cars quickly and safely. Special care and consideration must be shewn to children and elderly or infirm passengers. Where it is necessary for passengers to enter cars at halts provided with raised platforms by the door at the engine end, the Fireman will assist by opening and closing the door. It will be arranged for passengers to enter and leave the cars by separate doors where the traffic is heavy and circumstances admit of this being done. Where necessary the Fireman of the leading car must attend to the door at the Engine end. Passengers must not be allowed to travel on the platforms at the ends of the cars, or in the centre vestibule.

If necessary, passengers may be permitted to leave Cars through the door leading to the driver's compartment, provided that it is not opened until the car has come to a standstill at the platform.

In the case of cars with centre vestibules, the door leading to the Driver's compartment must be kept locked, except at starting stations when it is found necessary

to load the cars quickly.

Doors giving access to the luggage compartment must be kept normally locked. Guards must in all cases give the signal to the Driver to start by means of the electric bell communication provided for the purpose, and not by hand signal. In case of failure of the bell communication, the Guard must use a green flag by day and green light by night, and must report to his Station Master that the bell communication has failed, the Station Master to immediately notify the Telegraph Lineman.

In the event of bell communication between the Guard and Driver not being available from any cause the Guard must ride in the vestibule portion on the Auto-Car, where the Driver's application valve is fixed, in order to be able to apply the brake in case of emergency. Before giving the Driver a signal to start the Guard must examine the brake application valve and satisfy himself that the cotter pins can easily be removed in case of necessity. The Guard must also see that the cotter pins are in proper position before he vacates the vestibule end.

Trailer Coach.

2. When an ordinary coach is attached to a car, passengers without tickets joining at halts must not be allowed to travel in the coach, provided there is room for such passengers to ride in the car, the object being to enable the Guard to issue tickets en route.

Where Guards are to ride.

3. On Services worked by cars with centre vestibules, Guards must ride in the centre vestibule.

Guards on Auto-Cars must not, IN ANY CASE, ride in the same compartment as the Driver, and, unless other accommodation is provided, they must ride in the passenger compartment of the car.

Cars run light.

4. When cars are not being run as passenger trains, it will not be necessary to provide a Guard, but they will be treated as light engines. Except where there are instructions to the contrary, when a trailer or other vehicle is attached, it will be necessary to provide a Guard or other man to act as Guard, who must ride in the rear vehicle. It will not, however, be necessary to provide a Guard for an auto engine and one trailer without passengers.

#### AUTO-CAR INSTRUCTIONS-Continued.

Passengers without tickets joining the car at halts will be booked by the Tickets. Guard after entering the car. Each ticket issued by the Guard must, before being handed to the passenger, be punched by a special punch opposite the name of the station (on the Up or Down side of the ticket, as the case may be) to which it is available. Special care must be exercised to punch the tickets correctly so that they may not be made use of beyond the proper station or halt. Ordinary card tickets (where in use) must be dated by the Guard before being handed to the passenger.

Tickets for stations must be collected from passengers by the ticket collecting Examination of staff at stations in the usual way (unless otherwise specified), but tickets issued for tickets halts where there is no staff must be collected by each Guard when passengers leave

the car.

Tickets collected by Guards must be cancelled and placed in a box provided on Collected tickets. the car. Unless instructions are given to the contrary, this box must be cleared after each journey by men appointed for the purpose.

6. Stock tickets for the use of Guards and men in charge of halts will be ordered Stock tickets. and kept by Station Masters as required. Application must be made to the Station Master for a fresh supply in good time. Car tickets must be kept apart and recorded separately in the station stock ticket register. The Guards and men in charge of Ordering tickets. halts must keep a record of all tickets supplied to them. They must be careful to see that they have a sufficient number of tickets of each series ready for use, and will only issue tickets to places at which the car is booked to stop.

7. A list of fares must be exhibited in the cars wherever tickets are issued on Fares and fare the cars. At halts where staff is employed, fare lists must be exhibited.

8. Cash collected by Guards must be paid in at the station as directed. Each Cash collected by Guard will hand in form M12 with the cash, the form to be initialled by the person Guards. to whom it is paid.

9. Guards must announce the names of stations and halts to the passengers Announcing 9. Guards must announce the names of stations and haits to the passengers in the cars in a CLEAR AND DISTINCT VOICE just before reaching the stations or stations and

halts.

When a car and trailer or trailer coach, not connected by vestibuled gangways, are coupled together, and run in charge of one Guard, he must ride on the car and announce the name of the stopping place from the platform.

10. When staff is not employed at halts, Guards will be held responsible for Where no staff at halts. dealing with passengers at those places.

 The lighting and extinguishing of lamps at halts must be carried out in Lighting and ex-tinguishing lamps accordance with instructions.

12. District Inspectors must see that the platforms at the halts are kept clean Platforms at halts by the Permanent Way staff, who will also clear snow from halt platforms and clearing helts of approaches whenever necessary. Guards must report on their journals any omission in these respects.

Clearing halts of

13. The movable steps on cars are only to be used at halts not provided with Movable steps at halts not provided raised platforms.

The steps at the engine end must not be used, neither must the doors at this form. end be used for the purpose of allowing passengers to enter or leave the cars. Care must be taken to see that these doors are kept locked.

After the car has stopped at a halt not provided with a raised platform the step Manipulation of lever must be released, the steps put into position, and securely fastened by key pin movable steps. or spring eatch (whichever is provided) and the door then opened. The door must not be opened until this has been done.

with raised plat-

Before giving the signal to the Driver to start, the Guard must close the door and then withdraw the steps, and be careful to see they are securely fastened in running

14. Guards must see that the cars are lighted as, and when, required. When there are long intervals between trips, the gas must, whenever possible, be turned down low, and turned up again in reasonable time, and the Guard will be held responsible for this.

Lighting of cars.

15. The Driver will be responsible for changing the lamp at the leading end of Lamps. the car, and the Guard for the lamp at the rear, except when an Auto engine is at the rear, in which case the tail lamp will be fixed by the fireman.

Tail lamp to be carried on stand at bottom of chimney during the time the lamp is not lighted and in the centre of buffer plank when alight.

(G.A.12. 4/43. O.M.12070.)

### AUTO-CAR INSTRUCTIONS-Continued.

Fireman to deal with Train Staff, etc., on Single Lines. 16. On Single lines the Driver must see the Electric token or staff in all cases before proceeding into the onward section, but the Fireman will be held responsible for picking up and setting down the electric token, staff or ticket, as the case may be, and in those instances where the Driver is travelling alone in the leading compartment the Fireman, each time he takes or gives up the electric token, staff or ticket, must sound the engine whistle so as to make the Driver aware that he has done so. If the Driver does not hear the whistle sounded he must give four rings on the electric bell, and immediately stop the car to ascertain what is wrong.

Where electric bell communication is not available, the Driver must give four short whistles.

Destination or Route Boards, 17. Guards must see that the proper Destination or Route Boards are exhibited and placed in the proper position on the cars.

Cleaning cars and trailers.

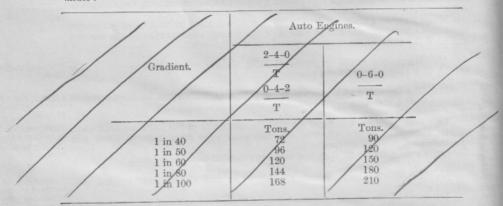
18. The cars must be well cleaned daily; this will be done by the Locomotive Department staff, but the Station Masters at the terminal stations must arrange for the cars to be swept out as often as is necessary to keep them thoroughly clean during the time they are in service. The Guards to report on their journals when this is not done.

Whenever possible the Ticket Inspectors must examine the tickets of the passengers in the cars.

Branch and Local Services worked by auto-engines.

19. Whenever it is necessary to attach additional vehicles on trips when the auto-car would be leading, the Auto-engine need not be uncoupled and placed in front, but may be allowed to propel not more than two vehicles provided the regulator is coupled through from the engine to the leading end. It may at the same time also pull vehicles which, including those propelled, will make up a tonnage as shewn below:—

The maximum loads of Auto-Cars can be computed on a tonnage basis as under:—



The vehicles attached to Auto Engines should be calculated for maximum load purposes at their tare weight, plus the usual allowances when conveying milk, fish, parcels, etc.

The following diagram will illustrate what is meant:-

ORDINARY VEHICLES (Drawn). (PROPELLED),

AutoEngine. Car. -

Driver to ride in front at \( \ldots \) when Auto-Car is propelled.

An Auto-Engine may be uncoupled for shunting purposes when necessary.

When the wheels behind the auto-engine exceed the number shown in the Table on page 109 a brake vehicle must be placed at the rear with a Guard in it.

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GENERAL INSTRUCTIONS TO BE OBSERVED IN CONNECTION WITH AUTO-CAR SERVICES.—Pages 128-131.

The following to be substituted for the table shown in paragraph 19 on page 130:-

	Auto	Engines
Gradient	2-4-0 T	2-6-2 T
	0-4-2 T	0-6-0 T
l in 40 l in 50 l in 60 l in 80 l in 100	Tons 72 96 120 144 168	Tons 90 120 150 180 210

(G.A.30 Op.-9/54 T.D./P.R.P.)

## AUTO-CAR INSTRUCTIONS-Continued.

20. It often becomes necessary for auto-engines with auto-cars attached to take Auto-engines water when the auto-car is being propelled, and sometimes when the auto-engine platform water is drawn up opposite the water column the auto-car is standing in advance of a stop cranes. signal and fouling other lines or crossings,

In such cases it is to be distinctly understood that the auto-car must not be propelled past the stop signal at danger except upon the authority of the Signalman, or the lowering of the proper signal; and immediately after water has been obtained the Driver must, on receipt of a hand signal from the Guard or other authorised person, set back with all vehicles clear of the stop signal, and, when he has done so, whistle three times as an intimation to the Signalman that the vehicles are all inside the signal unless he receives instructions to proceed on his journey and the signals are right for him to do so. When a signal is lowered for this purpose it must not be replaced at "Danger" until the Driver has whistled to signify all vehicles have been set back clear.

Signalmen must be careful not to accept trains on lines which are liable to become fouled by shunting operations of this nature until they have satisfied themselves the vehicles have been drawn to the rear of the stop signal, or have proceeded on their

21. When auto-engines with vehicles attached are working as passenger trains, the Driver must always work the regulator at the leading end. When any other movement is required to be made, and there are no passengers in the cars, it will not be necessary for the Driver to be at the leading end, but the Guard or Shunter must be in the leading end of the leading vehicle, keeping a good look-out and prepared to hand signal to the Driver. At places where the shunting operation involves several reversing movements and difficulty or delay would result if the Guard or Shunter had to change his position for each movement, or at places where a Guard or Shunter is not available, special instructions will be issued.

## INSTRUCTIONS TO BE OBSERVED BY DRIVERS AND FIREMEN IN WORKING AUTO TRAINS WHEN THE DRIVER IS DRIVING FROM THE VESTIBULE END AND THE FIREMAN REMAINS ON THE FOOTPLATE.

It is essential that there should be a proper understanding between the Driver and Fireman as to the working of the engine.

The Driver should satisfy himself that the Fireman properly understands the working of the reversing gear, vacuum brake. lubricator and sanding gear, and the management of the fire and boiler.

On receipt of a signal to start from the Guard, the Driver must sound the whistle which the Fireman must acknowledge, and the Driver must not start until the Fireman has acknowledged his signal. Before acknowledging the Driver's signal the Fireman must satisfy himself that the brake is off and that the reversing lever is in the correct position.

When approaching signals or terminal stations, the Fireman must be on the look-out and be prepared to act in case of emergency from any cause.

If a Fireman discovers any fault in the working of the engine he must inform the Driver at the first stopping place, but if the fault is such that it is necessary to stop before reaching the stopping place the Fireman must call attention of the Driver by applying the vacuum brake.

The Fireman must not leave the footplate or the engine-room without the consent of the Driver.

The following is the code of electric bell signals between the Driver and Fireman and Guard:-

.. To start. 1 ring

.. Fireman to blow brake off. 2 rings

.. To stop. 3 rings

## WORKING OF AUTO TRAINS ON BRANCH LINES, WITHOUT A GUARD.

On certain Branch Lines, where specially authorised, a Guard will not be employed on auto trains provided the number of coaches on the train does not exceed three vacuum-fitted vehicles, two of which must be fully vacuum brake fitted, and one vehicle may be piped only.

When auto trains are being worked without a Guard the following General Rules and Regulations

are modified as shewn below :-

Rules 120 and 129.—The Station Master, or other authorised person, must see that the tail lamp is in position (and burning properly when necessary) before the train leaves the starting point.

Rule 141 (b).—The signal for starting will be given by the Station Master or other authorised person.

Rule 141 (e).—In the event of the train being stopped by accident, or other exceptional cause, the Driver must satisfy himself that all is in order before again proceeding with the train.

Rule 179 (i).—In the event of an accident, or failure, the train must be considered as coming within the category of a light engine.

## Regulations for Working the Vacuum Brake.

Clause 3. - General Appendix, page 98.

The duty of ascertaining whether the brake is properly coupled up and in working order throughout the train, and between the vehicles and engine or rail motor, and of testing the brake, must be performed by the Station Master, or other authorised person.

Regulations for Working Chain Communication between Passengers and Guard and Driver by means of the Vacuum Brake.

The duties devolving on the Guard as shown in the instruction under the above heading on page 101 of the General Appendix must be carried out by the Fireman.

Conveyance of Mails, etc.

Mails, parcels and miscellaneous traffic must be locked in the luggage compartment.

## Diesel

## GA19

## WORKING OF STREAMLINED RAIL CARS.

The instructions under this heading on pages 132 to 134 to be cancelled and the following substituted:—
In connection with the running of streamlined tail cars the instructions for Auto Car Services shown on pages 128 to 131 must be observed except as varied below:

Clause 1. (Fifth paragraph.)

Guards must in all cases give the signal to the Driver to start by means of the electric bell communication provided for the purpose, and not by hand signal. In case of failure of the bell communication, the Guard must give a verbal message and must report to his Station Master that the bell communication has failed, the Station Master to notify the person who normally attends to the Bell communication on Auto trains, Dining Cars, etc., in the district. (Guard should not ride with Driver.)

Clause 4.

A Guard or other man to act as Guard must always be provided for streamlined rail cars.

## Clause 16. Exchange of Electric Train Token.

As the Driver is unable to exchange the train token whilst the car is in motion, it must be brought to a stand at the Signal Box, or other convenient point, and the Signalman must go to the car and exchange the token with the Driver.

When issuing a token for the car the loop carrier must not be used.

#### Clause 21.

When any movement with passengers in the car is required, the Driver must always be at the leading end.

When there are no passengers in the car and bell signals can be given in the rear driving compartment by the Guard or Shunter from the leading end, the shunting movement may be performed with the Driver at the rear end, but the Guard or Shunter must ride at the leading end, keep a sharp look-out and be prepared to signal to the Driver by means of the bell push provided for the purpose.

The bell codes to be used are :-

"Go ahead"									1 .
40.1 10					90.00	***	***	***	I ring.
DCL Dack	B 8 A	***	200						7 .
"Stop"			***	 					3 rings

In the event of the bell communication failing the Driver must ride at the leading end,

Streamlined rail cars must not be relied upon to operate track circuits, and Guards or Shunters will always be held responsible for carrying out Rule 55 in regard to reminding the Signalman of the presence of the car where it is stopped at signals, irrespective of whether the line is track circuited or not. The car must not be allowed to draw up to Advanced Starting signals or Starting signals in advanced positions to await acceptance from the Box in advance, but must be brought to a stand as near the Signal Box as practicable well clear of Junction points and within the protection of the Home signal as soon as possible.

The duty of going to the Signal Box must be performed by the Guard or Shunter, and to enable this

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#### Rule 55. Diesel

Streamlined and cars must not be relied upon to operate track circuits, and Guards or Shunters will always be held responsible for carrying out Rule 55 in regard to reminding the Signalman of the presence of the car where it is stopped at signals, irrespective of whether the line is track circuited or not. The car must not be allowed to draw up to Advanced Starting signals or Starting signals in advanced positions to await acceptance from the Box in advance, but must be brought to a stand as near the Signal Box as practicable well clear of Junction points and within the protection of the Home signal as soon as possible.

Clause (c).

The duty of going to the Signal Box must be performed by the Guard or Shunter, and to enable this to be done a Guard or Shunter must always accompany the car.

Use of "Vehicle on Line" Switch.

In the event of the stream and car having to stand on a platform line where a "Vehicle on Line" switch is provided, it must be protected by means of the switch, and the Station Master concerned will be responsible for seeing that this is done.

Instructions to Signalmen.

At Signal Boxes where an additional Home signal is provided, the Signalman must not acknowledge the "Is Line Clear?" signal for the streamlined rail car until permission can be given for the car to proceed to the Inner Home signal. Streamlined rail cars must not be detained at the outermost Home signal except in case of emergency.

Where Intermediate Block signals controlled from the Signal Box in rear are provided, the car must not be allowed to leave the Signal Box in rear until the "Is Line Clear?" signal has been accepted by the Signalman at the Box in advance, nor must any train be allowed to follow the car towards the Intermediate Block signal until the "Train out of Section" signal has been received for it.

## Trains Stopped by Accident, Failure or Obstruction or Other Exceptional Cause.—Rules 179 to 181.

The Guard must act in accordance with the Rule, and where necessary the protection usually performed by the Fireman must be carried out by the Driver. Clauses (g) and (h) of Rule 179 and (h) of Rule 181 will apply to streamlined rail cars.

Should it be necessary for the Guard and Driver to leave the car unattended, the Driver, before leaving the car, must stop the engines and apply both hand brakes. On falling gradients he must place the engine in reverse, and select and engage bottom gear; on rising gradients bottom forward gear must be engaged. The gear selector lever must be locked up by the Driver in the tool box. All doors giving access to the Driver's compartment must be locked before the Driver leaves the car to carry out the provisions of these Rules.

## Driving Compartments.

No one is allowed to ride with the Driver in the leading driving compartment of a streamlined car unless he is in possession of an engine pass.

No one but an official of the Company holding an engine pass or a guard in the performance of his duty is allowed to travel in the rear driving compartment.

When it is necessary for anyone to travel in this compartment, care must be taken not to interfere with any of the controls.

The following to be added after the word "run" in line two of the last paragraph of these instructions (See G.A.25):—

".....or if a Diesel Car is required to work in a service which is normally scheduled to be worked by a train or auto-car on a route over which Diesel Cars are authorised to be worked....."

(G.A.30 Op.—9/54 LKI/8847/372)

The following paragraph to be added to the end of these instructions :-

If it is absolutely necessary for a Diesel car to work over a section of line where it is not normally scheduled to runtand a printed or stencilled notice cannot be issued in sufficient time to ensure that 48 hours' notice is given to all concerned, the Drivers of such Diesel cars must be notified of the circumstances and must then sound the horns when entering and emerging from tunnels; also when approaching curves, level crossings, barrow crossings, overbridges, gangers' huts and other buildings adjacent to the line upon which the car is run.

(G.A.25—1/50. L.K.1/8847/40.)

The instructions under this heading to be amended to read—"WORKING OF DIESEL CARS."

The reference in these instructions to "Streamlined Rail Car" to read "Diesel Car" throughout. The following to be added at the end of the instructions:—

When a Diesel car is required to work over a section of line where it is not normally scheduled to run, prior advice must be issued to all concerned, including the staff of other Departments, e.g., Permanent Way men, of the intention to make such movement.

When scheduled Diesel car working has been suspended for a short period, i.e., not exceeding seven days, in connection with repairs, etc., a train or Auto service being substituted temporarily, it will not be necessary for a special advice of resumption of Diesel car working to be issued to all concerned.

When scheduled Diesel car working has been suspended for a period in excess of seven days, a notice to all concerned must be issued before Diesel car working can be resumed.

(G.A.19-10/48. L.K.1/8847/40.)

#### WORKING OF DIESEL CARS.-Page 132.

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The following to be added at the end of the second paragraph under the heading "Instructions to Signalmen.":—

"The Train Entering Section" signal for the Diesel Car must be sent when the car leaves the signal box controlling the Intermediate Block Signal.

(G.A.31-7/56 L.K.1/12510/372)

## WORKING OF STREAMINED RAIL CARS.—page 132.

The following to be inserted as the final paragraph under heading "Head and Tail Lamps":

A spare oil tail lamp must be carried on Cars Nos. 1 to 18 inclusive, and will be branded with the number of the Car to which allocated. These lamps are provided to enable the provisions of Rule 204 to be complied with, and Guards will be held responsible for satisfying themselves that the lamps are properly trimmed.

(G.A. 18. 11/47. L.K. 1/8684/40.)

## WORKING OF DIESEL CARS-Page 132.

The following additional paragraph to be inserted after the instructions headed "Use of emergency coupling-Cars I to 17 inclusive":-

Tail traffic may only be hauled by Diesel Cars numbered 18 upwards.

The maximum tail tonnage in all circumstances must not exceed 60 tons. In the case of the Twin Diesel Units formed with an intermediate coach, the maximum tail tonnage must not exceed 30 tons.

The tail load must be restricted to 30 tons where there is a rising gradient of 1 in 60 or steeper, except where the diesel car is used for Engineering Department tunnel inspections when the restriction applies on rising gradients of I in 40 or steeper.

When steam heating is in use not more than one passenger carrying vehicle may be attached to the Diesel Car or Twin Diesel Units.

(G.A.30 Op.-9/54 LKI/12009/372 T.D.288)

The following to be inserted at the end of the clause headed "Instructions to Guards":--All passenger Diesel Cars, with the exception of Diesel Car No. 1, may carry additional passengers up to a number not exceeding half the normal seating capacity.

## Exceptions:

- (a) The normal seating of Diesel Car No. I (69 passengers) must not be exceeded.
- (b) The loading of Diesel cars passing through the Severn Tunnel must not exceed the seating
- capacity. (c) The maximum permissible loads in Luggage compartments of Diesel Cars must not exceed the maximum indicated on the Notice exhibited in the Car.

(G.A.30 Op .- 9/54 T.39,119 G/I)

The following additional paragraph to be inserted on page 133:

Working over Permissive Lines.

When empty Diesel Cars are required to be worked over permissive lines the following instructions must be observed:-

Trains which are admitted to a permissive line immediately following a Diesel Car must be brought to a stand at the Signal Box in accordance with clause 9 of the Permissive Block Regulations and the Driver verbally told that a Diesel Car is in the section ahead. The Driver of a train which is so admitted must proceed with caution and must take care not to buffer up to the Diesel Car. (G.A.3.—12/37. O.M.11767.)

## WORKING OF STREAMLINED CARS-Continued.

## Failure of Car on a Single Line.—Electric Train Token Regulation 14.

In the event of a streamlined rail car becoming disabled on a section of single line between two Token Stations the Driver must take the token to the nearer token station for the purpose of obtaining assistance and as he proceeds place detonators on the rail as laid down in Rule 179; the Guard must proceed in the opposite direction, protecting his train as directed in this Rule.

## Use of Emergency Coupling, Cars 1 to 17 inclusive.

No additional vehicle can be attached to these cars.

A special coupling is carried in the above cars, for use in emergency only, by means of which the car can be pulled or propelled by an engine. Whenever the car has to be pulled or propelled the special coupling

The special emergency coupling must not be used except for the purpose of hauling or propelling the loaded car to the first available station, at which it can be shunted clear of the running line. If, however, serious delay or inconvenience to passengers would result from the car being shunted clear at the first available station which might be avoided by hauling or propelling the car to another station reasonably near, this may be done.

When the special coupling is used the streamlined rail car may be drawn by any type of engine, tender or coach and, where the line is straight, this may be done at normal speed. On sharp curves the speed must not exceed 5 m.p.h. Propelling movements must be made very carefully and must not, in any case, exceed 5 m.p.h.

## Head and Tail Lamps.

The car will not carry the standard G.W. head lamps, but the standard tail lamp will be carried.

After sunset, or during fog or falling snow, or when proceeding through a tunnel, the tail lamp must be alight, and the two white lights (horizontally at platform level) must be exhibited at the leading end as "A" headlamps. When "B" headlamps are required, one white light (at top-centre) will be used.

The switching on of headlamps after sunset, during fog or falling snow, or before passing through tunnels, must be performed by the Driver.

The Guard or man acting as Guard will be responsible for seeing that the tail lamp is placed in position and lighted when necessary.

## Instructions to Guards.

The "right-away" signal must invariably be given by means of the vestibule bell and must not be given until all doors are closed and securely fastened.

The doors next to the adjoining running line must not be used except under unavoidable circumstances, and in such cases special care must be exercised to see that no other train is approaching.

The number of passengers in and out of each station must be recorded on the Guard's journal.

## Warning of the Approach of Car.

These cars are fitted with two pairs of horns at each end, the two horns of each pair sounding notes of different tones, which are operated alternately. The warning is distinctive and not liable to be mistaken for that given by a road vehicle.

All concerned must warn men employed under their supervision who may be required to work on the permanent way or to walk upon or cross running lines of the importance of observing the warning, and that they must be prepared for the car to approach quietly and at high speed. Upon hearing the warning the Driver should be given an acknowledgment whenever possible.

It is important that men engaged on permanent way work, etc., shall move promptly to a point of safety upon sighting or receiving audible warning of the approach of the car.

## Working Over Fermissive Lines.

Trains which are admitted to a permissive line immediately following a streamlined rail car must be brought to a stand at the Signal Box in accordance with clause 9 of the Permissive Block Regulations and the Driver verbally told that a streamlined rail car is in the section ahead. The Driver of a train which is so admitted must proceed with caution and must take care not to buffer up to the streamlined rail car.

## Automatic Train Control Apparatus.

Streamlined rail cars are equipped with A.T.C. apparatus at each end. Before leaving one end to drive from the other, the Driver must put the apparatus out of use by operating the lever provided. The movement of this lever isolates the apparatus from both the battery and the brake system, puts the vacuum gauge in that vestibule out of use, and brings the "Not in use" flag into view. Upon entering the vestibule at the other end, the Driver must bring the A.T.C. apparatus into use, and until this duty has been performed, no reading will be obtainable on the vacuum gauge in that vestibule.

## Gas and Water.

Gas will be required for the buffet car and a supply of water for lavatories and buffet where provided. L.K.1/4839/41.) (G.A. 5.-2/39.

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## WORKING OF STREAMLINED CARS\_Continued.

Automatic Train Control Apparatus.

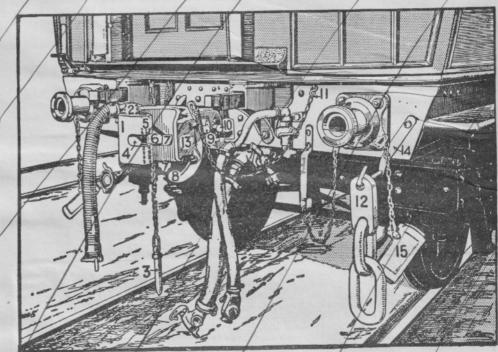
Streamlined Rail Cars are equipped with A.T.C. apparatus at each end. After a Car has been standing for a considerable hime the vacuum in the reservoir may be destroyed, thus opening both the standing for a considerable hime the vacuum in the reservoir may be destroyed, thus opening both the A.T.C. brake valves must be closed to enable the Driver A.T.C. brake valves must be closed to enable the Driver destroyed, the character to enable the Driver must ask the Chard to raise the brake restoring handle on the apparatus at the rear end of the Car until live mehes of vacuum has been created.

Gas and Water.

will be required for the Buffet Can and a supply of water for lavabories and buffet where provided.

AUTOMATIC COUPLERS.

1. The following instructions apply to vehicles fitted with automatic couplers and Pullman type gangway The automatic coupler and arxiliary fittings are shewn in the illustration below :-



- 1. Knuckle.
- 2. Knuckle Pin.
- 3. Emergency Coupling Pin.
- Slot for Emergency Coupling.
   Holes in Knuckle for Emergency Coupling Fin.
- Knuckle Tongue.
- Vertical Lock.
- Uncoupling Lever.
- 9. Coupler Support Vin.
- 10. Pivot Pin.
- 11. Uncoupling Chain.
  12. Emergency/Link Coupling.
- 13. Coupler Head.
- 14. Headstook Hook.
- 15. Buffer Saddle.

3. The rehicles are fitted with movable buffers, instead of those of the ordinary pattern. buffers must be placed in the "Shorr" position when the Automatic Coupler is in use and in the "Long" position when attaching a coach fitted with the ordinary screw couplings to Automatic Coupled stock.

Changing Position of Movable Buffers.

(a) To change the side buffers from the "Short" to the "Long" position, pull the buffers out as far as they will come and place the saddles flat on top of the buffer spindles, taking care to see that THE SADDLE ENTERS THE GROOVES OF THE BUFFER HEAD AND THAT THE LUCE OF THE SADDLE ARE PLACED TOWARDS THE BUFFER SOCKETS.

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o, pull the les, taking E LUE OF The instructions on pages 134 to 136 under headings AUTOMATIC COUPLERS and METHOD OF USING EMERGENCY COUPLINGS to be deleted and the following substituted:—

## "BUCKEYE" AUTOMATIC COUPLERS AND PULLMAN GANGWAYS.

In dealing with vehicles fitted with "Buckeye" automatic couplers and Pullman gangways, the following instructions must be observed. The illustration will provide a convenient means of reference to the various parts mentioned in the instructions.

WARNING.—When coupling together vehicles fitted with automatic couplers, or coupling vehicles so fitted to an ordinary carriage or engine, the staff employed must not in any circumstances stand between the vehicles, but must wait until they have been brought together before passing under the buffers to connect the brake pipes, etc.

On ex L. & N.E. "Buckeye" fitted stock the emergency coupling pin, emergency link coupling and buffer saddle, items 3, 12 and 15 respectively, are fixed on the headstock to prevent rattling, as shewn in the illustration, and are not allowed to hang. An emergency screw coupling is carried in each guard's compartment.

On ex S.R. standard "Buckeye" fitted stock emergency link couplings are not used and an emergency screw coupling is carried in each guard's compartment.

On B.R. standard carriages fitted with "Buckeye" couplings, emergency link couplings are not used and an emergency screw coupling is carried at one end of each coach fixed to the underframe headstock. In addition, an emergency screw coupling is carried in each guard's compartment.

## (I) Changing Positions of Movable Buffers.

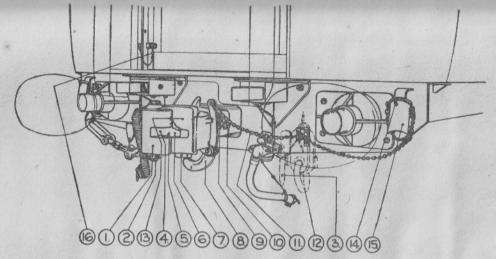
- (a) TO CHANGE THE SIDE BUFFERS FROM THE "SHORT" TO THE "LONG" POSITION, pull the buffers out as far as they will come and place the saddles flat on top of the buffer spindles, taking care to see that the saddle enters the grooves of the buffer sleeve and that the lips of the saddle are placed towards the buffer sockets.
- (b) TO CHANGE THE SIDE BUFFERS FROM THE "LONG" TO THE "SHORT" POSITION, remove the saddles and push the buffers back as far as they will go.
- (c) When not in use, the saddles must be hung on the hooks provided for them on the headstocks.

## (2) Coupling Instructions.

(a) WHEN THE VEHICLES TO BE COUPLED TOGETHER ARE BOTH FITTED WITH THE AUTOMATIC COUPLER, the procedure is as follows:—

The gangway shields must be off and the buffers in the "short" position.

Each coupler head must then be secured in the position shewn in the illustration by withdrawing the support pin, lifting the coupler head as high as it will go, and replacing the support pin. The utmost care must be taken to ensure that the TAIL-PIECE of the pin falls downwards.

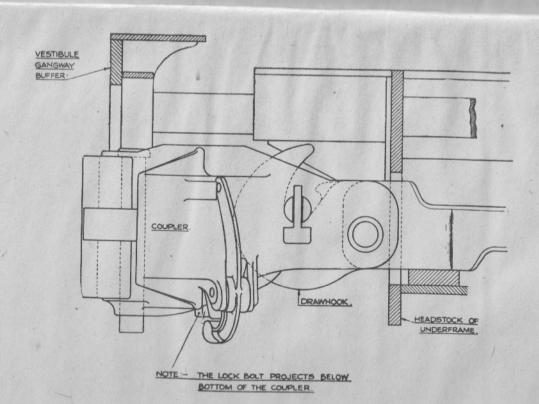


- L KNUCKLE
- 2. KNUCKLE PIN
- 3 EMERGENCY COUPLING PIN.
- 4. SLOT FOR EMERGENCY COUPLING.
- 5. HOLES IN KNUCKLE FOR EMERGENCY COUPLING PIN.
- 6. KNUCKLE TONGUE.
- 7. VERTICAL LOCK.

- 8. UNCOUPLING LEVER
- 9. COUPLER SUPPORT PIN.
- 10. PIVOT PIN.
- II. UNCOUPLING CHAIN.
- 12. EMERGENCY LINK COUPLING.

SIDE BUFFERS FROM THE

- 13. COUPLER HEAD.
- 14. HEADSTOCK HOOK.
- 15. BUFFER SADDLE.
- IG FRENCH PIN.



The knuckle of the coupler head of ONE of the vehicles must then be opened by pulling the uncoupling chain, which operates the lock.

The knuckle of the coupler of the OTHER coach should be kept closed, EXCEPT WHEN THE COUPLING IS DONE ON A CURVE, in which case it may be necessary to open the knuckle of both couplers.

Automatic couplers will NOT engage on ACUTE curves.

The stationary vehicle must have the brake hard on or be prevented otherwise from moving.

The vehicle to be attached should be brought up steadily against the stationary vehicle and pressed against it without shock, until the gangways are compressed sufficiently to allow the coupler knuckle to close and the lock to drop.

Staff must satisfy themselves that the couplers have engaged properly by looking or feeling underneath to make certain that the vertical lock on each coupler is projecting below the coupler head, and that the knuckles of the coupler are clasping each other. In addition it is necessary to make a test by a slight pull from the engine.

The brake, and, when necessary, the heating pipes, bell and lighting cables, GANGWAY CURTAINS (or wind guards), must then be connected.

(b) WHEN COUPLING A VEHICLE FITTED WITH SCREW COUPLINGS OR AN ENGINE, TO A VEHICLE FITTED WITH AUTOMATIC COUPLERS, first place the buffers of the vehicle which is fitted with the automatic couplers in the "long" position, then slightly lift the coupler head, withdraw the coupler support pin, lower the coupler head, and replace the support pin in the coupler head, taking care to see that the tail-piece of the support pin is turned properly backwards. Place the screw coupling shackle on the "Buckeye" hook and screw up in the usual way.

### (3) Uncoupling Instructions.

Before uncoupling two vehicles fitted with automatic couplers, the gangway curtains (or wind guards) and electric bell couplings inside the gangways must be disconnected; the brake and heating pipes and electric cables must also be uncoupled.

The driver should then be signalled to set back slightly, and the uncoupling chain firmly pulled and held. This will release the lock and allow the knuckle to open when the engine draws ahead.

After the coupling has been disengaged, the vehicles should be drawn a few yards/apart sufficiently clear to allow of a safe working margin.

When the necessary gap between the vehicles has been made the shunter must exhibit a hand danger signal to the driver to indicate that he wishes to proceed between the vehicles and he must obtain an acknowledgment of this hand signal working in close cooperation with the Driver before going between the vehicles.

If, in the operation of dividing, there is any easing backwards or forwards, no attempt must be made to prevent the "Buckeye" couplers from re-engaging, and the train must again be divided.

### (4) General Instructions.

- (a) The vehicles must not be fly-shunted, and they must not be bumped against each other or against buffer stops, as this causes damage to the gangways.
- (b) WHEN VEHICLES FITTED WITH AUTOMATIC COUPLERS ARE BEING SHUNTED AGAINST STOCK FITTED WITH SCREW COUPLINGS, OR ARE LEFT IN A POSITION WHERE STOCK FITTED WITH SCREW COUPLINGS COULD BE SHUNTED AGAINST THEM, THE SADDLES MUST BE ON THE BUFFERS, AND THE COUPLER HEAD IN THE "DOWN" POSITION.
- (c) WHEN A VEHICLE FITTED WITH AUTOMATIC COUPLERS BECOMES THE REAR VEHICLE OF A TRAIN, THE SADDLES MUST BE PLACED ON THE REAR BUFFERS AND THE REAR COUPLER HEAD MUST BE IN THE "DOWN" POSITION.
- (d) In cases where vehicles fitted with automatic couplers are being shunted for connecting purposes to a vehicle similarly fitted, the saddles must be off the buffers, and the coupler head in the "up" position.
- (e) In the event of a buffer sticking on a vehicle fitted with automatic couplers or the couplers failing, the carriage examiner's attention should be called immediately to the matter. All cases of difficulty must be reported to the C. & W. Engineer's Outdoor Assistant by the C. & W. Department Inspector.



## (5) Emergency Screw and Emergency Link Couplings.

An emergency screw coupling is carried in the guard's compartment of all brake vehicles stred with automatic "Buckeye" couplers, and on ex L. & N.E. vehicles an emergency link coupling is carried on the headstock at one end of all the vehicles fitted with automatic "Buckeye" couplers.

Ex S.R. standard vehicles do not carry an emergency link coupling.

All B.R. standard vehicles fitted with automatic "Buckeye" couplers carry an emergency screw coupling on one headstock in addition to the emergency screw coupling carried in the guard's compartment of brake vehicles.

When, from any cause, the "Buckeye" couplers cannot be used, the emergency SCREW coupling must be utilised whenever possible, and screwed up tightly. The gangway doors must be locked.

IN THE EVENT OF TWO VEHICLES OR VEHICLE AND ENGINE FITTED WITH THE "BUCKEYE" COUPLER BECOMING UNCOUPLED, ON NO ACCOUNT MUST ANY ATTEMPT BE MADE TO COUPLE UP AGAIN WITH THE "BUCKEYE" COUPLER.

When the emergency SCREW coupling is used, due to mishaps as detailed below, the station where it is put on the carriage must advise, by wire or telephone, the destination station where the defective vehicle will be taken out of the train, and the staff at the latter point must replace the emergency SCREW coupling in the brake compartment or on the headstock of the vehicle to which it belongs.

## METHOD OF USING EMERGENCY COUPLINGS.

Nature of Mishap.	Coaches to be joined.	Emergency Coupling to be used	Method of Coupling.
One or both couplers defective and draw-hook broken.	One ex L. & N.E. and any other coach fitted with "Buckeye" Couplers.	Ex L. & N.E. LINK Coupling (to withdraw coach with broken hook from train).	If sufficient hook is left on the vehicle with the broken draw-hook to hold the coupler in the "Up" position, leave the coupler in that position and carry out the following operations:—  EXTEND SIDE BUFFERS ON BOTH VEHICLES.  Place the oval end of the emergency link coupling on the sound draw-hook. Place the flat end of emergency link into the slot of vehicle with the broken draw-hook (the slot referred to is shewn by figure 4 on the illustration). The hole in the flat link must be in line with the hole which runs through the coupler knuckle and when the emergency coupling pin (No. 3 on illustration) is inserted through this hole the operation of joining the two vehicles is then complete LOCK THE GANGWAY DOORS ON EACH VEHICLE and, in the case of ex S.R. and B.R. Coaches use french pin (No. 16 on illustration).  If there is not sufficient hook left to support coupler head, it must hang on link coupling.
One or both couplers defective and draw-hook broken.	Two ex S.R. or B.R. Coaches or one ex S.R. and one B.R. Coach fitted with "Buckeye" Couplers.	-	Drop Couplers. Extend side buffers on both vehicles. Lock gangway doors of each vehicle, and in the case of ex S.R. and B.R. coaches use french pin (No. 16 on illustration) Withdraw from service.

## AUTOMATIC COUPLERS-Continued.

(b) To CHANGE THE SIDE BUFFERS FROM THE "Long" to THE "SHORT" POSITION. remove the saddles and push the buffers back as far as they will go.

(c) When not in use, the saddles must be hung on the hooks provided for them on the headstocks.

5. The following instructions must be observed in coupling and uncoupling by means of the Automatic Coupler:—

Coupling.—Before vehicles are brought together the Guard or Shunter performing the work must, if the line or siding at the point is straight, see that the knuckle of one of the couplers is open by pulling the incoupling chain. If the line or siding is on a curve the knuckles of both couplings must be opened. The vehicles are then to be brought slowly together, when the couplers will connect automatically. The Driver must then be instructed to make a slight forward movement in order to ensure that the couplers are properly locked, and, if so, the vacuum brake pipes (and, if necessary, steam heating pipes) must be connected, and the gangway curtains fastened in position. The Guard or Shunter performing the work must then place the sangway mat in position, and the Guard will be responsible for seeing that this is done before starting the train.

The gangways of vehicles coupled by means of Automatic Couplers are fitted with springs which keep the gangways properly compressed, and gangway clips are not used.

When coupling vehicles fitted with Automatic Couplers the Guard or Shunter performing the work must not under any circumstances stand between the vehicles, but must wait until they have been brought together and coupled before proceeding to connect the vacuum brake pipes, etc.

6. Uncoupling.—Before the Automatic Coupled vehicles are uncoupled, care must be taken to see that the Electric Cable connections are parted, also that the curtains of gangways are folded back and secured in position by means of straps provided.

After the vacuum brake and steam heating hose-pipes have been disconnected the Driver must be instructed to "ease up," when the uncoupling chain must be pulled to unlock the coupling. This operation will lift the lock, allow the knuckle to open, and permit the coaches to be separated.

7. In the event of a vehicle fitted with Automatic Couplers having to be detached for repairs the movable buffers must be placed in the "Long" position before the vehicle is moved.

After this has been done the coupler support pin must be withdrawn, when the coupler will hang down on the pivot pin to enable the ordinary hook to be used. The support pin must not be allowed to hang loosely, but be replaced in the holes in the coupler.

Assistance will be given by the Carriage and Wagon Department Staff, where available, in adjusting the buffers and couplers.

The same procedure must be carried out when attaching Automatic Coupled stock to a vehicle fitted with ordinary screw couplings.

A limited number of G.W. vehicles are fitted with special gangway levers for coupling to Pullman type gangways.

Vehicles not fitted with special gangway levers must have vestibule doors locked and blank doors fitted.

A stock of gangway adaptors is kept at Swindon.

8. Emergency Screw and Emergency Link Couplings.—Vehicles fitted with Automatic Couplers earry an emergency screw coupling for use in the event of the Automatic Couplers parting or becoming defective in any way, and in some cases an emergency link coupling is also carried. When both couplings are provided the screw coupling is carried in the brake compartment of the vehicle and the link coupling on the headstock, as shewn in the illustration. When the emergency screw coupling only is provided it is carried on the headstock.

When, from any cause, the Automatic Couplers cannot be used, the emergency Screw coupling must be utilised whenever possible and screwed up tightly to keep the vestibules together for use.

In the event of two vehicles fitted with the Automatic Coupler becoming uncoupled, on no account must any attempt be made to couple up again with the Automatic Coupler.

When the emergency SCREW or emergency LINK coupling is used, the station where it is put on the carriage must advise by wire the destination station, where the defective vehicle will be taken out of the train, and the staff at the latter point must replace the emergency coupling which has been used in its correct position on the vehicle to which it belongs.

### METHOD OF USING EMERGENCY COUPLINGS.

-			
Nature of defect.	Coaches to be joined.	Emergency Coupling to be used	Method of Coupling.
		to be used.	
. /	- / .	1 -/-	
One or both	Two coaches	Link.	If sufficient hook is left on the vehicle with the
couplers defec-	fitted with	/	broken draw-hook to hold the coupler in the
tive and draw-	Automatic	/	"up" position, leave the coupler in that
hook broken.	couplers.		position and carry out the following opera-
/	/	,	Extend side buffers on both vehicles.
		/	Place the oval end of the emergency coupling link
/		/	on the sound draw-hook. Place the flat end
/	/	/	of the emergency link into the slot of the
/	/	/	vehigle with the broken draw-hook. (The slot
/	/	/	referred to is shewn by figure 4 on the illustra-
	/	/	tion.) The hole in the flat link must be in line
		/	with the hole which runs through the coupler
/	/		knuckle (Figure 1) and when the emergency
	/	. ,	coupling pin (Figure 3) is inserted through this
/	/	/	hole the operation of joining the two vehicles
	/	/	is then complete.
/	/	/	Lock the vestibule doors on each vehicle.
/		/	If there is not sufficient hook left to support
/		/	coupler head, it must hang on link coupling.
/		/	/ / / / /
Draw-hook	One vehicle	Link.	If sufficient hook is left on Automatic Coupled
broken on	fitted with		vehicle to support coupler-head, fix latter in
Automatic	Automatic	/	"up" position. Extend side buffers on
Coupled ve-	Coupler and	/	Automatic Coupled vehicle and place the oval
hicle.	one ordinary	/	end of emergency coupling link on the draw-
	Screw/Coup-	/	hook of the ordinary vehicle.
/	ling,	/	Place the flat end of the emergency link into the
/	/	/	slot of the vehicle with the broken draw-hook.
/	-/	/	(Figure 4). The hole in the flat link must be
	/		in line with the hole which runs through the
	/		coupler knuckle (Figure 1), and when the
	/		emergency coupling pin (Figure 3) is inserted
	/		through this hole the operation of joining the
	/	/	two vehicles is then complete.
	/	/	Lock the vestibule doors on each vehicle.
	/	/	If there is not sufficient hook left to support
	/	/	coupler head it must hang on link coupling.
. /		-	1 1 1 1 1 1 1 1 1 1 1 1 1 1 1 1 1 1 1 1
One or both	Two coaches	Screw.	Drop couplers, extend side buffers on one vehicle
/couplers defec-	fitted with		only place short link of serew coupling on hook
tive,	Automatic	/	first, then long link on opposite hook, and screw
	Couplers.		up as tightly as possible.
Broken or de-	One vehicle	Screw.	Shorten side buffers on Automatic Coupled
fective Screw	fitted with	Berew.	vehicle, place short link of screw coupling on
Coupling on	Automatic		first, then long link on opposite hook, and
ordinary ve-	Coupler and	/	screw up as tightly as possible.
hicle.		/	screw up as tightily as possible.
myle.	one ordinary Screw Coup-	/	
	ling.	/	
	MIIE.		

## CENTRAL BUFFERS AND DRAWGEAR ON COACHES.

Sets of Coaches fitted with Central Buffers and Drawgear are provided for local working on certain sections of the Line.

The coaches are coupled together by a fixed coupling bar which passes through the central buffers and is secured by a coupling pin in each coach. The leading and rear ends of each set of coaches are provided with ordinary buffers and drawgear for connecting to the engine or ordinary vehicle.

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## METHOD OF USING EMERGENCY COUPLINGS—continued.

Nature of Mishap.	Coaches to be joined.	Emergency Coupling to be used.	Method of Coupling.
Draw-hook broken on "Buckeye" vehicle.	One ex L. & N.E. vehicle fitted with "Buckeye" Coupler and one with ordinary Screw Coupling.  One ex S.R. or B.R. Coach	ex L. & N.E. Link Coupling (to withdraw coach with broken hook from train).	If sufficient hook is left on "Buckeye" vehicle to support coupler-head, fix latter in "Up" position. Extend side buffers on "Buckeye" vehicle and place the oval end of emergency coupling link on the draw-hook of the ordinary vehicle.  Place the flat end of the emergency link into the slot of the vehicle with the broken draw-hook. (The slot referred to is shewn by figure 4 on the illustration). The hole in the flat link must be in line with the hole which runs through the coupler knuckle and when the emergency coupling pin (No. 3 on illustration) is inserted through this hole the operation of joining the two vehicles is then complete.  LOCK THE GANGWAY DOORS ON EACH VEHICLE and, in the case of ex S.R. and B.R. Coaches, use french pin (No. 16 on illustration).  If there is not sufficient hook left to support coupler head it must hang on link coupling.  Lock gangway doors of each vehicle and, in the case of ex S.R. and B.R.
" Buckeye " vehicle.	fitted with "Buckeye" Coupler and Coach fitted with ordinary Screw Coupling.		Coaches, use french pin (No. 16 on illustration). RETAIN SIDE BUFFERS ON "BUCK-EYE" VEHICLE, in extended position. Withdraw from service.
One or both couplers defective.	Any two Coaches fitted with "Buckeye" Couplers.	SCREW.	Drop couplers, EXTEND SIDE BUFFERS ON BOTH VEHICLES, place SHORT link of screw coupling on one hook first, then long link on opposite hook, and screw up as tightly as possible.
Broken or defective Screw Coupling on ordinary vehicle.	Any vehicle fitted with "Buckeye" Coupler and one with ordinary Screw Coupling.	SCREW.	Lock the gangway doors on both vehicles if both are gangway fitted and, in the case of ex S.R. or B.R. Coaches, use french pin (No. 16 on illustration). RETAIN SIDE BUFFERS ON "BUCKEYE" VEHICLE, in extended position. Place short link of SCREW coupling on one hook first, then long link on opposite hook, screw up as tightly as possible.

## (6) Automatic Couplers Separating.

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When the Automatic Couplers become divided in service they must be kept as far as possible in the condition in which they are found so as to assist in the discovery of any defect.

(G.A.28.Op.-4/51. LK1/9172/Gen.E.)

Special Instructions Relating to "Grave" and "Deep dene" are cancelled GF 24

See Circular O/RR in front of GF

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## SECTION II. (b).

## INSTRUCTIONS CONCERNING BOTH PASSENGER AND FREIGHT TRAINS.

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Reference to the following to appear on page 137 :-SPECIAL INSTRUCTIONS TO BE OBSERVED IN CONNECTION WITH THE WORKING OF TRAINS DESIGNATED BY THE CODE WORD "DEEPDENE." GENERAL INSTRUCTIONS TO STATION MASTERS, INSPECTORS, ENGINE DRIVERS, GUARDS, SIGNALMEN, LENGTHMEN AND OTHERS CONCERNED. I.-ENGINE HEAD LAMPS. The Engine, or where more than one is used, the leading Engine, must carry express passenger train headlamps. 2.-TRAIN TAIL LAMPS Two tail lamps must be carried on the rear vehicle of the train.

Block Regulation 19 "Train passed without Tail lamp" need not be carried out unless both lamps are missing or both lights are out when they should be burning. 3.-BLOCK SIGNALLING. The train must be signalled on the Block Instrument by a special "Is line clear?" signal of 12 bears on the bell given thus :-4-4-4

4.—TRAINS RUNNING UPON, CROSSING, OR FOULING, THE LINE ON WHICH THE TRAIN CODED "DEEPDENE" RUNS.

(a) A train running in advance of the train coded "Deepdene" on the same line of rails must not be allowed to leave or pass any place at which it can be shunted, unless there is time for the train to arrive at the next place and be shunted minutes before the coded train is expected to pass the signal box in the rear, or, in the case of a train not continuing on the same line of rails as the coded train, unless such train can pass the Junction at which it will leave the line upon which the coded train will run 5 minutes before the coded train is expected to pass the signal box in the ear.

(b) No train or vehicle must be allowed to occupy, cross or foul, the line upon which the coded train will run for 5 minutes before the coded train is expected to pass the signal box in in

(c) A train which cannot be permitted to have precedence over the coded train must not be allowed to approach the Junction from a conflicting direction until after the coded train has passed unless there are trap points or other points set for another line to prevent et the line upon which the coded train is being run being fouled, or Outer and inner Home signals are provided on the converging line in which latter case the train must be held at the Outer Home signal.

(g) On single lines Block Regulation 4 must appry and at organic list authorised, this must not be used in connection with the coded train or any train requiring \*to cross the coded train.

5.- SHUNTING OPERATIONS.

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Shunting operations on any lines or sidings next adjoining the line on which the coded train will run must be suspended for 5 minutes before the coded train is expected to pass the signal box in the rear and until it has passed.

6.-ROAD LEVEL CROSSINGS.

(a) All Occupation and Accommodation Level Crossings must be specially protected.

(b) At all Level Crossings where Crossing Keepers are stationed, the person in charge must be on duty 30 minutes before the coded train is due to pass and remain until 10 minutes

At Public Road Level Crossings where a Crossing Keeper is not regularly employed. Permanent Way staff provided with the necessary hand signals and detonators must be placed in charge 30 minutes before the train is due to pass until 10 minutes after it has passed, unless instructions are issued for the gates to be locked.

(c) Nothing must be allowed to cross the line at any Public Road Level Crossing for 10 minutes before the coded train is expected to pass and until it has passed, and at any Occupation or Accommodation Level Crossing for 30 minutes before the coded train is due to pass and until it has passed.

The District Permanent Way Inspectors will be responsible for providing the extra men required.

## 7.-SUSPENSION OF ENGINEERING WORKS.

All Engineering work on the line apon which the coded train will run must be stopped 15 minutes before the train is due to pass and until it has passed.

#### 8.-STATION MASTERS.

Station Masters at all stations on the route of the coded train must be on duty, and watch the passage of the train. They must satisfy themselves that the whole of their staff concerned thoroughly understand these instructions.

(G.A.20-1/49. R.E. Stand :- 0/RR/6072/26.)

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- (d) A train must not be run on to a diverging line, nor must a train be allowed to stand on a platform, bay, or other converging line (except at terminal stations) for the coded train to pass, unless there are trap points, or other points set for another line, to prevent the line upon which the coded train will run being fouled.
- (e) Should advice be received that the coded train is running out of course, the above-mentioned instructions as to trains not being allowed to precede it or cross the line upon which it will run must be carried out according to the time it is expected to pass.
- (f) Permission must not be given for a train following the coded train to approach on the same line of rails or on a converging line at a Junction until the "Train out of Section" signal has been received for the coded train from the signal box in advance, unless there are Facing Points which are set for another line.
- (g) On single lines Block Regulation 4 must apply and at signal boxes where Regulation 5 is authorised, this must not be used in connection with the coded train or any train requiring to cross the coded train.

Reference to the following to be made on page 139:-Deleted GF24 ENGINES FITTED WITH ELECTRIC HEADLAMPS.

Certain engines have been equipped with small electric lamps for the purpose of headlamp indication. The electric headlamps (white light) are fixed permanently at each end in all positions in which they may be required. Brackets on which oil lamps or discs can be placed for use in the circumstances described below are also provided.

The appropriate electric lamps must be illuminated under all the conditions in which ordinary

il headlamps should be lighted, as provided for in Rule 119.

In the event of the failure of individual electric lamps required for the prescribed code, they should be replaced temporarily by lighted oil lamps placed on the appropriate brackets.

When it is not necessary for the electric lamps to be illuminated, the appropriate headlamp code indication must be given by the display of discs placed on the brackets provided.

Engines fitted with electric lighting equipment must carry

(a) An oil tail amp which must be used at all times when it is necessary to carry out Rule 122.

(b) An oil headlamp to replace, when necessary, an individual electric headlamp that may have failed.

(c) A red shade to enable the oil headlamp referred to in clause (b) to be used for single line working, etc., purposes. Should it prove impossible to display the full lamp code required either by electric lamps or temporarily with the assistance of oil lamps, such measures must be taken as will enable the full code

to be displayed at the earliest possible moment. Certain engines are equipped with electric headlamps at the front end only, and in the case of these engines travelling tender first the headlamp code must be given by means of oil lamps. (G.A.23—7/49. R.E. Op. Com. Min. 29—14/10/43.) tha

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### ENGINE HEAD LAMPS.

(1) The thick type letter shown against each class of Head Lamp Indication thus:—"A, B, C," etc., is placed at the head of the columns in the Working Time Tables to indicate what head lamps that particular train is to carry.

Note.—The Trainmen will be responsible for seeing that their trains carry the proper Head Lamps

according to class.

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(2) The same system of head lamps and marks applies on relief lines as on main lines.

(3) The object of the distinguishing head lamps on the engines is to ensure prompt and sufficient advice being given to stations in advance in order that the trains may be dealt with according to their importance. Signalmen and all others concerned are therefore requested to see that trains are signalled in accordance with the head lamps, and the appropriate "Is Line Clear?" bell codes.

(4) Head lamps carried by trains assisted by an engine in front must be as under:—
For distances up to 12 miles .. One head lamp only to be carried at foot of chimney by the leading engine. Train engine to carry its usual, i.e. booked head lamps.

For distances over 12 miles ... Front engine only to carry head lamps, and those to be the head lamps applicable to the train.

(5) Except where special arrangements are in force to the contrary, other Companies' Engines working over the Great Western system must also use these head lamps, and Station Masters, Signalmen and others concerned must report to their Superintendent or District Traffic Manager any instances where this is not done.

(6) No alteration in the Standard head lamps is permitted unless specially authorised, and if it be found necessary in practice for Signalmen at congested points to know when other classes of trains are in the neighbourhood, such exceptional instances must be immediately reported to the Divisional. Superintendent or District Traffic Manager, who will arrange "Bell" signals to meet them.

(7) The head lamps will be carried on the same lamp irons day and night, the difference being that they will be extinguished by day and lighted at night and during fog or falling snow.

## DIAGRAM SHOWING CLASSIFICATION, HEAD LAMPS AND CORRESPONDING BELL SIGNALS.

Class of Train.	Head Code (White Lights)	Description of Train:	Beats on Bell.	How to be Given.
A	<u>Ö.</u>	Express passenger train, newspaper train or breakdown van train or snow plough going to clear the line or light engine going to assist disabled train Officers' Special train not requiring to stop in Section Express diesel car	4 4 8	Consecutively.  Consecutively. 4 pause 1 pause 3.
В	0	Ordinary passenger train, mixed train or breakdown van train NOT going to clear the line or loaded rail motor train; †*Branch passenger train Ordinary passenger or parcels diesel car	4 4 9	3 pause 1. 1 pause 3. 5 pause 1 pause 3.
С	5	Parcels, fish, fruit, horse, livestock, meat, milk, pigeon or perishable train composed entirely of vehicles conforming to coaching stock requirements.  Express freight, livestock, perishable or ballast train pipe fitted throughout with the automatic brake operative on not less than half of the vehicles.  Empty coaching stock train (not specially authorised to carry "A" headcode) or empty rail motor train;	5	I pause 3 pause I  3 pause I pause I  2 pause 2 pause I

# DIAGRAM SHOWING CLASSIFICATION, HEAD LAMPS AND CORRESPONDING BELL SIGNALS—continued.

Class of Train.	Head Code (White Lights)	Description of Train.	Beats on Bell.	How to be Given.
D	0	Express freight, livestock, perishable or ballast train partly fitted with the automatic brake operative on not less than one-third of the vehicles	5	Consecutively.
E	<u>Ö.</u>	Express freight, livestock, perishable or ballast train partly fitted with NOT less than four braked vehicles connected by vacuum pipe to the engine Express freight, livestock, perishable or ballast train with a limited load of vehicles NOT fitted with continuous brake	5	I pause 2 pause 2. I pause 2 pause 2.
F	<u></u>	Express freight, livestock, perishable or ballast train NOT fitted with continuous brake	5	3 pause 2.
G	0	Light engine or light engines coupled Engine with NOT more than two brake vans	5	2 pause 3. I pause I pause 3.
н		Through freight or ballast train not runing under class "C," "D," "E" or "F" headcode	5'	I pause 4.
J	٥	Mineral or empty wagon train	5	4 pause 1.
к	<u>Ö.</u>	Freight, mineral or ballast train stopping at intermediate stations †*Branch freight train Freight, ballast or Officers' Special train, requiring to stop in section	3 3 7	Consecutively. I pause 2. 2 pause 2 pause 3.

<sup>†-</sup>To be used only where authorised by the Operating Superintendent.

<sup>\*—&</sup>quot; B" Head Codes are subject to alteration in the case of through trains working over Branches, as shewn in the Working Time Tables.

<sup>‡—</sup>The term "rail motor" includes "auto-train."

Signalmen entering trains in the Train Register Book must, in addition to any information that may be necessary to identify the train, insert in the first column the proper "Is Line Clear?" bell signal. (G.A.26 Op.—5/50. R.E. Op. Com. Min. 176.)

## DIAGRAM SHOWING CLASSIFICATION, HEAD LAMPS AND CORRESPONDING BELL SIGNALS.—Page 139.

Insert § against D headcode trains and the following at the foot of page 140:

§ The proportion of vehicles on which the automatic brake must be operative to be based on the equivalent load, Class 3 traffic. (G.A.30 Op.—9/54 E.84669 H(2-C)).

Engine Head Codes.

The following to be added to the "description of Trains" carrying "E" Head Codes:-

How to be given 1 pause 2 pause 2 Beats on Bell 5 Weed killing trains when both running and spraying (G.A.30 Op .- 9/54 LKI/10661/417E).

Reference to the following to be made on page 141:-

## REVERSING OF ENGINES TO START.

Whenever it is necessary for a Driver to reverse his engine to start, and in setting back is likely to foul any other movement, another running line, or a level crossing, proper authority must be obtained before setting the train back.

In cases where, after a detachment from a train composed of coaching stock, the engine or engine with vehicles, has moved forward and come to a stand and it is subsequently necessary for the Driver to reverse his engine in order to start, he must, before setting back for that purpose, satisfy himself that the person who performed the uncoupling has moved (G.A.29.Op.-5/52. LK1/E.) clear of the vehicles.

## TIMING OF EMPTY COACHING STOCK TRAINS.—Page 141.

The following to be added at the end of the 2nd paragraph:-

"on sections of the line where the ruling gradient is less than I in 100 rising."

(G.A.30 Op.-9/54 T.38964 G/7).

Reference to the following to be made on page 141:-

## WORKING OF GAS TURBINE ENGINES.

When a gas turbine engine is required to work over a section of line where it is not normally scheduled to run, prior advice must be issued to all concerned, including the staff of other Departments, e.g., Permanent Way men, of the intention to make such movement.

When the scheduled working of a gas turbine engine has been suspended for a short period, i.e., not exceeding seven days, in connection with repairs, etc., it will not be necessary for a special advice of resumption of normal working to be issued to all concerned.

When the scheduled working of a gas turbine engine has been suspended for a period in excess of seven days, a notice to all concerned must be issued before normal working is resumed.

If it is absolutely necessary for a gas turbine engine to work over a section of line where it is not normally scheduled to run, or if a gas turbine engine is required to work in a service which is normally scheduled to be worked by a steam locomotive on a route over which the gas turbine engines are authorised to be worked and a printed or stencilled notice cannot be issued in sufficient time to ensure that 48 hours' notice is given to all concerned, the Drivers of such engine must be notified of the circumstances and must then sound the siren when entering and emerging from tunnels, also when approaching curves, level crossings, barrow crossings, overbridges, gangers' huts and other buildings adjacent to the line upon which the gas turbine engine is run.

(G.A.30 Op.-9/54 LKI/8847/372)

## Approaching Geographical Junctions and requiring to proceed through Junction.

On Main line and requiring to proceed to right On Slow or Goods line and requiring to proceed On Slow or Goods line and requiring to proceed The appropriate route code whistle to be given at	to right	1 long 2 short 2 long 1 short 2 long 2 short enumerated in the local
Appendices.	Main line	5 short

To or from Goods line or Slo	w line or	Loop an	d Main lir	ne	5 short
To cross from Main to Main				***	4 short 1 crow 1 long
T from Boy or Platform	ines		CONTRACTOR OF THE PARTY OF THE		I crow
Down Main or Fast Slow or Go	ods or Loo	p to Dov	wn Sidings	•••	2 short pause 3 short
Dawn Main or Fast Slow or (100	ods or Loop	to Ob SI	dilligs		3 short pause I short
Up Main or Fast, Slow or Go	ods or Loc	p to O	own Siding		3 short pause 2 short
Up Main or Fast, Slow or Go	or vice ver	19 10 0	3,4411 313111,		3 short pause 3 short
Up Sidings to Down Sidings Train ready to leave Sidings	of vice ver.				2 short pause I short
Shunt from Sidings to Main L	ine				2 short pause 2 short
To an from Loco					2 short
Express trains requiring fresh	engine at ne	xt stopp	oing place		3 crows

Express trains requiring fresh engine at next stopping places the fire on lineside	
Keeper's hut.	I long pause 3 short

Keeper's hut.	I long par
Engine requiring water	
The distance light angine is clear of points which require to be turned	1 31101 €
To indicate that train or light engine has been shufted creat of	1 crow 1
To indicate that train or light engine has been shunted clear of	

all running lines-(Rule 69) Before starting train assisted by engine in rear—(Rule133 clause c)

(G.A.30 Op .- 9/54 LKI/9593/E).

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The following items to be inserted on page 141:-

## PROVISION OF ENGINE CONDUCTORS.

In all cases of engine conductors being requisitioned, and particularly when required from an intermediate point, in order to prevent delay in carrying out the operation, all concerned should be notified precisely where the conductor is to be picked up.

(G.A.28.Op.-4/51. LK1/5112/30.)

#### COUPLING AND UNCOUPLING OF ENGINES OF PASSENGER TRAINS.

## --- agreed upon between the Traffic and Locomotive Departs DUTIES OF ENGINE CONDUCERS.

If a Driver, a passed Fireman acting as a Driver, or Motorman, is no thoroughly acquainted with any portion of the Line over which he has to run, he must obtain the services of a

When the Conductor is familiar with the type of engine employed, he must work the engine.

When the Conductor is not familiar with the type of engine employed, he will give n to the Train Driver the necessary instructions in regard to the signals, curves, gradients, speed restrictions, etc., applicable to the Line over which they are working, and leave the actual driving entirely in the hands of the Train Driver.

The Conductor will be responsible for the due observance of signals, speed restrictions, etc., and safe working of the train.

In every case the Train Driver must study the signals, speed restrictions, etc., for that part of the line over which he is being conducted.

The Conductor will be responsible in cases where it is necessary for the Fireman to carry out the provisions of Rule 55, for seeing that this is done.

(G.A.28.Op.-4/51. LK1/10600/20.E.) -l'an at all stations, lunctions are

## STANDARD WHISTLE CODE.

Drivers requiring Assistant Engine or Fresh Engine en route.

In order to minimise delays to trains because of partial or total engine failure, the attention of all Drivers and Signalmen is directed to the standard instructions in the Sectional Appendices which provide for the use of the following whistle code:

Express trains requiring a fresh engine at next stopping place ..... 3 crows.

The sounding of this whistle code will denote to Signalmen that another engine is required at the next station, if available there, or at the next Motive Power Depot, and

Signalmen must immediately pass forward an advice, giving title and head code of train in difficulty to the appropriate Control Office, who will decide what action is necessary and issue instructions accordingly. In cases where a fresh engine is required from a Motive Power Depot, an advice must be passed forward by the Control without delay, and, on receipt of such an advice, Motive Power Depot staff must take steps at once to provide the

(G.A.31—7/56 T.38, 113.G.1 7.38, 417.G.1 M.1953)

## ENGINE HEAD LAMPS FOR FREIGHT TRAINS WITH THROUGH LOADS.

When an ordinary or special Freight train carrying " \* head lamps makes up a full load through to destination at an intermediate point en route, the Guard should inform the Engineman of the fact, who should then alter the engine head lamps to "A" A GRZL

The person in charge at the point where the train is made up to a full load should advise the Signalman of the change in the character of the train, and also advise any places in advance where the train would call in the ordinary course to exchange traffic, so that as far as possible trains stopping intermediately may be kept clear.

## TIMING OF EMPTY COACHING STOCK TRAINS.

main line stock in normal in Standard Passenger Load for engine.

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main line stock in normal cent, in excess of Standard Passenger loads for engine.

Trains composed of 8-wheeled \ 4-6-0 engines—Express Passenger Speeds.

running condition and with- 2-6-0 and 4-4-0 engines with 5 ft. 8 in. wheels-Ordinary Passenger Speeds.

Trains composed of 8-wheeled 4-6-0 engines - Ordinary Passenger Speeds.

running condition, with 2-6-0 and 4-4-0 engines with 5 ft. 8 in. loads not more than 20 per wheels—"D' Head lamps—Freight train speeds.

Note.—The code "Pug" should be used in applying to the Locomotive Department for power, stating approximate tonnage when practicable and headlamps to be carried.

## COUPLING AND UNCOUPLING OF ENGINES OF PASSENGER TRAINS.

At certain Stations, which have been agreed upon between the Traffic and Locomotive Departments, arrangements have been made for the Firemen to perform the work of coupling and uncoupling the engines of Passenger Trains in certain circumstances. (See Local Appendices for full details.)

## ENGINES TAKING WATER AT WATER COLUMNS SITUATED AT BOXES WHICH ARE SWITCHED OUT.

When it is necessary for Engines to take water at Water Columns situated near Boxes which are switched out, Engine Drivers, before proceeding to such Water Columns, must stop at the nearest Box in the rear where a Signalman is on duty, and inform the latter that they intend to stop in the section for that purpose.

#### ENGINE WHISTLES

## ENGINE WHISTLES-Page 141.

The table and instructions under this heading to be deleted and the following substituted:-

#### STANDARD CODE OF ENGINE WHISTLES

The following code of engine whistles applies at all stations, junctions and sidings, not otherwise specially provided for in the local Code of Engine Whistles shown in the table following the

In order to avoid annoyance to passengers at stations and residents in the neighbourhood of the Railway, Drivers are requested not to make more frequent use of the engine whistles than is absolutely necessary to ensure safe and efficient working in compliance with the Rules and Regulations.

Note: The term "Slow line" includes Relief line.

Description Whistles 1 long \*Main or Fast lines \*Line next to Main line (Slow or Goods) 2 long \*Line next to Slow or Goods 3 long

(One additional long whistle to be given for each additional line farther away from the Main

\*These codes to be given when approaching signals at Danger or when necessary to indicate when ready to proceed on same line.

#### WORKING OF ENGINES IN STEAM COUPLED TOGETHER.

Not more than two engines (for "King" class engines see clause (D) below) may be run coupled together over the undermentioned Main Line routes :-

### (1) London to Penzance:-

Via Lavington, Somerton (including Athelney Loop), and Millbay.

Via Melksham and Somerton.

Via Box and Badminton (including Weston Loop) and Bridgwater. Kingswear Branch.

#### (2) London to Birkenhead :-

Via Didcot, Oxford, Worcester and Wolverhampton. Via Bicester.

#### (3) London to Fishguard :-

Via Swindon, Gloucester, and Landore.

Via Box or Badminton, Severn Tunnel, and Felin Fran.

## (4) Newport, Hereford, and Shrewbury.

Such running to be subject to the following prohibitions :-

(A) The speed in both directions between 106m. 46c. on the Eastern side of Bath Station and 107m. 23c. on the Western side of Bath Station must not exceed 30 m.p.h.

### (B) Over Royal Albert Bridge, Saltash:-

(1) The maximum speed of all engines passing over the structure is 15 m.p.h.
(2) Two engines of the "Red" classification must not run coupled.
(3) "Red class engines may only be assisted by engines of the 44-0 33XX and 34XX classes, diagrams A.23 and A.25, or of the "Yellow" and "Uncoloured "classes.
(4) Only one engine may be pulling whilst on the bridge.

## (C) Over River Wye Bridge, Chepstow :-

(1) The maximum speed of trains drawn by one engine only is 25 mp.h. (2) The maximum speed of trains drawn by two engines is 15 m.p.h. (3) Only one engine may be pulling whilst on the bridge.

## (D) 4-6-0 60XX "King" class engines :-

(1) Engines of the 60XX "King" class may not run coupled together.

(2) "King" class engines may run coupled to any engine of the tender type, except those in the "red" group, but 4-6-0 engines of the 68XX class, and 2-6-0 engines of the 83XX and 93XX types are specially authorised. Tank engines of the "yellow" group only may run coupled to a "King" class engine. Special permissions for certain routes appear in paragraph 4 hereunder.

(3) When any tender engine is run coupled to a "King" engine, they must not be coupled funnel to funnel. KEYHAM

## (D) 4-6-0 60xx "King" Class Engines.

On all routes authorised for 60xx "King" class engines (except the goods loops scheduled for use in emergency by a "King" class engine singly at a maximum speed of 5 m.p.h.) two "King" class engines may run coupled together, or may be assisted by any other Western Region engine,

(b) A "King" class engine may be assisted by any engine (other than "King") in the Down direction only, between Taunton and Whiteball. Burlescombe

(G.A. 8-5/41. C.E.-J2/46819.)

(E) CUSTOMARY RUNNING SPEEDS OF TRAINS WITH ONE ENGINE MUST NOT BE EXCEEDED BY TRAINS HAULED BY TWO ENGINES COUPLED TOGETHER.

Note.—On the remaining sections of line on the System not more than two engines in steam, of the appropriate types in the group or groups authorised to work, may be coupled together and work at customary speeds, except in those cases where special regulations are laid down to govern the working of engines coupled on the section of line concerned. These cases form the subject of local instructions and the speed limitations, where specified, must be strictly adhered to.

These instructions do not cancel those shown in the Appendix to No. 4 Section of the Service Time Tables in regard to engines with or without brake vans working through the Severn Tunnel.

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(D) 4-6-0 60xx "King" Class Engines. On all routes authorised for 60xx "King" class engines (except the goods loops scheduled for use in emergency by a "King" class engine singly at a maximum speed of 5 m.p.h.) two "King" class engines may run coupled together, or may be assisted by any other Western Region engine. subject to the following regulations :-

(I) When any tender engine is run coupled to a "King" engine, they must not be coupled chimney to chimney. (2) When a tank engine in the "red" or "blue" groups assists a "King" engine, the tank

engine must be coupled to the tender of the "King" engine. (3) Not more than one engine may be coupled to a "King" engine without special authority.

(4) The foregoing permissions are subject to the observance of the special overall speed

restrictions laid down on certain routes as applicable to the working of a "King" class

(G.A.19-10/48. C.E.-K.2/46819.) engine singly.

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## WORKING OF ENGINES IN STEAM COUPLED TOGETHER.—Page 142.

The instruction under heading (B) "Over Royal Albert Bridge, Saltash" (see Supplement G.A.30) to be cancelled and the following substituted :-

- (1) The speed of all engines passing over the structure must not exceed 15 m.p.h.
- (2) Not more than two permitted engines may work coupled together.
- (3) The instructions shewn on pages 144 and 145 of the General Appendix to the Rule Book in respect of the assisting or double heading of trains must be observed.

(G.A.31-7/56. T.D.95/41)

TIXX, DIXX, DIX Both groups of engines may also be assisted by engines of the "Yellow" and "Uncoloured"

Assisting tender engines with a leading bogie may be coupled either in front of the train engine or between the train engine and the train. In all other respects the general instructions for assisting or double heading of trains apply.

(G.A.30 Op.-9/54 TD95/41)

## (C) OVER RIVER WYE BRIDGE, CHEPSTOW.

- (1) The maximum speed of all trains, auto cars and light engines is 15 m.p.h.
- (2) Two "Red" tank engines must not run coupled together.
- (3) When a tank engine and a tender engine (both of the "Red" classification) are coupled together, the tank engine must be coupled to the tender of the other engine.

(G.A. 18. 11/47. C.E.-K2/54299.)

#### MIXED TRAINS-Continued.

All Mixed trains must stop at stations to avoid a longer run than 10 miles without stopping; but in the case of stations more than 10 miles apart it will not be necessary for a stop to be made between such stations

5. When trains are run for the conveyance of horses, cattle, or other stock, and vehicles are added for the conveyance of passengers, the vehicles containing the passengers must be placed in front of all goods vehicles, be provided with the continuous brake worked from the engine, and the train must be run subject to the conditions applying to Mixed trains. Drovers, Grooms, or other persons travelling in charge of live-stock are not, for the purpose of this instruction, regarded as passengers, but when a separate passenger vehicle is provided for their accommodation, it must be formed next to the engine and be provided with the continuous brake worked from the engine.
6. The regular trains authorised to be run as "Mixed" trains are headed "Mixed" in the Service

Time Tables.

If the goods work entails more than one shunt at any station the vehicles containing passengers must, if practicable, be placed on another line or siding and secured before the shunting is commenced. At places where towing is authorised a goods vehicle must not be towed whilst vehicles containing

passengers are attached to the engine.
8. When forming a Mixed train, if the next vehicle to the passenger coaches is fitted with a threelink coupling, that coupling should be used, but when the leading non-vacuum vehicle is fitted with a screw coupling the proper emergency screw coupling must be used in accordance with the instructions on page 145.

9. When the number of wheels on which the vacuum brake can be applied from the engine is less than one-fourth of the total wheels of the train-exclusive of van and engine brakes-Mixed trains must stop at stop boards for the purpose of applying as many wagon brakes as may be necessary in accordance with the Standard Incline Instructions shewn on page 179.

10. Tank wagons can only be conveyed by Mixed trains, subject to the instructions shewn on

page 177

11. These Regulations do not apply to troop trains.

### INSTRUCTIONS TO BE OBSERVED WHEN ARRANGING THE ASSISTING OR DOUBLE HEADING OF TRAINS.

ASSISTING OR DOUBLE HEADING.

Assisting or Double heading may be arranged only over those sections of the line and with those types of engines which have been approved by the Chief Engineer, see Main Line Route instructions re "Working of engines in steam coupled together," page 142, and Section Service Time Tables for Routes other than Main Line. Tender engines when so employed must always run chimney first, except where otherwise specially authorised.

2. PASSENGER TRAINS.

(a) Assisting from the bottom to the top of inclines.—As a general rule, any type of engine with coupled wheels not less than 4' 6" diameter may be used to assist a passenger train from the bottom to the top of an incline. Except where assisting such trains in the rear during clear weather is authorised as shewn in the Local Appendices to the Service Time tables, the assistant engine must be placed in front of the train engine and must be detached at the signal box at the summit of the incline.

(b) Assisting or Double heading on the level or on falling gradients.—Subject to Clause 1, when the points between which a passenger train requires assistance are so situated as to make it necessary for the assisted train to run some distance on the level or on a falling gradient before reaching or after having surmounted the incline; or when it is necessary for any purpose to attach a second engine to one of these trains, the following instructions must be observed:-

(i) Engines of the 4-6-0 and 4-4-0 types may be coupled in front of any engine (subject to special regulations respecting the "King" class), and, if available, an engine of one of these types should always be the leading engine.

i) If the engine to be attached is not of the same type as the train engine or of the 4-6-0. O type, it must be placed between the train engine and the train.

(iii) Except where shewn below (\*) engines of the 2-6-0 and 2-6-2 types with wheels 5' 8"

in diameter may assist in front of any engine except the 60XX "King" class between the following points only :-

Bristol to Badminton, Severn Tunnel Junction and Badminton. Pilning and Severn Tunnel Junction. Stroud to Sapperton Siding.

Newton Abbot and Brent. Totnes and Newton Abbot. Stourbridge Junction and Birmingham. Stratford-on-Avon and Earlswood Lakes.

• Millbay or North Road to Hemerdon—2-6-0 type (5' 8" wheels) or l
\* Kemble to Brimscombe—2-6-2 (5' 8" wheels) type only, (G.,

Taunton to Burlescombe (G.A. 10. 3/42. T.29,880. G/1)

(c) Assisting or Double Heading "King" Class Engines.—No engine must run coupled to a "King" (60XX) class engine, except as indicated on page 142 re" Working of Engines in Steam coupled together" and then the position of the engine must conform to clause 2 (b) Sub-section (ii) of these

In certain cases individual engines of a colour group superior to the route colouring are authorised to work, but it must be understood that such authorisation is not intended to cover these engines working coupled together or assisted by any other engine. Specific authority must be given in each individual case before such working is permitted.

(G.A.13. 9/43. E. 83395/H.)

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Clause 2-Passenger Trains (b) (ii) to be amended to read :-

(ii) In all other cases, an engine with a pony truck (i.e. 2-8-0, 2-6-0, 2-4-0, 2-8-2T, 2-8-0T, or 2-6-2T type) if available, should be the leading engine. Otherwise the more powerful engine must be placed in front.

(G.A.19-10/48... L.K.1/3/174/8.)

## INSTRUCTIONS TO BE OBSERVED WHEN ARRANGING THE ASSISTING OR DOUBLE-HEADING OF TRAINS—Page 144.

2. Passenger Trains.

The following to be added as paragraph (iv) to Clause (b):-

(iv) Engines of the 41xx and 51xx 2-6-2T types and of the 43xx 2-6-0 type may assist in front of any authorised engine between the following points:—

Par-Newquay Newquay-Par.

(G.A.30 Op.-9/54 TD.95/166).

Reference to the following to be made on page 145:-

## WORKING OF LOCOMOTIVES WITH TENDER LEADING.

Tender locomotives must not exceed a speed of 45 m.p.h. when running with the tender leading either when attached to a train or when running light. (G.A.30 Op.—9/54 LKI/E).

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(c) Vacuum fitted or partly vacuum fitted Freight trains.

Except where authorised by the Operating Superintendent and the Motive Power Superintendent vacuum fitted or partly vacuum fitted freight trains must not be double headed except for the purpose of assisting from the bottom to the top of an incline and through the Severn Tunnel.

NOTE.—For the purpose of this instruction the following classes of trains come within the category of "Freight":—

Vacuum fitted or partly vacuum fitted livestock and perishable (including fish, meat and fruit), freight, mineral, ballast, sleeper and Engineering Department trains. (G.A.26 Op.-5/50.)

## PASSENGER AND FREIGHT TRAIN INSTRUCTIONS. INSTRUCTIONS TO BE OBSERVED WHEN ARRANGING THE ASSISTING OR DOUBLE HEADING OF TRAINS-continued.

For sections of line over which passenger trains may be assisted in rear, in clear weather, see Local Appendices.

Note.—For the purpose of this instruction the following classes of trains come within the category of "Passenger":—

Passenger :Passenger (including Troop), Mail, "Mixed," Empty Coaching stock, Horse, Parcels,
Stores, Show Traffic, Pigeon, Hound, Cattle and Perishable (including Fish, Meats, Fruit and
Flowers) formed throughout with vacuum fitted stock. The store of the store o

(a) Assisting from the bottom to the top of inclines.—As a general rule any type of engine with coupled wheels not less than 4'0" diameter may be used to assist a freight train from the bottom to the top of an incline.

Particulars of the sections over which freight trains are usually assisted, and the position in which the assistant engine must be placed, are shewn in the Appendices to the Service Time Tables.

(b) Assisting or Double Heading on the level or on falling gradients.—Subject to clause 1, when the points between which a freight train requires assistance are so situated as to make it necessary for the assisted train to run some distance on the level or on falling gradient before reaching or after having surmounted the incline; or when it is necessary for any purpose to attach a second engine to one of these trains, over such gradients the assisting engine, if it is not of the same type as the train engine, or of the 4-6-0 or 4-4-0 types, must be placed between the train engine and the train.

(c) Partly Vacuum-fitted Freight trains.—Except where authorised by the Superintendent of the Line and the Chief Mechanical Engineer, partly vacuum-fitted freight trains must not be double headed except for the purpose of assisting from the bottom to the top of an incline and through the Severn

-For the purpose of this instruction the following classes of trains come within the category of "Freight":-NOTE .-

Partly vacuum filed Live-stock and Perishable (including Fish, Meat and Fruit), Freight,
Mineral, Ballast Sleeper and Engineering Department trains. The Canal State of the Service Time Tables give the only sections of line over which an assisting engine may be placed at the rear of a passenger train, and in each case when the state of the service of it is shewn whether the engine, when at the rear, is to be coupled to the train or not. When it has to be coupled the vacuum brake pipe must also be coupled up.

When the train has to be assisted at the rear it must be stopped at the appointed place mentioned in the instructions. The assisting engine must then come to the rear and, after the enginemen have exchanged signals in the manner indicated in Clause 3 on page 178 of the General Appendix to the Rule Book, the train must start from a state of rest.

On arrival at the appointed place at the top of the incline the train must stop for the assisting engine to be uncoupled, except where the assisting engine is authorised to be run uncoupled, in which case the assisting engine will cease to push at the top of the incline, and the train will run forward

During fog or falling snow passenger trains must never be assisted at the rear, but the assisting engine must be attached in front in accordance with the provisions of clause 2. Where authority is given for Freight and Mineral trains to be assisted in the rear, the assisting engine must during fog or falling snow be coupled to the rear of the train IN EVERY CASE.

ASSISTING ENGINES RETURNING LIGHT TO THE BOTTOM OF INCLINES.

Assisting engines of the 2-6-2 T. type—31XX, 51XX, and 61XX classes, after having assisted trains to the top of inclines, must not return to the bottom again at a greater speed than 45 miles per hour. All assisting engines of other types or classes must not exceed a speed of 30 miles per hour when returning. Where a lesser speed than mentioned above is ordered in the Service Time Tables, Appendices thereto, or Temporary Speed Restriction Notices, this must be complied with.

TRAINS ENTERING REFUGE SIDINGS (ENGINE WHISTLES).

When a train has been placed in a refuge or other siding for the purpose of clearing the main line, the Driver must give three short sharp whistles as soon as the whole of his train is placed clear of the throw-off point, or points leading to the dead end, as the case may be, in order to give the signalman an intimation that the engine is clear of such points, and that they may be turned so as to prevent the train from eeming out of the siding on to the running line.

### EMERGENCY SCREW COUPLINGS.

Three types of loose screw couplings are in use, as follows: No. 4. For connecting Carriage stock (2 ft. 11 in. maximum length extended).

No. 5. For connecting Carriage and Wagon stock and wagons to wagons if either or both are fitted with screw couplings. (2 ft. 7½ in. maximum length extended.)

No. 6. For connecting Wagon stock when both are fitted with 3-link couplings (2 ft. 11 in. maximum length extended).

Proper couplings must be used for their various purposes, and the screw, shackle pins, etc., of the emergency couplings must be well lubricated. Station Masters, Inspectors and Foremen must satisfy themselves that the screw couplings are in good condition and work freely before they are put into use.

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INSTRUCTIONS FOR DEALING WITH FIRES OR ACCIDENTS INVOLVING OR THREATENING EXPLOSIVES, INFLAMMABLE LIQUIDS, DANGEROUS CHEMICALS, COMBUSTIBLE MATERIAL, ETC.

When accidents or fires occur involving or threatening dangerous goods in the possession of the Railway Company, the first aim must be to prevent injury to life, and the next to prevent the damage extending to merchandise or to other property.

The steps to be taken depend upon the nature of the goods involved, and this should be, if possible, ascertained at the outset. Trucks containing inflammable liquids are labelled with the "Inflammable" label, and those containing other dangerous goods with the "Dangerous" label, as per specimens shewn below:—

GREAT WESTERN	RAILWAY. (3140 A)
INFLAMMA	BLE 5
DATE19TRAIN FROM TO	5
PLACE AS FAR AS PRACTICABLE FROM ENGINE, BRAKE-VAN AND VEHICLES LABELLED "EXPLOSIVES"	KEEP LIGHTS  AWAY FROM  THIS TRUCK.  LOAD and UNLOAD  OUTSIDE  GOODS SHEDS.
This Label to be used for INFLAI	MMABLE LIQUIDS.

GRE	AT WESTERN RAIL	WAY. (3140)
7	DANGEROUS	
SHUNT WITH CARE	DATE 19 TRAIN  FROM  TO  Via  SHEETS IN or ON Wagon. Total No.  Owner & No. of Wagon  Consignee	3

THIS LABEL TO BE USED FOR ACIDS AND ALL OTHER DANGEROUS
GOODS EXCEPT INFLAMMABLE LIQUIDS AND EXPLOSIVES.

The following to be inserted as paragraph 5 under the heading "General Procedure":-5. In the event of a fire occurring in a Yard and there being explosives in the vicinity, the Fire Brigade must be notified of the presence of explosives in the area in which the outbreak of fire has occurred. (G.A.23-7/49. L.K.1/6133/13.)

## INSTRUCTIONS FOR DEALING WITH FIRES OR ACCIDENTS INVOLVING OR THREATENING EXPLOSIVES, INFLAMMABLE LIQUIDS, DANGEROUS CHEMICALS, COMBUSTIBLE MATERIAL, ETC.—Page 147.

The following to be inserted as the third paragraph of the "Special" instructions on this page:-

In the event of a serious leakage occurring with Petrol loaded in tins or with a loaded Petrol Tank or if such loaded Petrol Tank has been overturned, the N.F.S. must be advised immediately and requested to attend and stand by until there is no further risk of fire or the tank, wagon or wagons have been removed to a place of safety. These arrangements must not prejudice the prompt clearance of the line. If there is a serious leakage in a consignment of Petrol loaded in tins, the wagon or wagons affected should not be unloaded, unless instructions to the contrary are issued, but should be removed to the nearest safe place to be isolated until such time as the leaks dry out, when the vehicle may be reforwarded.

(G.A.15-12/44 L.K.1/7564/5A)

Reference to the following to be made on page 147:-

#### DENSITY HYDROGEN INSTRUCTIONS FOR DEALING WITH HIGH PEROXIDE IN RAIL TANK WAGONS.

(1) Identification.—A longitudinal band 8 inches wide, made up of white outer bands 11 inches wide, with a light Brunswick green centre band 5 inches wide, painted on the bottom edge of the cowling (or on an approximate similar position on the tank where no cowling is fitted) at each side, and the words "Corrosive Liquid" inserted in the band in 4-inch white letters.

(2) The tanks bear one star.

(3) Each vehicle bears the "Dangerous" label.

(4) The vehicles must not be marshalled next to wagons known to contain explosives or inflammable liquids.

(5) Loose shunting, either with the tanks concerned, or with other vehicles against the tank wagons, is prohibited.

(6) Spillage or Leakage of Contents.—In the event of accident or mishap involving spillage or leakage, water must be used liberally. Ashes or ballast must not be used.

The instructions as to the method of dealing with fires or accidents involving acids and corrosive liquids, as shewn on this page, must be observed. (G.A.29.Op.—5/52. E.77314.H.)

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INSTRUCTIONS FOR DEALING WITH FIRES OR ACCIDENTS INVOLVING OR THREATENING EXPLOSIVES, ETC .- Continued.

#### GENERAL PROCEDURE.

- 1. Isolate the wagon on fire at once, and unless the fire can be effectually dealt with by buckets of water from the engine tender or by smothering by ballast or other such means, remove to the nearest source of water, such as water column or fire station.
- 2. When possible, the packages on fire should be removed from the wagon before the fire is extinguished, so as to avoid unnecessary damage to other goods by water.
- 3. After the fire has been extinguished, the wagon or packages involved must be kept under observation as a precaution in case of any further outbreak.
- 4. When, as the result of a fire, any goods are left at the side of the line, the Station Master at the nearest station must be advised immediately, and he will become responsible for their removal to his station without delay.

#### SPECIAL.

Motor spirit, petroleum and other inflammable liquids are generally lighter than water and will not mix with it; consequently such inflammable liquids will continue to burn on the surface of the water. Instead of extinguishing the fire, water may thus extend the area of the fire when motor spirit, petroleum, etc., are alight. Foam-producing extinguishers, or sand and earth are specially useful in putting out such fires.

When inflammable liquids are burning in the open, use sand or earth plentifully to prevent the flowing away of the burning liquid, especially towards watercourses.

Acids and Corrosive Liquids.—Nitric acid may itself set sawdust or straw alight. Water can be used to extinguish any such fire; but some acids, especially sulphuric acid and oleum, spit out dangerously when they come into contact with water, so that if they are involved, the water must be directed on the fire from a safe distance.

(Corrosive liquids in contact with the hands or body may cause burning. Wash the hands or parts of the body affected with plenty of water.)

Cylinders of compressed gases are liable to explode if near a fire, and there are dangers, both from the cylinders themselves and from the escaping gases. If the fire is very near such cylinders, keep everyone far away.

Celluloid fires become suddenly very fierce. Use water or chemical extinguishers promptly.

Charcoal may fire spontaneously, especially if damp, but the fires are not sudden. Unload the portion alight before putting out fire by water. Forward the portion remaining dry and undamaged Store the wet portion in open pending instructions. to destination.

Matches.—So long as the outside package remains sound it is unusual for a fire to follow the ignition of matches inside boxes. Generally any inside fire smothers itself. Do not open the case or attempt to extinguish unless the contents are continuing to burn.

Spent oxide of iron from gas works sulphur.—Such traffic, like charcoal, is liable to fire spontaneously, and fires are not sudden. The sulphurous fumes are objectionable. Water may be used, but the loads must be afterwards watched, as fires tend to recur.

Saltpetre, nitrate of soda, and other nitrates do not burn, but if they come into contact with a fire the latter becomes very fierce. Special efforts should be made to remove such materials from the zone of fire.

Cotton or wool in bales or bags. Keep the bales intact and do not open out the material. After putting out the fire keep under observation, as such fires tend to break out again.

Lime itself does not burn, but if it is insufficiently cool when loaded, or if it comes into contact with moisture, rain, or other water, it may produce so much heat as to set other goods alight. The heated portion should be removed from the truck and care should be taken to prevent water reaching the remaining lime.

DON'T USE WATER to extinguish fires involving:-

Calcium Carbide, Metallic Sodium. Metallic Potassium, Metallic Calcium,

but use sand, dry powder, or, where available, the Standard No. 5 Extinguisher.

The above suggestions as to conditions to be observed in dealing with fires, etc., are based on the chemical properties of the materials dealt with.

The fire-extinguishing appliances available at any particular locality will be ascertainable from the Fire Warden. н 2

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## INSTRUCTIONS FOR DEALING WITH VEHICLES, CONTAINING EXPLOSIVES, DERAILED OR OTHERWISE INVOLVED IN A MISHAP.

1. All vehicles containing explosives are labelled with Special Explosives labels (see specimen below):—

		SHUNT
DATE	19 TRAIN	WITH
FROM		43121-11-11
ТО		- UKEAI
VIA	COY	CARE
SHEETS IN or ON WAG	ON. Total No.	
Owner & No. of Wagon	water and the state of the stat	LUAD and UNLO

In the event of vehicles so labelled becoming derailed or breaking down in transit, every possible care must be taken in handling them.

The following precautions are the most important :-

- 2. The first precaution is to prevent fires whether from lights or sparks.
- 3. Smoking in the neighbourhood is strictly prohibited.
- 4. Matches must not be struck near the scene.
- 5. If naked lamps or lights are necessary, they should be placed on the windward or windy side of the vehicles, and far away from explosives and inflammable liquids.
- 6. Any vehicle containing Explosives, whether damaged or not, involved in a mishap, must be sent to the nearest Goods Depot or Siding, to be examined, and, if necessary, re-loaded in accordance with existing instructions before going forward.
- 7. Avoid all shocks in moving the Explosives. Packages containing Explosives must as far as practicable be passed from hand to hand and not rolled upon the ground. They must not be thrown or dropped down.
- 8. Every vehicle to which Explosives have to be transferred must first be swept quite clean. Packages of Explosives from one vehicle must not be mixed with those from another. Keep them apart both when placing on the side of the line and in re-loading into other vehicles.
- 9. Should packages of Explosives be scattered on the line, place them carefully on the side of the line and protect them with sheeting.
- 10. A responsible person must remain with the Explosives until the arrival of a competent representative to supervise their re-loading.
  - 11. All wagons into which Explosives are re-loaded must be labelled with the "Explosives" labels.
- 12. If any loose Explosive has been spilt on the line, drench it thoroughly with water and scatter earth over it.
- 13. These Instructions are taken from the Bye-Laws relating to explosives embodied in the Special Classification of Dangerous Goods.

Liskeard (Looe Branch)  The following stock not conveying passengers nover the connection from the Main Line to the Liske Branch, and on to Looe, provided a special speed 5 m.p.h. over the connection Main Line to the Looi is observed:  All types of four-wheeled vehicles.  Six-wheeled stock not exceeding 34ft, 6ins. to	et must not work ov 
and 21ft. wheelbase.	ay be worked ard and Looe restriction of Branch Line

## RESTRICTION AS TO THE WORKING OF STOCK OVER CERTAIN BRANCH LINES-page 149.

The instructions under heading of Princetown Branch to be amended to read:

Branch	RESTRICTION
Princetown Branch	Six-wheeled coaching and freight stock must not work over this Branch.

(G.A. 7.-3/40. R6/2757/A.)

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# RESTRICTION AS TO WORKING OF STOCK OVER CERTAIN BRANCH LINES.—Page 149.

The instructions on page 149 are amended as follows:-

Branch.	Restriction.
Highworth	No vehicle of greater length than 60 ft. over buffers, nor exceeding 9 ft. in width over body, 12 ft. 6 in. high in centre, and 11 ft. at sides, must be worked over this Branch.  (G.A.1. 3/37. C.E.J2/34395.).
Eastern and Western Valleys	. Add "except as between Risea Junction and Newport." (G.A.1. 3/37. C.E.J2/34395.)
Quaker's Yard H.L. and Merthyr	Coaches must not be wider than 9 ft. 3 in. over panels at waist nor exceed 63 ft. 6½ in. in length over buffers at this width, but coaches 9 ft. wide are unrestricted as to length.  (G.A. 1 3/37. R6/1543.A.)
Ross and Monmouth	Trailers 12 ft. 6 in. from rail to roof, together with 73 ft. long by 9 ft. 6 in. wide stock must not work over this Section. (G.A. 1. 3/37. R6/355.S.B.)
Bridport Branch	Passenger stock not exceeding 64 ft. 5\(\frac{3}{4}\) in, in length over buffers is permitted to work over this Branch. Passenger stock exceeding 64 ft. 5\(\frac{3}{4}\) in, in length over buffers may be worked into Bridgor 64 ft. 5\(\frac{3}{4}\) in, in length over buffers may be worked into Bridgor Station by prior arrangement with the Station Master, who will be responsible for ensuring that the adjoining road is not occupied by any stock during the period the long vehicles are at the station.  (G.A.1. 3/37. R6/1387.A.)
The following to be added:— Blackmill Branch	Coaches 9 ft. wide or over must not pass a train conveying similar stock over the curve between Tondu Middle and Tondu Ogmor Jct. Signal Boxes, nor at Brynmenyn Station.  (G.A. 1. 3/37. R6/1444.A.)

Branch.	Restriction.
Brynamman	W.R. Coaching Stock 9 feet wide, may work to Brynamman West Station, subject to service restrictions, and excluding Brynamman Loading Bank subject to service restrictions, and excluding Brynamman Loading. Such stock must not be allowed to work under the Brynamman Road Overbridge, unless specially authorised.
	(G.A.26 Op.—5/50. L.K.2/36310/Gen.)

RESTRIC	TIONS AS TO	WORKING OF STOCK OVER CERTAIN BRANCH LINES.	
Bran	CH.	RESTRICTION.	
Ealing and Shepl Railway.	herd's Bush	Electric Stock only to be worked over this line between Wood Land Station and Flyover Bridge, and over platform lines at Ealing Station.	
Veymouth Quay	Coaching S sions can wor 63ft. 6ins 64ft. 6ins W.R. sto Non-corrid	ck 66ft. 8ins. in length over buffers by 8ft. I lins. in width over body. or sets permanently close-coupled together by special type buffing must not be attached to a train for Weymouth Quay under any	rs, on ce th et. re ch
		Passenger Train vehicles of the corridor class worker Over the Tramway must have all the Gangways and Hoods properly disconnected, and the doors secured (after which the screw couplings are to be slackened out and taken off for the loose couplings to be put on) before the Train leaves Weymouth Junction.  The Station despatching a Train formed with Coaches or vans of this description, or if worked from a Foreign Line, the last Stopping Station before reaching Yeovil must were the Weymouth Station Master to arrange suit	8 2 2 2 2 2 2 2 2 2 2 2 2 2 2 2 2 2 2 2
		ABLE STAFF TO PERFORM THE WORK OF UNCOUPLING.  COACHING STOCK WITH CLOSE BUFFERS AND CONNECTIONS MUST NOT BE ATTACHED TO A TRAIN FOR WEYMOUTH QUAY UNDER ANY CIRCUMSTANCES.	
Easton		All trains must be composed of four-wheel or bogic vehicles only. In all cases the last vehicle when leaving Portland must be a brake var or brake coach. No restriction for goods vehicles, except six wheeled trucks, with a wheel base of 22 feet.  Special eight-wheel passenger coaches are provided to work on the Brane	n - /
Culm Valley  Devonport Docky	vard Lines	Ordinary eight-wheel stock for passenger use, vehicles over 60 ft. in leng and also six-wheel stock (with the exception of six-wheel milk tanks) a prohibited.  Six-wheeled wagons cannot be dealt with on the lines of H.M. Dock	th, ire
		yard at Devonport unless previous arrangements have been mad for their reception.	е
Princetown Bran Coleford	ch	Six-wheeled coaching stock must not work over this branch.  Four or eight wheeled vehicles to work over this line as a rule, but si wheelers may be run in cases of necessity.	x
Eastern and Wes Machen (B. & M		Passenger coaches 9 ft. 6 in. in width not to work over these lines of the second of the second of the second of the second overbridge situated at 4 mile 794 chains, no coaches of 60 ft. long by 9 ft. wide or over must be passed by another train of any description between Machen Sout and Machen Junction.	c c
Brynamman		Eight-wheel stock not exceeding 60 ft. over buffers by 9 ft. wide ma work over the branch and use the loop siding at Brynamma Station.	
Quakers Yard an (Joint Line). Golden Valley	d Merthyr	Coaches longer than 60 feet over buffers and 9 feet over body mus not travel between these points.  70 feet stock may lot travel over this Branch.	t
New Radnor Eardisley Presteign		Passenger Stock working over these branches must not exceed 63′6″ i length over buffers and 9′3″ in width over body.	n
Bridgort Branch		Passenger stock including rail motors and trailers—70 feet ton	g
Cricklade (M. & S	k.W. Section)	Coaching Stock 9 ft. 0 in. wide over feotboards, having a greate distance than 42 ft. 0 in. between bogic centres is prohibited from working into the Goods Shed Siding at Cricklade, and this type of stock must not be placed on the siding referred to.	n

Vehicles exceeding 64 ft. 6 in. in length over buffers and "Red Triangle" stock must not work to this Depot.

(G.A. 8.-5/41. R.6/2581/A.)

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Wood Lane Milk Depot ...

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Weymouth Quay ...

Buffers of Passenger Stock must be a minimum of lft. 6 ins. in width.

Coaching Stock (W.R. or other Regions) not exceeding the following dimensions can work to Weymouth Quay :—

63ft. 6ins. in length over buffers by 9ft. 3ins. in width over body.

64ft. 6ins. in length over buffers by 9ft. 0ins, in width over body.

W.R. stock 66ft. 8ins. in length over buffers by 8ft. 1 lins. in width over body.

Non-corridor sets permanently close-coupled together by special type buffing and drawgear must not be attached to a train for Weymouth Quay under any

circumstances.

extended buffers and 9ft. 3in. extreme width may work to Weymouth Quay but must be fitted on all occasions with gangway curtains.

Automatic coupled corridor stock not exceeding 67ft. Iin. in length over

(G.A.23-7/49. R.6/1619.A.)

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## Paddington to Penzance via Westbury, including:-

West Ealing to Greenford. Hanwell Loop. Uxbridge Vine Street Branch. Staines Branch. Windsor Branch and Slough West Curve. Maidenhead to High Wycombe. Marlow Branch. Henley Branch. Basingstoke Branch. Brentford Branch. Newbury to Didcot. Lambourn Branch. Winchester Branch. Westbury Loop. Frome Loop. Salisbury Branch. Castle Cary to Weymouth Town Station. Abbotsbury Branch. Bridport Branch (to Bridport). Yeovil to Curry Rivell Junction. Chard Branch. Minehead Branch. Barnstaple Branch'(to Barnstaple). Triverton Branch.
Stoke Canon to Morebath Junction.
Teign Valley Branch (includes Exeter Railway). Moretonhampstead Branch. Kingswear Branch. Brixham Branch. Ashburton Branch. Kingsbridge Branch. aunceston Branch. Plymouth North Road to Millbay Pier. West Cornwall Loop. Bodmin Branch (to Bodmin Town). \*Fowey Branch. Newquay Branch (to Newquay). Chacewater to Newquay. Falmouth Branch. Helston Branch.

St. Ives Branch.

\*Lostwithiel to Fowey. (Subject to speed restriction of 10 m.p.h. between 282 m.p. and 282m. 65c).

\*—Station staff to call attention to 13in. gap between centre of continuous footboard and Fowey platform.

## Paddington to Penzance via Bristol, including:-

Bristol Relief Lines.
Wallingford Branch.
Faringdon Branch.
Calne Branch.
Thingley Junction to Westbury.
Holt Junction to Patney and Chirton.
Bathampton to Holt Junction.
Bradford West Junction to Bradford South Junction.
Limpley Stoke to Camerton.
Bristol to Frome.
Portishead Branch via Parson Street Junction.
Clevedon Branch.
Yatton to Witham.
Blagdon Branch.
Weston-super-Mare Loop.
Narroways Hill Junction to Ashley Hill Junction.

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#### PASSENGER AND FREIGHT TRAIN INSTRUCTIONS.

#### RESTRICTIONS ON THE WORKING OF GREAT WESTERN WIDE STOCK-continued.

Can be worked over West London Lines to Addison Road.

Coaches with these dimensions are marked with a red triangle placed at each end of the vehicle.

of the vehicle

73 ft. long by 9 ft. wide. Whots Wd.

P

Cannot be accepted by any other Company, except that the L.M. & S. agree to it passing over the Standish Junction and Yate Section, also over the main line between Horseley Fields Junction and Dudley. The Southern Company will allow such stock to work on G.W. trains to and from Salisbury S.R. Station, but not to Southern Stations beyond Salisbury. It must not be worked over the Severn and Wye Joint Line, West Kirby Joint Line, Eastern Valleys, between Risea and the top of Western Valleys, Newport to New Tredegar and Brecon, also Machen to Caerphilly, over Cardiff Valleys Section (including Cardiff to Barry via Dinas Powis or via Penarth Town), Midland and South-Western Junction Section, over Kingswinford Branch, and on Central Wales Section may be worked only between Whitchurch and Aberystwyth and between Aberdovey, Dolgelley and Pwilheli. also between Aberthrythans Carmara acced.

63ft. 6ins. long by 9ft. 53ins. and 9ft. 7ins. wide.

Coaching Stock of these dimensions, which is distinguished by a red triangle placed at each end of the vehicle, cannot be accepted by any other Company, and can only be employed for through working on the passenger running lines between the points shewn below. The dimensions given on the solebars are 63ft. 6ins. long by 9ft. 7ins. wide on sleepers, diners and kitchen cars, while the other vehicles are shewn 63ft. 6ins. long by 9ft. 5\frac{3}{4}ins. and 9ft. 7ins. wide.

Any departure from through running on the permitted routes that may be required from time to time, e.g. other platforms at stations, sidings, etc., are subject to enquiry on each occasion and can only be dealt with by pre-arrangement.

The majority of the vehicles are formed in expresses to and from Paddington, but some are spare vehicles, and on occasions may be used in other services. It is very important that none should be formed in trains which may have to traverse sections of the line over which the vehicles are prohibited. They must not be formed in the Chester or Birkenhead portion of any train.

It is important to note that the schedule specifying the lines over which this stock is authorised to work does not, inter alia, include the following sections of the principal routes of the system:

Between Maindee Junction East and Little Mill Junction. Between Andoversford Junction and Red Posts Junction.

Tiverton Branch.

Stock 73ft. long by 9ft. wide.-Page 151.

DELETE "Standish Junction" in the second line and substitute "Abbots Wood Junction." INSERT after "Yate Section—"via Cheltenham and Gloucester South Junction only."

(G.A.30 Op.—9/54 R.6/5515).

The last sentence to be amended to read:-

"......and on the Central Wales Section may be worked only between Whitchurch and Aberystwyth, Aberystwyth and Carmarthen, Dovey Junction and Pwllheli (speed restriction of 25 m.p.h. through tunnels Aberdovey to Dovey Junction).

(G.A.30 Op.-9/54 R.17/355W.).

Stock 66ft. 8in. long by 8ft. Ilin. wide.-Page 151.

The entry under heading "L.M.S. Railway Lines prohibited" in respect of Swansea Victoria Station to be deleted.

(G.A.30 Op.-9/54 R6/-)

Stock 63ft. 6in. long by 9ft. 53in. and 9ft. 7in. wide.-Page 151.

The entry under heading "Central Wales" in respect of Pwllheli to Dovey Junction to be amended to read:—

Pwllheli to Dovey Junction (Speed restricted to 25 m.p.h. through tunnels Aberdovey to Dovey Junction).

Holt to Patney. Bathampton to Holt. (G.A.30 Op.—9/54 R.17/355W).

Yatton to Witnam,
Blagdon Branch,
Weston-super-Mare,
Ashley Hill Let, and Avonmouth Poeks Sto

Weston-super-Mare.
Ashley Hill Jct. and Avonmouth Docks Station
on Joint Line.

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RESTRICTIONS ON THE WORKING OF GREAT WESTERN WIDE STOCK-Continued.

Paddington to Fishguard, Neyland and Milford Haven. AND PEMBROKE DOCK Winterhourne to Bristol and Avonmouth Dock. Stapleton Road to Ashley Hill Junction. Malmesbury Branch. Cardiff (General) to Bridgend via Dinas Powis Bristol Loop. and Barry. Patchway to Filton. Filton West Loop. Pilning Junction to Hallen Marsh Junction.

† Trains composed of or containing any of the above mentioned stock must not be passed by trains on the adjoining line between the Boxes set out below and can only be dealt with by prearrangement :

Cardiff (General) to Llancaiach via Ystrad Mynach and Cardiff East Branch. When travelling in either direction between Cardiff General and Queen Street Stations.

Llandough Lower and Cogan Sidings. Barry Junction and Barry Sidings. Llantwit Major and Llandow. Scutherndown Road in Up Middle and Up Platform Lines. Duchy Quarry and Ewenny Quarry. Cowbridge Road Junction and Bridgend East,

Paddington to Oxley Sidings, via Bicester or Oxford, Uxbridge (High Street).

Oxford to Princes Risborough. Fairford Branch. GA13 Oxford to Churchdown via Kingham. Kingham to King's Sutton.

Kingham to Høneybourne. Honeybourne to Churchdown. Gloster F13 Honeybourne to Worcester. Honeyhourne to Hatton.

Honeybourne to Tyseley. Alcester Branch. Worcester to Leominster,

Worcester to Hartlebury. Must not

Hartlebury to Handsworth. Stourbridge Junction to Priestfield. Worcester to Hereford.

Abingdon Branch.

Woodstock Branch.

Swindon to Gloucester. Kemble to Circnester Town. Gloucester to Severn Tunnel Junction. Gloucester to Hereford. Chepstow to Monmouth. Ross to Little Mill Junction.

Little Mill Junction to Leominster.

over + M.5 Coys Maintain once
Churchdown and Gloster between GAIZ The following is the position in regard to the acceptance of this Stock by other

DUFFRYN ISAF

63 ft. 6½ ins. long by 9 ft. 3 ins. wide.

Companies :-Southern Company-Will not accept. L. & N.E. Company—Will not accept.
L.M. & S. Company—Will accept on following Sections:—

loads

Western Section. Shrewsbury and Crewe.

Crewe and Holyhead. Crewe and Liverpool (Lime Street).

Crewe and Manchester (London Road). Chester and Liverpool via Halton Junction. Chester and Manchester (Exchange) via Warrington.

Crewe and Glasgow (Central).

Crewe and Edinburgh (Princes Street). Crewe and Blackpool (Central).

Crewe and Blackpool (Talbot Road Station only, not Excursion Platform). 9 ft. 3 ins. wide Stock may work over Sirhowy Line via Nine Mile Point.

Midland Section. Standish Junction and Yate Junction. Bristol and Birmingham.

Bristol and Bradford. Gloucester and Birmingham.

Cheltenham (Lansdown) and Birmingham.

Such stock is also prohibited from working on the following Sections and Branches:

Highworth Branch. M. & S.W Junction Railway Section.

Culm Valley Branch. Bassaleg to Barry Junction (B. and M.).

Machen to Caerphilly. Vale of Glamorgan.

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Wre Lam RESTRICTIONS ON WORKING OF WESTERN REGION WIDE STOCK-page 151. 66 ft. 8 in. long by 8 ft. II in. wide

Paddington to Fishguard (via Landore and or Swansea High St.), Neyland, Milford Haven and Pembroke Dock, including:-

Swindon to Gloucester via Kemble. Kemble to Cirencester Town. Gloucester to Severn Tunnel Junction.

Gloucester to Hereford,

Newent Branch (Over Junction to Ledbury).

Chepstow to Monmouth. Ross to Little Mill Junction.

Little Mill Junction to Shrewsbury.

Winterbourne to Bristol.

Winterbourne to Avonmouth Dock Station (Joint) via Hallen Marsh Junction.

Stapleton Road to Ashley Hill Junction.

Malmesbury Branch.

Bristol Loop

Patchway to Filton. Filton West Loop.

Pilning Junction to Hallen Marsh Junction.

Swansea District Lines. Court Sart Junction to Llanelly via Skewen East Junction, and Lonlas Junction (also via Neath Junction R. & S.B. and Neath Loop, Lonlas Junction). Morlais Junction South and Llandilo Junction.

Morriston Branch. Felin Fran West to Hafod Junction.

Trains composed of or containing any of the above stock must not pass nor be passed by any stock on the adjoining line between the boxes set out below in either direction and can only be dealt with by prearrangements-

Section Cardiff General to Bridgend via Dinas Powis and Barry

Cardiff West and Penarth Curve South (inclusive). Llandough Lower and Cogan Sidings. Biglis Junction and Gas Works Junction at Barry Docks. Barry Junction and Barry Sidings. Llantwit Major and Llandow Southerndown Road in Up Middle and Up platform lines.

Boxes

Duchy Quarry and Ewenny Quarry. Cowbridge Road Junction and Bridgend East (inclusive).

Cardiff General to Hengoed H.L. or Llancaiach via Cardiff East Junction Branch and Ystrad Mynach

Cardiff East to Cardiff Queen Street South. Caerphilly East-over double junctions Main to Relief Lines.

Caerphilly West-over double junctions of Relief lines with Main Lines.

Paddington to Saltney Junction via Bicester or Oxford and Aynho Junction, including:

Uxbridge High Street Line.

Abingdon Branch.

Oxford to Princes Risborough. Woodstock Branch.

Fairford Branch.

Oxford to Gloucester via Kingham.

Kingham to Kings Sutton.

Kingham to Honeybourne.

Honeybourne to Gloucester and Cheltenham St. James.

Honeybourne to Worcester.

Honeybourne to Hatton.

Honeybourne to Tyseley.

Alcester Branch.

Worcester to Leominster. Worcester to Hartlebury.

Hartlebury to Handsworth. Stourbridge Junction to Priestfield.

Worcester to Hereford.

Shrewsbury to Buttington.

Ruabon to Barmouth Junction. Must not pass out-of-gauge loads over L.M.S. Company's Maintenance between Churchdown and

Gloucester. Central Wales.

Moat Lane to Brecon and Dolygaer. Whitchurch to Aberystwyth.

Oswestry to Gobowen.

Aberystwyth to Bryn Teify.

Pwllheli to Dovey Junction. (Speed restricted to 5 m.p.h. through tunnels Aberdovey to Dovey Junction.)

Wrexham to Ellesmere. Lampeter to Aberayron.

(G.A.16. 5/46, R.6/4073A.)

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### RESTRICTIONS ON THE WORKING OF GREAT WESTERN WIDE STOCK .- page 151.

Reference to the following to be inserted on page 151:

66 ft. 8 in. long by 8 ft. 11 in. wide.

#### Lines prohibited:

Ealing and Shepherds Bush.

Hammersmith and City Line.

Highworth Branch.

M. & S.W. Junction Line throughout.

Culm Valley Branch (Tiverton Junction to Hemyock).

Princetown Branch (Yelverton to Princetown).

Liskeard and Looe Line.

Ex Brecon and Merthyr (Bassaleg Junction to Duffryn Isaf).

Caerphilly Branch (Machen Junction to Caerphilly (exclusive).)

Burry Port and Gwendraeth Valley Railway and Branches.

Llanelly Up Bay Platform Line.

Brynamman Branch.

Severn and Wye Joint Line throughout.

GA 23

Golden Valley Line (Pontrilas to Hay).

Gloucester Docks-Prohibited with the exception of the following sidings:

Nos. 3 and 4 (Coal sidings).

No. 5.

No. 6 (Marshalling sidings).

No. 7.

Shipston-on-Stour Branch.

Pontcysyllte Branch (Rhos to Trevor).

Wrexham and Minera (Brymbo to Coed Poeth).

Cleobury Mortimer and Ditton Priors Light Railway.

South Wales Docks Lines.

Must

### Lines on which restrictions are necessary.

Weymouth Tramway ... Speed to be restricted to 4 m.p.h. and screw couplings to be slackened 3 or 4 threads.

Lostwithiel and Fowey

Speed to be reduced to 10 m.p.h. between 282 M.P. and 282 65c. Fowey

Passing Restrictions.—Must not pass, nor be passed by, any Passenger Train, or Train conveying passenger stock, or any out-of-gauge load on the opposite (or adjacent) running line (either direction) between the following Box-to-Box Sections:

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## RESTRICTIONS ON THE WORKING OF WESTERN REGION WIDE STOCK —page 151.

66 ft. 8 ins. long by 8 ft. 11 ins. wide.

Delete the following entry from the list of lines prohibited:—

M. & S.W. Junction line throughout.
The following new entry to be inserted:—

This stock may work over the M. & S.W. Junction line subject to the undermentioned stipulations:

Prohibition.

Cricklade—Goods Shed Siding (in accordance with the entry on page 149).

Passing Restrictions.

Not to pass nor be passed by similar passenger stock and/or freight trains on

adjoining lines at the following points:—
Swindon Town, between Boxes "A" and "B".
Chiseldon, except in loops at Marlborough end of station.

Marlborough, within platforms.

Collingbourne, within platforms. Ludgershall, within platforms.

(G.A.27.Op.—1/51. R.6/3926.)

# Passing Restrictions. The last paragraph of these Restrictions dealing with the Newport Engineering District as listed in G.A.26 Op. to be deleted and the following inserted:—

These coaches are prohibited from passing other coaching stock and out-of-gauge loads between the following points in the Newport Engineering District:—

Maesycwmmer Station (inclusive) to Pengam Station.

The existing restrictions so far as other Regions are concerned will continue to apply as already shown in G.A.18.

63 ft. 6½ ins. long by 9 ft. 3 ins. wide.—Page 152.

Delete the following from the list of lines over which this stock is prohibited:—

Vale of Glamorgan.

(G.A.31-7/56 R.6/3926)

Crumlin H.L. inclusive and Penar Junction. Hengoed H.L. and Penalitau Junction.

Nelson & Llancaiach and Quaker's Yard H.L.

Cresselley Crossing and Middle Duffryn.

Must not pass, nor be passed by, any Passenger Train or Train conveying passenger stock, or any out-of-gauge load on the opposite (or adjacent) running line (either direction) between the following Box-to-Box sections :-

From	То	То		
Pontymister South Aberbeeg Junction Risca Junction (inclusive) Rock Vein North Pontnewynydd Junction Hengoed H.L. Long Dyke Junction Cardiff East Cardiff West	Risca Junction  Aberbeeg North Brickworks South Nine Mile Point Abersychan L.L. Ystrad Mynach North Tyndall Street Crossing Clarence Road Cardiff (Queen Street) South Penarth Curve South		Mains. Mains and Reliefs. Mains.  "" "" ""	

ese coaches are prohibited from passing other coaching stock and out-of-gauge loads between owing points in the Cardiff Valleys Engineering Division :-

gan Junction to Cogan Sidings.

Junction to Cadoxton, Nos. 1 and 2 lines (inclusive).

rry Dock (exclusive) to Gas Works Junction. rry to Barry Island.

waker's Yard L.L. Station (inclusive) to Black Lion. rw Branch Junction to Trehafod Junction.

vieillon Lower to Rhondda Cutting.

puble Line portion at Maerdy.

ixon's Crossing to Mountain Ash. C. and N. Junction to Groeswen.

strad Mynach North to Hengoed L.L.

aesycwmmer Station (inclusive) to Pengam Station. ver junction curves at Treforest Junction.

adyr Junction Box (inclusive) to Radyr Station (inclusive).

he existing restrictions so far as other Regions are concerned will continue to apply as already in G.A.18. (G.A.26 Op.—5/50. R.6/3926.A.)

## RESTRICTIONS ON WORKING OF WESTERN REGION WIDE STOCK-page 151. 66 ft. 8 in. long by 8 ft. II in. wide.

The restrictions which apply, so far as the Western Region is concerned, have been revised, and the following should be substituted for those set out in G.A.18.

Lines Prohibited :---

Ealing and Shepherds Bush. Hammersmith and City Line.

Highworth Branch.

S.W. Junction Line throughour.

Culm Valley Branch (Tiverton Junction to Hemyock). Princetown Branch (Yelverton to Princetown).

Liskeard and Looe Line.

South Wales Docks Lines.

Ex Brecon and Merthyr (Bassaleg Junction to Duffryn Isaf).

Caerphilly Branch (Machen Junction to Caerphilly (exclusive) ). Dowlais Junction and Dowlais Cae Harris (Taff Bargoed Branch .

Burry Port and Gwendraeth Valley Railway and Branches.

Llanelly Up Bay Platform Line.

Golden Valley Line (Pontrilas to Hay).

Gloucester Docks. Prohibited with the exception of the following sidings :-

Nos. 3 and 4 (Coal Sidings).

No. 5 No. 6. (Marshalling Sidings).

No. 7. Shipston-on-Stour Branch.

Pontcysyllte Branch (Rhos to Trevor).

Wrexham and Minera (Brymbo to Coed Poeth). Cleobury Mortimer and Ditton Priors Light Railway.

Lines on which restrictions are necessary :-

Weymouth Tramway ... Speed to be restricted to 4 m.p.h. Loss withiel and Fowey Line ... Speed to be reduced to 10 m.p.h. between 282 m.p. and 282m. 65c. Fowey Station,

Passing Restrictions :-

Must not pass, nor be passed by, stock of identical dimensions, or by trains conveying passenger 73 feet long by 9 feet 6 inches wide, or by stock 73 feet long by 9 feet wide, also by trains stock of similar dimensions and/or out-of-gauge loads, between the following Box-to-Box

Postypool Road Station South and Trosnant Junction (Clarence Street). Cef Crib and Crumlin Junction inclusive. (This section includes Glyn Tunnel.)

(continued)

These coaches are prohibited from passing other coaching stock and out-of-gauge loads between the following points in the Cardiff Valleys Engineering Division:

Cogan Junction to Cogan Sidings. Biglis Junction to Cadoxton, Nos. 1 and 2 lines (inclusive). Barry Dock (exclusive) to Gas Works Junction. Barry to Barry Island. Quakers Yard L.L. Station inclusive to Black Lion. Eirw Branch Junction to Trehafod Junction. Gyfeillon Lower to Rhondda Cutting. Double Line portion at Maerdy. Nixon's Crossing to Mountain Ash. P.C. and N. Junction to Groeswen. Dowlais Junction to Dowlais Cae Harris. Ystrad Mynach North to Hengoed L.L. Maesycwmmer Station (inclusive) to Pengam Station. Over junction curves at Treforest Junction.

Radyr Junction Box, inclusive, to Radyr Station (inclusive).

#### L & N.E. Railway.

#### Lines Prohibited:

Ferryhill to Hartlepoo . North Leith Branch. Penicuick Branch. Eyemouth Branch. Newcastle Central.

## To following sections by prior arrangement only:

Between Sheffield and Manchester. Between Marks Tey and Long Melford. Between St. Dunstans and Queensbury. Between Darlington and Shildon. Between Durham and Bishops Auckland. Between Wormit Leuchars and Taport. Newbiggin Branch. Lincoln Station. Richmond (Yorks) Station. Whitby Station.

#### LMS Railway.

#### Lines Prohibited:

Between Carnforth and Whitehaven. Between Maryport (exclusive) and Carlisle. Between Bingley Junction Shipley and Bradford Junction Shipley. Between Halifax East Junction and Ovenden. Between Beaufort S.B. and Ebbw Vale Junction. Dursley Branch. Thornbury Branch. Nailsworth Branch. Meltham Branch Churnet Valley Platforms at Uttoxeter Station. Loop Line Etruria to Kidsgrove. Buxton Branch. Havfield Branch. Widnes and St. Helens Line. Penistone Station. Dunstable Bay Line. Swansea Vale Line and Branches. Swansea Victoria Station. Brynmawr, Blaenavon Bay. Brynmawr (M.T. & A. Line) Up Line through station. Hampstead Junction Line. West Hampstead Up Fast Platform. Willesden High Level Up Fast Platform. Highbury No. 1 Down Steam Line. St. Pancras Station. Delston Station Poplar Branch. Broad Street. Bow Station Down Platform (Tilbury Junction Bow to Bromley Junction Line)
Marchesfield (Central) Station platform roads (LMS and L. & N.E. Joint). Seaford No. 2 Bay Platform Line.

#### LMS Railway

#### Lines Prohibited :- continued.

Bradford (Exchange) Nos. 1 and 10 Platforms. Keighley (Worth Valley Branch Platform Lines). Invergordon Harbour Branch. Dornock Branch. Balerno Branch. Dumbarton Central Loop Lines. Barncluith Tunnel. Severn and Wye Joint Line.

#### Southern Railway.

### Lines Prohibited:

Between Tonbridge and Battle.
Between Deptford and Woolwich Arsenal.
Between Dormans and Kingscote.
Between Canterbury West and Whitstable Harbour.

(G.A. 18. 11/47, R.6/3926A.)

Stock 60ft. long by 9ft. wide.-Page 153.

The following to be added to the entry under heading "L.M. & S. Company (Western Section)": Swansea Victoria and Craven Arms (provided the length over buffers does not exceed 63ft. and the width over projections does not exceed 9ft. 4in.) subject to the following

Swansea Victoria—Wash road in the Carriage Sheds.

Builth Road-Bay Platform.

(G.A.30 Op.-9/54 R.6/-).

Reference to the following to appear on page 153:-

## COACHING STOCK OF CROSS-COUNTRY DIMENSIONS.

New coaches constructed to modified dimensions for cross-country working are now being sent into traffic.

The vehicles are distinguished by a small YELLOW disc placed on the ends near the tonnage figures, and they are interchangeable with the other companies as shewn below :-

#### L.&N.E.

Accept generally over Main Lines.

#### L.M.&S.

Can work generally, except over the following restricted sections:-

Dursley Branch. Thornbury Branch.

Nailsworth Branch. Halesowen Branch.

Churnet Valley Line Platforms at Uttoxeter Stations

Loop Line Etruria to Kidsgrove. Disley Tunnel on Buxton Branch.

Widnes and St. Helens Line.

Barrow (Ramsden Dock) and Coniston. Whitehaven, Bransty Station (except by special arrangement).

Halifax (Nos. 4, 5 and platform lines).

Penistone (Through station).

Balerno Branch.

Maryport (exclusive) and Carlisle.

Hampstead Junction Line.

Leadhills Branch.

Bingley Junction, Shipley and Bradford Junction, Shipley.

Dunstable Bay Line. Macclesfield (Central) Station Platform Roads. On the Hereford, Hay and Brecon section must work over main running lines.

Accept generally, except between :-

Tonbridge and Battle.

Gipsy Hill and Crystal Palace Low Level

Charlton and Plumstead.

Dartford and Strood.

Canterbury West and Whitstable Harbour.

(G.A.3.—12/37. R.6/1358.A.)

#### RESTRICTIONS ON THE WORKING OF GREAT WESTERN WIDE STOCK—Continued.

Western Valleys (Risca to Ebbw Vale and to Nantyglo).

Western Valleys (Nine Mile Point Branch)

South Wales Mineral Junction Railway (P.T.R.).

South Wales Mineral Railway (P.T.R.).

Ogmore Valley Extension Railway.

Dafen Branch.

Llanelly and Mynydd Mawr Railway.

Burry Port and Gwendraeth Valley Railway and Branches.

Gwendraeth Valley Line.

Pontcysyllte Branch.

Cleobury Mortimer and Ditton Priors Light Railway.

Vron Branch.

Tanat Valley Branch.

Nantmawr Branch.

Kerry Branch.

Van Branch.

Plasmadoc Branch.

Cawdor Branch.

Eastern Valleys-Pontypool (Crane Street)-Trevethin Junction to

Abersychan and Talywain. Apr To De Cyfarthfa Junction to Quakers Yard (H.L.). BLAENAVON L.L.

The L. & N.E. Company for general working will only accept stock not exceeding 9 feet 3 inches in of ft. lon width over hardles, which includes old type clerestory roof coaches (without projections in guard's compartment), modified elliptical roof stock and modern "Yellow disc" stock. In special circumstances, provided prior advice is given, they will take stock up to 9 feet 4 inches in width over handles, but this concession does not apply to the following :-

Destinations via Newcastle or Carlisle. Colne Valley Branch.

Ferryhill and Castle Eden Section.

(G.A.15-12/44 R.6/3603A)

#### L.M. & S. Company.

(Western Section) accept between Shrewsbury and Crewe.

Crewe and Holyhead.

Crewe and Liverpool (Lime Street).

Crewe and Manchester (London Road).

Crewe and Carlisle.

Chester and Liverpool (via Halton Junction).

Chester and Manchester Exchange (via Warrington). Crewe and Glasgow (Central).

Crewe and Edinburgh (Princes Street).

Crewe and Blackpool (Central).
Crewe and Blackpool (T. Road). Station only. Not Excursion platforms.

Stock 9 ft. 3 ins. wide may now work over the Sirhowy Line via Nine Mile

Midland Section) accept only as between Standish Junction and Yate Section.

Bristol and Birmingham.

Bristol and Bradford.

Gloucester and Birmingham.

Cheltenham Lansdown and Birmingham.

(Brake vehicles with projections must not be worked to the Midland Section).

## (North Stafford Section) accept as shewn below :-

Market Drayton and Stoke. Stoke and Macclesfield.

Stoke and Wellington and Burton.

Stoke and Colwich and Norton Bridge.

Macclesfield and Wellington (via Churnet Straight).

Ashbourne Branch.

#### Southern Company.

L & S.W. Section) accept for general working.

L.B. & S.C. Section) accept, except as between Gypsy Hill and Crystal Palace

S.E. & C. Section) cannot accept for general working.

Somerset and Dorset Joint Line accept, providing vehicles have not to be taken into Sidings adjacent to their Docks,

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DIMENSIONS OF VEHICLES TO STATIONS ON S.E. & C. AND L.B. & S.C. SECTIONS OF SOUTHERN RAILWAY.

"Double-ended" Beetles (B. & C.), Horse Boxes (Pacos B. & C.). brake coaches with side lookouts, large Milk Trucks (Siphons G. & J.), and other vehicles which exceed the dimensions set out below cannot work over the following sections of the Southern Railway:

Through Crystal Palace Low Level Tunnel. Tonbridge & Crowhurst. Streatham Hill & West Norwood. Hastings and Winchelsea. Between Charlton and Plumstead.

Whitstable Harbour Branch. .. 50 feet 0 inches. .. .. Length of body .. .. 8 ,,  $0\frac{3}{4}$  ,, Width of body at waist ... .. 8 ,, 4 Width across side cornices of roof ... .. .. 8 ,, 10 .. .. 8 ,, 4 ,, 4 Width across bottom stepboards on bogie truck ... .. . 10 81

Height of top of side cornice from rail level ... This restriction does not apply to Tunbridge Wells West (L.B. & S.C. Section) via Oxted, nor to Hastings via Polegate.

The "double-ended" beetles, luggage bodied and horse boxes, with luggage compartment, also brake coaches with side look-outs are not within these dimensions.

Coaches with an elevated or dome roof, or covered carriage trucks, must not work through via Victoria to the L. & N.E. and L.M. & S. Lines, but the latest pattern G.W. Horse Boxes may be used for traffic to the L. & N.E. and L.M. & S. systems via Victoria.

G.W. brake coaches and vans with guards' projections are not accepted by the L.B. & S.C. section of the Southern company. Stock wider than 9 feet over all should, therefore, not be used for stations on the L.B. & S.C. section of the Southern Railway.

PASSENGER TRAIN STOCK FOR ILFRACOMBE BRANCH.—No coaches with lower centre-step boards will be accepted from the Great Western Company at Barnstaple for working over the Southern line to Ilfracombe.

The Southern Co. have agreed to L. & N.E. Co.'s coaching stock 59 feet long (63 ft. 7 ins. over buffers), and 9 feet wide working to Ilfracombe via Barnstaple.

#### OTES MUST NOT BE LOADED AS UNDER :-

STATION TO	VEHICLES MUST NOT BE FIRM.	VEHICLES NOT TO BE LOADED.
Gloucester (Docks, L.M. & S. Side)	Ashbee & Co. Fielding & Platt Gloucester Joinery Co. Griggs & Co., Ltd. Haine & Corry, Ltd.	
	H. W. Ingram & Co., Ltd.  Matthews & Co., Ltd.  Meggitt & Jones  Nicks & Co.  Price, Walker & Co.  J. Romans & Co., Ltd.	30-ton rail and timber trucks. ("Macaws B. "C," "D" & "E.")
	Sessions & Sons, Ltd.	
London :— East India Dock	Western Trading Co., Ltd. J	Six-wheeled vehicles (passenger or goods) with
Hast Hala Dock		shipment traffic. Four-wheeled vehicles must be used.
		Well trollies
		("Crocodile " "Crocodile A." &c.), Bogie
		rail and timber trucks ("Macaws B," "C," "D" and "E"), girder and boiler trucks
		/" Pollen " " Pollen A." &c.), and " Loriot."
Millwall Dock	All firms	Six wheeled vehicles (passenger or goods) With
Millwan Dock	An arms	shipment traffic; four-wheeled vehicles must be used. Vehicles with wheelbase over 29 ft.
		or 8 ft. 3 in. wide.
		Well trollies ("Crocodile." "Crocodile A," &c.
		borie rail and timber trucks (" Macaws B,
		"C," "D," and "E"), girder and boiled trucks ("Pollen," "Pollen A," etc.), and
		"Toriot"
*Royal Albert Dock	All firms	cir whooled vehicles (nassenger or goods) with
South Dock	* (Except Motor Packing Co.,	shipment traffic. Four-wheeled venicles mus
Swansea :-	South Dock)	be used. Fruit vans.
King's Dock		Fruit vans.
Prince of Wales' Dock		a 1 '11 mand on bodies
South Dock		. Cattle wagons. Covered vans with wooden bodies
Gloucester (Docks L.M.	(S. side) Ashbee, Sons & Co.	Ltd. Bogie Bolster vehicles and vehicles with a wheel base exceeding
	Gloucester Joinery	15 ft. GAIG
	Griggs & Co., Ltd. Meggitt & Jones, Lt	
	Nicks & Co.	
	Price, Walker & Co.	.1
	Romans J. & Co., L	Id
		(CA 12 4/43 C.G.M. W.T./X. 57600.)

## Reference to the following to appear on page 155:— CONVEYANCE OF ROAD-RAIL MILK TANK TRAILERS BY RAIL.

Road-rail milk tank trailers are fitted with an adjustable screw spring stop above the centre of each bearing spring to prevent further deflection of the springs, and thus avoid rolling during transit. These stops must be screwed down until they are in contact with the springs before commencing a journey by rail, and the loading staff must see that this is done in all cases, whether the tanks are loaded or empty.

Care must also be taken to see that the chains provided for securing the road trailer to the rail vehicle are not twisted when finally tightened up, in order to ensure that they will not become slack en route.

When an empty road rail tank is required to be loaded with milk whilst on the rail vehicle, the adjustable screw spring stops must first be screwed back sufficiently clear of the springs to allow for any deflection of the tank springs likely to take place during loading, and after loading has been completed the screw stops and chains must be properly tightened and adjusted. In the case of a loaded road-rail tank requiring to be emptied whilst on the rail vehicle, the chains must be slacked back before emptying in accordance with the painted instructions on the underframe of the rail vehicle.

(G.A. 5.—2/39. C.M.E.—31299.)

Reference to the following to appear on page 155:-

#### SIDE CHAINS ON WARFLATS AND WARWELLS.

Side Chains on Warflats and Warwells must not be coupled between vehicles.

(G.A.15-12/44 L.K.1/7688/6A

The following to be substituted for the fifth paragraph of these instructions:-

When coaching stock is propelled on running lines or from running lines to sidings, the Guard or Shunter should ride on the leading vehicle or the nearest suitable vehicle, keep a good lookout and be prepared to hand signal to the Driver or Fireman. If this is impracticable the Guard or Shunter should place himself in such a position on the ground that he can plainly signal to the Driver.

(G.A.30 Op.-9/54 O.M.12725).

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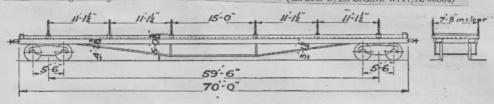
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MACAW C Nos. 84995 and 84996.

These wagons must not be loaded to any other Company's line or sent loaded or empty on to the G.W. Docks Lines without the authority of the Chief Goods Manager being first obtained.

(G.A.16. 5/46, C.G.M. W.T./X, 99864)



#### TAIL LAMP ADAPTORS FOR L. & N.E.R. VEHICLES.

8/6A

#### SHUNTING INSTRUCTIONS.

#### SHUNTING, &c., IN STATIONS AND STATION YARDS.

Signalmen must exercise every care in working points (especially in station yards where the points and signals are interlocked and worked from signal boxes) so as to avoid accidents occurring.

A train, or portion of a train, must never be shunted in a yard (except where the points are close to a signal-box) unless a Shunter, or some competent person, accompanies it for the purpose of seeing that the last vehicle is over the points, and to give a hand-signal to the Signalman to turn the points; the Signalman must not turn the points until he has received a hand-signal from the Shunter that the whole of the train is over them, nor must the Engineman move his engine until he has received a signal from the Shunter that he is to do so.

If there is no Shunter or Porter available, this duty must be performed by the Guard of the train, whether passenger or freight.

Passenger stock not provided with hand brake must not be loose shunted, but must remain attached until the movement has been brought to a stand for the unbraked vehicles to be detached.

When empty coaching stock is propelled on running lines or from running lines to sidings, the Guard or Shunter must ride on the leading vehicle or the nearest suitable vehicle thereto, keep a good look out and be prepared to hand signal to the Driver.

The Driver of a light engine, not accompanied by a Guard or Shunter, must satisfy himself that the points are in proper position before moving his engine over them. After moving over points the Driver must also satisfy himself that the engine is clear, and give an indication to the Signalman by

The person conducting the shunting must see that the points are in proper position before giving the signal for the train to move in either direction.

Signalmen must satisfy themselves that the points and signals are set in their proper position for shunting operations, as well as for the safe passage of trains, and they will be held responsible for reporting every case of omission on the part of Shunters or Guards to comply with these instructions.

Enginemen must give notice to all persons removing goods into or out of trucks and to men engaged in repairing wagons, by whistling once before moving wagons that are standing in mileage Sidings, or which may be under repair; and Shunters and others, before giving the signal to the Enginemen to move such wagons, must always walk the whole length of wagons and personally caution each individual who is engaged as described, and at the same time make him understand at what time it will be safe to resume his work; any neglect to obey this regulation will be severely dealt with.

Signalmen, Foremen, Shunters and Guards, when at work in a yard with engines, must instruct

Signalmen, Foremen, Shunters and Guards, when at work in a yard with engines, must instruct the Drivers verbally, as far as possible, with respect to the movements of the engines, and not trust to hand signals.

When an engine is shunting between a siding and a running line, the Signalman must not reverse the points until he has had an understanding with the person in charge of the shunting, and, in the event of there being no one in charge, then an understanding must be arrived at with the Driver.

Shunters, Shunt-Horse Drivers and Capstan men must exercise care in the movement of trucks into or out of goods sheds. Trucks must not be moved until it has been ascertained that the doors of the wagons are securely fastened, and that persons working in connection with trucks have been duly warned.

#### SHUNTING INSTRUCTIONS-Continued.

SHUNTING, &c., IN STATIONS AND STATION YARDS-Continuea.

Shunters must be careful not to move passenger trains or carriages whilst men

(G.A. 8.—5/41. LK1/6406/5.) The staff concerned should exercise great care in order to prevent damage to goods by rough shunting.

Milk Traffic, etc.—Guards, Shunters and others concerned must take the necessary care to ensure the proper handling of this traffic.

Shunting of Acids in Glass Carboys. - Instructions have been issued to the manufacturers in regard to the proper loading, etc., of this traffic.

Nitric Acid, if spilt upon the straw packing of the carboy, may set it on fire, and the fracture of one carboy may lead to the fracture and loss of acid from a large number of others. In addition to this. Nitric and other acids are liable to cause serious injury to persons or damage to merchand's: and rolling stock.

Wagons containing Acids in carboys bear distinctive labels, e.g., "Dangerous Goods," "Acids in Glass-Shunt with Care," and it is essential that the instructions in regard to shunting should be strictly observed in dealing with the wagons so as to prevent the breakage of the carboys.

The trucks containing acids must be formed as far from the engine as possible and loose shunting of, or against, such trucks is prohibited.

The stations must see that the "Dangerous Goods" and the "Acids in Glass-Shunt with care" labels are affixed to both sides of the truck in which Acids are loaded before despatch.

Station Masters and Goods Agents are enjoined to see, as they go about the station yards, that the regulations herein laid down are strictly observed by the staff concerned.

#### DEFINITION OF THE SEVERAL MODES OF SHUNTING WAGONS.

In order that there may be no misunderstanding as to the meaning of the shunting terms "Double Shunt" and "Fly Shunt" in common use among Shunters, Guards and others, the following definitions are to be noted :-

- Double Shunt .- This means the propelling of two lots of wagons (uncoupled from the engine propelling them, but coupled together in two separate lots) from one line of rails on to two different lines of rails; that is, one lot going through one set of points on to one line of rails, and the other lot going through another set of points on to another line of rails. The term "Double Shunt" also applies to the case of an engine propelling wagons when the wagons are turned on to one line of rails and the engine turned on to another line.
- Fly Shunt .- This means that while an engine is drawing wagons attached to it towards a set of facing points, the wagons are uncoupled from the engine or from each other, and the engine or engine and trucks, is run on to one line of rails, after which the following wagons are run on to another line of rails.

Double Shunting is strictly prohibited except when done by engines used specially for the purpose of shunting, attended by experienced shunters.

Fly Shunting is strictly prohibited unless the circumstances do not permit of the shunt being performed in any other manner, and, even then, the operation must be performed by an engine specially used for shunting and attended by an experienced shunter.

#### ENGINE DRAWBAR HOOKS AND COUPLINGS.

Engine screw connections, when not in use, must always be placed on the hook provided on the buffer beam for the purpose. They must not be allowed to hang down, nor be thrown back over the engine draw-bar hook.

### CARRIAGE AND WAGON DRAWBAR HOOKS AND COUPLINGS.

Carriage or Wagon screw connections, when not in use, must be placed upon the hook provided on the headstock for that purpose. They must not be allowed to hang down or be thrown back over the drawbar hook.

## FASTENING OF WAGON DOORS-page 156.

These instructions to be amended as follows:-

Guards must see that doors of all wagons formed on their trains or attached during the journey are securely fastened. It will not, however, be necessary for the end doors of 10-ton and 12-ton colliery wagons to be fastened when returning empty from docks to local collieries on this Company's system in South Wales, also to the Forest of Dean or to local collieries situate on the L.M.S. Company's Swansea Vale Line, and as far north as Llandovery on the L.M.S. Main Line. (G.A.3.—12/37. L.K.1/4081/6.)

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### SHUNTING, ETC., IN STATIONS AND STATION YARDS-Page 156.

The following to be inserted as the third paragraph on page 156:-

Steam Rollers and Tractor Engines.—Loose shunting of vehicles loaded with steam rollers or tractor engines, is prohibited: they must be brought to a state of rest whilst coupled to train or shunting engine.

(G.A.19—10/48. E.85921.H.)

### Reference to the following to appear on page 156.

#### SHUNTING INTO PRIVATE SIDINGS

In order to avoid the possibility of injury to persons who may be working near lines in private sidings or walking along such sidings or over level crossings, a man should, when practicable, accompany or precede wagons being propelled, and no engine or wagons should be moved until warning by means of the engine whistle, or otherwise, has been given to such persons. After dark or during fog a light should, where practicable, be exhibited on the engine or leading wagon when shunting into private sidings.

(G.A. 7.-3/40. LK1/5418/6.)

## TELEPHONE BOXES IN YARDS AND RUNNING LOOP LINES, ETC.

Arrangements have been made, as and when repainting becomes necessary, for telephone boxes to be painted with black and white diagonal stripes.

(G.A.12. 4/43. L.K.1/7014/7.)

Reference to the following to be made on page 156:-

## REGULATIONS FOR THE WORKING OF DIESEL ELECTRIC SHUNTING ENGINES OVER RUNNING LINES.

Diesel Electric Shunting Engines working in Yards are normally manned with Driver only on the footplate.

When such Engines are required to work over the running lines the Driver will be held responsible for obeying fixed signals, and the engine must be accompanied by a second person competent to carry out Rule 55, i.e. "Detention of Trains at Signals" and also "The Protection of Running Lines in the event of an Emergency" in accordance with Rules 178 to 181. The second person will normally be provided by the Locomotive Foreman, but where difficulty or delay would occur in so doing the Traffic Department representative in charge must provide a competent person.

In the event of a section being obstructed by a derailment or disabled train, a Diesel Electric engine, when a steam engine is not available, may be used to clear the section providing it is accompanied by a second person competent to carry out Rules 55 and 178 to 181, subject to conforming to the speed restriction laid down for this class of locomotive. (G.A.23—7/49. L.K.1/9369/Gen.)

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SHUNTING INSTRUCTIONS-Continued.

#### SHUNTING POLES.

Stations at which goods shunting is done must keep a supply of shunting poles on hand; these can be obtained by requisition from the General Stores, Swindon. A shunting pole must always be used for coupling and uncoupling wagons whenever it is possible to do so. The poles issued will in every case be fitted with hooks. When any pole becomes worn out, or breaks, the iron hook and the pole must be returned to the General Stores, Swindon, a new pole complete being obtained from the station stock. The iron hook must in no case be retained, nor must the station staff in any case attempt themselves to fix the iron hooks on the poles. The use of shunting poles for any purpose other than coupling and uncoupling of vehicles is strictly forbidden, and the poles must not be thrown upon the tops of sheeted loads.

The shunting poles must be examined as soon as they are received from Swindon, and any which appear to be cross-grained or otherwise defective should be returned to the General Stores, Swindon, immediately, an advice being sent to the Stores Superintendent of the number returned.

#### IMPROPER USE OF SHUNTING POLES.

The use of shunting poles for manipulating brake levers or wagons or for any purpose other than that of coupling or uncoupling vehicles is strictly forbidden.

Inspectors and others must immediately report to their superior officer any infringement of this

regulation which may come under their observation.

## USE OF BRAKE-STICKS AND SHUNTING POLES ON WAGONS FITTED WITH EITHER-SIDE

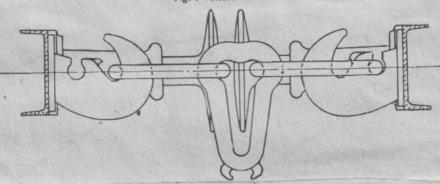
Guards, Shunters and others concerned must not use brake sticks or shunting poles in the looped handles of the Either-side brakes as this causes damage to the brakes. The brakes can be put on full power by hand without the need of a brake-stick or shunting pole.

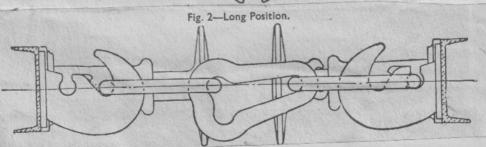
### "INSTANTER" PATENT COUPLER.

### INSTANTER COUPLINGS.

A number of vehicles are fitted with Instanter Couplings, which can be in either of two positions, as shewn in the following diagrams :-

Fig. 1-Short Position.





## "INSTANTER" PATENT COUPLER .- Page 158.

The paragraph following figure 2 to be amended to read:-

The coupling can be used for shunting operations as an ordinary loose coupling as shewn in figure 2. When the centre link is required to be placed into the short or close-coupled position the following procedure to be observed.

(G.A.31-7/56 L.K.1/9003/Gen.E.)

Referenc

BRAKE STIC

All b of June returned usual ch Reference to the following to be made on Page 158 :-

#### BRAKE STICKS

All brake sticks on hand at stations and in yards must be examined during the first week of June and December and any which shew signs of having become defective must be returned to Swindon Stores. Requisitions for replacements must be submitted through the usual channels.

(G.A.31-7/56-L.K.1/13121/420)

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#### PASSENGER AND FREIGHT TRAIN INSTRUCTIONS. SHUNTING INSTRUCTIONS—Continued. "Instanter" Patent Coupler—continued

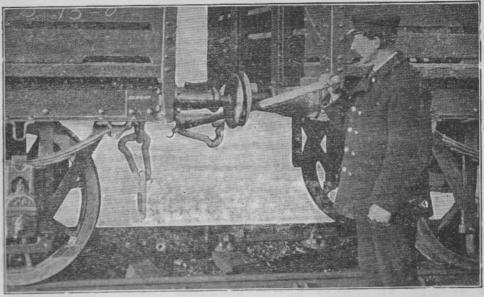


Fig. 3.

- (a) The shunter must face the same way as the claw end of the link.
- (b) After placing the ferrule end of the shunting pole in the claw end of the centre link from the under side of the buffer rod, the shunter with an upward and forward movement (the shunting pole pressing against the inside of the buffer head to obtain the necessary leverage, as shewn in Fig. 3) slides the centre link into the short position, see Fig. 1.

This operation can be carried out with one hand only on the shunting pole, and can be performed easily with a little practise.

To uncouple with the link in the short position, the shunter knocks the centre link into the long position, and with the same stroke lifts the end link off the draw-bar hook. This can be done with the shunting pole either above or below the buffer.

To short couple, or uncouple when on a curve, it is necessary for the buffers of the vehicle to be touching or slightly compressed.

To facilitate the working of the coupling a little grease may, if necessary, be used on the inner side of the centre link.

Vehicles fitted with Instanter couplings may be attached to trains as shewn below :-

## (a) Trains Conveying Passengers.

One vehicle only, fitted with Instanter couplings, may be attached to trains conveying passengers provided it conforms in other respects to coaching stock requirements and subject also to the instructions relative to the conveyance of four-wheeled vehicles on passenger trains. In such circumstances the screw couplings of the adjacent vehicles must be used. This does not modify the authority (where given) to attach at the extreme rear one vehicle not conveying passengers and not fitted with the continuous brake or through pipe.

## (b) Coaching Stock Trains Not Conveying Passengers.

Vehicles fitted with Instanter couplings and conforming to coaching stock requirements may be conveyed in trains composed of coaching stock, other than those conveying passengers, but the screw couplings of the vehicles on each side of the Instanter-fitted vehicle must be used. When two or more vehicles fitted with Instanter couplings are marshalled together in such trains, the screw couplings of the vehicles on each side of the Instanter-coupled vehicles must be used and the Instanter coupling, or couplings, in use, must be in the short position.

The instructions respecting the conveyance of four-wheeled vehicles will apply in respect of the running of Instanter-fitted vehicles on empty coaching stock trains.

#### (c) Freight Trains.

The couplings must be in the long position during shunting operations, and also when used in ordinary freight trains except when the vehicles are conveying cattle when the couplings of the cattle wagons must be in the short position.

Vehicles with Instanter couplings can be used in fitted freight trains in exactly the

## (b) Coaching Stock Trains Not Conveying Passengers.

Vehicles fitted with Instanter couplings and conforming to coaching stock requirements may be conveyed in trains composed of coaching stock, other than those conveying passengers, but the screw couplings of the vehicles on each side of the Instanter-fitted vehicle must be used. When two or more vehicles fitted with Instanter couplings are marshalled together in such trains, the screw couplings of the vehicles on each side of the Instanter-coupled vehicles must be used and the Instanter coupling, or couplings, in use, must be in the short position.

The instructions respecting the conveyance of four-wheeled vehicles will apply in respect of the running of Instanter-fitted vehicles on empty coaching stock trains.

## (c) Freight Trains.

The couplings must be in the long position during shunting operations, and also when used in ordinary freight trains except when the vehicles are conveying cattle when the couplings of the cattle wagons must be in the short position.

Vehicles with Instanter couplings can be used in fitted freight trains in exactly the same way as screw-coupled vehicles, but when used in the fitted portion of the train the screw coupling of the adjacent vehicle or vehicles should be used if possible. If this is not possible the Instanter coupling must be in the short position.

Under no circumstances must an Instanter coupling be used for attaching to vehicles fitted with the Buck-eye coupling, and a screw coupling must be used.

(G.A.27.Op.-1/51. L.K.1/9003/Gen.E.)

- made not be used for attaching venicles to the rear of a passenger train.

#### REGULATIONS FOR THE USE OF CAPSTANS.

1. No persons, except those duly authorised, must be allowed to work capstans. Members of the staff found working them without proper authority will be liable to dismissal.

2. The person in charge of a capstan must give warning to anyone who may be passing, or at

work in the vicinity, before moving wagons.

3. Before setting a capstan in motion, the person in charge must see that the tow rope is clear of the running rails and cannot be run over; he must also see that the tow rope hook is properly

Except where specially authorised to the contrary the hook of the capstan rope should not be placed in the fork of the axle guard wing but should be attached to the back of the axle guard below the axle box and great care must be exercised to prevent damage. The hook must not be attached to, or detached from, a wagon whilst the capstan is working.

A rope with knots or one which has become stranded or is otherwise defective must not be used.

## PASSENGER AND FREIGHT TRAIN INSTRUCTIONS. SHUNTING INSTRUCTIONS-Continued.

#### Operating Instructions.

(a) Loose chain or rope hydraulic capstans.

Draw the rope or chain tight by hand and wind three coils round the capstan drum. Apply foot treadle or hand lever gradually until the required speed is obtained. It is undesirable to and the canacity of the

4. The numbers of empty and loaded wagons which may be hauled at any one time will depend on the nature of the track (i.e., whether straight, curved, or up an incline) and must be fixed locally and exhibited at or near the capstan.

5. Wagons must not be checked or stopped by means of the ropes or chains on the capstans. Wagon brakes or scotches only must be used for such purpose, and care must be taken in drawing wagons towards the capstan that the rope hook does not fly off.

6. The ropes of all types of capstans must be left clear of the running rails.7. Proper care must be taken in getting between trucks for the purpose of coupling or uncoupling them, and shunting poles must be used whenever practicable. Under no circumstances should anyone be allowed to pass between the buffers of trucks; nor should any person get under the buffers of trucks without first informing the capstan-man that it is necessary to do so.

8. Under no circumstances must packing pieces be placed under treadle plates.

10 3. Any defect in a capstan must be reported to the Chief Mechanical Engineer's Department, who must certify that the capstan is in proper working order before it is used again.

## TOWING VEHICLES BY MEANS OF ENGINES AND DRAWING VEHICLES BY HORSES, TRACTORS OR CAPSTANS.

Where the towing of vehicles by engines is authorised, it is objectionable to attach the rope or chain to the draw-bar hook or the axle guard, the proper course being to attach the hook on the rope to the towing loop, or to place it in the hole in the sole bar of the vehicle nearest the engine, and, when ready to move, the engine must always start gently.

The towing loop on the sole-bar, however, is only of sufficient strength to tow the wagon itself, and, if it be necessary to move more than one vehicle at a time, the hook of the towing rope or chain must be connected to the draw-bar hook; but when it is so used great care must be exercised to prevent

it becoming entangled in the wheels.

When vehicles are drawn by HORSES, TRACTORS OR CAPSTANS the draw-chain or rope should be attached to the tow-loop on the sole-bar whenever practicable, but, when it is not possible to do this, the chain or rope may be attached to the draw-bar hook or axle guard, and great care must be exercised when this is done in order to prevent damage. Should the axle guard become bent, or other damage be done during the towing operation, the attention of a Carriage Department Examiner must be at once called to the matter to ensure the defect being rectified before the vehicle again passes into traffic. Not more than one wagon should be towed at a time by the axle guard.

The number of vehicles to be towed at one time must be regulated by the weight of the loads they contain, the gradients, and other circumstances, care being taken that no greater number of

vehicles than can be towed with safety are moved at one time.

Shunters and others must not pass in front of moving vehicles for the purpose of hooking or unhooking draw-chains or ropes, except in those cases where it is absolutely necessary for them to do so. REG chain Operating Instructions.

(a) Loose chain or rope hydraulic capstans.

Draw the rope or chain tight by hand and wind three coils round the capstan drum. Apply foot treadle or hand lever gradually until the required speed is obtained. It is undesirable to surge a capstan rope in endeavouring to move a load beyond the capacity of the capstan or to vary the coils of rope on the capstan head while it is in motion. When a capstan is finished with or left unattended, the power should, whenever possible, be shut off, but in cases where this is not convenient, the push for the treadle, where not a fixture, should be removed to a place of safety. After use, the rope must be taken off, and not left on the drum, and no capstan must be used to coil slack ropes.

(b) Fixed chain hydraulic capstans.

After pressure has been turned on and hook attached to wagon, apply foot treadle gradually until required speed is obtained. Capstan must be stopped and chain released before wagon to which it is attached reaches capstan. Pressure must be turned off at the end of each shift or when a ship finishes. When finished with chain must be coiled around capstan.

(c) Free rope type electric capstans.

Close switch or circuit breaker supplying power to the capstan, and operate as for hydraulic capstan. When a capstan is finished with, release foot treadle so that it falls into non-working position and cut off electric supply by means of switch or circuit breaker. Treat rope as for hydraulic capstan.

(d) Fixed wire rope type electric capstans (lever operated).

Close circuit breaker controlling electric supply to the capstan. Insert hand lever into slot provided for it in the capstan decking and see that the tongue of the hand lever is in the "neutral" notch. This is the position in which the capstan head is free for the rope to be unwound from the capstan head and attached to wagons. To operate capstan, release tongue of hand lever from "neutral" notch and move hand lever over to "running" position. The lever should be held in this position until it is desired to stop the capstan. When operating a capstan care must be taken to take up gradually any slack in the rope before full power is applied to move wagons.

To stop capstan, move hand lever away from the "running" position as far as it will go. This action switches off power from the motor, disengages the bollard clutch, and applies a brake to the capstan head.

After using a capstan the electric supply must be cut off by means of the circuit breaker controlling it and the hand lever removed from the slot and stowed inside the capstan box or other place provided for it. Under no circumstances must the lever be left in the slot unattended when work with the capstan has been completed. When finished with the rope must be left neatly coiled round the capstan drum.

(e) Fixed wire rope type electric capstans (foot treadle operated).

Close circuit breaker controlling electric supply to the capstan. Place arrow on foot treadle in "neutral" position and release dog lever on capstan top. This will free rope for attachment to wagons.

When ready to haul, lift foot treadle into "forward" position as shown by arrow, reverse dog lever into "closed" position and press treadle. To stop capstan take off foot, lift treadle into "reverse" position and press treadle until capstan stops. Open circuit breaker (power switch) at end of shift or finish of job. When finished with wire rope should be coiled around capstan.

Note:—No load must be hauled by a fixed wire rope capstan unless at least two turns of rope are round the capstan head.

(G.A. 5.—2/39. G.M.O.—C.79451.)

REGULATIONS FOR THE USE OF CAPSTANS-page 160.

The following to be added as an additional paragraph at the end of Clause (a)—loose chains or rope, hydraulic capstans:—

"Special care must be exercised by the capstan men to see that the safety catches and other safety devices attached to the bedplate of hydraulic turnover capstan engines are properly secured in all cases."

(CA 29 On 5/52 M & F.E. 98890 45 WT) (G.A.29.Op.-5/52. M. & E.E. 98890. 45.WT.)

9. Capstans must not be used for verailing wagons

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## SHUNTING INSTRUCTIONS-Continued.

## GUARDS' HAND LAMPS TO BE LIGHTED THROUGH CERTAIN TUNNELS.

With reference to Rule 129 (iii) of the Rule Book, Guards must have their hand lamps lighted during the whole of the time their trains are in the Tunnels on the Metropolitan Railway, or any cf the tunnels included on page 120.

## LIST OF TUNNELS (2 CHAINS AND OVER IN LENGTH) ON G.W.R. AND ON JOINT LINES.

Name.		No. of From lines		То		Length in	
		through.	Miles.	Chains.	Miles.	Chains.	110111111
Reading to Taunton direct.				****	107	003	1053
Somerton		2	126	584 -	127	$26\frac{3}{4}$	1000
Reading to Bristol and Creech June	etion		00	10	100	78	3212
***************************************		2	99	$\frac{12}{38\frac{1}{2}}$	101	471	198
		2 2	101	$\frac{30\frac{1}{2}}{24}$	106	271	77
		2	106	281	106	33	99
		2	100	3	109	15	264
- 11 0 - 10 11		2	111	57	111	65	176
		2	115	581	116	243	1017
71 TU 1 N O		2	116	41	116	48	154
THE TO		2	123	611	123	66	110
Creech Junction to Perzance.		-	120				
2277 1 2 11		2	173	13	173	123	1092
		2	206	34	206	434	209
N 1		2	206	$52\frac{1}{2}$	206	63	231
		2	206	664	206	683	55
CO. 1 1		2	206	72	206	75	66
		2	207	18%	207	42	512
		2	217	621	217	741	264
Marlon J Up	Line	1 1	} 227	62	228	22	869
	WILLIAMS	2	231	59	231	61	47
10		2	245	32	245	401	183
5		2 2	248	301	248	42	125
		9	254	61	254	27	452
73 ()		2 2	275	16	275	20	88
		2	279	181	279	441	565.
T) 1		2	297	493	297	76	581
D 1 1 1		2	299	10	299	241	320
*** 1 m		2	301	10	301	134	70
11 1		2	309	62	309	C4 ½	47
City Line.							1 220
Paddington (Suburban)		2	0	0	0	5	110
		2		At (	66		117
West London Extension Line.					0	~11	92
Branch No. 1.		2	2	471	2	511	10
Main Line.		2	2	53	2	57	10
Victoria Branch.		-	0	0	0	10	48
Mitre		2	0	8	.0	10	40
Reading.		0		1 + 95	10		70
7		2		At 35	42		
Wycombe Branch.		1	14	191	14	37 1	524
		1	14	131	17	1712	
Newbury and Winchester Branch		1	24	753	25	16	441
Winchester		1	24	101	20	10	
Devizes Branch (B. and H. Line)		1	85	53	85	62	190
10.1111.0		1	00	00	00		
Bristol Harbour (Redeliffe)		1 & 1 Sdg.	118	571	118	71	292
Bristel Harbour (Redeliffe)		to I saig.	110	0.4	1		

# PASSENGER AND FREIGHT TRAIN INSTRUCTIONS. LIST OF TUNNELS (2 CHAINS AND OVER IN LENGTH)—Continued.

Name.					No. of lines through.	From		To.		Length in Yards
						Miles.	Chains.	Miles.	Chains.	in rard
Portishead Branch. No. 1 Clifton Bridg	*0				1	122	23	122	253	59
No 0				***	1	122	523	122	631	232
Sandstone					1	123	771	124	11	88
TV-11					î	125	331	125	631	665
radford Branch.			***			1	004			
Bradford					2	7	184	7	$25\frac{1}{2}$	159
amerton and Limpl		ke Br	anch.							
					1	6	$74\frac{1}{2}$	6	$77\frac{1}{2}$	66
heddar Valley Line						~.		0=	-	100
					1	24	77	25	5	180
astle Cary to Weym					0	140	101	140	621	311
Evershot					2	149	48½ 70	149 157	191	660
Frampton					2 2	156 161	3	161	15	264
Poundbury					2 2	164	441	165	11/2	814
Bincombe	and De	:1			2	104	4.12	100	*2	011
leymouth and Portl Rodwell					1	169	42	169	441	51
almesbury Branch.						100	-		4	
Malmesbury					1	93	693	93	741	105
hard Branch.							4			
Hatch					1	3	72	3	79	154
arnstaple Branch.										The state of
Bathealton					1	175	141	175	343	447
Venn Cross					1	177	61	177	171	243
Nightcote					1	186	38	186	40	44
Castle Hill					1	199	353	199	501	321
xeter Railway.										
Perridge					1	4	8	4	46	836
Culver					1	4	751	5	63	253
orquay Branch.								222	011	405
Greenway					1	226	9	226	311	495
lingsbridge Branch.						10	00	10-	58	638
Sorley					1	10	29	10	98	050
aunceston Branch.					,	5	20	5	34	307
		***			1	7	491	7	781	641
Yelverton		***		•••	ì	10	62	10	79	374
Grenofen					1	10	02	10	,,,	
owey Branch.					1	284	21	284	741	1178
ewquay Branch.	***								-	
Luxulyan (Bridge-	.)				1	285	443	285	463	56
					î	299	23	299	25	44
oonbarrow Branch.										
Goonbarrow (Stens	alees)				1	289	$53\frac{1}{2}$	289	69	341
almouth Branch.										
Sparnick					1	302	68	303	10	491
Perran					1	306	23	306	40	374
adminton Line.								0=		PO
Alderton				***	2	97	34	97	57	506
Alderton Chipping Sodbury					2	101	6	103	48	4444
evern Tillinei Line	D. alliu	D. W.		ne)		0	50	7	56	1760
Patchway New (U	p Line	:)	***	**	1	6	56	7 7	443	1246
Old (Do	wn Lir	1e)			1	6 7	68‡	1	56	62
,, Short (I		ane)	***		1	7	53	10	543	97
	***			***	2	10	504	15	291	7668
Severn	Y			***	2	11	03	10	201	7000
Avonmouth and Filt		е.			2	113	783	114	121	302
Charlton			***	**	4	110	109	111		1

# PASSENGER AND FREIGHT TRAIN INSTRUCTIONS. LIST OF TUNNELS (2 CHAINS AND OVER IN LENGTH)—Continued.

Name.				No. of lines	From		То		Length	
					through.	Miles.	Chains.	Miles.	Chains.	in Yards
	То	wie C	langer	ton						
Swindon to Severn Tu Kemble	n. oc.	YIA U	iouces	ter.	. 2	90	403	90	$59\frac{1}{2}$	415
Sapperton (Short)					2	94	501	94	664	352
Sapperton (Long)					2	94	$69\frac{1}{2}$	95 125	74 19	1860 232
Newnham Severn Tunnel Juncti	to	Fishe	hand		2	125	81/2	120	10	202
		r ising	uaru.		2	158	71	159	25	742
					2	158	70	159	25	762
					2	216	273	216	631	789 189
Whitland				***	2 2	257 274	$\frac{11}{401}$	257 274	9章 51章	243
				***	2	214	402	211	019	210
Swansea District Lin Lonlas		***			2	1	73	1	49#	925
Llangyfelach					2	4	33	5	121	1953
Penllergaer					2	6	413	6	57莹	287
berystwyth Branch.						,	011	1	-66	100
Bryn Teify					1	29	$\frac{61\frac{1}{2}}{21\frac{1}{2}}$	29	251	86
Tyn-y-Craig		Class Israel	Dunne	 In	1	29	212	20	209	-
Pencader	asue I		Dram	ш.	1	257	58	258	23	988
					1	265	74	266	$1\frac{1}{2}$	168
North Pembrokeshire		ich.				200	wo.1	050		100
Castle					1	269	$79\frac{1}{2}$	270	4	100
Llanelly Line.					1	5	123	5	163	88
Pontardulais	Puny	anh	***	•••	1	0	124	0	104	
Pembroke and Tenby Narberth	Drai	1011.			1	264	16	264	$28\frac{1}{2}$	273
Pembroke					1	285	$5\frac{1}{2}$	285	$26\frac{1}{2}$	460
Tale of Neath.								10	41	709
Quakers Yard					1	16	9	16 29	41	703 526
Pencaedrain					2.	28	$64\frac{1}{2}$	29	81/2	020
Taff Yale Extension.					2	4	59	4	72	280
44 "		***			2	9	46	9	64	398
Newport to Hereford										
Redhill			***		2	3	15	3	$17\frac{1}{2}$	50
Merthyr Branch.							0=1		F03	2497
Merthyr		1			1	3	371	4	703	2491
Forest of Dean Branc					1	1	53	1	54	1064
Bullo Bradley Hill					1	1	66	i	792	299
Blue Rock					1	2	60	2	65	109
Grange Court and He								-08	0.01	700
Micheldean (Lea)					1	126	701	127	261	782 540
					1	136 137	$\frac{32}{28\frac{1}{2}}$	136 138	$\frac{56\frac{1}{2}}{3\frac{1}{2}}$	1208
77.1					1	141	49	141	541	110
Dinedor Ross & Little Mill.					1	171	40	1	0.14	
Lydbrook					1	4	$62\frac{3}{4}$	5	$11\frac{1}{2}$	630
Symonds Yat					1	7	21	7	403	434
	***				1	13	$12\frac{1}{2}$	13	191	148
					1	24	794	25	$11\frac{1}{2}$	256
Bridgend to Abergwy	rnfi.				1	10	171	11	10	1591
Cymmer					1	10	1/2	11	10	1001
Gaer					2	159	47	159	65	403
Porthcawl Branch.	***			1		-				
37 //			***		1	8	56	8	583	63
Halls Road L. Section							40		50	990
Penar					1	4	42	4	53	239

# PASSENGER AND FREIGHT TRAIN INSTRUCTIONS. LIST OF TUNNELS (2 CHAINS AND OVER IN LENGTH)—Continued.

Name,		o, of ines	From		То		Length
		ough.	Miles.	Chains.	Miles.	Chains.	in Yards
Coleford Branch.							
TTT1 '4 1'0'		1	. 4	$35\frac{1}{2}$	4	38	55
Wye Yalley Branch.							1100
from the state of		1	1	35	2	9	1188
Tintern		1	4	221	4	$30^{3}_{4}$	182
Didcot to Chester.		0	100	40	100	501	73
		2	100	49	100 129	$\frac{52\frac{1}{2}}{19}$	596
		2	128 129	72 66	129	$72\frac{1}{2}$	135
		5	129	76	130	31	160
31		2	135	54	135	73	412
		9	141	30	141	47	377
	• •	2 2	157	75	158	161	471
		2	192	343	192	37	52
		2	194	71	194	91	46
		2	200	424	200	451	64
		2	209	482	209	511	53
THE PARTY OF THE P							
		2	213	144	213	241	216
		4	213	301	213	35	106
Ruabon and Dolgelley Branch.							
		1	7	551	8	61	682
		1	24	473	24	55	1531
- · · · · · · · · · · · · · · · · · · ·							
11 37 1		2	0	524	0	77	545
- 11 37 0		2	1	723	1	76	71
Barry Line.							
day Control of the Co		2	2	75	3	5	223
- 4 1		1	9	103	9	231	280
· · · · · · · · · · · · · · · · · ·							
		2.	3	221	4	281/2	1,868
		2	14	17	14	774	1,323
Fenrhos Branch.							
		2	9	14	9	36	490
0 1 111		2	6	$5\frac{1}{2}$	7	133	1,941
Cardiff Railway.							
Tongwynlais		1	4	123	4	21	180
Port Talbot to Blaengarw.							1 010
Bryn (Cwmcerwyn)		1	5		5	534	1,010
		1	5	$64\frac{1}{2}$	6	35	1,109
Swansea and Treherbert							-
Danygraig		2	23		23	59	79
Gelli		21	4	711	4	79	169
Rhondda		1	1	45	3	411	3,443
Andoversford and Redpost Junction.			1			-	1
Chedworth		1	53	64	54		49
Marlborough		2	18	501	18	793	64
Moat Lane and Talyllyn Line.							-
Marteg		1	70		70	381	37
Rhayader		1	74	$14\frac{1}{2}$	74	$26\frac{3}{4}$	27
Brecon and Dowlais Branch.							
Talyllyn		1	3		3		67
Torpantau		1	13	$52\frac{1}{2}$	14	23	66
Dovey Junction to Pwllheli.							
No. 1 Aberdovey		1	81		81		20
No. 2 Aberdovey		1	82		82		21
No. 3 Aberdovey		1	83		84		19
No. 4 Aberdovey · · · · ·		1	84		84		53
2101 - 12001 - 1	1000	1	100	73	100	11	7

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# PASSENGER AND FREIGHT TRAIN INSTRUCTIONS.

# LIST OF TUNNELS (2 CHAINS AND OVER IN LENGTH)-Continued.

Name.				No. of lines	From		То		Length
				through.	Miles. Chains.		Miles. Chains.		in Yards
Ketley Branch.				1	164	301	164	33	59
Horsehay Oxford, Worcester,	Wolverham	nton.	via		101	4			
Worcester.		·F,							00-
Campden				2	97	47	98	71	887
Worcester (Rainboy	w Hill)			2	120	781	121	81/2	212 949
Dudley				2	147	364	147	79‡	949
Ashendon and Aynho	Railway.					202	2	901	193
Brill (Rushwood)				2	2	293	15	$\frac{38\frac{1}{2}}{65}$	1147
Ardley				2	15	13	10	0.0	111/
Worcester and Herefo					190	101	131	391	1567
					130	48 <u>1</u> 48	131	401	1589
				,	130	15	135	751	1323
				1	135	10	100	tot	1020
Kidderminster Loop.				1	136	683	137	104	480
Bewdley		- h.i 1			100	004	10,	102	
Birmingham and Nort	th warwick	snire	Line.	2	8	621	8	701	176
Wood End (Tanwo	orth)			-	0	0-1			
Galton Branch.				2	133	43	133	46	64
Galton			***	-	100	2.0	1		
Halesowen Branch.				1	137	46	137	53	151
Haden Hill Stourbridge Extension	n		***		101				
()ld Hill				2	136	39	136	791	887
Banbury and Chelten	ham Franc	h.							
Hook Norton	III ZICII			1	92	351	92	54	418
Chipping Norton				1	97	36	97	67	685
Andoversford				0	101	623	102	0	384
Cheltenham to Honey	bourne.								
Greet (Wineheom)	e)		***	. 2	12	$25\frac{1}{2}$	12	57	693
Hunting Butts				. 2	19	193	19	$24\frac{1}{4}$	97
Severn Valley Branc									100
Mount Pleasant					136	4	136	$9\frac{1}{2}$	123
Bridgnorth				. 1	150	13	150	38	550
Much Wenlock Brand	ch.							201	100
Presthope				. 1	-166	591/2	166	681	198
Joint Lines.									
G.W. and G.C. Joint.									040
White House					13	121	13	28	348 84
Saunderton (Up Li				. 1	23	303	23	$34\frac{1}{2}$	0+
G.W. and L.M.S. Joi	nt.				1				
Clifton Extension	Line.					10	0	25	289
Montpelier Clifton Down				. 2	0		0 2		1738
Clifton Down		***		. 2	1	52	2	91	1700
Severn and Wye J	oint Line.				1 1 1	011	17	$72\frac{1}{4}$	242
Miery Stock (Ly	dbrook Bra	men)		,	17	611	17 12	70	503
Moselev Green (	Loop Line			1	12	$\frac{47\frac{1}{2}}{62}$	6	5	506
Sararn Bridge				. 1	5	02	0	0	0.70
Shrewsbury and	Hereford J	oint 1	line.	0	07	46	27	52	132
Ludlow	0-0 000	***		. 2	27 42	671	43	353	1051
Dinmore				. 2	42	0/2	40	0.7	2001
Chester and Birke	nhead Join	it Line	Э.	0	13	581	14	31/2	565
Woodside				2	13	201	1.2	- 52	500
	ington Join	It Line	e.	-	10	61	10	101	86
Chester and Warr				2					

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#### PASSENGER AND FREIGHT TRAIN INSTRUCTIONS.

## CONTROL AND DISTRIBUTION OF ROLLING STOCK.

The control and distribution of the Company's Rolling Stock is in charge of the Rolling Stock section of the Office of the Superintendent of the Line at Paddington, which department is also responsible for the working and distribution of sheets and ropes.

As far as Passenger Train stock is concerned, the Superintendent of the Line exercises complete and absolute control, but with regard to freight train vehicles, sheets and ropes, the duty is exercised

in conjunction with the Chief Goods Manager.

For Divisional distribution each Divisional Superintendent has a local Rolling Stock section under his direct supervision. These sections are stationed as follows:-

Worcester Reading Gloucester Bristol Newport Birmingham Chester Exeter Cardiff Oswestry Swansea Plymouth

Station Masters, Goods Agents, &c., must make their applications for carriage stock, wagons, sheets, and ropes to the Rolling Stock section of the division in which their stations are situated. Should the stock ordered not be promptly supplied the requisition must be repeated and followed up with the Divisional Superintendent or District Goods Manager as the case may be.

Each station must forward to the Rolling Stock section of the division by agreed train daily, or

by telegraph or telephone, the undermentioned information on the W.G. return:

(a) The number of carriages, wagons and other vehicles, loaded and empty, on hand at the

station, or in the sidings connected with it.

(b) The number of such vehicles that will be required for use at the station on the following day; and what, if any, can be spared for use elsewhere.
(c) What additional stock, if any, is required.

This information must be sent by train (on Form 2,000A) in all cases where it can be forwarded sufficiently early to reach the Rolling Stock section by 2.0 p.m., so that it may be dealt with the same afternoon. Where this cannot be done, or where the Rolling Stock section requires it, the information must be sent by telegraph (on Form 450-4 or 450-5) or telephone at 4.0 p.m.

Private Owners' Wagons must not appear on these returns.

Station Masters and Goods Agents must, as far as possible, anticipate their requirements and advise their Rolling Stock section in good time of all exceptional demands that are likely to be made upon the stock, and if from any unforeseen circumstances a station runs short of stock, the Rolling Stock section must be immediately advised by telegraph or telephone, in order that arrangements may be made for a supply to be sent.

Station Masters and Goods Agents must see that the Daily Stock Returns are prepared by a competent member of the staff, and the instruction that the return is to be signed by the Officer in charge

or his Chief Clerk to be strictly adhered to.

All stock must be properly and fully defined in accordance with the returns and telegraph codes. In applying for stock, particulars of the date and time required for loading must be given, and the

destination of the traffic whenever possible.

The local Rolling Stock section will instruct the stations daily, by telegraph or otherwise, as to the disposal of the stock not required for use, and all instructions must be promptly acted upon and replied to if necessary. No spare stock must be sent away without instructions from the local Rolling Stock section, or the Office of the Superintendent of the Line at Paddington.

All loaded vehicles must be released as quickly as possible after arrival; and the consignees of station to station traffic must be required to unload the wagons consigned to them as soon as possible

after their receipt.

The vehicles, when unloaded, must at once be made available for forwarding traffic, or placed at

the disposal of the Rolling Stock section if not required for immediate use.

Every effort must be made to place vehicles in position for unloading immediately on arrival, special care being taken to prevent the unloading sidings being blocked with Private Owners' trucks.

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# CONTROL AND DISTRIBUTION OF ROLLING STOCK .- Page 166.

The instructions in G.A.9 under this heading to be cancelled and substituted by the

The existing instructions for the Supply and Distribution of Freight Stock are shown in booklets BR.87237 and 87238 "Standard instructions to Stations and Standard instructions to District Officers" respectively. (G.A.30 Op.-9/54 R.).

#### PREPARATION OF GOODS GUARDS JOURNALS.

The attention of Goods Guards is directed to the importance of accurately filling up and promptly rendering their journals as instructed immediately on completion of a journey.

All entries should be made in a clear and distinct manner.

#### MARSHALLING.

Instructions as to the correct method of marshalling trains running through two or more divisions are contained in the pumphlets issued for the purpose. Local trains should be dealt with in accordance with the instructions issued by the Divisional Superintendents or District Traffic Managers.

The following should be noted:-

1. Shunting at local stations by train engines should be reduced to a minimum, and in order to assist in this connection, various trains are shewn in the marshalling instructions pamphlet to attach traffic at one shunt. When a train is detained at a local station waiting a margin to proceed, or from any other cause, advantage should be taken of such periods to marshal the train as far as possible in such a way as will assist the work in the marshalling yards.

The work at the stations where trains are timed to call varies considerably, and in many cases

it can be performed in less time than is shewn in the schedules. It is expected that advantage will

be taken of this to expedite the running.

Station Masters must personally, where practicable, supervise freight train working and make

such arrangements as will obviate waste of power.

2. Trains should be confined to wagons for destinations specified in the Marshalling Instructions pamphlet and must be formed in the order shewn.

3. Unless authority to the contrary is given by the Divisional Superintendents or Controls concerned, the formation of trains must not be varied from that shewn in the Marshalling Instructions pamphlet, neither must trains stop for traffic purposes at stations except where booked to do so. Any difficulty experienced in carrying out the marshalling shown in the pamphlets should be reported to the Divisional Superintendent or District Traffic Manager.

4. The ruling principle is that through traffic must be worked by through trains to destination

and not short to intermediate stations.

5. Traffic for the longest distances must have preference by through trains.

6. Wagons containing explosives must always be marshalled as near the middle of the train as possible.

#### CONVEYANCE OF PASSENGER TRAIN STOCK ON FREIGHT TRAINS.

Passenger vehicles 65 feet or more in length must not be conveyed by freight train

2. Passenger coaches, passenger brake vans and parcel vans must not be conveyed by freight trains of 60 wagons or over. If absolutely necessary for traffic reasons one vehicle only, either a scenery van, 8-wheeled carriage truck or siphon F, G, H, or J may be conveyed.

The following to be substituted for clauses 1, 3 and 4:-

1. Coaching Stock exceeding 65ft. in length may be conveyed on a through freight train, with "F" or inferior headcode only, subject to being formed next in front of the rear Goods Brake Van, with an additional Goods Brake Van or 15-ton open Goods (TUBE) wagon, or other vehicle 24ft. over headstocks or longer, formed next in front of the coaching stock.

Freight trains conveying such coaching stock must not enter sidings not normally used for such stock, and the stock must not be conveyed on trains passing by routes over which the stock is prohibited except when notified by the Operating Superintendent giving Civil Engineer's Conditions of passage.

This instruction does not apply to stock passing from private building works, particulars of which are notified by the Operating Superintendent as out of gauge or exceptional loads with e the applicable conditions of passage.

3. If essential for traffic reasons 8-wheel passenger train vehicles when conveyed by freight 1) trains must be formed immediately in front of the brake van except in the case of local trains and er pilot trips conveying not more than equal to 25 wagons of Class I traffic, when they may be placed in any position on the train.

If essential for traffic reasons 4 and 6-wheel passenger train vehicles may be formed in any ig position on freight trains.

(G.A.30 Op.-9/54 Ex.5299/52)

The following to be added as clause 6:-

6. Empty Parcels Vans Siphons 'F', 'G', 'H' or 'J' must not be conveyed on freight trains except where essential for traffic requirements on Branch Line services as arranged by the local District Officer. (G.A.30 Op.-9/54 E.1088H (2-C) R.8/-)

SECTION II (c)

SECTION III ( CHANDISE

The following the General Appe

> Existing Code Word

Fruit Fruit A Fruit B Fruit C .. Fruit D ...

Macaw Macaw A Macaw B Macaw C Macaw D Macaw E Macaw F Macaw J Macaw G & H Mex Mex B Mica Mica A & B Mink Mink A .. Mink B .. Mink C & D Mink F & G

Serpent Serpent ( Serpent D

Mite B ..

Open A ..

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- (1) No train consisting of more than 70 freight vehicles (excluding the brake van, may have any vehicles coupled with the vacuum-brake pipes to the engine.
- (2) No freight train may have more than 60 loose coupled vehicles (including the brake van) behind the vacuum-braked vehicles connected with the vacuum pipe to the engine. If this number be exceeded, the train must be run as a non-vacuum train.
- (3) Shunting.—Vacuum-braked vehicles which are connected with the vacuum pipe to the engine are to be kept on the engine during shunting operations in the same way as a shunting truck, where this can be done without delaying trains or disorganising the vard working.

SECTION II (c)—INSTRUCTIONS CONCERNING FREIGHT TRAINS.—page 167, and SECTION III (a)—INSTRUCTIONS CONCERNING LOADING, CONVEYANCE, ETC., OF MERCHANDISE TRAFFIC.—Page 188.

The following alterations in Rolling Stock code words have been made and references to such code words in the General Appendix to be amended accordingly:—

Existing Code Word	New Code Word	Interpretation
Fruit	Pasfruit	Passenger Fruit Van, 10ft. wheel base, load 6 tons.
Fruit A	Fruit	Ventilated Fruit Van.
Fruit B	Banana	Insulated Banana Van.
Fruit C	Pasfruit C	Passenger fruit van, gas lit, 12ft, 6in. wheel base, load 10 tons.
Fruit D	Pasfruit D	Passenger fruit van, gas lit, 28ft. 6in. long, over body, 18ft. wheel base, load 10 tons.
Macaw	Single	Single Bolster Wagon.
Macaw A	Double	Double Bolster wagon.
Macaw B	Bobol C	Bogie Bolster wagon with length over headstocks 45-47ft. 6ins.
Macaw C	Borail C	Eight-wheel bogie timber truck (30 tons) 70ft. long.
Macaw D	Borail D	Fight-wheel hogie timber truck (40 tons), 45tt, long, extreme width 8ft, 8ins.
Macaw E	Borail B	
Macaw F	Double	Double bolster wagon.
Macaw J	Borail F	Eight-wheel bogie bolster truck (40 tons) 62ft. long.
Macaw G & H	Bobol A	Bogie Bolster wagon with length over headstocks 33-35ft.
Mex	Ox	
Mex B	Oxfit	Cattle Wagon. V.B. complete.
Mica	. Meat	
Mica A & B .	Ins	Insulated Meat Van.
Mink	. Van	Covered Goods Van, 12 tons and under.
Mink A	. Vanfit	Covered Goods Van, 12 tons and under. V.B.
Mink B		
Mink C & D .	. Vanfit	Covered Goods Van, 12 tons and under. V.B.
Mink F & G .		
Mite		
Mite B	. Twin	Twin Bolster wagons.
Open		High sided Goods wagon (over three planks).
Open A	. Hybar	High-sided Goods Wagon fitted with permanent sheet supporter.
Open B	. Hybarfit	High-sided Goods wagon fitted with permanent sheet supporter V.B.
Open C	. Tube	Long wagon with inside length varying between 20ft 62 ins. and 30ft, 12 ins.
		inside height varying between 2ft. 4ins. and 3ft. 13ins., for carrying tubes.
Serpent	. Cartruck	Open Carriage Truck under 20ft, in length (freight stock).
Serpent C .	000	Open Carriage Truck under 20ft, in length V.B. (treight stock).
Berpent D .	. Cartruck A	O C .: T 200t in langth (traight stock)

(G.A.15-12/44 L.K.1/7433/5A)

# CONVEYANCE OF PASSENGER TRAIN STOCK ON FREIGHT TRAINS.—Page 168.

The following to be inserted as Clause 5:—
"Passenger coaches and passenger brake vans should not be conveyed on freight trains without the consent of the Control Offices concerned. The freight trains scheduled in the Marshalling Instructions for the conveyance of coaching stock are excepted from this arrangement."

(C. A.1. 3/37, E.75344/5.G.)

Passed vehicles 65 ft. o more in length over the buffers (except stock passing from private build; works, particulars of which are notified by the Operating Superintendent as out of gauge exceptional loads with the applicable conditions of passage) must not be conveyed by Freight (GA 19—10.48 F 86061/H.)

## CONVEYANCE OF PASSENGER TRAIN STOCK ON FREIGHT TRAINS .- Page 168.

The following to be added as paragraphs 7 and 8:-

- Eight-wheel ex-passenger stock converted as tool vans, mess vans, etc., when conveyed on freight trains must be formed immediately in front of the brake-van or with other 8-wheel vehicles so formed.
- 2. Four- or 6-wheel ex-passenger stock converted as tool vans, mess vans, etc., may be formed in any position on freight trains, but unless piped or fitted with the automatic vacuum brake they cannot be conveyed on fully fitted trains, nor in the fitted portion of a partially fitted train.

(G.A.31-7/56 L.K.1/12693/232)

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Reference to the following to be made on page 168:-

RIVERS' TICKETS. It is very important that Drivers' Tickets are correctly and fully completed as the information is the basis of Railway Statistics called for by the Transport Act of 1947. There is evidence to show that Column 13 "Description of Trip" is not being accurately compiled in that "loaded" or "empty" freight trains are not being properly described. It is important to remember that if there are one or more loaded wagons on a train, that train must be regarded as a "loaded" train.

Guards are responsible for informing Drivers as to the "Description of Trip" (Column 13 of Drivers' Ticket) and the number of wagons and the equivalent load (Column 21 of Drivers' Ticket). It is imperative that care should be taken to see that the Driver is properly informed as to:-

- (a) Whether the train is "loaded" or "empty". Number of vehicles and equivalent number of wagons at starting point.
- (G.A.30 Op.-9/54 E.).
- (c) Any alterations to load en route.

SCREW COUPLINGS ON VEHICLES FORMED IN FREIGHT TRAINS-page 169. The following to be added as the first paragraph of these instructions :-In the event of two vehicles, one fitted with screw couplings and the other with Instanter couplings coming together, the latter should be used for joining up whether formed in the vacuum portion or not, but in the case of two vehicles being together, one with screw couplings and the other with ordinary three-link couplings, the former should be used for joining up if formed in the vacuum portion, but not otherwise.

(G.A.3.—12/37. E.72899.H.)

Reference to the following to be made on page 169:-

# WORKING OF ENGINEERING DEPARTMENT SLEEPER TRAFFIC.

Trains arranged for conveyance of sleepers, whether loaded in sleeper wagons or crocodiles (also odd wagon

oads or small lots), must conform with the following conditions:

Timing .. Not to exceed speed for "ordinary" freight trains, viz. 25 m.p.h.

Loads .. Not to exceed those authorised for "mineral" trains, subject to the following restriction in the case of trains comprising sleeper crocodiles:

Group A. engines restricted to 20 loaded crocodiles.

,, 23 ,, 25 ,, 30 D. ., 40 E.

\* 20 ton brake vans to be provided when these loads are conveyed.

All sleeper carrying vehicles to be calculated in accordance with the table shown in the Service Time Tables (G.A.1. 3/37. E.53194.H.) headed "Calculation of Special Wagons-Loaded and Empty.

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SCREW COUPLINGS ON VEHICLES FORMED IN FREIGHT TRAINS—page 169.

SCREW COUPLINGS ON VEHICLES FORMED IN FREIGHT TRAINS-Page 169.

The following to be substituted for the instructions appearing on page 169 and the amplification contained in Circular G.A.5 :-

Vehicles working in the vacuum portion of the partly vacuum fitted freight trains, are fitted with either screw couplings or Instanter couplings. The screw couplings, if fitted, must be used.

The screw couplings, however, must not be used in non-vacuum freight trains, nor in the nonvacuum portion of a partly vacuum-fitted freight train, unless both vehicles to be coupled are fitted with screw couplings, or difficulty would arise in using the three link couplings owing to difference in length of buffers. If, however, considerable delay would occur in attaching or detaching the brake

van if the link coupling were used, the screw coupling of the last wagon, if so fitted, may be used.

(G.A.19—10/48. L.K.1/9003/Gen.4.)

Consequence of the difference in the length of the buffers, it is not possible to get the link coupling over the drawbar-hook of the screw fitted vehicle until the buffers have been considerably compressed and considerable telay in attaching and detaining would result, it should be understood that the rule may be refixed, provided the screw coupling it well oiled and works heely, so that it is not likely to lift off the drawbar-hook on the journey.

INSTRUCTIONS FOR WORKING BALLAST TRAINS.

INSTRUCTIONS FOR WORKING BALLAST TRAINS.—Page 169.

The instructions under this heading to be cancelled and the following substituted:-

INSTRUCTIONS FOR WORKING OF CIVIL ENGINEERING DEPARTMENT BALLAST AND MATERIAL TRAINS.

Additional instructions relating to the working of fully fitted Ballast trains are shown under separate heading "Fully fitted trains with Ballast loaded in Engineering Department Hopper Wagons and Plough or Freight Brake Van from Quarries to unloading sites and return of empty Hoppers.

Arrangements for running.

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194.H.)

The District Operating Superintendent or District Traffic Superintendent from whose District the trains will start will make all arrangements for the running, and advise the other District Operating Superintendents concerned. In his requirements, the District Engineer must indicate the approximate duration of the Engineering Department work at site or sites to enable the Operating Department to arrange relief of trainmen when necessary.

Any emergency arrangements after office hours will be made by the respective Control Offices

Engine Power.

The District Operating Superintendent will arrange with the District Motive Power Superintendent for the provision of the necessary engine power.

When making application for the running of these trains or engines in connection with relaying, reconstruction of bridges, or other Engineering Department work, the District Engineer must advise the District Operating Superintendent or District Traffic Superintendent (giving type of engine or engines if necessary), stating whether during any interval, the engines may be utilised for other purposes, and, whenever possible, name the time when it is expected an engine will be available to return from the site of the work.

picking up men for unloading.

When these trains are required to call at places en route to pick up men for unloading the District Operating Superintendent or District Traffic Superintendent must be advised at the time arrangements are made for the running of the train.

order to avoid auplication of telegraphic advices, e.g. of the cancellation of trains, telegrams, where circumstances permit, may be despatched in the joint names of the Divisional Officers concerned.

The Traffic Department will arrange with the Locomotive Department for the Engine Fower, provision of the necessary engine power, but before ordering an engine to work a special ballast train, engines should be made as to whether an engine returning to its home station is available to run in the direction of the ballast train, and can work

When making application for the running of trains or engines in connection with When making application for the funning of trains or engines in connection with relaying, reconstruction of bridges, or other Engineering Department work, the Divisional Engineer must advise the Divisional Superintendent or District Traffic Manager giving type, if practicable) and whether, during any interval, the engines may be utilised for other purposes, and, whenever possible, name the time when it is expected an engine will be available to return from the site of the work.

The running of ballist trains on main passenger train routes should be curtailed as funning of Ballist Trains are as possible throughout the period of summer traffic (during Bank Holiday periods, during summer period.), especially on Mondays, Fridays and Saturdays.

When ballast trains are required to call at places en foute to pick up men for Ballast trains unloading, the Divisional Superinterment or District Traffic Manager must be advised picking up men at the time arrangements are made for the running of the train.

# INSTRUCTIONS FOR WORKING BALLAST TRAINS-Continued.

Instructions to Guards. and Signalmen as to site for unloading. The section or station and mileage of the site with a description of the line at which such a train is to be worked must be shown in the Civil Engineer's weekly ballast programmes, Speed and Engineering Notice, or other notice, and the guard of the train must be instructed accordingly. Where it is necessary to unload some or all the material on another site and time will not admit of the Guard being notified in the usual way, information must be given by the Permanent Way Inspector or Ganger to the Signalman at the commencement of the section affected, and he in turn must inform the Guard of the train accordingly.

Ballast not to be deposited on points, rodding wires, connections, or A.T.C. Ramps.

When ballast or other material is being unloaded, great care must be exercised to see that it is not deposited on point rodding wires or connections, or automatic train control ramps.

Wagons put off for repairs. When an empty wagon is put off for repairs it should be labelled by the Guard to the home station, and when a loaded wagon is detached for repairs it is to be labelled to the destination of the train.

Should any loaded wagons be put off for repairs, etc., the Guard must give particulars in writing to the man responsible for unloading at destination, in order that he may be aware that the complete train has not arrived, thus obviating the risk of error in the certification of the advice note.

The guard must show on the back of the engine ticket (form No. 2009), Engineering Department service vehicles, wagons put off for repairs, etc., giving the name of station and wagon numbers, and stating if loaded or empty and how labelled.

Guards to examine trains.

Guards must examine their trains immediately after unloading or on taking charge, also before proceeding to or leaving a Contractor's temporary road, and in the event of a wagon having been damaged, a report of the circumstances must be sent to the District Operating Superintendent or District Traffic Superintendent.

Reversing on incline. Trains which have to reverse on a steep incline must be provided with a brake van at each end. The provisions of Rule 151 must be observed.

Propelling on incline.

The amplification of Rule 149, exception (vii) as shown on page 20 of the General Appendix to the Rule Book to apply.

Marking of wagons.

The marking of Engineering Department wagons is not to be altered without authority from the Civil Engineer.

Guard's Journal. The Guard of each train must send a sectional journal on form B.R.87210 to the Operating Superintendent of each District through which his train passes.

Journals need not be rendered to the Engineering Department.

Engine Ticket. The Guard of each train must prepare engine ticket (form No. 2009), including on the back thereof particulars of the loading of the train, and immediately the last journey for the day is completed, it must be forwarded to the District Engineer in whose District the train originated.

The Guard must also give an engine ticket (form 2009A) to each Driver for the time the Driver was on the train.

Each Driver must obtain from the Guard an engine ticket and attach it to his daily record.

Guards are responsible for rendering an engine ticket to the Driver after the completion of the work, covering the whole of the time the engine has been in traffic. The Driver must communicate with the Guard after completion of the work in order to obtain this engine ticket.

Engines returning home.

When engines which have been used for working loaded trains are not required on the return journey and return home light, the time will be debited to the Engineering Department, but should they be utilised for ordinary traffic working no charge will be made.

Guard to inform Driver formation and number of wagons. Before starting, the Guard must inform the Driver in charge the total number of wagons on the train, and what proportion of it is composed of vacuum fitted wagons, the brakes of which can be applied from the engine. VACUUM-FITTED VEHICLES MUST BE FORMED NEXT TO THE ENGINE.

Vacuum pipes out of use. On arrival at site when these vehicles are disconnected the Guard must see that any loose vacuum pipes are placed on the stop plugs provided.

Headcodes to be carried.

Engineering Department trains must carry "F" headcodes except in the case of trains fully or partially vacuum fitted which should carry "C," "D" or "E" headcodes respectively.

Reference to the following to be made on page 171.

# FULLY FITTED TRAINS WITH BALLAST LOADED IN ENGINEERING DEPARTMENT HOPPER WAGONS AND PLOUGH OR FREIGHT BRAKE VAN FROM QUARRIES TO UNLOADING SITES AND RETURN OF EMPTY HOPPERS.

Loaded trains must run under the following conditions:-

- (i) Carry "C" headcode.
- (ii) Maximum speed-50 m.p.h.
- (iii) Engine provided to have not less than 5ft. 8in. diameter coupled wheels.
- (iv) Vacuum brake on all wagons to be operative, also plough or freight brake van to be fitted or piped.
- (v) It will not be necessary to observe incline instructions.
- (vi) Maximum load not to exceed 22 20-ton loaded Engineering Department hopper wagons. On rising gradients the standard loading for Class I traffic shown in the Service Time Tables must not be exceeded. Engineering Department 20-ton hoppers loaded with ballast to be calculated as "Two equals Three I0-ton wagons of Class I traffic."
- (vii) Instanter couplings must be in short position.

Procedure to be adopted in arranging trains. The Civil Engineer will issue a weekly programme to all concerned on Tuesdays showing the requirements for fully fitted trains of ballast to run from the following Monday to Sunday (both days inclusive).

The District Operating Superintendent or District Traffic Superintendent concerned with the despatch of the ballast must confer with the District Engineer and agree departure time of trains, according to the unloading point for all the trains listed and advise other District Operating or Traffic Superintendents concerned by telegram on Wednesdays full details of the trains arranged for the following week,

The District Engineer involved with the unloading of the ballast to consult the District Operating Superintendent or District Traffic Superintendent in whose District the unloading will take place, on Wednesdays and agree at what time this can be accomplished. The District Operating Superintendent or District Traffic Superintendent will order a fresh engine, if this is necessary, to undertake the work of discharging the ballast and bringing back the empties, and arrange departure time for a special train conveying the empty hoppers back to the Quarry.

Train Schedules. Standard schedules will be laid down for these trains, where practicable, and must be adhered to. In the case of journeys where a standard schedule cannot be applied, throughout timings must be pre-arranged in the case of loaded trains to arrive at unloading site by 7.30 a.m. unless otherwise specified in the Civil Engineer's weekly programme.

District Operating Notices. Details of the loaded and empty trains required to run including timings must be issued in the District Operating or Traffic Superintendent's notices, copies of which must be sent to the Operating Superintendent's T.D. Section.

Engine Power and trainmen.

In general where the transit is within 100 miles of the quarry, an engine and trainmen must be diagrammed to cover the loaded and empty journeys throughout, although in the case of branch lines it may be necessary to utilise a smaller type of engine from the junction in which case the engine used to convey the ballast from the Quarry, or nearby marshalling yard, must be available for working back the empty hoppers to the Quarry.

has been placed in position the plough is lowered to the rails, but not screwed hard down. That the French keys in the centre and right-hand levers of the wagons are then removed and the key of the left-hand lever kept in.

That the hopper wagon next to the engine is discharged first, the door being opened by removing the key from the left-hand lover and the continuous continuous and the continuous continuou

Lubrication.

The lubrication, which includes doors, pins and slides of hopper ballast wagons and plough vans will be done by the C. & W. Engineer's Department staff at the loading points.

Closing doors of hopper wagons. Should any difficulty be experienced in closing the doors of hopper wagons, force must not be used, but the doors swung back against the stop on the solebar to dislodge any fine ballast from the ledge on the "Z" iron.

(G.A.30 Op.—9/54. E.82672H. (12-C.).

(f) Before proceeding with the empty train the doors of the hoppers of the wagons are closed and secured with the French keys which are to be properly fixed in the three levers, the keys being inserted in the holes from the left-hand side.

In the case of longer distance transits or where it is considered guaranteed arrivals at unloading sites within reasonable limits are uncertain, it is desirable that ballast should be despatched from Quarries as soon as possible after loading has been completed, and be stabled at a convenient point near to unloading site. The Operating Superintendent will diagram engine and trainmen to work to stabling point and back to the Quarry.

A fresh engine and trainmen will be diagrammed to carry out the unloading of the ballast unless the District Engineer has made other arrangements.

Prior arrangements must be made in order to ensure that enginemen and guards are available to work the loaded and empty trains at the recognised changing points en route.

eturn of mpty hoppers o Quarries.

Owing to limited supply of hopper wagons for ballast traffic, it is important no delay occurs in returning these wagons, when empty, to Quarries in order that the programmed supplies of ballast may be maintained, and movement must be undertaken by special trains.

mpty hopper

Empty trains may run under "C" headcode freight train conditions not exceeding 50 20-ton hoppers. Engineering Department hopper wagons being calculated on the basis of 20/21 ton wagons shown in the Service Time Tables.

# ORKING OF HOPPER BALLAST WAGONS AND PLOUGH.

uard to iderstand orking of ipper trains. Whenever practicable, a loaded hopper ballast train must be worked by a Guard who from previous instruction and experience, is qualified to take charge.

orking of ough to be dertaken Guard. The working of the hopper ballast plough for spreading the ballast is to be undertaken by the Guard, under direction of the Permanent Way Inspector or Sub-Inspector.

here ploughs a not to used. The hopper ballast plough is not to be used in station yards.

ried by pper trains. Hopper wagons are only to be loaded with material as authorised by the Civil Engineer and the plough must not be employed for spreading material other than crushed slag or stone, gravel and ashes suitable for top ballast.

scharging last from pper trains. When required to be unloaded on lines already opened for traffic, the Permanent Way Inspector of the district will give instructions where the ballast is to be discharged, but the Guard of the train and the Ganger of the length must satisfy themselves that there are no fittings, catch points, guard rails, signal wires, locking gear rods, detonator machines, automatic train control ramps, level crossings, or other works with which the plough can possibly come in contact on the lengths of line to be ballasted.

ponsibilities Guards charge of oper train.

The Guard will be held solely responsible for the proper working of the train and for satisfying himself that:—

- (a) Hopper wagons and plough vans are in good order and examined at the usual examining points en route.
- (b) When not engaged in unloading, the plough is in its normal position by being raised to the fullest extent, and the two keys inserted and secured by padlock.
- (c) When starting on a journey with a train which has to be unloaded in a tunnel, the plough is in the right direction, also that the hoppers are arranged so that all the levers are one side of the train, and that where unloading has to be performed in a double line tunnel, all levers face the 6-ft. way.
- (d) Upon arrival of the train at the place where it is intended to unload, and after it has been placed in position the plough is lowered to the rails, but not screwed hard down. That the French keys in the centre and right-hand levers of the wagons are then removed and the key of the left-hand lever kept in.

That the hopper wagon next to the engine is discharged first, the door being opened by removing the key from the left-hand lever, and the engine started at a speed of not more than four miles per hour. Before the whole of the contents of the first wagon to be discharged are run out, that the hopper door of the next wagon to it is lowered, and so on through the train.

The Permanent Way Inspector or Ganger will assist in the discharge of the train, one being on one side of the train and the Guard on the other.

- (e) When the contents of the whole train have been spread, and before resuming the journey, the plough is cleaned with the scrapers which are kept in the plough van, then raised to the fullest extent and secured by means of the keys and padlock.
- (f) Before proceeding with the empty train the doors of the hoppers of the wagons are closed and secured with the French keys which are to be properly fixed in the three levers, the keys being inserted in the holes from the left-hand side.

# INSTRUCTIONS FOR WORKING BALLAST TRAINS-Continued.

itopping at itop Boards to pin down prakes, and at out of nclines.

ALL TRAINS WITH THE EXCEPTION OF-THOSE CARRYING "C" HEADCODE, MUST COMPLY WITH THE INCLINE INSTRUCTIONS AS SHOWN ON PAGES 178 AND 180 OF THE GENERAL APPENDIX TO THE RULE BOOK, MUST STOP AT STOP BOARDS AND PIN DOWN BRAKES AS REQUIRED IN ACCORDANCE WITH THE "GENERAL INSTRUCTIONS FOR WORKING INCLINES."

Co-operation secween Operating and ingineering Departments. It is of importance that close co-operation should exist between the District Operating Superintendents and District Engineers so that Engineering Department trains shall be scheduled at a time when they will cause least interference with the ordinary booked train services. This is especially necessary where the trains have to work on, or pass over, single lines.

lunning of rains during ummer. On main passenger train routes the running should be curtailed as far as possible throughout the period of summer traffic (during Bank Holiday periods, etc.) especially on Mondays, Fridays and Saturdays.

(G.A.30 Op .- 9/54 E.8267 H(12-C. ).

and the regulations will admit of this being done.

Freight or ballast trains, wholly or partly composed of vacuum fitted stock, not be coupled to trains not provided with the vacuum brake except in of GREAT EMERGENCY when the vacuum brake pipe must be disconnected from leading engine and both trains worked as ordinary non-vacuum fitted trains.

Ballast Guards are responsible for rendering a ballasting ticket to the Driver the completion of the work, covering the whole of the time the engine has been brivers of ballast trains must communicate with the Guard after completion work in order to obtain the ballasting ticket and attach same to the daily

It is of importance that close co-operation should exist between the Traffic and Departments, so that ballast trains shall be scheduled at a time they will cause least interference with the ordinary booked train services. This especially necessary where the trains have to work on, or pass over, single lines.

Co-operation between Traffic and Engineering Departments.

together.

# WORKING OF HOPPER BALLAST WAGONS AND PLOUGH.

Whenever practicable, a loaded hopper ballast train must be worked by a Guard from previous instruction and experience, is qualified to take charge.

The working of the hopper ballast plough for spreading the ballast is only to be dertaken by the Guard, and each van is to contain a notice to this effect.

The hopper ballast plough is not to be used in station yards, nor on any but the sleeper road, keyed on the outside.

Hopper wagons are only to be loaded with material suitable for top ballast, and plough must not be employed for spreading material other than crushed slag or gravel and ashes suitable for top ballast.

When required to be unloaded on lines already opened for traffic, the Permanent Inspector of the district will give instructions where the ballast is to be district, but the Guard of the train and the Ganger of the length must satisfy selves that there are no fittings, catch points, guar rails, signal wires, locking rods, detonator machines, automatic train control ramps, level crossings, or works with which the plough can possibly come in contact on the lengths of the beballasted.

The Guard will be held solely responsible for the proper working of the train and a satisfying himself that:—

(a) Hopper wagons and plough vans are in good order and examined at the usual examining points en route.

(b) When not engaged in unloading, the plough is in its normal position by being raised to the fullest extent, and the two keys inserted and secured by padlock.

(c) Refore the hopper wagons are taken to be loaded, the French keys are properly fixed in the three levers, the keys being inserted in the holes from the left hand side.

Guard to understand working of hopper train.
Working of plough to be undertaken by Guard.
Where ploughs are not to be used.

Materials carried by hopper trains.

Discharging ballast from hopper trains.

Responsibilities of Guard in charge of hopper train.

#### WORKING OF HOPPER BALLAST WAGONS AND PLOUGH-Continued.

(d) When starting on a journey with a train which has to be unloaded in a tunnel, the plough is in the right direction, also that the hoppers are arranged so that all the levers are one side of the train, and that where the unloading has to be performed in a double line tunnel, all the levers face the 6-ft. way.

(e) Upon arrival of the train at the place where it is intended to unload, and after it has been placed in position, the plough is lowered to the rails, but not screwed hard down. That the French keys in the centre and right-hand levers of the wagons are then removed and the key of the left-hand lever kept in. That the hopper wagon next to the engine is discharged first, the door being opened by removing the key from the left hand lever, and the engine started at a speed of not more than four miles per hour. Before the whole of the contents of the first wagon to be discharged are run out, that the hopper door of the next wagon to it is lowered, and so on through the train.

The Permanent Way Inspector or Ganger will assist in the discharge of the

train, one being on one side of the train and the Guard on the other.

(/) When the contents of the whole train have been spread, and before resuming the journey, the plough is cleaned with the scrapers which are kept in the plough van, then raised to the fullest extent and secured by means of the keys and padlock.

(q) Before proceeding with the empty train the doors of the hoppers of the

wagons are closed and secured with the French keys.

Inbrication.

The lubrication, which includes doors, pins and slides of hopper ballast wagons and plough vans will be done by the Chief Mechanical Engineer's Department at the loading points.

Closing doors of hopper wagons.

Should any difficulty be experienced in closing the doors of hopper wagons, force must not be used, but the doors swung back against the stop on the solebar to dislodge any fine ballast from the ledge on the "Z" iron which may be resting there.

#### BALLAST TRAINS WORKING IN SECTION AND BACKING.

Where it is necessary for a ballast train, which has to perform work in a block section, to move back a short distance in the wrong direction for the purpose of ploughing out the ballast, or for any other object, such movements in the wrong direction must be made with great care, especially on sections of the line where gradients exist and where catch points are fixed. The Guard in charge will be held responsible for seeing that the second Guard (or if no other Guard accompanies the Train, some other competent man) is in the van ready to apply the brake whilst the wrong direction movement is being made. The Guard in charge must remain outside, as may be necessary, in order to fulfil his responsibilities in connection with the discharge of the wagons, and he must also keep a sharp look out for eatch points.

Under no circumstances must a ballast train set back in the wrong direction to plough out ballast

Attention is directed to the instructions set out on page 265 in regard to securing catch points before a train is run over them in a facing direction.

#### WORKING OF VACUUM AND PARTLY VACUUM FITTED FREIGHT TRAINS.

# Freight Trains carrying "D" Headlands

Freight trains composed partly of vacuum brake-fitted vehicles booked to carry "D" head and run at the times set out in the Service Time Tables for such trains, which are based on average speeds of 40 to 45 m.p.h., and a maximum under suitable conditions of 55 m.p.h., must be formed and worked in accordance with the following regulations:—

(1) All the vehicles formed in a partly vacuum-braked freight train carrying "C" headlamp must be fitted with OIL AXLE-BOXES. Note.—On-certain trains oil axle-boxes packed with horse hair or waste-are prohibited.

(2) The MAXIMUM number of vehicles which may be formed on a partly vacuum-fitted freight train is 70, excluding the brake van.

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# ORKING OF VACUUM AND PARTLY VACUUM FITTED FREIGHT TRAINS -page 172.

The following to precede Clause (A) of the existing instructions:-

Freight Trains carrying "C" Headcodes. The working conditions applicable to "C" Headcode Freight Trains are:-

- 1. Maximum load equal 50 wagons (10-ton carrying capacity) of Class 3 traffic exclusive of brake van.

  - 2. Maximum speed-60 miles per hour. 3. Normal vacuum reading to be not more than 26 inches and not less than 23 inches
- recorded on the engine gauge and not less than 18 inches on the goods brake van gauge. 4. Fixed goods broke wan must be provided? Pyet or fitted freight train brake range A3D
  - 5. May run 160 miles without intermediate examination. 6. The vacuum brake to be operative on not fewer than 75 per cent. of the total
- vehicles on the train, excluding the goods brake van.
- 7. They will not be required to stop at Stop Boards. 8. Where assistance is necessary this to be performed under the same conditions applicable to ordinary freight trains, except that the bank engine vacuum pipe must be
- coupled to the train, when the assisting engine is coupled in the rear. 9. Special "Z" stop at Pilning for Severn Tunnel examination (not Carriage and Wagon Department examination) to be made as in the case of other freight trains.
- Smilar arrangement to apply at Severn Tunnel Junction in respect to Up Freight Trains. 10. Wagons conveyed must all be vacuum fitted or piped and fitted with oil axle boxes not packed with horsehair or waste.

Existing Clause "(A) Freight Trains carrying 'D' Headcodes" to be relettered "(B)." Existing "(B) Freight Trains carrying "E" Headcodes" to be relettered "(C)." (G.A.29.Op.-5/52. E.89083.H. (30-G.).)

SECURING OF BOTTOM DOORS—HOPPER WAGONS, Etc.

The attention of all concerned is drawn to the importance of the French keys or locking pins which secure the bottom doors of Hopper wagons, etc., being in the proper position to prevent the doors opening

in transit and depositing some, or all, of the load upon the track. The French keys, with a small nib which registers with a slot in the plate through which the key is passed, must be positioned so that the keys require to be turned through an angle (up to a full half-turn in some cases) before they can be withdrawn. Inspection should easily show if there is any likelihood of the key working out because it has not been turned round to the "secure" position, i.e. the nib and slot should be out of line whilst the vehicle is in traffic.

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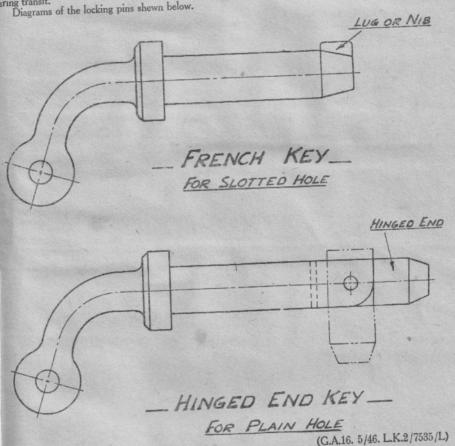
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t the force lodge Similarly, locking pins, with a hinged end, should be seen to be passed right through the hole so that the end is dropped into the "secure" position.

The pins or keys are sometimes behind the solebar and not easily visible without looking over or under the top or bottom of the solebar, but in all cases these bottom doors are locked with a locking pin, and though it is the responsibility of the loader to see that the pins are properly secured at the time of loading, it is the duty of the guards and examiners to see that the pins have not worked round to the "withdraw" position during transit.



Note.—Or certain trains oil gate-boxes packed with horse hair or waste are prohibited. These trains are marked with a B in the service timetables, which significant that in addition to the restriction regarding axle-exes packed with horse hair or waste, the trains travel laster than the average speeds of 40-45 miles per hoar and or travel more than 112 miles without examination.

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# WORKING OF VACUUM AND PARTLY VACUUM FITTED FREIGHT TRAINS-Page 173

The following to be inserted immediately after note "Z":-

Note. For the purpose of giving effect to this instruction the minimum number of wagons to be coupled with the vacuum pipe to the train engine to be based on the equivalent load for Class 3 traffic, e.g., should a load of 44 wagons of Classes 2 and 3 traffic be equal to 60 wagons of Class 3 traffic the minimum number of wagons to be coupled with the vacuum pipe to the engine to be 20 and not 15.

(G.A.30 Op.—9/54. E.84669H. (2-C.)).

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Clause (1) of the instructions under heading "(B) Freight Trains carrying "Head to be soon page 173 to be amended to read :

(1) A number of loaded vacuum brake fitted vehicles (or empty wagons as provided for in the note below

must be coupled with the vacuum pipe to the train engine in accordance with the following table

No alteration to be made in the table.

The following to be inserted after the table at the end of Clause (1): Note. In the event of empty wagons being included in the fitted portion of a "E" headlestrain the number coupled to the engine by the vacuum pipe must not be fewer than one-sixth (with a minimum of 7) or more than one-half of the total number of vehicles on the train.

(G.A. 18. 11/47. E.84563H.)

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(2) In the event of the MINIMUM proportion of loaded vacuum-prakeu venue.

# WORKING OF VACUUM AND PARTLY FITTED FREIGHT TRAINS-continued.

(3) The Minimum and Maximum proportions of vacuum brake-fitted vehicles required to be connected with the vacuum pipe to the train engine, if the train is to run at its booked speed, except in cases where a different proportion is specially authorised, are given below:

No. of vehicles on train (excluding Brake Van).	vehicles re-	uum-braked quired to be h the vacuum train engine.	No. of vehicles on train (excluding Brake Van).	No. of Vacuum-braked vehicles required to be coupled with the vacuum pipe to the train engine.		
Not exceeding	Minimum.	Maximum.	Not exceeding	Minimum.	Maximum.	
	5		40 wagons	13	20	
5 wagons	6	See	42 ,,	14	21	
10 ,,	7	Note Z	44 ,,	15	22	
12 ,,	7	below.	40	15	23	
14 ,,	0	Dolow.	40	16	24	
16 ,,	0	11	50	17	25	
18 "	8	11	F0	17	26	
20 ,,	9	12	~1	18	27	
22 ,,	9	12	=0	19	28	
24 "	9		50	19	29	
26 ,,	10	13		20	30	
28 ,,	10	14	60 ,,	21	31	
30 ,,	10	15	62 ,,	21	32	
32 ,,	11	16	64 ,,	21 22	33	
34 ,,	11_	17	66 ,,		34	
36 ,,	12	18	68 ,,	23	35	
38 ,,	13	19	70 ,,	24	30	

Z-The whole of the train may if considered necessary be coupled with vacuum pipe to the train engine,

Note.—In no case must the number of vacuum-fitted wagons coupled to the train engine exceed 35.

(4) Any partly vacuum brake-fitted freight train carrying "D" head lamps having less than the required proportion of vacuum brake-fitted vehicles in accordance with Clause (3), or any such train which in the course of its journey detaches vacuum brake-fitted vehicles so as to reduce the number of such vehicles coupled with the vacuum pipes to the engine below the minimum required, must run at such reduced speed as will enable the driver properly to control the train with the brake power at his disposal. "the headlamps being reduced to poor as the case may be."

(5) At the commencement of the journey and at any station at which the number of vacuum

braked vehicles may have been altered, the Guard must inform the Driver of the actual number of vehicles coupled up with the vacuum brake to the engine, and the Driver must not start until he has received this information.

(B) Freight Trains carrying "B" Headlamps.

Freight trains carrying "E" headlamps may run at a maximum speed of 35 miles per hour subject to the following regulations :-

(1) A number of loaded vacuum brake-fitted vehicles must be coupled with the vacuum pipe to the train engine in accordance with the following table :-

No. of Vehicles on Train (excluding Brake Van).		No. of Loaded Vacuum-Braked Vehicles Required.								
	Un	to	and	not e	exceedir	ng 4()	Not	less	thar	in 4)
			,,		22	45	,,	,,	,,	
			,,		,,	50	,,	,,	,,	
					,,,	50 55	,,	,,	**	7 more than
					,,	60	,,	,,	,,	
	**			,,	,,	64	,,	,,	,,	
	**				**	70	,,	99	99	, 11!

(2) In the event of the MINIMUM proportion of loaded vacuum-braked vehicles not being coupled with the vacuum pipe to the engine, the train should run at the point-to-point times for express goods "F" headlamp trains, provided the load does not exceed 70 wagons of Class 3 traffic or equivalent.

If the train exceeds 70 wagons, it must run at the point-to-point times as set out in the Service Book for "ordinary" freight trains, and no vacuum-brake fitted vehicles must be coupled with the vacuum pipe to the engine.

(3) Whenever it is necessary for GREASE AXLE-BOX vehicles to be formed in trains of this class, the following conditions must be observed :-(i.) The WEIGHT of the load must not exceed ONE-HALF of the registered carrying

capacity of the wagon.

(ii.) The load must be evenly distributed. (iii.) Examiners must give special attention to grease axle-boxes at examining stations.

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General.

"The foregoing instructions apply to Railway Companies' vehicles. Privately

owned Requisitioned wagons fitted with oil axle boxes and loaded with Class 3 traffic les (or empty) may be conveyed by all classes of Freight Trains. The term 'vehicle' must be understood to mean a four-wheeled vehicle."

(G.A. 10. 3/42. E.78000/15.H.) (b) No partly vacuum-fitted freight train may have more than 60 loose-coupled venicles (in cluding brake van) behind the vacuum-braked vehicles connected with the vacuum pipe to the engine. If this number be exceeded, the train must be run as a non-vacuum train.

(c) The Driver must apply the vacuum brake gradually so as to avoid damage to the train and to the contents of the wagons.

#### WORKING OF SPECIAL FREIGHT TRAINS.

The working of special freight trains including stops for examination (in accordance with the regulations) will be arranged by the Controls, in Divisions where such exist, and in the absence of Control by the person deputed by the Divisional Superintendent or District Traffic Manager.

Controllers or others responsible, must forward all necessary advices in accordance with the instructions relating to "Freight train advices," including particulars of head-lamps, and point to

point allowances applicable.

Special freight trains must not be run without the consent of all Controls or Divisions through whose area the train has to pass, so that every opportunity may be taken of securing a reasonable path and utilising power to the best advantage.

In the event of specials terminating short of the arranged destination, the Controller, or person, making the arrangement, must advise all concerned accordingly.

As long notice as possible must be given the Locomotive Department when power is required for special trains.

# WORKING OF FREIGHT TRAINS .- Page 174.

The instructions under the above heading to be cancelled and the following substituted:-

# EXAMINATION OF FREIGHT TRAINS.

Unless otherwise specially agreed between the Operating Superintendent and the Carriage & Wagon Engineer, freight trains must not run more than the following distances shown under the various headcodes without stopping for Carriage and Wagon examination.

Class of Freight Train	Maximum Distance in Miles, train is allowed to run without full Carriage & Wagon Examination	Type of Axle Box				
"C" "E" "F" and below. "F"	160 125 125 85 85 85	Oil Axle Box Oil Axle Box. Oil Axle Box. Grease Axle Box. Oil and Grease Axle Box. Wagons (loaded or empty). Oil or Grease Axle Box empty wagons only*				

\* Full train loads of empty wagons capable of being run under "C", "D" or "E" headcode conditions to be so arranged. (G.A.30 Op .- 9/54. E.81019. H(7-B.) ).

and utilising man and engine power to the best advanta In the event of trains terminating short of the arranged destination, the Controller, or person making the arrangement, must advise all concerned accordingly.

As long notice as possible, minimum three hours, must be given to the Locomotive Department when power is required for special trains.

(G.A.19—10/48. E.81019/H. (7—B) make arrangements for its return in accordance with the following instructions:

1. Unbalanced engines, other than those referred to in these instructions as "engines of seasonal traffic specials," must not be detained at a "foreign" depot for a longer period than 10 hours, unless there is a reliable prospect of a suitable service being found for them in the homeward direction, when they may be retained for a period not exceeding 12 hours.

2. Engines must be despatched from a foreign depot in the direction of the home station only, and

must normally be worked to the home depot by the most direct route.

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Note. If a train is a booked "D" head and train, and the only reason it is reduced to a "D" headlamp train is because there is an insufficient number of vacuum-fitted vehicles connected with the vacuum pipe to the train engine to conform to "O" head bed conditions, it can run as scheduled so far as examination is concerned, i.e. up to 115 miles between examinations. Controllers or others responsible, must forward all necessary advices in respect to the running

the originating Division to agree with all other interested Controls the point or points at which special freight trains must be examined. Special freight trains must not be run without the consent of all Controls through whose area the train has to pass, so that every opportunity may be taken of securing the best practicable path

and loading of both booked and special freight trains. It is the responsibility of the Controller in

As long notice as possible, minimum three hours, must be given to the Locomotive Department when power is required for special trains.

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making the arrangement, must advise all concerned accordingly.

(G.A.19-10/48, E.81019/H. (7-B)

and utilising man and engine power to the best advantage. In the event of trains terminating short of the arranged destination, the Controller, or person

tion, all cor

The instructions on page 174 under heading PROVISION OF ENGINE CONDUCTORS to be deleted (see page 141). (G.A.28.Op.—4/51)

#### FREIGHT TRAIN RUNNING TIMES.

Whenever the maximum load exceeds that published, or the class of vehicle or traffic conveyed in a Freight train is unsuitable to allow of the booked timing being maintained, the Driver should be instructed to run his train in the appropriate standard point-to-point timing to suit the altered circumstances.

# REGULATIONS TO BE OBSERVED IN RETURNING OR BALANCING ENGINES OF SPECIAL. CANCELLED OR DIVERTED TRAINS.

#### (A) Engines of Special Trains.

When an engine belonging to another station becomes available for which there is no immediate return service, the Locomotive Depot concerned must advise the Traffic Department or Control and make arrangements for its return in accordance with the following instructions:—

1. Unbalanced engines, other than those referred to in these instructions as "engines of seasonal traffic specials," must not be detained at a "foreign" depot for a longer period than 10 hours, unless there is a reliable prospect of a suitable service being found for them in the homeward direction, when they may be retained for a period not exceeding 12 hours.

2. Engines must be despatched from a foreign depot in the direction of the home station only, and

must normally be worked to the home depot by the most direct route.

# WORKING OF EX-PRIVATELY OWNED WAGONS ON FREIGHT TRAINS.

The conditions under which ex-privately owned wagons are to be conveyed on freight trains are given below :-

Wagons fitted with Oil Axle-boxes. Class of Traffic.

Head Codes.

\*Class 1 ... ... \" F" or inferior.

> Wagons fitted with Grease Axle-boxes. Headcodes. Class of Traffic.

Inferior to "F."

Class 2

"F" or inferior. Class 3 Empty ...

(G.A.26 Op.-5/50.)

The following additional instruction to be inserted on page 174 immediately preceding the instructions headed "Working of Special Freight Trains":

# HEADLAMPS OF SPECIAL TRAINS CONSISTING OF EMPTY RAILWAY-OWNED STOCK.

The following instructions must be observed in respect of head and to be carried by special trains of empty railway company's wagons:

D' Headendos to be carried and trains signalled accordingly when the instructions on pages 172, 173 and 174 of the General Appendix can be complied with.

(2) Headbades to be carried and trains signalled accordingly when the instructions in the General Appendix on pages 173 and 174 affecting such trains can be complied with. The number of wagons coupled to the engine by the vacuum pipe must not be fewer than one-sixth (with a minimum of 7) or more than one-half of the total number of vehicles on the train.

(3) In the event of suitable engines not being available, or if the instructions regarding the proportion of vacuum stock or suitable wagons cannot be complied with, the special trains must be run at ordinary speed and be samalled as headle of trains.

(G.A. 18. 11/47. E.84563H.)

GA26

Reference to the following to be made on page 174:-

# RUNNING OF SPECIAL FREIGHT TRAINS.

When an Inter-District Special freight train is required (other than those pre-arranged by notice or other means) the request to be passed by the originating District Control to Headquarters Control, Paddington, with details of headcode, load to be conveyed and time the Special train is required to start. The Headquarters Control to make the necessary arrangements with the other District Controls including points at which Carriage and Wagon examination is to be carried out and agree or otherwise with the District Control making the application whether or not the Special train can run.

As long notice as possible, minimum 4 hours, must be given to the Motive Power Department to enable preparation to be made in the provision of power. A serial number will be given by Headquarters Control to all Special freight trains agreed and this number to be quoted in all ressages, telephonic or telegraphic, sent in connection with the train concerned.

(G.A.30 Op .-- 9/54. E.81019 H(7-B.)).

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# Wagons fitted with Oil Axle-boxes. Class of Traffic.

Head Codes.

\*Class 1 ... " F" or inferior. ... All classes of freight trains except those

... | marked ... Empty ... ...

\* The maximum number of ex privately owned or former railway company owned wagons of Class I traffic permitted on "F" head code trains is ten, and they must be (G.A.27.Op.—1/51. E.78000/15.W.) fitted with oil axle-boxes,

# REGULATIONS TO BE OBSERVED IN RETURNING OR BALANCING ENGINES OF SPECIAL, CANCELLED OR DIVERTED TRAINS-Continued.

3. Unbalanced engines must not be ordered away with trains the first portion of the journey towards the home depot, and the remainder away from it, unless arrangements are made for the unbalanced engine to be taken off the train at a suitable point en route.

4. Unbalanced engines, when once despatched on a train destined for a point short of the home depot, or taken off a train in accordance with Clause 3, must not be detained at such intermediate point, but must be worked to the home depot with the least possible delay.

5. Unbalanced engines must not be worked on regular services unless and until arrangements have been made between the Locomotive Depots concerned to balance the engine working satisfactorily.

## (B) Engines of seasonal traffic trains.

1. Engines which have worked seasonal traffic and ocean specials are excepted from the operation of Clause 1 of the foregoing instructions relating to the retention of unbalanced power for the purpose of finding a return loading. Such engines must be returned to their home depots in accordance with the special instructions appearing in the working notice relating to the particular traffic, or, where no notice is issued, with the least possible delay.

2. Engines to which the foregoing clause applies are those which have worked seasonal traffic, viz., flower, fruit, broccoli, potato and other vegetable specials, and ocean specials (passenger or mails).

## (C) Engines of cancelled trains.

1. When a regular freight train is cancelled, the Control or Yard Master must confer with the destination Control and ascertain whether :-

(a) When the train is worked by an engine belonging to the home station, the engine

is to be sent light to the opposite end to return with the balancing train, or

(b) When the train is worked by a "foreign" engine, the "foreign" engine must be returned home light immediately or, alternatively, it may be retained to work another service in the direction of its booked service.

and advise the result of such inquiry to the Locomotive Department.

2. If consent is obtained to either the home engine or the "foreign" engine, as the case may be, being retained to work a service other than scheduled in the direction of the balancing point, a clear understanding must be arrived at as to how such engine is to work forward to its balancing point, and particularly as to the time required there.

# (D) Engines of diverted trains.

1. In the event of a regular long distance freight train being loaded at its starting point to a destination other than booked, or diverted intermediately, the engine being required at the booked destination to work a balancing train, the Control arranging the diversion, after conferring with the Locomotive Depot, will be responsible for advising in ample time all Controls and Locomotive Depots concerned the arrangements which are necessary to maintain the scheduled balance of engines.

2. If a train is diverted away from its booked destination, the engine should be dealt with at the

nearest junction as indicated below:i. Train diverted to a distant station, i.e. more than 35 miles beyond junction point.

Fresh engine to be provided to work train forward and return as ordered. Train engine to run to booked destination to work balancing train.

ii. Train diverted to a near station, i.e. not more than 35 miles beyond junction point.

Train engine to work through to destination of train, returning immediately to junction point and then be sent to booked destination for balancing train.

3. If the train is ordered to a destination short of that booked, the receiving Control and Locomotive Depot will be responsible for sending the engine forward to its booked destination for the balancing train.

## RESTRICTIONS ON RUNNING OF WAGONS LOADED WITH BOILERS, GIRDERS, OR OTHER HEAVY TRAFFIC.

1. Great Western Company's oil-box wagons of the following types, also other Companies' wagons of the similar types (if fitted with the Railway Clearing House, 1923, or subsequent standard AERO PROPELLER WAGON oil-axle boxes) :-

Pollens, Crocodiles (except Crocodile L), Rectanks, Loriot, Loriot A, B, C, D, E, L, M, and R, Loriot W (when fitted with removable buffing struts), Hydras, Serpents (except Serpent D), Corals, Morels (except No. 41994), Beavers, Totems, Open C, Macaw A, B, C, D, E, F, G,

(except as shewn in Clauses 5 and 6) when loaded with boilers, girders, or other heavy traffic, must not travel on freight trains carrying superior to "E" headen to GAZO

The following to be inserted as Clause 1A:—
"Scorpions" must not be conveyed on partially fitted Vacuum trains which are timed in excess of 40 miles per hour, or which run 115 miles or more without examination. On partially fitted Vacuum trains which are not timed in excess of 40 miles per hour, or run over 115 miles without examination "Scorpions" may be conveyed under the same conditions as "Serpents." "Scorpions" 409 and 410 must not be conveyed on (G.A.1. 3/37. E.63527.H.) partially fitted Vacuum trains of any description.

## RESTRICTIONS ON THE RUNNING OF WAGONS, ETC .- Continued.

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2. Loriot W (when not fitted with removable buffing struts), Morel No. 41994, Loriots F, G, H, J, K, when loaded, Serpent D, Crocodile L (loaded or empty), and Loaded Foreign Companies' oil axle-box wagons similar to those enumerated in clause I, except those fitted with R.C.H. 1923 or subsequent standard oil axle-boxes, must not travel on freight trains carrying superior than " h" head and a content of the conte

3. Empty Great Western and empty "foreign" Companies' oil axle-box special wagons of types enumerated above must not travel on any freight train carrying superior than "E" headlamps.

4. Before a loaded Foreign Company's oil-box special wagon is attached to a freight train carrying "F" headign to the Traffic Department Inspector in charge of the Yard must personally authorise the wagons being attached, and before this authority is given he must ascertain from the Locomotive Department Examiner if the wagon is suitable to travel on such a train.

5. Great Western Company's oil-axle box wagons and Foreign Companies' wagons of the following types (if fitted with Railway Clearing House, 1923, or subsequent standard oil-axle boxes):-

WAGON

Hydra, Hydra C, Hydra D, all Loriots (except F, G, H, J, K and W), Open C, Serpents (except PROPELLEA Serpent D), when loaded with traffic, the weight of which does not exceed 50 per cent. of the marked carrying capacity of the vehicle and the load is equally distributed, also Bocars, Macaws B, D, E and H loaded up to 10 tons, may be conveyed on partially-fitted vacuum trains carrying "C" headlamps (except those timed faster than the standard vacuum speed, i.e. 40 miles per hour, or those booked to run more than 115 miles without intermediate examination).

6. Ships' plates or similar traffic, loaded up to 10 tons on Macaw B, Macaw D, Macaw E or Macaw H, provided the plates are loaded on the bolsters and carefully secured between the stanchions of the trucks, may be conveyed on partially-fitted vacuum trains carrying "C" headlamps (except those timed faster than the standard vacuum speed, i.e. 40 miles per hour, or those booked to run more than 115 miles without intermediate examination).

7. Any of the wagons mentioned in Clauses 1 and 2 when loaded with exceptionally heavy machinery, or other articles exceptionally heavy or lengthy, must be examined and passed (and, if considered necessary, accompanied) by one of the Chief Mechanical Engineers' Department Inspectors from Swindon or Wolverhampton, and, if the train by which the wagons are conveyed is booked to run 50 miles or more without stopping, it must be stopped specially after covering 25 miles or at the nearest station to the 25-mile limit, in order that the wagons and loads may be specially examined, and, if found to be running all right, the remainder of the journey may be taken without further special stoppage. In such circumstances the examining station must be advised by telegraph of the fact that the train is stopping there for special examination of the load.

And Twenter 8. Tenton loaded roll wagons and any wagons other than those mentioned in Clauses I when loaded with exceptionally heavy machinery or any other articles exceptionally heavy or lengthy, may only be allowed to travel on trains carrying "#," "J" or "K" headless and booked as such in the working Time Tables, and they must not be attached to any train that runs at a greater speed than 25 miles an hour, or that travels more than 25 miles without a stop, unless the loads and wagons have been specially examined and certified as fit to travel by one of the Inspectors of the Chief Mechanical Engineers' Department from Swindon or Wolverhampton, who must also accompany the train. In every such case, however, the train must stop as soon as the wagons have run 25 miles, or at the nearest station to the 25-mile limit, in order that the vehicles and loads may be re-examined by the Inspector of the Chief Mechanical Engineers' Department accompanying the train, and if the wagons are found to be in good running condition they may, upon his authority, work to destination at the ordinary booked speed of the train without further special stoppages.

9. The Guard in charge of any train conveying traffic of the description mentioned in Clauses 1, 2, 7,

and 8 must examine the wagons and the loads at every station at which the train stops.

10. Crocodile L (loaded or empty), and all wagons used for the conveyance of exceptionally heavy machinery or any other article exceptionally heavy or lengthy, must always be placed next in front of rear brake van (except as shown in Clause 11, and in those cases where an application is made by the Chief Mechanical Engineer for a second brake van to be provided next the engine with the exceptional load marshalled immediately behind it). Such loads must be carefully watched while running.

Cases where doubt exists as to loads being considered exceptionally heavy or lengthy, should be referred to the Chief Mechanical Engineer or his Divisional Superintendent concerned, giving par-

ticulars of load and wagon proposed to be used.

11. When loaded with traffic of a light nature, as shown in Clause 5, or running empty, the vehicles enumerated in Clauses 1, 2 and 5 may be marshalled in their proper order (except Crocodile L).

12. For details of procedure to be followed in connection with acceptance and conveyance of articles exceeding the published gauge dimensions or of exceptional shape, dimensions or weight, see pages 190

See pages and Instructions enumerated below:

Page 191.—Loading of Round Timber. ,, 194.— ,, ,, Baulk Timber. ,, 194.—

" Channel, Bulb and angle iron.....scrap, etc. 211.--" Traffic over 46 feet.....tubes, rails, etc. " 218.— "

# CONVEYANCE OF CONTAINERS LOADED ON SINGLE PLANK WAGONS-page 177.

Delete and substitute the following :--

One plank wagons having no means of securing containers other than ropes, are prohibited from running on "C" Headcode trains, or "D" Headcode trains marked \*. (G.A.26 Op.—5/50.)

# RESTRICTIONS ON THE RUNNING OF WAGONS, ETC .- page 176.

Clause 3 to be amended to read:

Great Western and other Companies' empty oil axle-box special wagons of types shewn in Clauses 1 and 2 must not be conveyed on any freight train carrying superior than "E" headlamps, except the following in temporary use: GA 23

Warflat

(G.A. 18. 11/47. E.78000/170H.)

3. Empty Great Western and empty "foreign" Companies' oil axle-box special wagons of the types enumerated in Clauses 1 and 2 must not travel on any freight train carrying superior than "E" headlamps.

The following to be inserted as clause 5A:-

5A. Consignments of any description loaded on bogic wagons must not be conveyed by vacuum or partially vacuum fitted freight trains, when the length of load exceeds that of the wagon upon which it is loaded or when the weight of load exceeds 10 tons. (G.A. 5.-2/39.

In the event of no examining station being near the 25 mile limit, the Chief Mechanical Engineer's Inspector must accompany the load and make a special examination of the wagon and load after travelling 25 miles.

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ohibited -5/50.) The following to be inserted between clauses 3 and 4:-

Note:—Clauses 1, 2 and 3 apply also to Road-Rail and demountable tanks loaded on wagons specially constructed for their conveyance, but in these cases the star or stars will be found painted on the rail chassis only. In no case must a Road-Rail or demountable tank containing Class "A" or other dangerous liquids be mounted on a rail chassis bearing three stars.

(G.A. 5.-2/39.

Tanks for the conveyance of highly inflammable liquids (Class A) are painted aluminium colour with a bright red band 6 inches wide, running round the ends; the red band is stopped short on the sides of the tank to provide space for the name of the owner or commodity. These tanks must be dealt with in accordance with Rule 240.

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## FREIGHT TRAIN INSTRUCTIONS. CONVEYANCE OF TANK WAGONS.

(1) Private Owners' tank wagons having one star painted on each side of each tank, former G.W.R. tank wagons and Messrs. Allsopps (Beer) tank cars, (loaded or empty), also Road/Rail and Demountable tanks (loaded or empty), loaded on wagons specially constructed for their conveyance with one star painted on the rail chassis only, may be conveyed on trains scheduled to carry "E" or inferior to "E" head codes, irrespective of the class of traffic the tanks contain, as indicated on the wagon label. (G.A.27.Op.-1/51. E.76159/5.W.)

on each side of each tank, also Road Rail and Demountable Tanks (loaded or empty) loaded on wagons the specially constructed for their conveyance with two stars painted on the rail chassis only, may be run in partially fitted vacuum freight trains scheduled to carry 'D' headledge except those marked in the Service Time Tables). Tanks to be marshalled at the rear end of the vacuum coupled portion of the train and ons next inside the last vacuum coupled vehicle.

3. Private Owners' and Railway Companies' fully vacuum brake fitted six-wheeled tank wagons nps (full or empty) carrying non-inflammable liquids and having three stars painted on each side of each tank, teralso Road Rail and Demountable Tanks (loaded or empty) loaded on wagons specially constructed for their conveyance with three stars painted on the rail chassis only, may be run in partially fitted vacuum freight trains scheduled to carry head except those marked in the Service Time Tables), also in passuring senger trains which do not exceed a speed of 60 m.p.h. at any point on the journey. Tanks to be marshalled in the vacuum portion of the train and inside the last vacuum coupled vehicle. IN NO CASE MUST A 17TY ROAD/RAIL OR DEMOUNTABLE TANK CONTAINING CLASS "A" OR OTHER DANGEROUS OUS LIQUIDS BE MOUNTED ON A RAIL CHASSIS BEARING THREE STARS.

4. Road/Rail Demountable Tanks (loaded or empty) loaded on wagons specially constructed for their conveyance with three stars painted on the rail chassis may be run in partially vacuum fitted freight trains scheduled to carry ' head bours and marked provided they are marshalled either:

(a) next behind the train engine coupled to the engine and its neighbouring vehicle by means nk of screw coupling with buffers slightly compressed.

(b) in any position in the vacuum fitted portion of the train always provided that it forms part of a number of vehicles formed next behind the train engine all of which must be screw coupled, and should the demountable tank be the last vehicle in the vacuum fitted portion of the train it must also that be coupled to the trailing vehicle by means of the screw coupling.

IN NO CASE MUST A ROAD/RAIL OR DEMOUNTABLE TANK CONTAINING CLASS "A" OR OTHER DANGEROUS LIQUIDS BE MOUNTED ON A RAIL CHASSIS BEARING THREE STARS.

5. Other Tank Wagons, loaded or empty, must only be conveyed on trains scheduled to carry or "K" head sous (Tank wagons for Class "A" inflammable liquids, dangerous liquids or compressed he liquified gases to be marshalled as near the middle of the train as possible.)

When no freight train service is available, tank wagons for inflammable liquids and highly inflammable liquids (loaded or empty) may be conveyed by Mixed trains. When such wagons are so conveyed by ith Mixed trains they must be marshalled next inside the rear brake van, and in all cases at least one ordinary be goods wagon must be placed between any vehicle containing passengers and the tank wagons. In no case rill must wagons conveying readily combustible traffic, such as hay or straw, be placed next the tank wagons.

The foregoing instruction modifies Clause 10 of Rule 240 in so far as the conveyance of these tank wagons is concerned. (G.A.16. 5/46. E.76159/5. W.)

(other than round timber), and tubes, may be conveyed on open goods wagons by partly vacuumfitted "D" headlamp trains (except those timed faster than the standard vacuum speed, i.e., 40 miles per hour, or those booked to run more than 115 miles without intermediate examination), also by trains carrying "E" head bodes under the following conditions:—

(a) The overhang must not exceed 4 feet beyond the end of the wagon.

The total weight of the wagon load must not exceed one-half the carrying capacity of the vehicle.

(r) The overhanging traffic to be well secured and the wagon next to the overhanging end must always be loaded, and of such height as to form a suitable match.

N.B.—Low-sided wagons—i.e. one or two-plank—must not be used for loading or matching traffic with overhanging ends on partly-fitted "D" or "E" Headles freight trains.

D" "E" WORKING OF GOODS BRAKE VANS.

I. Booked "I" and "I" head a trains must, as far as possible, be worked by Vans branded with the letter "S" and every effort must be made to keep these vans in balanced working to ensure suitable vans working "Express" Freight trains. At certain Depots vans are set aside for specific working and are branded R.U. (Restricted User); these vans must not be allowed to work other than on the local services for which they are appointed.

2. Station Masters, Yard Masters and Yard Inspectors are responsible for revealing on the morning report to the Control or Divisional Officer accurate particulars of Brake Vans on hand, in accordance with instructions, to enable a satisfactory distribution to be made.

3. When it is necessary for Freight trains to be stabled short of destination, i.e. at an outlying Refuge Siding or Loop, arrangements must be made by Control Staff, or other responsible Officer, for the disposal of the brake vans, in order to avoid any unnecessary delays thereto.

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COLUMN TO THE PROPERTY OF THE PARTY OF THE P Tanks containing highly inflammable liquids (Class A) are painted a light stone colour with a bright red based 6 inches wide running horizontally round the centre of the tank. These tanks must be dealt with in accordance with Rule 240.

WEIGHT OF TRAFFIC IN OWNERS' TANK WAGONS.

Loaded tank wagons will not be accepted for conveyance unless the tare is readily readable; the weight of the contents must be ascertained in all cases by deducting the painted tare from the gross weight of the wagon and contents on both the outward and return journeys. When returned vehicles contain an amount of residue the senders must declare the weight, and the carriage charges must be paid thereon.

Tank wagons must (subject to the allowance for expansion) be fully loaded, unless fitted with approved wash-plates, enabling them to be used for variable loads from half to full, but must not be loaded beyond their registered weight carrying capacity. Tank wagons, both full and empty, will not be accepted for conveyance unless all inlets and outlets are securely closed watertight.

Twin-Tank Wagons. - Both tanks must be equally loaded when the wagon is in motion.

CONVEYANCE OF OVERHANGING LOADS ON PARTLY VACUUM-FITTED FREIGHT TRAINS.

Subject to the Regulations governing Severn Tunnel, overhanging loads, consisting of deals, timber (other than round timber), and tubes, may be conveyed on open goods wagons by partly vacuumfitted "D" headlamp trains (except those timed faster than the standard vacuum speed, i.e., 40 miles per hour, or those booked to run more than 115 miles without intermediate examination), also by trains carrying "E" headbooks under the following conditions :-

(a) The overhang must not exceed 4 feet beyond the end of the wagon.

(b) The total weight of the wagon load must not exceed one-half the carrying capacity of the

wehicle. (c) The overhanging traffic to be well secured and the wagon next to the overhanging end must always be loaded, and of such height as to form a suitable match.

N.B.-Low-sided wagons-i.e. one or two-plank-must not be used for loading or matching traffic with overhanging ends on partly-fitted "D" or "E" Headles reight trains.

## To Shops other than Swindon.

When vans are labelled to Shops other than Swindon, the equipment must be retained in the Vans, which must be locked.

The Traffic Department Staff at the Station or Yard adjacent to such Shops must, before handing over the vans to the Chief Mechanical Engineer's Department, remove the:—

Set of side lamps. Tail Lamp. Oil Can. Bucket.

Shunting Pole. Handbrush. Fire Shovel.

The Traffic Department Staff must leave in the Vans the :-

Brake Stick. Wheel Sprags. Short Drawbar and Block.

The equipment removed must be placed in store to replenish missing equipment as the vans ex Shops are put into traffic.

The Chief Mechanical Engineer's Department will be responsible for ensuring that the Brake Stick, Wheel Sprags, Short Drawbar and Block are in the Vans before being put into traffic.

(G.A.13. 9/43. E.44407/H.)

4. Doors of Goods Brake Vans must be kept locked on the G.W. System when not in use.

Goods brake van doors are fitted with rim locks. Appointed Guards may, as a rule, retain possession of van door keys when resting but when on leave or absent from duty through illness or any other exceptional cause, the keys must be left at the depot or other appointed place. A number of spare keys for use by emergency men acting as Goods Guards are kept at the Depots and will be handed to these Guards as required, who must return them to the Depots from which they are obtained.

5. The Standard equipment of each van is as shewn below:-

Set of side lamps. Tail lamp. Oil can. Shunting Pole. Sprags (not less than two). Brake Stick. Short Drawbar and Block.

\*Hand Brush. \*Fire shovel. \*Bucket. \*-If any of these articles is missing this does not render the van useless for service.

These articles must be retained in the Van, and the set of Side Lamps and Tail Lampwhen not being used—must be placed inside the Van on the brackets provided. Special care must be taken to keep the Van door locked when the Van is not used for train working. In no circumstances must the equipment be taken from the Van except for replacement from Stores, neither must additional lamps be kept in the Brake Vans.

The locker provided with padlock (formerly used for A.R.P. equipment) must be used for the bucket, shovel, handbrush and oil can and Guards must lock these articles up in this receptacle when not in use.

6. Guards are responsible for the cleanliness and correct equipment of their vans. When leaving the Vans at the termination of trains they must place the side and tail lamps on the brackets inside the Van and see that the other equipment is placed inside and that the door is locked; they must report in the book provided for the purpose, particulars of any missing equipment or any matter requiring attention. They should keep their own property in the lockers provided in the Guards' room or cabins or deposit it with the Yard Inspector.

Note. In the event of a Guard being authorised to leave a train on a Goods line at a terminal point before the train is disposed of the person in charge must see that the side and tail lamps are deposited inside the van immediately the train is cleared from the Goods line and the door locked.

- 7. When Brake Vans are ordered to be sent from one Yard or Station to another, the equipment must be sent with them and they must be despatched without delay. Vans so sent must be labelled to the particular stations and particulars included on train advices.
- 8. Station Masters, Yard Masters and Yard Inspectors must arrange for the inspection of Goods Brake Vans weekly, and satisfy themselves as to their cleanliness and equipment, including the supply of suitable sand, reporting to the Divisional Superintendent any irregu-
- 9. Guards must not paint over the bottom panes of the windows of Goods Brake Vans thereby obstructing the look-out, neither must the construction of the vans be tampered with
- 10. Guards must, when leaving a brake van, satisfy themselves that there is sufficient sand and oil for the next journey and must report to person in charge of yard any deficiency. Guards must, before starting, test sanding appliances to ensure satisfactory working. Not more than I backetful of sand should be placed in the sand backet when supply & 11. Guards must show on journals particulars of vans working "light".
- 12. In the event of a brake van being marked off, or stopped for repairs, the Guard or the examiner (if marked off in the Yard) will be responsible for advising the Traffic Inspector, or person in charge, in writing of the fact. When vans have to be sent to C. & W. shops for repairs the vehicle must be locked up with all the equipment so that the van can be put into traffic immediately after repairs have been completed. The Yard Master or Station Master must also advise the Divisional Superintendent or District Traffic Manager the number of each van marked off, the shop sent to for repairs and the date.
- 13. When it is necessary to send Brake Vans to Shops for repairs the following instructions must apply :-

To Swindon Shops.

The standard equipment must not be removed from the Vans before being sent to Swindon Shops, but must be locked inside the Van which must be labelled. Upon arrival at Swindon Works the equipment will be removed from the Vans and transferred to the General Stores. When the Vans have been repaired arrangements will be made to re-equip the Vans in the Swindon Shops from the General Stores.

#### GENERAL INSTRUCTIONS FOR WORKING INCLINES.

#### Ascending Inclines.

1. The Assisting Engine must be placed as under:-

Freight or Mineral Trains.—Behind the rear van, except in certain cases where the varying nature of the gradient necessitates it being placed in front of the Train Engine. As a general rule the Assisting Engine must not be coupled to the rear van unless there be special circumstances which render it desirable. All exceptions to the general practice directed in this clause must be shown in the Appendix to the Service Time Tables or in Special Instructions issued by the Divisional Superintendents or District Traffic Managers.

On those sections of the line where authority is given for Freight and Mineral trains to be assisted in the rear, the Assisting Engine must, during Foggy Weather or Falling Snow, be coupled to the rear of the train in every case. The Bank Engine must also be coupled when there is a failure of the Block

and trains are being worked by time interval.

2. Whenever it is necessary for a Freight or Mineral train to have the assistance of an Assisting Engine up a rising gradient, the train must be stopped dead in order that the Assisting Engine may come to the tail of the train in readiness to assist it. On no account must an Assisting Engine come to the rear of any train whilst the train is in motion.

3. The signal to start must be given by the Guard to the Driver of the engine in the rear, and when there are two Guards this must be done by the rear Guard after he has exchanged signals with the Guard in front. When the Driver of the engine in rear has received the Guard's signal to start, he must call the attention of the Driver in the front of the train by giving two "crow" whistles which must be acknowledged by repetition from the front engine after the Driver of that engine has satisfied himself that the necessary fixed signal has been lowered, and until these "crows" have been given and acknowledged, neither the train engine nor the assisting engine must move forward.

# GR AT WESTERN RAILWAY.

Divisional Superintendent's Office, BRISTOL. (T.M). G.W.7.

Ref:- A1/28,000

38th. July, 1945.

Dear Sir,

# OR ING OF GOODS BRAKE VANS.

With reference to the instructions under the above heading on pages 177 and 178 (amended in G.A.13), of the General Appendix to the Rule book, the following addition should be added to clause 10 of the revised instructions:-

"Not more than one bucketful of sand should be placed in the sand boxes when the supply is exhausted".

Please bring this instruction to the notice of all Guards and others concerned.

Yours truly, for R.G.Pole of

## GENERAL INSTRUCTIONS FOR WORKING INCLINES-Continued.

12. In the case of a train having to do work at a siding connected with a running line situated on an incline, sufficient wagon brakes must be put down before commencing the descent to ensure the train stopping at the siding. If the siding itself is also on a gradient falling towards the running line, a sufficient number of wagon brakes must be put down before removing the wagons, and the Guard or Shunter must accompany the wagons from the siding to the running line, and put down additional brakes should it be necessary to do so, so as to ensure the wagons being kept under complete control whilst bringing them out of the siding on to the running line. When the wagons have been attached to the train, the instructions in Clause 8 must be strictly observed.

13. The Driver must stop his train at the foot of the incline, or where it may be necessary to

do so, for the wagon brakes to be released.

14. In all cases where it is possible, a minimum time will be fixed for descending inclines, and trains must not descend the inclines in less time than that stated.

15. All ballast and freight trains, even when they consist partly of vacuum stock, must stop at the foot of inclines as well as at the top.

16. Signalmen and others concerned are strictly enjoined to report every case in which these Regulations have not been strictly adhered to.

17. Special Regulations to be observed on Sections of the Line where two trains are authorised to be coupled together Down Inclines .- The General Instructions applying to a single train must be

observed with the following additions:—

(a) The Guard of each of the coupled trains will be held responsible for putting down a sufficient number of brakes on his own train in accordance with Clause 8. When sufficient brakes have been put down, the first Driver will give two short, sharp whistles to his Guard, and the rear Driver three short, sharp whistles to his Guard.

(b) When two trains coupled together are descending an incline, the Guard of each train must keep the couplings tight by a judicious application of the hand-brake in sufficient time to

prevent anything like a snatch if the train has to pass on to a rising gradient.

(c) Whenever, from any cause, either of the Drivers of the coupled trains finds it necessary to stop, he must sound his brake whistle as a signal to the other Driver to shut off steam and apply his brake.

Notes.—The term Mineral Train must be understood to include not only Coal, Stone, Ore, Steam Cranes &c., trains, but Engineers' Ballast, Earth and Material trains also. Special Trains See Instructions with regard to Hand-brake power on Passenger trains on page 109.

conveying

# BRAKING VACUUM-FITTED AND PARTLY VACUUM-FITTED FREIGHT TRAINS DOWN INCLINES.

1. With reference to the foregoing Incline Instructions freight trains COMPLETELY FITTED THROUGH-OUT WITH THE VACUUM BRAKE, AND CLOSE COUPLED WITH SCREW OR INSTANTER COUPLINGS, need not stop either at the top or at the foot of inclines. The Goods Brake Vans used on such trains must

In all cases be fitted bither with vacuum brake complete or with vacuum pipe and brake setter.

The Goods Brake vals used on such trains must in all cases be fitted bither. The Goods Brake vals used on such trains must in all cases be fitted to the complete or with vacuum pipe and brake setter.

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The Driver of EVERY partly vacuum-fitted freight train must also bring the train to a STAND AT

THE FOOT OF THE INCLINE.

\* See clause "A (3)" of the Instructions for working partly vacuum-fitted freight trains for proper proportion of vehicles fitted with vacuum brake and to be connected to the engine of a partly vacuum-fitted "C" headlamp freight train.

3. Any train booked as a partly vacuum-fitted "C" headlamp freight train, a "D" headlamp freight train (except as shewn in Clause 4 of these instructions), or any other freight train formed with vacuum-braked vehicles coupled with the vacuum pipe to the engine, but having less than the proper proportion of such vehicles in accordance with Clause "A (3)" of the instructions for working such trains, must run under the "Incline Instructions" applicable to "Ordinary" freight trains, and the hand brakes must be put down as required on the vehicles immediately to the rear of the vacuumbraked vehicles coupled to the engine.

4. Guards of "D" headlamp freight trains working over sections of line where the incline instructions necessitate the train being brought to a stand at a stop board, must satisfy themselves at the last booked stopping-place before reaching the stop board as to the number of loaded vacuumbraked wagons formed next to the engine with the brake pipes coupled, and they must in all cases give this information to the Driver before leaving that place.

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#### Descending Inclines.

Clause 15 amended to read:-

15. All ballast and freight trains, including those consisting partly of vacuum stock but excluding those which are fully vacuum fitted, must stop at the foot of inclines as well as at the top.

(G.A.30 Op .-- 9/54. E.82672. H.(12-C.)).

#### BRAKING VACUUM-FITTED AND PARTLY VACUUM-FITTED FREIGHT TRAINS DOWN INCLINES.—Page 181.

Clause 5 amended to read:-

5. These instructions do not apply to Ballast Trains partly composed of vacuum fitted stock which must work under the Incline Instructions.

(G.A.30 Op.-9/54. E.8267 H.(12-C.)).

## RAILWAYS COMPANIES' WAGONS STOPPED FOR REPAIRS .- Page 181.

agraph I to be amended to read:

1. The Carriage and Wagon Examiner must inform the Traffic Department Inspector, Guard, or Shunter where loaded wagons are marked off, and arrangements must be made immediately by the Traffic Department to berth the tracks in a Cripple Siding. The Carriage and Wagon Examiner must inform the Traffic Department Inspector, Guard, or Shunter directly loaded wagons conveying Class 3 traffic are marked off when arrangements will then be made by the Traffic Department to communicate quickly with the Goods Department by telephone or other means, to permit of telegraphic advices being issued to the forwarding and destination stations without delay. In cases where such wagons are marked off during the night, the Traffic Department to transmit particulars immediately the Goods Office is open.

(G.A. 1. 3/37. C.G.M.—W.T.4191.) Paragraph I to be amended to read :-

Reference to the following to be made on page 181:

#### WAGONS WITH BROKEN DRAW-BARS.

When a wagon with a broken draw-bar is put off in a siding by means of the short draw-bar and block, the Guard must place the defective wagon next to the stop block except in those cases where delay to the train or other circumstances make this impracticable. The short draw-bar and block must be left on the wagon, and a note made by the Guard on his journal showing where and on what wagon they have been left. The Carriage and Wagon Examining Staff will arrange removal of the short draw-bar and block from the wagon and will return them to the nearest Depot Stores. (G.A. 18, 11/47, LK1/8788/Gen. 4.)

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#### BRAKING VACUUM-FITTED AND PARTLY VACUUM-FITTED FREIGHT TRAINS DOWN INCLINES-Continued.

When the number of loaded vacuum-braked wagons connected to the engine of a "D" headlamp train is equal to the number required in the case of a partly vacuum-fitted "C" headlamp freight train, Clause 2 of these instructions will apply.

When the proportion of vacuum-braked vehicles connected with the engine of a "D" headlamp train is less than that required in the case of a partly vacuum-fitted "C" headlamp freight train,

Clause 3 of these instructions must be carried out.

5. These instructions do not apply to Ballast Trains, which must work under the Incline Instructions, whether formed completely or partly of vacuum-fitted steek.

#### WORKING INCLINES WITH A ROPE ON THE BALANCE SYSTEM.

In order to minimise the risk and danger of trucks running away owing to a broken link or drawbar hook they must be doubly connected, whenever practicable, when ascending and descending Inclines worked by means of a rope on the balance system.

#### RAILWAY COMPANIES' WAGONS STOPPED FOR REPAIRS.

When a wagon is stopped for repairs at a station where there is no Wagon Examiner appointed by the Chief Mechanical Engineer's Department, an advice of such stoppage must be immediately forwarded to the local representative of that department at the nearest examining station. If the wagon is loaded the forwarding and receiving station must be advised by telegram and, should it bear repairers' plates, the firm responsible for executing the repairs must be advised immediately particulars have been received from the Chief Mechanical Engineer's Department of the material required to carry out the repairs.

#### RAILWAY COMPANIES' WAGONS STOPPED FOR REPAIRS-Page 181.

The existing instructions to be cancelled and the following substituted :-

#### RAILWAY-OWNED AND EX-P.O. WAGONS STOPPED FOR REPAIRS.

The following arrangements must be carried out in connection with all Railway-owned and ex-P.O. Wagons stopped for repairs :-

1. The Carriage and Wagon Examiner must inform the Traffic Department Inspector, Guard or Shunter where a loaded wagon is marked off, and arrangements must be made immediately by the Traffic Department to berth it in a Cripple Siding. Arrangements will then be made by the Traffic Department to communicate quickly with the Goods Department by telephone or other means to permit of telegraphic advices being issued to the forwarding and destination stations as necessary. In cases where such a wagon is marked off during the night, the Traffic Department to transmit particulars immediately the Goods Office is open.

2. The Carriage and Wagon Examiner must place the red "Not to Go" card over the traffic label in the pocket or under the clip, but not to completely obscure the traffic label. If the traffic label is tacked to the side of the wagon the red card must be placed immediately by the side of it.

3. When a loaded wagon is marked off for repair, particulars of the stoppage must be entered by the Examiner in the "949" Book, which is to be handed to the Station Master or Goods Agent daily to enable the required advices to be sent promptly.

4. An advice of loaded wagons stopped for repairs must be issued to forwarding and receiving stations by the Goods Agent or Station Master immediately on receipt of the particulars in the "949" Book or of an advice from the Traffic Department. Form 2826 must be used for this purpose and must in all cases contain the following particulars :-

- (a) Name of Station or Depot.
- (b) Name and number of wagon,
- (c) Forwarding and destination station.
- (d) Traffic.
- (e) Consignees.

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4. An advice of loaded wagons stopped for repairs must be issued to forwarding and receiving stations by the Goods Agent or Station Master immediately on receipt of the particulars in the "949" Book or of an advice from the Traffic Department. Form 2826 must be used for this purpose and must in all cases contain the following particulars:—

- (a) Name of Station or Depot.
- (b) Name and number of wagon,
- (c) Forwarding and destination station.
- (d) Traffic.
- (e) Consignees.
- (f) Label date.

In the case of Urgent, Important, or Perishable traffics advices of the stoppage must also be sent to forwarding and receiving stations by telegram or telephone, the appropriate "2826" form being endorsed as in confirmation.

5. When a wagon is stopped for repair at a station where there is no Wagon Examiner appointed by the Carriage and Wagon Department, an advice of such stoppage must be immediately forwarded to the local representative of that department at the nearest examining station.

6. It will be the responsibility of the Carriage and Wagon Department to obtain the material for and carry out the repair to the vehicle.

7. The Carriage and Wagon Department, after the necessary repair has been carried out, will be responsible for intimating to the Traffic Department that the wagon is fit to continue its journey.

8. When a wagon containing a through load is found during transit to be unfit to travel and Carriage and Wagon Department men with suitable material are available at or near the spot, and the necessary repairs to the vehicle can be carried out without causing greater delay than would otherwise occur owing to transhipment of the load, the vehicle should be repaired while under load. The local Goods and Carriage and Wagon Departments must retain close contact to avoid the transit of the goods being unduly delayed. In all other circumstances the load must be transhipped, a record of the occurrence being kept at the transhipment station (at large stations in a separate book), and, in labelling the wagon into which the load is transhipped, such transhipping station must state on the labels the name of the original forwarding station, the date on which the

disabled wagon left there, and its name and number.

The transhipping station on the day of the occurrence must send a further advice on Form 2826

to the sending and receiving stations of the consequent transhipment.

(G.A.23-7/49. E.86585/H.)

RAILWAY COMPANIES' WAGONS STOPPED FOR REPAIRS-Continued.

repairs to the vehicle can be carried out without causing greater delay than would otherwise occur owing to transhipment of the local Goods. The local Goods owing to transhipment of the load, the vehicle should be repaired while under load. The local Goods and Carriage and Wagon Repartments must retain close contact to avoid the transit of the goods being unduly delayed. In all other circumstances he load must be transhipped, a record of the occurrence being kept at the transhipment station (at large stations in a separate book), and, in labelling the wagon into which the load a transhipped, such transhipping station must state on the labels the name of the original forwarding station, the date on which the disabled wagon left there, and its number. The transhipping station on the day of the occurrence must advise the sending and receiving stations of the failure of the wagon and consequent transhipment; also take the steps prescribed in such cases for its repair."

#### REGULATIONS AS TO PRIVATE OWNERS' WAGONS.

1. The Owners of all new or rebuilt vehicles intended to work upon the Great Western Railway must, before they are brought into use, communicate with the Chief Mechanical Engineer, Swindon, so that he may have them inspected without unreasonable delay, and if built, or rebuilt, in accordance with the Railway Clearing House Standa & Specification and addenda thereto, register plates, as described in the Specification, shall be for the with the contract of t

2. The name and address of the Own or Lessee, one wagon number, and the tare, shall be painted conspicuously on both sides of the wagon; the maximum load must also be clearly indicated on both

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term of not less than three months, the name and address of a board or plate on both sides of the wagon, and that when name and address of the Lessee shall either be so painted, gon label), on both sides of the wagon.

case may be, shall keep their wagons in good ming condition. nd examined and put into good repair before be endered to the

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substituted in central action of the conditions of the said Specification. shall be observed which for the proper and safe working it is necessary and charge t expenses incurred in effecting the same. and of Trade under the provisions of the Railway hen it is necessary in the ordinary course of business signee shall be placed upon any railway wagon, such

of such wagon; and no Private Owner's Wagon will be way unless so labelled or directed on both sides. on Companies and their repairers, when labelling defective learly set forth on the labels the station or siding from and and hand in a pro Consignment Note or written forward-

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hout prejudice, to dispense with a Consignment Note or written wagons painted with a yellow star in accordance with the proaulage Scheme, la led to Repair Works.

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at of premises in the occupation of Private Wagon Repairers, a charge services, except wh a higher charge is now made, in which case

ximum charge under these regulations. to the owner, or his agent, in respect of standing room for any wagon

or repairs at the rate of 6d. per wagon per day, which will be each lated ys, exclusive of the late of the advice note, Sundays, and Bank Holidays, ced at the disposal of the Owner or Repairer, and to terminate when the h after repair.

to not apply when the owners are parties to the "omnitted Shunting and, in which case the wagons are plated or stencible." C.C."

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Reference to the following to be made on page 182:-

#### REGULATIONS AS TO PRIVATE OWNERS' WAGONS.

The Owners of all new or rebuilt vehicles intended to work upon the Western Region .. must, before they are brought into use, communicate with the Carriage & Wagon Engineer, Swindon, so that he may have them inspected without unreasonable delay, and if built, or rebuilt, in accordance with the Railway Clearing House Standard Specification and addenda thereto, register plates, as described in the Specification, shall be forthwith affixed to each.

The name and address of the Owner or Lessee, the wagon number, and the tare, shall be painted conspicuously on both sides of the wagon; the maximum load must also be clearly indicated on both sides of the wagon.

When wagons are let on hire the Lessee will, for the purpose of these Regulations, be regarded as the Owner.

Provided that when the hire is for a term of not less than three months, the name and address of the Lessee shall be painted or exhibited on a board or plate on both sides of the wagon, and that when the hire is for less than three months, the name and address of the Lessee shall either be so painted, or exhibited on a card (other than the wagon label), on both sides of the wagon.

The Owners or Lessees, as the case may be, shall keep their wagons in good working condition, and have them properly lubricated and examined and put into good repair before being tendered to the Western Region for transit.

The British Transport Commission may remove the register plates from any wagon if wheels, axles, or any other materials of less dimensions or strength than those provided for by the Railway Clearing House Standard Specification are afterwards substituted in contravention of the conditions of the said Specification.

5. If in transit any defect shall be observed, which for the proper and safe working it is necessary to repair before the vehicles are allowed to proceed further, the British Transport Commission may, with the consent of the Owners, make such repairs, and charge them with all expenses incurred in effecting the same.

In pursuance of the rules made by the Board of Trade under the provisions of the Railway Employment (Prevention of Accidents) Act, 1900, when it is necessary in the ordinary course of business that any label or direction as to destination or consignee shall be placed upon any railway wagon, such label or direction must be placed on both sides of such wagon; and no Private Owner's Wagon will be accepted for conveyance by the Western Region unless so labelled or directed on both sides.

All wagon Owners, representatives of wagon Companies and their repairers, when labelling defective wagons at railway stations and depots, shall clearly set forth on the labels the station or siding from and to which the wagons are required to travel, and hand in a proper Consignment Note or written forwarding instructions.

The British Transport Commission agrees, without prejudice, to dispense with a Consignment Note or written forwarding instructions in the case of wagons painted with a yellow star in accordance with the provisions of the Commuted Empty Haulage Scheme, labelled to Repair Works.

Note-Not applicable to wagons working in Scotland.

7. When wagons, for the purpose of repair, are required to be shunted into and out of Sidings belonging to the British Transport Commission and/or into and out of premises in the occupation of Private Wagon Repairers, a charge of 2s. 8d. per wagon will be made for such services, except where a higher charge is now made, in which case such higher charge shall be the maximum charge under these regulations.

Siding rent will be chargeable to the owner, or his agent, in respect of standing room for any wagon detained at a station or siding for repairs at the rate of ls. 5½d. per wagon per day, which will be calculated from the expiration of three days, exclusive of the date of the advice note, Sundays and Bank Holidays, from the time the wagon is placed at the disposal of the Owner or Repairer, and to terminate when the wagon is labelled for despatch after repair.

The foregoing charges do not apply when the owners are parties to the Commuted Shunting and Siding Rent Charge Scheme, in which case the wagons are plated or stencilled "C.C."

8. Any authorised servant of the British Transport Commission may detain any wagon which may appear to him unfit to run until it has been put into proper repair and passed by an authorised person of the Western Region Carriage & Wagon Engineer's Department.

The British Transport Commission will not be responsible for any damage to Private Owners' Wagons left unprotected in an imperfect state by the Owners, nor for any injury that may occur to wagon repairers, who will be required to execute an indemnity before they are allowed to work on British Transport Commission's premises.

Private Owners' Wagons running over the British Transport Commission lines must not, apart from a reasonable description of the contents of the wagon, be used for advertising purposes, but the British Transport Commission will not object to a description (to be approved by them) of a product of the Owner's manufacture being painted thereon.

11. Nothing contained in these Regulations shall prejudice or affect any legal liability to each other of the actual Owners or Lessees of Wagons and the British Transport Commission.

(G.A.30 Op .-- 9/54. E.86585 H.10-F.).

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#### REGULATIONS AS TO PRIVATE OWNERS' WAGONS-Continued.

9. The Company will not be responsible for any damage to Private Owners' Wagons left unprotected in an imperfect state by the Owners, nor for any injury that may occur to wagon repairers, who will be required to execute an indemnity before they are allowed to work on the Company's premises.

10. Private Owners' Wagons running over the Railway Company's lines must not, apart from a reasonable description of the contents of the wagon, be used for advertising purposes, but the Railway Company will not object to a description (to be approved by them) of a product of the Owner's manufacture being painted thereon.

11. Nothing contained in these Regulations shall prejudice or affect any legal liability to each other of the actual Owners or Lessees of Wagons and the Railway Companies.

#### PRIVATE OWNERS' WAGONS STOPPED FOR REPAIRS.

# PRIVATE OWNERS' WAGONS STOPPED FOR REPAIRS-Page 183.

The existing instructions under this heading to be deleted and the following substituted :-

## PRIVATELY OWNED WAGONS STOPPED FOR REPAIRS.

The following arrangements must be carried out in connection with privately owned wagons stopped for repairs:—

1. The Carriage and Wagon Examiner must inform the Traffic Department Inspector, Guard or Shunter where loaded wagons are marked off, and arrangements must be made immediately by the Traffic Department to berth the trucks in a Cripple Siding.

2. The Carriage and Wagon Examiner must place the red "Not to Go" card over the traffic label in the pocket or under the clip, but not to completely obscure the traffic label. If the traffic label is tacked to the side of the wagon the red card must be placed immediately by the side of it.

3. When a wagon is stopped for repairs at a station where there is no Wagon Examiner appointed by the Carriage and Wagon Department, an advice of such stoppage must be immediately forwarded to the local representative of that department at the nearest examining station.

4. The Carriage and Wagon Department, after the necessary repairs have been carried out, will be responsible for intimating to the Traffic Department staff that the wagons are available to continue their journey.

5. When a privately-owned wagon, either loaded or empty is stopped for repairs particulars of the stoppage must be entered by the Carriage and Wagon Examiners in "949" Book, which is to be handed to the Goods Agent or Station Master daily to enable the necessary advices to be sent.

fafter the expiration of six days no action has been taken by a private owner or repairer to undertake repairs to the worm advised, the Examiner must re-enter particulars in the "949" Book of the wagon stopped and mark the entry "Second advice" or "Third advice" as the case may be.

#### REGULATIONS AS TO PRIVATE OWNERS' WAGONS-Continued.

- 9. The Company will not be responsible for any damage to Private Owners' Wagons left unpro
- 6. Immediately upon receipt of the particulars in the "949" Book the Goods Agent or Station Master must, in the absence of any special instructions to the contrary, despatch an advice upon Form 2827/I as follows:—
  - (a) To the freighter upon whose account the wagon is running.

(b) To the owner of the wagon as indicated by the iron plate affixed to the vehicle, in cases

where the freighter and owner are not one and the same.

Where special instructions are given upon the wagon as to the firm to whom the advice is to be addressed, such instructions must be carried out, or if the owners or freighters have issued any standing order in writing as to the repair of their wagons such orders must be complied with.

At the same time an advice upon Form 2826 must be despatched to :-

(a) Receiving station.

(b) Forwarding station where known.

7. The advices issued on Form 2827/1 must in all cases contain the following particulars :-

(a) Name of station or depot.

(b) Name of owner or lessee of wagon.

(c) Wagon Number.

(d) Name of destination station.

(e) Nature of defect as far as possible. (f) Full particulars of any material required.

The advices issued to forwarding and receiving stations must in all cases contain the following particulars in accordance with Form 2826:—

(a) Name of station or depot.

(b) Name of owner or lessee of wagon.

(c) Wagon number.

- (d) Forwarding and destination stations.(e) Nature of defect as far as possible.
- 8. When a wagon containing a through load is found during transit to be unfit to travel and Carriage and Wagon Department men with suitable material are available at or near the spot, and the necessary repairs to the vehicle can be carried out without causing greater delay than would otherwise occur owing to transhipment of the load, the vehicle should be repaired while under load. The local Goods and Carriage and Wagon Department must retain close contact to avoid the transit of the Goods being unduly delayed. In all other circumstances the load must be transhipped, a record of the occurrence being kept at the transhipping station (at large stations in a separate book), and in labelling the wagon into which the load is transhipped such transhipping station must state on the labels the name of the original forwarding station, the date on which the disabled wagon left and its name and number.

The transhipping station, on the day of the transhipment, must send a further advice on Form

2826 to the sending and receiving stations of the transhipment.

9. In cases where vehicles are stopped with hot boxes or require other light repairs which are executed by the Executive by arrangement with the Owner, care must be taken to rule out the remark on the 2827/I advice asking for instructions to be given with regard to the repair and to substitute a remark to the effect that the repairs are in hand.

10. Wagons ordered from one point to another for the purpose of repair must not be sent forward until a Consignment Note, or written forwarding order, specifying the route it is desired the wagons should travel, has been obtained from the owner or other party responsible, except when

painted with a yellow star as referred to in Clause 11.

II. Wagons must not be sent forward from a Western Region Station without a Western Region label being affixed, bearing name of the forwarding station and the route shewn on the Consignment Note, except in the case of wagons painted with a yellow star (which indicates that they come within the provisions of the Commuted Empty Haulage Scheme), which may, when labelled to Repair Works, be worked forward to the Repair Works without Railway Executive labels being affixed.

12. Wagons labelled for repairs must not be accepted from another Region at an exchange

function unless the labels indicate the name of the station from which it was forwarded.

13. Guards must not take forward from a station, junction or siding, Private Owners' wagons

bearing labels which do not comply with Clauses 11 and 12 hereof.

- 14. In cases of wagons labelled at local stations or sidings, payment of any charges due in accordance with the Rolling Stock Regulations in the General Classification must be obtained before the wagons are sent forward, except in cases where the senders have an authorised ledger account or the owners are parties to the Commuted Charge for empty haulage arrangement, in which case the vehicles bear a yellow five-pointed star, six inches in diameter.

  15. A charge for shunting of 1s. 8d. per wagon must be made in every case of a wagon stopped
- 15. A charge for shunting of Is. 8d. per wagon must be made in every case of a wagon stopped for repairs, whether required to be shunted into a private siding or into a siding belonging to the Executive. The charge of Is. 8d. to cover the shunting both into the siding for repairs to be carried out, and out of the siding after the repairs have been completed.

The foregoing charges do not apply when the owners are parties to the Commuted Charge for Shunting and Siding Rent arrangement, in which case the wagons are plated or stencilled "C.C."

Wagons conveyed from one station to another upon which haulage charges, as per General Classification Scale, are entered by invoice will not be subject to a charge for shunting at the destination station.

#### PRIVATE OWNERS' WAGONS STOPPED FOR REPAIRS-Continued.

9. A sharp look-out must be kept for wagons labelled empty to collieries for loading but intercepted en route and labelled to other points for repairs, in order to ensure the instructions contained in Chauses 4, 5, 7, and 8 being strictly observed.

in Clauses 4, 5, 7, and 8 being strictly observed.

10. A charge for shunting of 12 per wagon must be made in every case of a wagon stopped for repairs, whether required to be shunted into a private siding or into a siding belonging to the Company. The charge of 1/t to cover the shunting both into the siding for repairs to be carried out, and out of the siding after the repairs have been completed.

The foregoing charges do not apply when the owners are parties to the Commuted Charge for Shunting and Siding Rent arrangement, in which case the wagons are plated or stencilled "C.C."

If after the expiration of six days no action has been taken by a private owner or repairer to undertake repairs to the wagon advised, the Examiner must re-enter particulars in the "949" book of the wagon stopped and mark the entry "Second advice" or "Third advice," as the case may be.

Wagons conveyed from one station to another upon which haulage charges, as per General Classification Scale, are entered by invoice will not be subject to a charge for shunting at the destination station.

In cases of wagons (other than those included in the computed scheme for shunting and siding

16. In cases of wagons (other than those included in the commuted scheme for shunting and siding rent clause 15) detained for repairs at a station, or in one of the Executive sidings other than a siding let to a wagon repairing firm at a rental, siding rent at the rate of 10½d. per wagon per day must be charged after the expiration of three days (exclusive of the date of advice note, Sundays and Bank Holidays).

Example:—A wagon stopped and advice despatched on Monday. The three free days will be Tuesday, Wednesday and Thursday. Siding Rent will commence on Friday and will continue until the date on which repairs are completed, irrespective of where the wagon has been standing, less

Sundays and Bank Holidays, and accounts must be made up on this basis.

17. In cases where the Executive are unable to place wagons in their usual cripple Sidings, or in sidings where the needful protection can be afforded to men engaged on repairs, owners have been informed that the Executive is prepared to consider on its merits any cases where it can be proved the detention was not due to the fault of the owner or repairer of the wagon, and therefore, to safeguard the Executive's interests, it is very important the following information be recorded:—

1. The actual date the wagon is placed in position for repairs.

2. If the wagon cannot be placed on the date of stoppage in the cripple siding, or any other siding where the repairs can be executed under protection, the advice to the Owners and/or repairers should intimate that a further advice will be issued when the wagon is in a position where the repairs can be safely carried out, and a daily record must be kept of the numbers of the Firms' or Traders' wagons already stopped, both standing in and out of position.

3. As the majority of Private Owners' Wagons bear repairers' plates, the number of the wagons,

both in and out of position, for each repairer is to be recorded daily, and the number repair-

ed daily by each repiarer.

4. Any special application made for a wagon to be placed in such a position that it can be repaired must be recorded both as to date and time of the application.

18. Siding Rent is also to be charged on Private Owners' wagons waiting access to a wagon repairers' private siding or a siding rented by a repairer from the Executive. Owners and repairers must be advised on arrival, and the next three days will be the free period. (G.A.23-7/49. E.86585/H.)

# LABELLING OF NEWLY LIFTED WAGONS—Page 184.

The existing instructions to be cancelled and the following substituted ;-

WAGONS TABELLED TO PASS ON SLOW TRAINS

When wagons have received attention for hot axle boxes en route, they should be labelled with the Railway Executive "Not to run on Fast Trains" label (No. 521/2). Thereafter, on the authority of this label, they may only work to destination or repair depot as the case may be, on trains tipned at ordinary speed and which do not run more than 25 miles between atops. Such wagons must not be returned to normal traffic until they have received a proper repair. (G.A.23-7/49. E.86274/H. (2-C.))

#### RAILWAY COMPANIES' OR PRIVATE OWNERS' WAGONS LABELLED FOR REPAIRS.

Private Owners' wagons bearing green "For Repairs" labels must not be worked on Vacuum or Partly-Vacuum fitted freight trains but Railway Companies' wagons may be worked on any freight train irrespective of the headlamps carried, except as follows:

Wagons bearing the Green Label endorsed "Not to run on Fast Trains" must be restricted to trains carrying "J" or "K" headlamps.

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#### WAGONS LABELLED TO PASS ON SLOW TRAINS .- Page 184.

The instructions under this heading to be cancelled.

(G.A.30 Op .-- 9/54 E.86585H.).

# RAILWAY COMPANIES' OR PRIVATE OWNERS' WAGONS LABELLED FOR REPAIRS. —Page 184.

The instructions under this heading to be cancelled and the following substituted:—

#### WAGONS LABELLED FOR REPAIRS.

Wagons fitted with oil axleboxes and bearing green "FOR REPAIR" labels, indicating defects other than those appertaining to axleboxes, axleguards, wheels and axles may be conveyed by trains running under "D" or inferior headcodes.

Wagons fitted with grease axleboxes and bearing green "FOR REPAIR" labels, indicating defects other than those appertaining to axleboxes, axleguards, wheels and axles may be conveyed by trains running under "F" or inferior headcode conditions.

Wagons bearing green "FOR REPAIR" labels indicating a defective axlebox, axleguard, wheel or axle, the nature of which does not call for the wagon to be loaded upon another vehicle, should only be conveyed by trains running under "H" or inferior Headcode conditions.

(G.A.30 Op.--9/54. E.86585H.)

The following instruction to be inserted on page 184:-

# PRIVATE OWNERS' WAGON REPAIRERS MAKING USE OF "REPAIR WHEN EMPTY" CARDS.

When a firm attach a "Green Card" to a loaded wagon giving instructions for the vehicle to be sent to a Private Wagon Repairer after discharge, the label should be tacked to the wagon within about six inches of transit label, except in the case of all steel wagons when they should be placed in the ticket holder. (G.A.3.—12/37. R.C.H.Min.4870.)

#### LABELS FOR VEHICLES WITH DEFECTIVE BRAKES.

It is sometimes found that owing to wagon brakes being defective or requiring adjustment, the hand brake lever when lifted from the rest and released from the grasp falls to the bottom of the rack or guide.

Shunters and others discovering such a defect must replace the lever in the rest and secure it

to the guide, so as to prevent any attempt being afterwards made to operate the brake.

Should the vehicle be empty at the time it must be detained and the attention of the Station Master, Inspector, or any other responsible person in charge called to it, so that the Carriage

and Wagon Examiner at the nearest point may be advised.

In the event of a defect of this kind being discovered on a loaded wagon in transit, in addition to seenring the lever, the guard, on arrival at the next examining station must report the matter to the Examiner, who will, if possible, give the brake the necessary attention, or label the truck for repairs with a special card provided for the purpose, overprinted in red lettering "Defective Brake," as shown below :-

									Railw	ay.
FO	R		RE	EP	AI	R	S	_	/F	-
From				_		F	R	A	KE	-
Те	_	-	7	VI	E	-	-		_	
Date_DEF	E	C	, 1	-	_					
Defects DE		_								
Any unauthoristo which it has been prosecution.					ng thi	s Ca imse	rd fr	om t	he veh	nicle inal

Wagons bearing these special labels must in no circumstances, after reaching their destination be reloaded until the brake has been put into good working order.

#### IRON FITTINGS OR APPLIANCES FALLING FROM ENGINES, CARRIAGES OR WAGONS.

Couplings, brakework and other fittings on freighters' wagons break off in the ordinary course

of travelling, and are not returned for repairs in all cases with the wagons.

When the fittings become detached while the trucks are running, they cannot of course in all cases be sent home with the trucks, but when the parts of the wagon become detached while the trucks are empty and when being shunted at Stations or at any place where the fittings can be put into the wagons, it is the duty of the Guards and Shunters or other persons concerned to see that such portions of the trucks as break away are placed into the wagons so that they may be sent to the proper Stations for repairs.

In cases of the failure of a coupling, or of ironwork of any kind falling from a loaded private wagon, the Guard should take possession of the broken parts, and hand them over to the Carriage Department Staff at the first examining station, as if put on top of the load they are liable to be lost while the

vehicles are running or being unloaded.

#### EMPTY PRIVATE WAGONS.

EMPTY PRIVATE WAGONS, bearing neither label nor painted instructions for disposal, must be held and application made to the owners for forwarding orders, care being taken to see that payment of the haulage charges due is obtained, in accordance with the regulations in the General Railway Classification, unless the owners are parties to the commuted charge for empty haulage arrangement, in which case, the vehicles bear a yellow five pointed star 6 inches in diameter.

#### COUPLING WAGONS WITH DRAW-BAR HOOKS OF VARYING HEIGHTS.

Goods Guards, Shunters and others concerned should, as far as they can consistently with reasonable despatch, adopt the principle of placing the coupling of the lower hook over the higher hook when coupling wagons having hooks of varying heights.

#### COUPLINGS NOT PROPERLY DROPPED INTO DRAW-BAR HOOKS.

When coupling wagons Goods Guards and Shunters must see that in all cases the coupling link drops properly into the hook, and not allow it to ride on the "Gedge" slot.

#### WAGON DRAWGEAR BREAKING.

Breakages of drawgear on wagons may be caused by the train being started with the brake of a heavy brake van hard on, thus causing a severe snatch. Enginemen should exercise every care in starting their trains in order to avoid undue snatching of the couplings and drawgear, and when their trains are started Goods Guards should be careful to ease off the hand brake before the engine picks up the full load.

Drivers sometimes apply the steam brake to check trains when the hand brake would be sufficient for the purpose. Drivers should be careful not to use the steam brake unnecessarily, nor (except in emergencies) in such a manner as to cause a violent jerk on the train.

It is the duty of the Guard of any train in which heavily loaded vehicles, such as "Crocodiles," "Macaws," &c., are formed, to advise the Driver how many such vehicles there are and their position in the train.

In the event of a train dividing without any portion of the drawgear breaking, the Guard, when recoupling the divided portion of the train, must double cauple at both ends of both vehicles in connection with which the separation has occurred; the Examiner's attention being called to the matter as usual at the first examining station in order that one or both of the vehicles may, if necessary, be labelled for repairs, which can be effected on arrival at destination.

#### INSTRUCTIONS TO GUARDS OF TRAINS, LEFT "DEAD" EN ROUTE.

In the event of a train being left "dead" in a refuge siding or place not attached to a station or depot, the guard will be responsible for seeing that an advice is dispatched to the Control of any cattle or perishable traffic on his train, in order that arrangements may be made for working it forward promptly.

#### TRANSIT OF STEAM AND HAND TRAVELLING CRANES.

Steam and hand travelling cranes must be conveyed from place to place, as far as possible, on slow freight trains, and then subject to the following conditions:—

(a) Not more than one cranc of dead weight exceeding 30 tons may be attached to the rear of any freight train, next to the brake van, and then only provided no other vehicle conveying an exceptionally heavy consignment is also attached.

(b) Cranes of greater lifting capacity than 25 tons must not be conveyed by ordinary freight services. When not forming part of "breakdown" or "Engineering Department train," they must always be worked from point to point as a special train.

#### TRANSIT OF STEAM AND HAND TRAVELLING CRANES.—Page 186.

The following to be added as clause (c):-

A special train conveying a 45 ton steam crane complete with match truck and Stokes bogies, which are piped, when travelling within the Western Operating area to or from the site of Engineering Department Operations, may carry "B" headcode, provided the Crew's van is also piped and the other vehicles comprising the train are fully vacuum fitted. Such trains must not exceed a speed of 45 m.p.h. at any point.

G.A.30 Op.—9/54. LKI/12204/417. EX.3509/53.).

#### SECTION III.

CENERAL INSTRUCTIONS AFFECTING THE LOADING AND CONVEYANCE OF MERCHANDISE TRAFFIC, ALSO LIVE STOCK BY PASSENGER AND FREIGHT TRAINS.

(a) Loading, conveyance, etc., of merchandise ... ... ... ... ... ... 189 to 246

(b) Loading, conveyance, etc., of live stock by passenger and freight trains ... ... ... ... ... ... 248 to 258

# SECTION III. (a).

# INSTRUCTIONS CONCERNING LOADING, CONVEYANCE, ETC., OF MERCHANDISE TRAFFIC.

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\* Serpents conveying containers need not be labelled with "Examine Load" label, provided special attention is given to the securing of the containers to the rail vehicles.

(G.A. 8.-5/41. E.81567/H.)

# LOADING, ETC., OF MERCHANDISE TRAFFIC.

#### SELECTION OF SUITABLE WAGONS.

Suitable wagons should be selected for the conveyance of Freight traffic according to the nature of the consignment to be carried. If proper attention is paid to this point and to care in loading, the risk of loads shifting during transit will be minimised.

#### CARE IN LOADING.

Goods Foremen, Loaders, Shunters and Guards must take the utmost care to see that the contents of all wagons are evenly distributed, safely and properly loaded and secured where necessary. Doors must be correctly secured and no wagon loaded beyond its registered capacity.

Wagons tendered by another Company must comply with the foregoing conditions before going

Improper loading or overloading must be reported at once by the person in charge to the District Goods Manager.

#### EXAMINATION OF LOADS.

Guards must strictly comply with Rules 129, 131, 156, 157, 158 and 171 of the Rule Book, which lay down the absolute need for careful examination of all loads before starting and during the journey. Loads received at junctions with other Companies' lines should be carefully examined by the Yard Inspectors, Foremen, Shunters and Guards, in order to ascertain whether they have become in any way displaced on the journey, and to make sure that the binding chains, ropes, etc., are secure and in proper condition.

Guards should not refuse to take wagons loaded from other Companies' lines because the loading is not in accordance with the Regulations shewn in this Company's General Appendix, pages 189 to 246 inclusive, provided the Guard is satisfied that the loads are in good order and fit to travel. If he is not satisfied, he should call the attention of the person in charge of the yard or other responsible official to the load, and explain to him the reasons for not taking the traffic forward, so that the load may

be properly inspected.

#### ARRANGEMENTS FOR LOADING AND DESPATCH OF SPECIAL CONSIGNMENTS.

Wagons of the types enumerated below must, before despatch under load, be labelled with "Examine Load" labels as per illustration appended.

" Pollens." "Loriots." "Crocodiles." "Serpents." "Rectanks." "Scorpions." "Hydras."

" Corals."

" Morels." " Beavers."

"Totems." "Beavers."
Bogie bolster wagons ("Macaw" and "Gane" type) when loaded with articles such as built up girders, cylinders, or consignments of exceptional character as regards length, breadth, height or weight, including loads the conveyance of which is specially authorised by the Chief Goods Manager, must be labelled with the "Special Load" label.

In cases of doubt the Loading Staff have authority to utilise the special label.

EV	Vestern Railway Comp	
	TQLbs	
TO TO GO FORWARD FROM LOADING POINT INTIL INSPECTED AND PASSED AS PROPERLY OADED AND SECURED	DATE19	This load must be examined during transit and at all exchange
EXAMINED BY-	Via	pcints.
	SHEETS IN or ON Wagon. Total No	. 8
DATE	Owner & No, of Wagon	

"Examine Load," "Shunt with Care" and Figure "3" appear on the label in red print.

Load, to the

/H.)

#### LOADING, ETC., OF MERCHANDISE TRAFFIC.

#### ARRANGEMENTS FOR LOADING AND DESPATCH OF SPECIAL CONSIGNMENTS-Continued.

The "Examine Load" label must be signed, in the space provided for signature, by a member of the Company's staff to whom this responsibility has been delegated by the local officer. This applies in the case of Goods Department traffic proper, or for departmental traffic leaded on behalf of the

Engineering, Locomotive, Stores, or Road Transport Departments.

The signing of special labels for wagons loaded by the Engineering Department will be undertaken by the Permanent Way, Bridge and Mechanics' Inspectors, Sub-Inspectors and Foremen in those cases where one of these is available for the purpose. In other cases the Engineering Department to communicate with the District Goods Manager, who will arrange for an authorised person to examine the load and certify the special label either where loaded or at the nearest suitable point. In the latter event the Engineering Department to affix over the special label an ordinary label to the point of examination stipulated by the District Goods Manager.

The respective departments mentioned will be responsible for the safe loading and despatch of

such consignments loaded, secured, and passed for transit by the staff under their control.

Yard Masters, Yard Inspectors, and Traffic Yard Supervisory Staff must satisfy themselves that special loads originating on the Great Western line (see clause I) are properly labelled in accordance with these requirements, and that the weight of the consignment is shewn on the label. They are not to allow special loads to go forward should labels not be in order as herein directed.

Loads arising on other Companies' lines exchanged to the Great Western Company at the various

junctions, will continue to be examined before acceptance by this Company.

G F1 10

#### ARTICLES EXCEEDING THE PUBLISHED GAUGE DIMENSION3.

The District Goods Managers will advise the Chief Goods Manager of any "out-of-gauge" consignments expected to pass. The Chief Goods Manager will at once furnish the Superintendent of the Line with a preliminary advice, and obtain assent for the passage from the other Companies concerned, also Engineering and other Departments affected by the passing of the traffic. After the foregoing has been obtained, the Chief Goods Manager will advise the Superintendent of the Line and the District Goods Managers concerned. Upon receipt of the sanction to the passage, the Superintendent of the Line will advise the Divisional Superintendents or District Traffic Managers concerned.

The District Goods Managers from whose district a consignment is being forwarded must give the Divisional Superintendent the longest possible advance notice of the time and date the consignment will be ready to travel and the latter will then issue the necessary instructions and advise the other Divisional Superintendents or District Traffic Managers concerned on what train the traffic will be despatched. If a consignment is forwarded on a train terminating in an intermediate Division, the Superintendent of that Division must advise the other Superintendents of the proposed connecting

The District Goods Manager must also advise the other Companies concerned of the despatch, and the Divisional Superintendents or District Traffic Managers must advise the Divisional Engineers

The points between which loads exceeding 10 feet in width may not pass nor be passed by coaching stock or "out-of-gauge" loads on the principal routes are laid down in the revised schedules for loads 10 feet (exclusive) to 10 feet 9 inches (inclusive) in width. It must be understood, however, that no load exceeding the standard gauge dimensions for sections of line concerned will be allowed to travel until a certificate for its passage has been given by the Chief Engineer to the Chief Goods Manager, which certificate will specify particular restrictions and requirements in each individual case.

The instructions mentioned above in respect of loads exceeding 10 feet in width passing coaching

stock must be issued to the Traffic Department staff concerned, in the following form:

"Must not pass or be passed by any passenger, parcels, milk, perishable or any other passenger rated train, or by any goods train conveying 'out-of-gauge' loads or coaching stock on the adjoining line, between the following signal boxes:—"

The Divisional Superintendents or District Traffic Managers must make such arrangements as will ensure the above instruction being observed, especially in regard to the passing of Freight trains conveying coaching stock on the adjoining line.

"Out-of-gauge" loads up to and including 10 feet in width may be conveyed by ordinary trains, provided certificates have been furnished in the usual way by the Engineering Department that they

Loads exceeding 10 feet in width may in certain instances and where practicable be conveyed by ordinary trains, providing the authority of the Superintendent of the Line is first obtained. In the event of special train working being necessary on a week-day or Sunday, a schedule must be agreed beforehand, at the point to point times laid down for ordinary freight trains, subject to all permanent, temporary and special speed restrictions being observed. The working of the special train must be so arranged as will comply with the restrictions imposed by the Engineering Department.

The loads of such specials may be made up with through traffic only to the maximum single engine

"The District Goods Manager must also advise the other Companies concerned of the dispatch and the Divisional Superintendents or District Traffic Managers must advise the Divisional Engineer concerned only when this action is stipulated in the conditions of passage by the use of the Code word 'DELOD'."

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"The special examination of the loads by Chief Mechanical Engineer's Inspectors at the exchange junction may be dispensed with in the case of out of gauge consignments arising on other Companies' Lines."

#### ARTICLES EXCEEDING THE PUBLISHED GAUGE DIMENSIONS-page 190.

The following to be added after the eighth paragraph on page 191:

"Should a Traffic Inspector not be readily available, and in order to obviate delay to traffic, the Divisional Superintendent or District Traffic Manager may arrange, for a competent man to undertake the duty.

"In the case of loads not exceeding 10 ft, in width, but which through exceptional length or other peculiarity, stringent conditions of passage, etc., in the opinion of the Divisional Superintendent, or District Traffic Manager, require the services of a Traffic Inspector, such to be provided by the Divisional Superintendent or District Traffic Manager. Should a Traffic Inspector not be readily available, arrangements to be made for a competent man to undertake the duty."

(G.A. 7.-3/40. O.M.11949.)

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#### LOADING, ETC., OF MERCHANDISE TRAFFIC

#### ARTICLES EXCEEDING THE PUBLISHED GAUGE DIMENSIONS-Continued.

All wagons used for the conveyance of exceptionally heavy machinery or any other article exceptionally heavy or lengthy must be placed next in front of the rear brake van, but when very exceptional loads are conveyed by trains carrying other traffic and accompanied by a Swindon Inspector, the Chief Mechanical Engineer will stipulate whether the "out-of-gauge" load must be marshalled at the front or rear of the train, and if in front a second brake van should be provided next to the engine. (Such loads must be earefully watched while running.) Special trains on a Sunday conveying "out-of-gauge" traffic must be booked for examination in accordance with what is shewn below. If there is no local Examiner available, the Swindon Inspector accompanying the "out-of-gauge" traffic will be responsible for the examination of the wagons.

In the event of ordinary traffic being conveyed on the "out-of-gauge" special train on a Sunday, such will be examined in the ordinary way at points where an Examiner is on duty, but in cases where no Examiner is on duty special arrangements must be made for examination by the Divisional Traffic and Loco. Superintendents.

Girders loaded on "Pollen" class wagons extended must be conveyed by special train with no other traffic attached, and one of the Chief Mechanical Engineer's Inspectors from Swindon must accompany the train to destination or point of exchange with other Company.

Loads which exceed the width of the truck on the 6-foot side must not be conveyed through any section where single line working is in operation, or through a station where there are platforms on each side of the line over which the loads will be conveyed or where there is an island platform on double roads, until proper enquiries and arrangements for working have been made.

A Chief Mechanical Engineer's Inspector must always examine and, where considered necessary, travel with "out-of-gauge" consignments up to 10 feet wide. A Chief Mechanical Engineer's Inspector from Swindon must always examine loads exceeding 10 feet wide, and travel with these

This must be done with consignments arising on the G.W. Line and those received from other Companies' Lines.

In all cases care must be exercised to see that the train is not shunted on to the opposite line for refuging or single line working purposes unless this can be safely done.

Traffic Inspectors thoroughly conversant with the working in each Division to travel with all loads exceeding 10 feet in width. The Inspectors must make themselves acquainted with all special arrangements applicable to the safe working of all trains, and must see that these instructions and any other special instructions issued are carried out.

Cases are too frequently occurring of wagons failing with hot axles in consequence of the bulk of the load being placed at one end of the vehicle. It is important that loads of all descriptions be as evenly distributed as the circumstances will allow.

In no circumstances must any specially constructed wagon be loaded with a consignment exceeding 75 per cent. of the marked carrying capacity of the vehicle; e.g., a wagon marked to carry 20 tons must not be loaded with more than 15 tons, except under the special supervision of an inspector from the Chief Mechanical Engineer's Department.

If the Swindon Inspector subsequently finds that a load which has been advised as "out-of-gauge" can with safety travel under conditions appertaining to loads within gauge, he will give authority for this in writing to a responsible Inspector in the Yard at which the load starts, and the latter must advise the Divisional Superintendent, who in turn will send the necessary advices covering the throughout journey so far as it relates to the Great Western system. If the Divisional Superintendent's or District Traffic Manager's Office is closed this duty will fall upon the Control or where a Control does not exist, a responsible Officer deputed by the Divisional Superintendent or District Traffic Managers. This instruction will not apply beyond the limits of the Great Western Railway.

Articles exceeding the published gauge dimensions must not be conveyed through the Severn nel. (See Appendix to No. 4 Section of the Service Time Tables.) Tunnel.

"It is essential that the greatest care be exercised at loading points in connection with exceptional and out of gauge loads to ensure that the dimensions do not exceed those for which passage authority has been given." (G.A. 10. 3/42. E.82066/H.).

LOADS EXCEEDING 80 FEET for G.W. Stations and 60 feet for other Companies' Stations, or the equivalent length thereof, are to be considered as of exceptional dimensions, and special authority for their conveyance must be obtained from the Chief Goods Manager before being sent forward. Consignments exceeding 60 feet must not be conveyed on Freight trains carrying "D" "E" or "F" head cooks" ol

m Note.—The equivalent length of a load is arrived at by multiplying the longest overhang measured from the headstock of the wagon by two and adding the distance over headstock of the vehicle. six inches

Denotes points at which load is chained to bolster or to wagon,

Denotes points at which chains or ropes are fastened loosely round load only. 0

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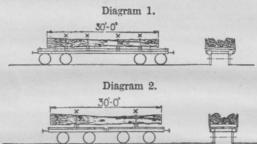
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#### LOADING, ETC., OF MERCHANDISE TRAFFIC.

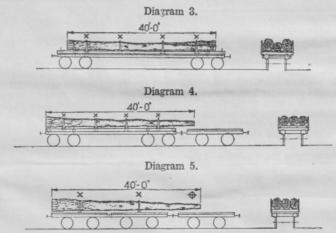
#### LOADING OF ROUND TIMBER-Continued.

#### INSTRUCTIONS FOR LOADING.

1. LOADS FXCEFDING 20 FEET AND UP TO 30 FEET IN LENGTH.—The timber must be loaded on 20 ton Rail and Timber Trucks (Macaw H) when available, with end bolsters placed in inner brackets as shewn on Diagram 1, and care taken to see that the butts overlap the end bolsters by at least one foct six inches. When Macaws H are not available Twin Timber Trucks (Mite) may be used as an alternative—see Diagram 2. The butts should, as a rule, be reversed, but where there is a difficulty in doing this, they may be loaded at one end. In either case the load must be securely chained at the bolsters and all stanchions used.



2. LOADS EXCEEDING 30 FEET AND UP TO 40 FEET IN LENGTH.—The timber must be loaded on Macaws B, D, or E, with end bolsters placed in inner brackets as shewn on Diagram 3, and care taken to see that the butts overlap the end bolsters by at least one foot six inches with butts reversed, or on Macaw H with match truck at one end to act as under-runner only and entirely free of load—Diagram 4. When Macaws B, D, E, or H are not available Twin Timber Trucks (Mites) with match truck at one end to act as under-runner only, may be used as an alternative, but the match truck must be entirely free of the load; where load exceeds 39 fect a match truck without bolster should be used—see Diagram 5. In the cases of Macaw H and Twin Timber Trucks (Mites), the butts must be loaded at one end of the load-carrying wagons and loads securely chained at the bolsters of Macaw H or Mites only, with all stanchions in position. When Twin Timber Trucks (Mites) are used, the smaller end of load should be bound round with a separate chain or rope when necessary to prevent spreading, but this chain or rope must not be secured in any way to the match truck.



LOADS EXCEEDING 40 FEET must not be conveyed on Twin Timber Trucks (Mites,

3. LOADS EXCEEDING 40 FEET AND UP TO 45 FEET IN LENGTH.—The timber must be loaded with butts reversed on Macaws B, D or E to Diagram 6, or on 20-ton Rail and Timber Truck (Macaw H) with match truck at one end to act as under-runner only, as snewn on Diagram 7. When the method shewn on Diagram 7 is adopted, the butts must be loaded at one end of Macaw H and load securely chained at the bolsters of this wagon, with all stanchions in position. Where load exceeds 43 feet in length, a match truck without bolster should be used. The smaller end of load should be bound round with a separate chain or rope, when necessary, to prevent spreading, but this chain or rope must not be secured in any way to the match truck.

#### LOADING OF ROUND TIMBER.

# INSTRUCTION 5. LOADS EXCEEDING 54 FEET AND UP TO 60 FEET IN LENGTH—page 193.

The following to be added at the end of the paragraph:

Macaw J may also be used but match truck will not be required.

(G.A. 7.—3/40. C.G.M.—W.T./X.14231.)

# INSTRUCTION 6. LOADS EXCEEDING 60 FEET AND UP TO 70 FEET IN LENGTH—page 193.

The following to be added at the end of the instruction:

Macaw J may also be used, but match truck will be required at one end only for lengths exceeding 62 ft. 6 inches. The small end of load only to overhang, as in Diagram 9, and the end bolster at that end should only be placed in the outer brackets, when the load exceeds 64 ft. in length.

(G.A. 7-3/40. C.G.M.-W.T./X.14231.)

# INSTRUCTION 7. LOADS EXCEEDING 70 FEET AND UP TO 80 FEET IN LENGTH—page 193.

The following to be added at the end of the instruction:

Macaw J may also be used and the foregoing conditions are to be observed.

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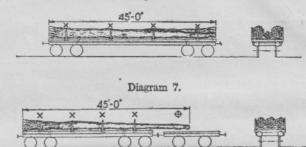
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#### LOADING, ETC., OF MERCHANDISE TRAFFIC.

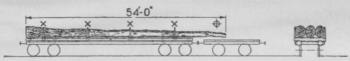
LOADING OF ROUND TIMBER-Continued.

#### Diagram 6.



4. LOADS EXCEEDING 45 FEET AND UP TO 54 FEET IN LENGTH.—The timber must be loaded on Macaws B, D or E to Diagram (8), with match truck at one end, when necessary, to act as under-runner only. Care must be taken, when a match truck with bolster is used, to ensure that load clears bolster when buffers are compressed. The smaller end of load should be bound round with a separate chain or rope, where necessary, to prevent spreading, but this chain or rope must not be secured in any way to the match truck.

Diagram 8.



5. LOADS EXCEEDING 54 FEET AND UP TO 60 FEET IN LENGTH.—The timber must be loaded on Macaws B, D or E, with end bolster at one end placed in outer brackets, as shown on Diagram 9, with match truck (without bolster) used at one end to act as under-runner only. The smaller end of load must be bound round with a separate chain or rope to prevent spreading, but this chain or rope must not be secured in any way to the match truck,

Diagram 9.



6. LOADS EXCEEDING 60 FEET AND UP TO 70 FEET IN LENGTH.—The timber must be loaded on Macaws B, D or E (overhanging not more than 9 feet at butt end of load), with end bolsters placed in outer brackets and a match truck (without bolster) used at both ends, as shewn on Diagram 10. The width of load must be confined to 4 feet 6 inches at small end, and when overhang over bolsters is 15 feet or over, see Diagram 10b. A separate chain or rope should be bound round small end of load, to prevent spreading, but this chain or rope must not be secured in any way to match truck.

#### Diagram 10.



7. LOADS EXCEEDING 70 FEET AND UP TO 80 FEET IN LENGTH.—The timber must be loaded with butts reversed on Macaws B, D or E, with a match truck (without bolster) at each end, and the width of load confined to 4 feet 6 inches, see Diagram 11. A separate chain or rope should be bound round both ends of load to prevent spreading, but these chains or ropes must not be secured in any way to the match trucks.

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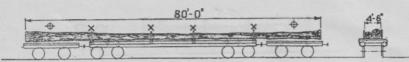
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#### LOADING, ETC., OF MERCHANDISE TRAFFIC.

#### LOADING OF ROUND TIMBER-Continued.

Diagram 11.



In loading sets of big timber it is important that the bottom tier should rest upon the bolsters and that, when the second tier would reach half its diameter above the tops of the stanchions, the trees should, as far as possible, be loaded saddle fashion, i.e., approximately, four in the lower tier, three in the second tier, and so on. In no case must any trees be allowed to rest on the top of the stanchions, and care must be taken that both the butts and the tops of the timber are so placed on

the bolsters as to ensure a clearance over sides and ends of wagons when passing over curves.

In all cases where the loads of timber extend to the full width of the bolsters, the "D" Shackles, where fitted, must be turned OUTSIDE the stanchions, and in every case careful inspection of the trees should be made before loading is commenced, so that the longest trees may be placed in the middle of the sets, to allow the maximum amount of play in rounding curves.

Before loading is commenced the chains and "D" Shackles attached to the trucks must be

thoroughly overhauled, as the safety of the load is to a large extent dependent upon them; and, in addition to the inspection of the loads by the Timber Loaders, they must, as far as possible, be care-

fully examined at the starting points by the Station Masters or Goods Agents as the case may be.

When twin timber trucks ("Mites") are used the bolsters must be greased underneath in the centre before loading and also the quadrants upon which they move, in order to give them free play. This duty rests with the Chief Mechanical Engineer's Department, but at stations where there are no Greasers or Examiners belonging to that Department, the greasing (or oiling) of the bolsters must be carried out by the Goods or Traffic Department Staff who perform the loading. The Inspector, or Foreman, or other person superintending the loading will be held responsible for seeing that the bolsters are properly greased or oiled before the wagons are loaded.

In loading timber on Branch Lines it is desirable that the butts should as far as possible be placed

so as to lead on to the Main Line.

Trucks loaded with Timber must be formed as near the end of the train as circumstances will

The loads must be examined by the Guards when taken on, and also by Guards and Shunters at each stopping place, where the chains should be adjusted if necessary.

#### LOADING OF BAULK TIMBER.

BAULE TIMBER EXCEEDING SO FEET IN LENGTH FOR G.W. STATIONS AND 60 FEET FOR STATIONS ON OTHER COMPANIES. LINES MUST NOT BE ACCEPTED FOR CONVEYANCE WITHOUT THE AUTHORITY OF THE CHIEF GOODS MANAGER.

X Denotes points at which load is chained to bolster or to wagon.

Gensignments exceeding 70 feet in length must not be conveyed on freight trains earrying "C," "D," "E" or "F" Headlamps.

Loads of Baulk Timber must overlap the Bolsters upon which they are carried by at least 1 foot 6 inches.

Baulk timber exceeding 60 feet in length must not be accepted for conveyance over the undermentioned sections of the Great Western Railway without the authority of the Chief Goods Manager :-

Barry and Bridgend and Coity Junction. Bassaleg to Caerphilly and Duffryn Isaf.

Blaenau Festiniog Branch, between Maentwrog Road and Blaenau Festiniog. Bristol Port and Pier Line.

Burry Port and Gwendraeth. Coleford Branch.

Cornwall Minerals Lines-

Lostwithiel to Fowey. Fowey to St. Blazey. Par to Newquay. Goonbarrow Branch. Bugle to Carbis Wharf.

Burngullow to St. Dennis Junction.

Treamble Branch. Retew Branch.

Culm Valley Line (Tiverton Junction to Hemyock). Eardisley Branch.

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#### LOADING OF TELEGRAPH POLES—page 194.

The following additional instructions to be added:-

Telegraph Poles exceeding 50ft. in length must be loaded tip to butt on Bobol "C" or Borails "B" or "D" with the end bolsters placed as near the headstocks as possible, and the load securely chained at the bolsters.

Poles up to 60ft in length may be loaded with the overhang at one end, but in such cases the load must not exceed threequarters of the written capacity of the wagon.

Poles exceeding 60ft, in length must be loaded centrally on the wagon, and when the overhang at each end exceeds 15ft, the width of the load must not exceed 4ft, 6ins.

Borails "F" or "C" may also be used, if available, but the overhang should not exceed 15ft. and if at one end of the wagon only, the load must not exceed threequarters of the written capacity of the wagon.

Loads exceeding 80ft, for G.W. stations and 60ft, for other Companies' stations, or the equivalen length thereof, are to be considered as of exceptional dimensions, and special authority for their conveyance must be obtained from the Chief Goods Manager before being sent forward. Consignments exceeding 60ft, must not be conveyed on Freight trains carrying "D", "F", or "H" headlesses GRZW

Note.—The equivalent length of a load is arrived at by multiplying the longest overhang measured from the headstock of the wagon by two and adding the distance over headstock of the vehicle.

Other Companies' Bobols "A", "B", "C" and "D" which are now in Common User may also be loaded subject to the arrangement of bolsters of these vehicles, allowing the foregoing instructions to be complied with in principle.

(G.A 16, 5/46, C.G.M. W.T./X66806)

#### LOADING OF TELEGRAPH POLES.

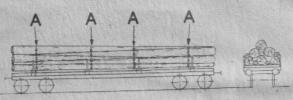
Methods of carrying Telegraph Poles tip to butt loading.

Telegraph poles should be loaded as self-contained load on bolster wagons as shewn in Diagram 12.

If suitable wagons for this purpose are not available, telegraph poles may be loaded on single bolster wagons as shewn in Diagram 13.

Care should be taken to ensure that the pc es are recurely chained together to prevent any disintegration of the load during transit.

#### Diagram 12.

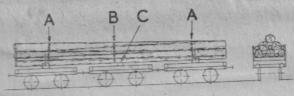


Self contained load.

#### May be loaded to full width of Bolster

Where shackles are not provided on bolsters, the load to be chained to the securing rings on the wagon frame as close as possible to the bolsters.

#### Diagram 13.



#### Three Wagon Set.

May be loaded to full width of Bolster.

Stanchions to be removed from Centre Wagon and load chained free of wagon.

Total weight of load not to exceed capacity of two wagons.

A—Loa d chained to Bolsters, Stanchions in position, Shackles outside Stanchions when, owing to load extending to full width of Bolsters, they (the Shackles) cannot conveniently be placed inside the Stanchions.

B-Load chained free of wagon.

C-Bolster to be retained.

(G.A.13. 9/43. C.G.M.-W.T./X.66806.)

the equivalent length of a load is arrived at by multiplying the longest overhang measured detack of the wagon by two and adding the distance over headstock of the vehicle.

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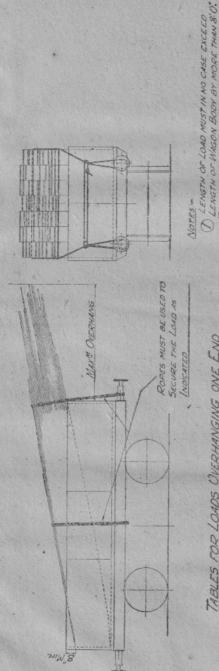
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LOADS EXCEEDING 80 FEET for G.W. Stations and 60 feet for other Companies' Stations, or the equivalent thereof, are to be considered as of exceptional dimensions, and special authority for their conveyance must be obtained from the Chief Goods Manager before being sent forward. Consignments exceeding 60 feet must not be conveyed on Freight trains carrying "D" "E" "F" or "H head lumbles GA 26

Note.—The equivalent length of a load is arrived at by multiplying the longest overhang measured from the headstock of the wagon by two and adding the distance over headstock of the vehicle.

(G.A.12, 4/43, C.G.M.-W.T./X, 68878.)



# TABLES FOR LOADS OVERHANGING ONE END

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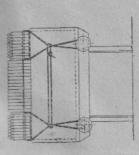
	ANY LOAD EXCEEDING 25-6 IN LENGTH MUST BE	C) LOADED FLAT ON OTHER SUITABLE STOCK OTHER THAN ORDINARY WAGONS.	WOTEXCEEDING SO! STEEL WOOD STEEL WOOD STEEL WOOD STEEL WOOD SOOKS ARE PROPERLY SECURED. THE OF THE SOURCE OF SOURCE OF THE SOOK STEED SOURCE OF THE DOOR	HINGE BAR & NOT EXCEED ZFT. NOR THE WASIMUM	& OVERHANG TO BE AT TRAILING END WHERE POSSIBLE	S ROPES MUST BE USED TO SECURE THE LOND AS INDICATED	6) LOADS NOT TO OVERHANG WAGON BY LESS THAN SINS	DEMOSS MUST BE LAID LONGITUDINALLY & NOT DEMOSONALLY ACROSS THE WAGON.
Statement of the last			WOOD FRAME	S	T	42	1	7
or serves	TONS	13 TON	STEEL FRAME.	0		0	4	01
とうちの からからないから	MAXIMUM LOAD IN TONS	MGON/	WOOD .	4	1	5 22 5 32 6	N	0
Sandyana,	W LOA	DION	STEEL FRAME	5		5	,	4
Street on our other	OXIMO	VAGON	WOOD FRAME	3	1	23	1,0	2 62 4 5 0
-	M	8 TON 1	STEEL	A	-	3	N	7
And the last of th	MAXIMUM	WERHANGS WITH 8 TON WAGON OTON WAGON 3 TON WAGON	VOTEXCEEDING SOLID	6FT & UNDER 4 3 5 4 6 5		767	-378	110

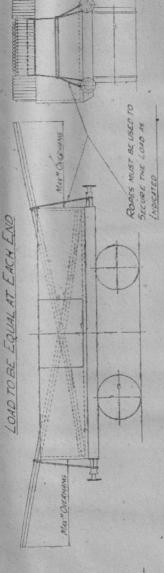
(SEE NOTE 3 FOR END DOOR WAGONS.)

DIACRAM X"

PIPES, ANGLES, BARS ETC, LOADED TO FULL WIDTH OF WAGGAS, WHICH MAY BE CONVEYED MAX "LOADS & OVERHANGS OF TIMBER.

IN ORDINARY 8,108,13,70N WAGONS. (GAIS CG.M.-WTX72780)





TABLES FOR LOADS OVERHANGING BOTH ENDS

41	02	500						
	Wear	WOOD FRAME	80	8	N	20	0	5
75NS	13 TON 1	STEEL FRANK	8 01	6	8	7/2	7	0
NI 04	VAGON	WOOD .	1	9	52	5	4	33
MAXIMUM LOAD IN TONS	OBNI	STEEL FRAME	8	1	62,	9	5	4
XIMU	46ON	WOOD	5 . 4	4	4	3	N	2
MA	8 TON WAGON 10 TON WAGON/3 TON WAGON	STEEL WOOD STEEL WOOD STEEL WOOD FRAME FRAME FRAME	5	5.	5	4	2	5
MAXIMUM	2	WITH EOUAL LENGTHS (SOLID LOAD)	17/ 1/17	2.67	357	457	5.67	, 6F

OVERHANGS WITH 8 TON WARDN OTON WARDN/37DN/VAGON UNEOUNL LENGTHS UNEOUN 502 57EEL NOOD STEEL NOOD OF LOJD TO BE FRAME FRAME FRAME FRAME 50LO) 6/T 8 UNDER 5 4 62 52 8 7 7/T 42 52 62 52 8 7
TEL WOOD ST THE FRAME FR 5 4.
5 4.
12 52
4 3 6 5 72 62

DIAGRAM "Y" OVERHAN

NOTES 
LENGTH OF LOAD MUST IN NO CASE EXCEED

LENGTH OF WAGON BODY BY MORE THAN 8'O

(SEE NOTE 5 FOR END DOOD WAGONS)

ANY LOAD EXCEEDING 25'6 IN LENGTH MUST BE LOADED FLATON OTHER SUITABLE STOCK OTHER THIN ORDINARY WASONS

E SEND DOOR WAGONS MUST NOT BE USED FOR SOLONDS OVERHANGING AT BOTH ENDS.

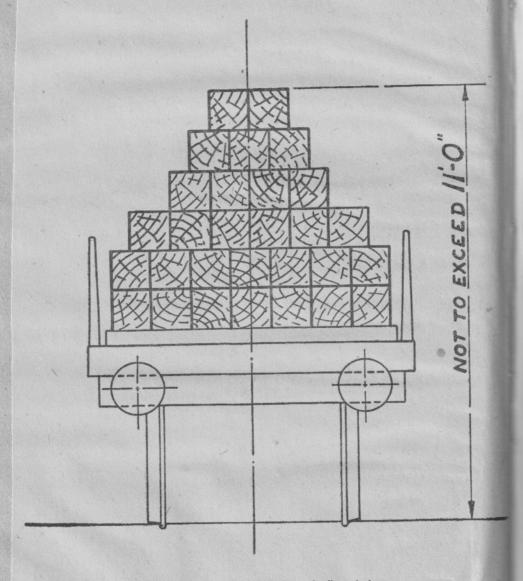
A RODES MUST BE USED TO SECURE THE LOAD AS INDICATED.

(S) LOADS NOT TOOVERHANG WAGON BY LESS THAN 9 INS.
LOADS MUST BE LAID LONGITUDINALLY 8 NOT.
(G) DIAGONALLY ACROSS THE WAGON

MAXM LOADS & OVERHANGS OF TIMBER, PIPES, ANGLES, BARS ETC. OVERHANGING BOTH ENDS OF WAGON, WHICH MAY BE LOADED IN ORDINARY 8 108/3 TON WAGONS. (CAIS. C.G.M. WTX. 12780) The following to precede the last paragraph on page 195 headed "Timber 20ft. to 30ft. (inclusive) in length":

#### TIMBER FOR BOLSTER WAGONS.

When baulk timber is loaded on Bolster wagons in the manner indicated on diagrams 1 to 9 on pages 196 and 197 of the General Appendix, and the load does not exceed the length of the carrying wagon, the load may extend to the full width of the bolsters between the stanchions, providing the base of the top full width tier is below the top of the stanchions. When above the stanchions the timber must be recessed, pyramid fashion, to a height not exceeding 11ft. from rail, as shewn in the following diagram:



The loads to be built up as far as possible, with the largest baulks at the base.

Loads exceeding the length of the wagon must not be built up higher than the stanchion and when loaded as shewn on Diagram 9 must not exceed 4ft. 6ins. wide.

(G.A. 18. 11/47. C.G.M.WT/X1/14353.

Divl. Supt's Office, Bristol. G.W.7.

2nd August, 1947.

Ref: - A1/52944.

Dear Sir,

Securing of imported timber through South Wales Ports.

In connection with the heavy importation of timber through South Wales Ports loaded in accordance with diagram 'X', Supplement G.A. 15 of the General Appendix to the Rule Book and having regard to the acute shortage of ropes now being experienced, it has been agreed by the Chief Goods! Manager, in conjunction with the Chief Mechanical Engineer, to dispense with the rope round the middle of the wagon (illustrated in diagram 'X') as a special case and until such time as the rope position improves, this relaxation to apply to traffic destined for G.W. destinations only, subject to such loads not passing through the Severn Tunnel.

It must be stressed that the roping illustrated in the General Appendix mentioned in the foregoing, must be strictly adhered to when the rope position again becomes normal.

Please note and advise all concerned.

Yours truly,
for R. G. Pole. H.

nclusive) in

on pages Swagon, the he top full recessed.

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TOTE for D L . 1160

destinations +100 ions only, Severn Tunnel

X1/14353.

#### LOADING, ETC., OF MERCHANDISE TRAFFIC.

#### LOADING OF BAULK TIMBER-Continued.

Eastern Valleys Line— Newport Dock Street to Trevethin Junction. Trevethin Junction to Blaenavon and local lines.

Trevethin Junction to Abersychan and Talywain and local lines.

Ely Valley Line-

Llantrisant to Penygraig.
Mwyndy Junction to Brofiskin.
Forest of Dean Branch, from Bullo Junction to Drybrook.

Garw Branch (Brynmenyn to Blaengarw).

Gwendraeth Valley. Halesowen Line.

Kerry Branch. Lambourn Valley Line. Liskeard and Looe Branch. Llanelly and Mynydd Mawr.

Llynvi Valley Branch (Tondu to Abergwynfi).

Nantmawr Branch. New Radnor Branch.

Ogmore Valley Branch (Tondu to Nantymoel). Plymouth (Millbay) Dock Lines.

Portland Branch.

Princetown Branch. Quaker's Yard and Merthyr (ex R.R. Section).

Rhydycar Junction and Morlais Junction.

Severn and Wye Joint Line.

Tanat Valley.

Tondu to Portheawl. Totnes Quay Branch.

Van Branch

Watlington Branch. West Cornwall Mineral Lines.—Portreath Branch-North Crofty Branch. North Roskear Branch.

Hayle Wharves. Tresavean Branch.

Western Valleys Line

Newport (Dock Street) to Aberbeeg. Aberbeeg to Nantyglo.

Aberbeeg to Ebbw Vale. Risca to Nine Mile Point Junction.

Hall's Tramroad, Upper and Lower Sections.

Cwmcarn Branch.

Llanhilleth Junction to Crumlin Junction

Abertillery Junction to Cwmtillery.

Weymouth Quay Line.

#### INSTRUCTIONS FOR LOADING.

TIMBER OF LENGTHS NOT EXCEEDING THE INSIDE LENGTH OF OPEN WAGONS .- This timber must be loaded flat in ordinary open goods wagons. When the load extends above the wagon the top tiers must have at least 4 inches engagement with the sides and ends of the vehicle to prevent them sliding. The loads must be firmly secured by ropes in all such cases.

#### TIMBER NOT EXCEEDING 22 FEET IN LENGTH.

With overhang at one end only. Loads not exceeding 22 ft. in length with weights in accordance with those laid down for the applicable overhangs, in table of Diagram "X", may be carried in ordinary 8, 10 or 12 ton open goods wagons, not exceeding 3 ft. 3 inches (five planks) deep, and without end doors. The loads must overhang the trailing ends of the wagons and be firmly secured by ropes as shewn on Diagram "X", except in cases where the overhang does not exceed 1 ft., when the load must be secured as shewn on Diagram 4, page 207.

Timber in lengths not exceeding 22 ft. with With overhangs at both ends. weights in accordance with those laid down for the applicable overhangs, in table of Diagram "Y", may also be carried in ordinary 8, 10 or 12 ton open goods wagons, not be exceeding 3 ft. 3 inches (five planks) deep and without end doors. Care must be taken to distribute equally the weight of load over each end of the wagon and to secure load firmly by ropes as shewn on diagram "Y". A suitable match truck must be used at each end of each loaded wagon.

(G.A. 7.-3/40. C.G.M.-W.T/X.14231.)

#### LOADING, ETC., OF MERCHANDISE TRAFFIC.

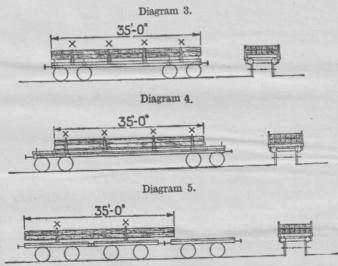
#### LOADING OF BAULK TIMBER-Continued.

bolsters placed in inner brackets as shewn on Diagram (1). When Macaws H are not available, Twin Rail Trucks (Mite) may be used as an alternative, see Diagram (2). The loads must be firmly secured by means of the chains attached to the bolsters of the vehicles.

# Diagram 1. 30'-0" Diagram 2. 30'-0"

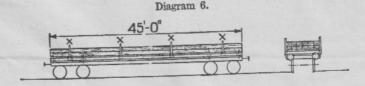
Timber in mixed loads, not exceeding 30 feet in length, may be carried in 25-feet deal wagons

TIMBER MORE THAN 30 FEET AND NOT EXCEEDING 35 FEET IN LENGTH.—The timber must be loaded on 20-ton Bogie Rail and Timber Trucks (Macaws H) when these are available—see Diagram 3. When Macaws H are not available, Bogie Rail Wagons (Macaws B, D or E) to be used, with the end bolsters placed in the inner brackets as indicated on Diagram 4, and care taken to see that load overlaps end bolsters by at least 1 foot 6 inches. In the event of Macaws B, D or E not that load overlaps end bolsters by at least 1 foot 6 inches. In the event of Macaws B, D or E not that load overlaps end bolsters by at least 1 foot 6 inches. In the event of Macaws B, D or E not that load overlaps end bolsters by at least 1 foot 6 inches. In the event of Macaws B, D or E not that load overlaps end bolsters by at least 1 foot 6 inches. In the event of Macaws B, D or E not that load overlaps end bolsters by at least 1 foot 6 inches. In the event of Macaws B, D or E not that load overlaps end bolsters by at least 1 foot 6 inches. In the event of Macaws B, D or E not that load overlaps end bolsters by at least 1 foot 6 inches. In the event of Macaws B, D or E not that load overlaps end bolsters by at least 1 foot 6 inches. In the event of Macaws B, D or E not that load overlaps end bolsters by at least 1 foot 6 inches.



TIMBER MORE THAN 35 FEET AND NOT EXCEEDING 45 FEET IN LENGTH.—The timber must be loaded on Bogic Rail and Timber Trucks (Macaws B, D or E), see Diagram 6.

For loads up to 40 feet in length, the end bolsters must be placed in inner brackets as indicated on Diagram 4. Loads to be firmly secured by the chains provided on the vehicles.



A FINALLY SECURED AS SHOWN. ROPED ROUND BUFFER CASTING TWO RIVES TO BE USED TO RUMES TO BE FIRST SECURED TO LOAD, THEN DOUBLE SECURE THE OVERHANS. MAXIN OVERHANG SEE NOTE 5 BELOW

TABLES FOR LOADS OVERHANGING ONE END

LENGTH OF LOAD MUST IN NO CASE EXCEED LENGTH

OF WASON BODY BY MORE THAN 8-0".

(SEE NOTE 3 FOR END DOOR WASONS.) ANY LOAD EXCEEDING 25'G IN LENGTH MUST BE LOADED FLAT ON OTHER SUITABLE STOCK OTHER THAN END DOOR WAGONS MAY BE USED PROVICING THE END

OKOMARY WAGONS.

GYCERD ZFT NOR THE MAXIMUM LOAD Z TONIS,

ROPES MUST BE USED TO SECURE THE OVERHANGING

S ROPE SHOWN AT THE CENTRE OF THE MAGON IS TO BE

USED WHEN LOAD IS ABONE RAVE OF WAGON AT THIS END OF THE LOAD AS INDICATED & THE ADDITIONAL

(6) LONDS NOT TO OVERHAND MAGON BY LESS THAN SINS.

MAX	OVER	WOTE OF LO	65					
	WAGON	WOOD	9	0	5	4	W	21
MAXIMUM LOAD IN TONS	13 Tori	STEEL WOOD STEEL WOOD STEEL WOOD FRAME FRAME FRAME FRAME FRAME	0	0	9	0	5	4
NI ON	WAGON	WOOD FRAME	0	5	4	n	25	2
m La	NOTON	STEEL FRAME	9 9 9	5	5	5	A	N
AXIMO	MEON	WOOD FRAME	5	A	W	12	2	121
M.	8 TON WASON 10 TON WASON 13 TON WASON	STEEL FRAME	.5	4	4	N	2	2
MAXIMUM	WITH FOUND	(SOLID LOAD)	1.67	267	3.67	457	5.67	65

COOKS ARE PROPERLY NOOD STEEL NOOD STEEL NOOD OF CORTION NUST REST ON THE DOOR HINGE BAR & NOT AGES OLD FRANKE FRANKE FRANKE FRANKE ENRING EXCERD OF THE MANNEY BAR & NOT HANGS WITH 8 TON WASON O'TON WASON 13 TON WASON 43 MAXIMUM LOAD IN TONS -0 0 35 4 M 5 5 M M M & UNDER 75 8FT MUM

DIAGRAM Z.

MAXIMUM LOADS & OVERHANGS OF TIMBER, PIPES, ANGLES, BARS ETC, NOT LOADED TO FULL WIDTH S, 10 & 13 TON WAGONS. (G.A.IS C.G.M.-WTX. 7278) (7) LOADS MUST BE LAID LONGITUDIMILLY & CENTRALLY & NOT CARGONALLY ACROSS THE MACON.

(G.A.15 C.G.M.-WT/X. 72780)

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mber cated

# TIMBER MORE THAN 50 FEET AND NOT EXCEEDING 70 FEET IN LENGTH —page 197.

Amplified to read:

The timber must be loaded centrally on Bogie Rail and Timber Trucks (Macaws B, D or E) with end bolsters placed in outer brackets, and a match truck used at each end as under-runners only. When loads exceed 60 feet in length match trucks without bolsters must be used—see Diagram (8). Macaw J may also be used. Lengths not exceeding 55 feet to be loaded centrally with the bolsters over each bogie placed in the bolster guides provided 4 feet from bogie centres towards centre of wagon. Lengths exceeding 55 feet but not exceeding 62 feet 6 inches to be loaded centrally with bolsters over each bogie placed in the bolster guides provided at the bogie centres. Lengths exceeding 62 feet 6 inches but not exceeding 66 feet to be loaded as in Diagram 7 with bolsters similarly arranged and match truck at one end. Lengths 66 feet to 70 feet to be loaded as in Diagram 7 with end bolster placed in outer brackets at overhanging end of load only, with match truck at that end. Loads must be firmly secured by the chains provided on the bogie vehicles.

(G.A. 7.—3/40. C.G.M.—W.T /X.14231.)

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#### LOADING OF BAULK TIMBER-Continued.

The method of loading baulk timber more than 30 feet and not exceeding 45 feet in length refers

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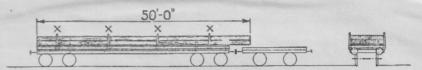
h d only to traffic arising at stations on the Great Western Railway.

Other Companies do not adopt this method, the practice being to use three single timber trucks loaded centrally with the middle bolster removed, and in order to avoid difficulty at junctions with other Railways it has been agreed that this traffic may be accepted at such junctions when so loaded, provided that the width of the load does not exceed 4 feet 6 inches; the trucks are short-coupled; the loads are secured on the end bolsters only and are satisfactory in all other respects.

TIMBER MORE THAN 45 FEET AND NOT EXCEEDING 50 FEET IN LENGTH.-The timber must be loaded on Bogie Rail and Timber Trucks (Macaws B, D or E) with a match truck at one end to act as under-runner only—see Diagram 7.

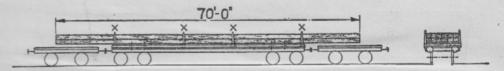
Loads must be firmly secured by the chains provided on the bogie vehicles.

#### Diagram 7.



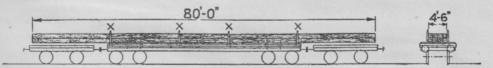
TIMBER MORE THAN 50 FEET AND NOT EXCEEDING 70 FEET IN LENGTH. The timber must be leaded centrally on Bogie Rail and Timber Trucks (Macaws B, D or E) with end bolsters placed in outer brackets, and a match truck used at each end as under runners only. When loads exceed 60 feet in length match trucks without bolsters must be used see Diagram (8). secured by the chains provided on the bogic vehicles.

#### Diagram 8.



TIMBER MORE THAN 70 FEET AND NOT EXCEEDING 80 FEET IN LENGTH,-The timber must be loaded on a Bogie Rail and Timber Truck (Macaw B, D or E) with end bolsters placed in outer brackets, and a match truck (without bolsters) used at each end-as shewn on Diagram 9. The width of load must be restricted to 4 feet 6 inches, and this can be effected by placing the stanchions in the inner stanchion sockets provided in the bolsters of the vehicles.

#### Diagram 9.



Before loading is commenced the chains and "D" shackles attached to the trucks must be thorougnly overhauled, as the safety of the load is to a large extent dependent upon them; and, in addition to the inspection of the loads by the Timber Loaders, they must, as far as possible, be carefully examined at the starting points by the Station Masters or Goods Agents, as the case may be. When Twin Timber Trucks (Mites) are used, the bolsters must be greased underneath in the centre before loading, and also the quadrants upon which they move, in order to give them free play. This duty rests with the Chief Mechanical Engineers' Department, but at stations where there are no Greasers or Examiners belonging to that Department, the greasing (or oiling) of the bolsters must be carried out by men of the Goods or Traffic Departments who perform the loading, and the Inspector, Foreman or other person superintending the loading will be held responsible for seeing that the bolsters are properly greased or oiled before the wagons are loaded.

### LOADING OF PITWOOD.

The following instructions shew how imported pitwood must be loaded at the Ports. Instructions relating to the loading of pitwood at stations other than Ports appear on page 204 after diagram No. 12. Guards must satisfy themselves that the wagons of pitwood are properly loaded and safe to travel before they are attached to trains.

(G.A. 18. 11/47. C.G.M.WT/X.1/12770.)

they are attached to trains.

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W

DEEP-SIDED WAGONS SHOULD BE USED FOR THIS TRAFFIC AS FAR AS PRACTICABLE. ONE, TWO, OR THREE-PLANK WAGONS, OR WAGONS WITH SIDES LESS THAN TWO FEET FOUR INCHES HIGH, MUST NOT BE USED.

PITWOOD INTENDED TO TRAVEL VIA THE SEVERN TUNNEL MUST BE SECURELY ROPED IN ALL CASES.

#### PITWOOD WITH BARK ON.

Diagrams Nos. 1, 2 and 3, shewing the mode in which Pitwood is to be loaded in Five or Seven-Plank Wagons.

When five or seven-plank wagons with fixed ends are used for pitwood in 9 ft. lengths, it must be loaded lengthwise in the middle of the wagon, with two stanchions on each side, as per diagram No. 1, each end of the wagon being filled, when possible, with pieces not exceeding 6 ft. 6 ins. in length, placed crosswise or on end, but lengths of 6 ft. 6 ins. may be loaded as shewn in diagram No. 2, with two stanchions to each length of pitwood on each side of wagon, or may be loaded on end throughout the wagon.

Diagram No. 1.



When five or seven-plank wagons with tip-end doors are loaded with 9 ft. lengths, as shewn in diagram No. 1, the fixed end of wagon may be filled with 6 ft. 6 in. lengths, crosswise or on end, but at the tip door end the 6 ft. 6 in. lengths must be loaded in the upright position and the Loaders must see that the tip door is properly fastened before the wagon is loaded. Lengths of 6 ft. 6 ins. are to be loaded as shewn in diagram No. 2. Lengths of 4 ft. 6 ins. may also be loaded as shewn in diagram No. 2, provided that the uprights or stanchions are comprised of 6 ft. 6 in. lengths.

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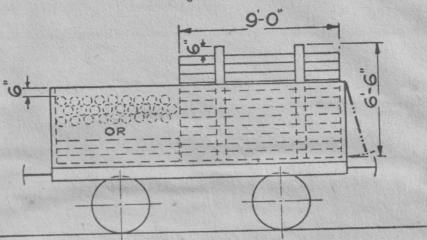
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n in end. ders are a in The following to be inserted as a fourth paragraph on page 198:-

PITWOOD FROM SOUTH WALES PORTS TO LOCAL COLLIERIES.

Pitwood may be loaded in accordance with the following diagram, the traffic being restricted to service from South Wales Ports to local Collieries.

Diagram "A"



This method of loading must conform to the following instructions:—

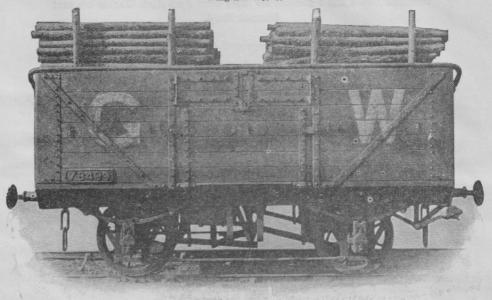
1. Not less than 7-plank ex P.O. wagons to be employed. The longitudinal timbers to be loaded at the "end door" of the vehicle and the space at the fixed end filled with timbers loaded athwart or gunshot.

3. Timber to be kept 6 in. below the tops of the stanchions and below the sides of the wagon at the fixed end.

(G.A.19—10/48. C.S.—W.T./XI/44660.)

# LOADING, ETC., OF MERCHANDISE TRAFFIC. LOADING OF PITWOOD—Continued.

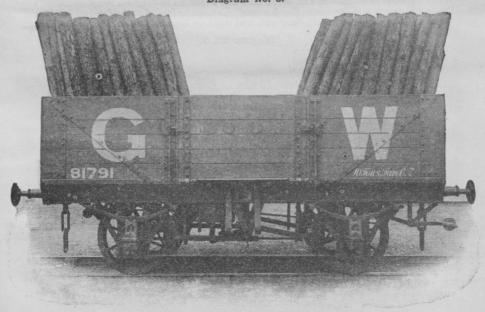
Diagram No. 2.



Pitwood, loaded in accordance with diagrams Nos. 1 and 2, must be securely roped, if, owing to the awkward shape of the wool or from other causes, there is any doubt as to the loads travelling

The highest point of the loads, as per diagrams Nos. 1 and 2, must be at least 6 ins. below the top of the side stanchions.

Pitwood, loaded ex ship by means of slings, in Five or Seven-plank Wagons. Diagram No. 3.



Pitwood, loaded direct into trucks from ship, by means of slings, may be loaded as per diagram No. 3, care being taken that sufficient pieces are placed crosswise in the centre of the wagon, from the bottom to well tighten the load.

# LOADING OF PITWOOD—Continued.

Diagrams Nos. 4 and 5 shewing the mode when Four-plank Wagons are used.

Diagram No. 4.

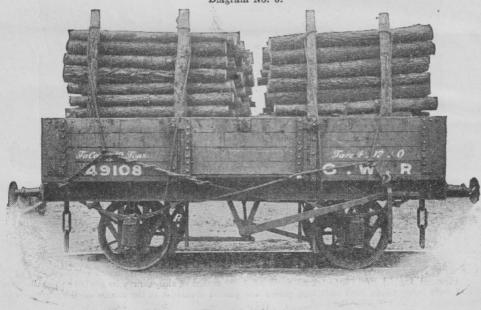


When four-plank wagons with fixed ends are used for pitwood in 9 ft. lengths the method of loading as per diagram No. 4 is to be adopted. Lengths of 6 ft. 6 ins. are to be loaded as shewn in diagram No. 5, but in both cases the loads must be roped without exception. The highest point of the loads, as per Diagrams Nos. 4 and 5, must be at least 6 ins. below the top of the stanchious.

the loads, as per Diagrams Nos. 4 and 5, must be at least 6 ins. below the top of the stanchions.

When Pitwood in lengths of 9 ft. and 6 ft. 6 ins., as shewn in diagram No. 4, is loaded together in a Truck which has a tip-end door, the shorter sticks must be loaded at the fixed end of Wagon only and not across the Truck at the tip-door end, and must be kept below the sides of the Truck.





Pitwood loaded crosswise must be kept well below the sides of the Wagon in all cases.

#### LOADING OF MERCHANDISE TRAFFIC-LOADING OF PITWOOD.-Page 201.

The fourth paragraph of the instructions headed "Pitwood with the Bark Stripped off, i.e. Pit Props," to be amended as follows:—

When loaded in accordance with diagram 7, wagons with fixed ends only must be used.

When the method of loading shown on diagram 8 is adopted, and a wagon having one tip end door is used, the overhang must be at the tip end with the lower ends of load towards the fixed end of wagon.

When loaded in accordance with diagrams 7 and 8 in wagons less than 4 ft. 3 in, deep inside, the transition of the securely roped.

When loaded in accordance with diagram 8 in wagons 4 ft. 3 in. deep inside and over, roping need not be insisted upon, provided the props do not exceed 8 ft. in length and the height of load does not exceed 10 ft. 6 in. from rail to top.

When loaded in accordance with Diagram 7, wagons with fixed ends or with one tip end door may be used but when the latter type of wagon is utilised, the props loaded lengthwise in the bed of the wagon must be at the tip door end and the lower ends of the sloping portion of load against the fixed end of the wagon.

When the method of loading shown on Diagram 8 is adopted, and a wagon having one tip end door is used, the overhang must be at the tip end with the lower ends of load towards the fixed end of wagon.

When loaded in accordance with Diagrams 7 and 8 in wagons less than 4 ft. 3 ins., deep inside, the traffic must be securely roped.

When loaded in accordance with Diagram 8 in wagons 4ft. 3 ins. deep inside and over, roping need not be insisted upon, provided the props do not exceed 8 feet in length and the height of load does not exceed 10 feet 6 inches from rail to top. (G.A.3.—12/37. C.G.M.—W.T/X16004.)

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LOADING OF PITWOOD-Continued.

### PITWOOD WITH THE BARK STRIPPED OFF, i.e. PIT PROPS.

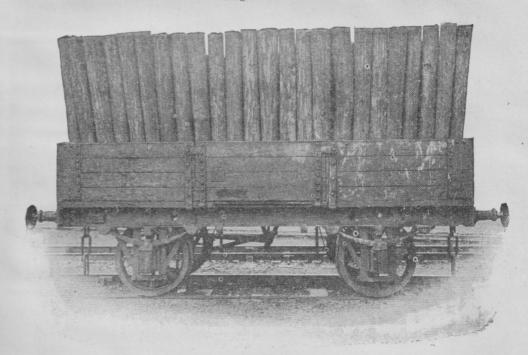
Pitwood with the bark stripped off must not be loaded as shewn in Diagrams Nos. 1, 2, 4 or 5, nor above the sides of Wagons, unless it is loaded as shewn in Diagrams Nos. 3, 6, 7 or 8, NOR IN WAGONS WITH SIDES OF LESS THAN FOUR PLANKS. FIVE AND SEVEN-PLANK WAGONS SHOULD BE USED WHEN AVAILABLE.

When loading, as per Diagram No. 6, pieces not exceeding 6 ft. 6 ins. in length, must be placed on end like stanchions, close together round the sides and ends of the Wagon, the load being made up by pieces placed either lengthwise or across the truck.

With a mixed load of pieces, 9 ft. and 6 ft. 6 ins. in length, the uprights must be of 6 ft. 6 ins. props, the 9 ft. pieces being loaded lengthwise, the space at the end or ends to be filled with lengths of 6 ft. 6 ins. The body of the load must be kept 12 ins. below the top of the uprights.

When loaded in accordance with Diagrams Nos. 7 and 8, the traffic must be securely roped, AND UNDER NO CIRCUMSTANCES MUST THE SLOPING LOT OF WOOD BE PLACED AT THE TIP DOOR END OF WAGONS.

Diagram No. 6.



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# LOADING, ETC., OF MERCHANDISE TRAFFIC. LOADING OF PITWOOD—Continued.



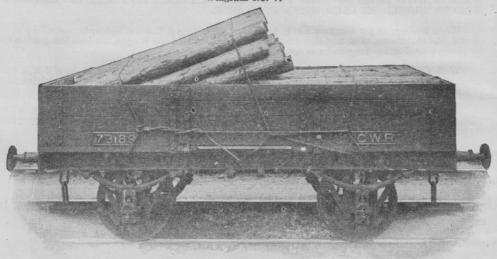
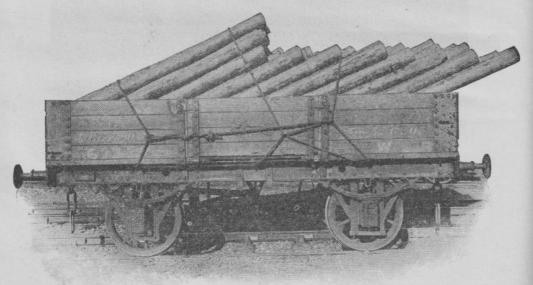


Diagram No. 8.



#### LOADING OF PITWOOD-Continued.

#### PITWOOD IN 20-TON MINERAL WAGONS.

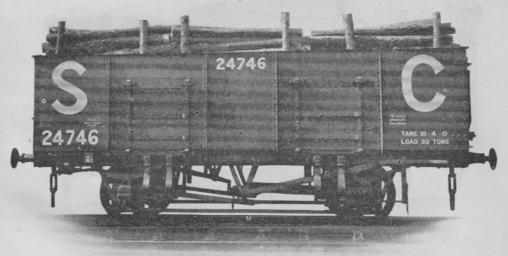
When pitwood is loaded in 20-ton mineral wagons, 6'6'' lengths must be loaded as shewn in diagram No. 10 or No. 11, 9'0'' lengths in accordance with diagram No. 9, and 6'6'' and 9'0'' lengths in accordance with diagram No. 12.

When loaded as shewn in diagrams No. 11 and 12 sufficient pieces must be loaded on the floor of the wagons at the ends to tighten the load, care being taken to see that the end doors are properly secured. 6' 6" lengths to be used as side stanchions.

Diagram No. 9.



Diagram No. 10.



LOADING, ETC., OF MERCHANDISE TRAFFIC.

LOADING OF PITWOOD—Continued.

PITWOOD IN 20-TON MINERAL WAGONS—Continued.

PITWOOD IN 20-10N MINERAL WAGONS—Commune.

Diagram No. 11.

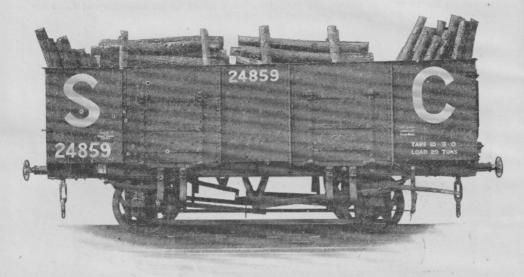
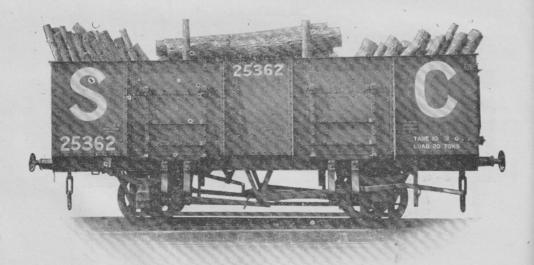


Diagram No. 12.



LOADS MUST NOT EXCEED THE CARRYING CAPACITY OF THE WAGONS.

## LOADING OF PITWOOD AT STATIONS OTHER THAN PORTS.

#### General Instructions.

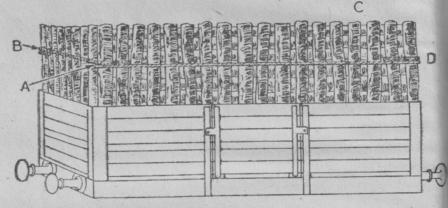
Loads must be tightly and securely roped to prevent any movement in transit. This is especially necessary in the case of loads shewn in diagrams 13, 14, 15 and 16 below, where it is essential to prevent the loss spreading.

Pitwood, with bark stipped off, must not be loaded above the sides of wagons, except as shewn in diagrams 13, 15 or 16 below.

- (a) Wagons with whole drop sides are not to be used for this traffic.
- (b) Wagons of less than 4-plank type are not to be used for this traffic.
- (c) Wagons with end doors must not be loaded in the manner shewn on diagrams 14, 16 and 17.
- (d) End-door wagons may be loaded in the manner shewn on diagrams 13 and 15.
- (e) Where horse-hooks are not fitted on wagons in the positions shewn on the diagrams, ropes may be secured to any convenient place on the wagon underframe.

The traffic differs in shape, length and girth, and the following diagrams are supplied as guides to goe and safe loading:

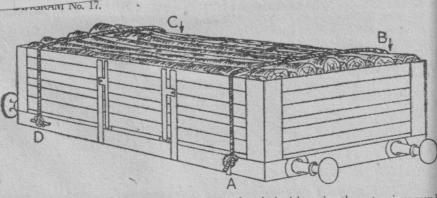
DIAGRAM No. 13. 4 ft. 0 in. to 6 ft. 6 in. lengths.



No spaces should be left between the upright pieces and the space inside those should be tightly packet. The inner layers must be at least six inches below the surrounding upright pieces.

Diagram No. 13 has been drawn to allow of the inner layers being seen but in practice no spaces show be allowed between the upright pieces.

Roping. Turn rope round two uprights at "A", "B", "C", and "D", both ends of rope being tied "A".



The butts should be alternated to secure rigidity, and to obtain tight roping, the centre pieces may be slightly arched above the rave of the wagon.

Roping. Secure rope at "A", over to hook at "B", along solebar to hook at "C", across load to "D" and the solebar to hook at "C".

(G.A. 18. 11/47. C.G.M.WT/X.1/12770.)

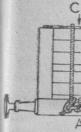
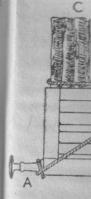


DIAGRAM N

The space left in Roping. Start ack to and tie at ".
The wagon flap
DIAGRAM No



The pieces placed
Roping. Secure
sten at "F" (opposit
The wagon flap n

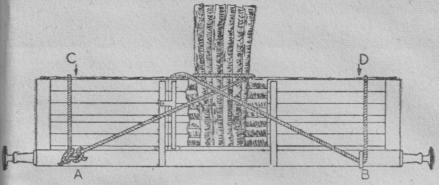
DIAGRAM No.



Pieces loaded flat r Roping. Secure re gon flap guard at "D

LC

DIAGRAM No. 14. Alternative method for 6 ft. 6 in. lengths.

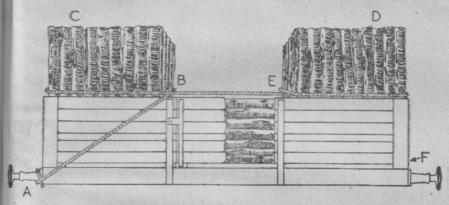


The space left in the centre between the pieces loaded lengthwise must be tightly packed.

Roping. Start at horse-hook "A", encircle uprights, across top to "D", back to "B", over to "C" to and tie at "A".

The wagon flap must be put up and secured before roping.

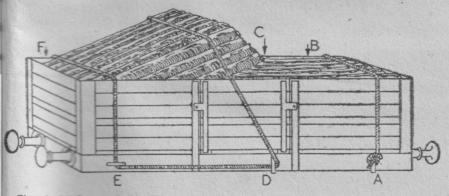
DIAGRAM No. 15. Alternative method for 6 ft. 6 in. lengths.



The pieces placed lengthwise must be loaded up to the rave and the spaces left at the ends packed tightlyRoping. Secure rope at "A", to "B", encircle uprights "C", across to "E", encircle uprights "D"
sten at "F" (opposite side to "A").

The wagon flap must be put up and secured before roping.

DIAGRAM No. 16. 7 ft. to 9 ft. lengths, also 6 ft. to 9 ft. lengths mixed.



Pieces based and must be kept below the rave of the truck.

Roping. See at "A", over load to hook at "B", to wagon flap guard at "C", over load to agon flap guard at "F".

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s 14, 16 and 17.

nd 15. rams, ropes may be

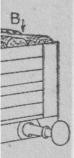
l as guides to good



d be tightly packed

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f rope being tied



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VT/X.1/12770.)

# LOADING OF DEALS, BOARDS, BATTENS, SCANFLINGS AND SIMILAR TRAFFIC.

The following Instructions must be observed .:-

Diagram No. 1. (See Instruction No. 1.)

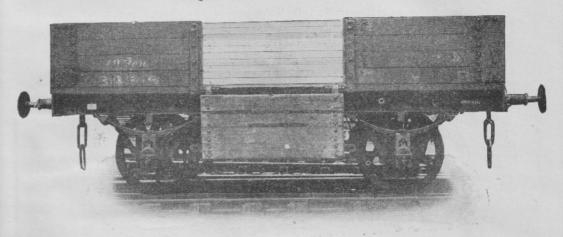


Diagram No. 2. (See Instruction No. 1.)



#### LOADING OF DEALS, BATTENS, ETC .- Continued.

1. Lengths that are less than the inside length of the wagon may be loaded in open wagons, with sides of not less than three planks, as shewn in Diagram No. 1, the timber being kept below the sides of the Wagons; or, as shewn in Diagrams Nos. 2 and 3, Wagons with sides of not less than four planks being used.

When loaded as shewn in Diagram No. 2, the timber must be of such a length as to permit of pieces being used as stanchions round the sides and ends of the wagon. These pieces must not exceed 6 feet 6 inches in length and must be placed on end, close together along the ends and as far as possible round the sides of the wagon and must rest on the floor of the wagon.

The timber inside the stanchions must be kept 6 inches below the tops of the stanchions. When sufficient upright pieces cannot be obtained to extend entirely along the sides, as well as the ends, of the wagon, the load must be protected by upright pieces placed close together at each end of the wagon, and by not less than ten upright pieces along each side, the load being firmly secured by ropes. Tip end door wagons must not be used for loads, as per Diagram No. 2.

When loaded in accordance with Diagram No. 3, two or more stout stanchions, not exceeding 6 feet long and of uniform size, must be used on each side of the load, and must extend upwards from the floor of the Wagon. The timber must be kept 6 inches below the top of the stanchions, and must be secured by passing one rope or more if necessary round the stanchions, and tightly across the load from stanchion to stanchion, also across the load in the centre, the top of the load being rounded off for the purpose of obtaining greater security from the roping. The well of the Wagon must be filled with Timber.

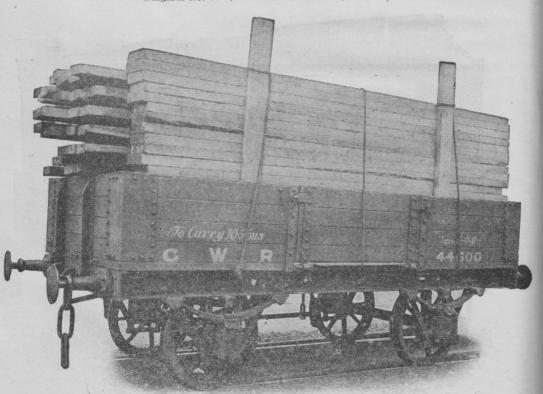


Diagram No. 3. (See Instructions Nos. 1, 2 and 3.)

LOADING OF DEALS, BOARDS, BATTENS, SCANTLINGS AND SIMILAR TRAFFIC .- Pages 205 and 210.

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Clause 1.—The following to be added as the fifth paragraph on page 206:—

Where practicable sawn planks and boards may be loaded 18 inches above the rave of wagons of not less than 4 planks, provided the ends of the wagons are completely fenced by timber 4 inches thick and not exceeding 7 feet high with 4 upright standards of similar dimensions down each side of the wagon, each end and each pair of standards being secured and nailed in position by transverse timbers. The fencing at the ends and sides of the wagons must rest on the floor of the wagon and the load must not come within 6 inches of the top of the shortest standard.

The timber must be so loaded as to ensure that the fencing at the ends and sides of the wagons is held rigidly in position.

When end-door wagons are used, every care must be exercised in ensuring that the end-doors are properly secured before loading is commenced.

Note.—Alternative method of loading from that shown in diagrams 2 and 3. (G.A.13. 9/43. Op.Com.Min.3950.)

The following additional instructions to be inserted on page 206:-

#### LOADING OF SLEEPERS (UNCHAIRED).

Where-possible, sleepers to be loaded end to end and flat on the floor of an open goods wagon, and tiered up to the level of the wagon rave.

Where this is not possible, a quantity of sleepers should be loaded flat and tiered at one end of the wagon up to the level of the rave, and at the opposite end sleepers may be loaded with one end resting on the floor of the wagon with the sleepers leaning against the ends of those loaded flat, and at an angle sufficient to form a compact load.

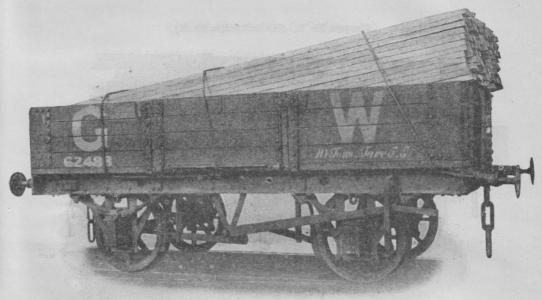
The sloping sleepers must be securely roped with the ropes braced against the sides of the wagons to preclude movement in transit. This method is similar to that used for the loading of Pitwood, and is illustrated on page 202, diagram 7.

If end-door wagons are used, the sleepers loaded in the slanting position must be placed at the fixed end.

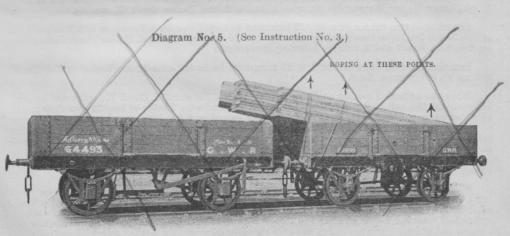
(G.A.13. 9/43. C.G.M.—W.T./X. 76389.)

# LOADING, ETC., OF MERCHANDISE TRAFFIC. LOADING OF DEALS, BATTENS, ETC.—Continued.

Diagram No. 4. (See Instruction No. 2.)



2. Lengths exceeding length of wagon by not more than one foot may be loaded and roped as shewn in diagrams Nos. 3 or 4. When loaded, as shewn in Diagram No. 3, the instructions in the preceding paragraph must be complied with. The low end of load shewn in Diagram No. 4 must be kept sinches below the top of the wagon. With weight in accordance Diagram X



Deleted GAY

# LOADING, ETC., OF MERCHANDISE TRAFFIC. LOADING OF DEALS, BATTENS, ETC.—Continued.

Diagram No. 6. (See Instruction No. 3.)



3A Leigths exceeding length of wagon by more than one foot but not exceeding 24 feet 6 inches:

Not exceeding 6 tons in weight must be loaded and roped or chained in accordance with Diagrams Nos. 3, 5 or 7, and the following instructions, or on a Double Bolster Timber Truck (Macaw A).

Exceeding 6 tons but not exceeding 10 tons must be loaded and roped or chained in accordance with Diagrams Nos. 3, 6 or 7, and the following instructions, or on a Double Bolster Timber Truck (Macaw A).

Exceeding 10 tons but not exceeding 14 tons must be loaded and roped or chained in accordance with Diagrams Nos. 6 and 7, and the following instructions, or on either a 20-ton Bogie Rail and Timber Truck (Macaw H) (see Diagram No. 8), or on a Double Bolster Timber Truck (Macaw A).

Exceeding 14 tons but not exceeding 20 tons must be loaded on a Twin Timber or Rail Truck (Mite), or on a 30-ton Timber or Rail Truck (Macaw B), or on 20-ton Bogie Rail and Timber Truck (Macaw H), see Diagrams Nos. 7 and 8, and the following instructions.

Exceeding 20 tons but not exceeding 30 tons must be loaded on a 30-ton Timber or Rail Truck (Macaw B), see Diagram No. 8.

When the method of loading as shewn in Diagram No. 3 is adopted, Check Wagons must be used as required; the well of the carrying Wagon must be filled with Timber of lengths less than inside length of wagon; two or more stout stanchions not exceeding 6 feet long must be used on each side of the load and must extend upwards from the floor of the Wagon; the Timber must be kept 6 inches below the top of the stanchions, and must be secured by passing one rope, or more if necessary, round the stanchions, and tightly across the load from stanchion to stanchion, also across the load in the centre, the top-of the load being rounded off for the purpose of obtaining greater security from the roping.

When the method of loading as shewn in Diagram No. 5 is adopted and there are not more than half a dozen pieces over 26 feet but not exceeding 28 feet in length, these may be included in the load, provided they are so mixed with other pieces of timber as to ensure safe transit. The average length of the load must not, however, exceed 21 feet, and an additional rope must be bound round the overhanging end of load. The low end of the load shewn in Diagram No. 5 must be kept 6 inches below the top of the wagon.

Deleted GAY

The following to be inserted as Instruction No. 3 on page 208.

3. Lengths which overhang the end of wagon by 1 foot or more. These may be carried in ordinary 8, 10 or 12 ton open goods wagons not exceeding 3 ft. 3 inches (five planks) deep and without end doors, provided the weights and applicable overhangs given in the table of Diagram "X" are adhered to and the load secured as shewn on that diagram. Alternatively, Double Bolster Trucks (Macaw A), Twin Rail and Timber Trucks (Mite) or 14 ton Open Wagons (Open C) may be used for loads not less than 20 feet in length. The loads to be roped or chained in accordance with Diagrams No. 6 page 208, and No. 7 page 209.

(G.A. 7.—3/40. C.G.M.—W.T/X.14231.)

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### LOADING OF DEALS, BATTENS, ETC.—Continued.

Loads on Twin Timber Trucks must be composed of lengths of not less than 20 feet resting on the Bolsters, and must be tightly chained at the Bolsters, a binding chain or rope being fastened round the centre of the load as shown in Diagram No 7. In the case of thin boards liable to sag, the lengths resting on the bolsters must be 21 feet or more. A load requiring the width of the bolsters between the inner stanchion sockets only, must be secured in the same way, the stanchions being fixed in the inner sockets of the bolsters. Single Timber Trucks must not be used for such loads.

Diagram No. 7. (See Instruction No. 3.)

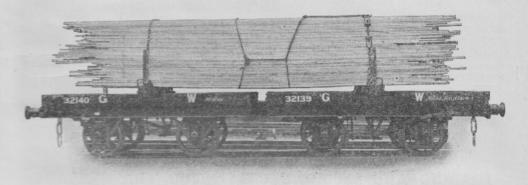
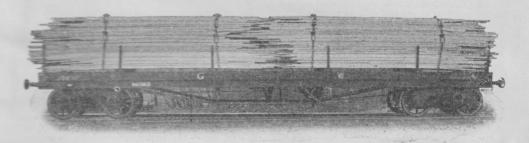


Diagram No. 8. (See Instruction No. 3.)



## INSTRUCTION No. 4. LENGTHS EXCEEDING 24 FEET 6 INCHES, ETC .page 209. To be amended to read:

Lengths exceeding the maximum laid down in table of Diagram "X" must be loaded on Twin Rail and Timber Trucks (Mite), 20 ton Bogie Rail and Timber Trucks (Macaw H) or Bogie Rail and Timber Trucks (Macaws B, D or E) with match trucks as required.

The overall length of double load as shewn in Diagram No. 8 must not exceed 46 feet for Bogie Rail and Timber Trucks (Macaws B, D or E) or 36 feet for 20 ton Rail and Timber Trucks (Macaw H) without the use of check wagons. The Tops of loads as per Diagrams 3, 6,7 and 8 should be rounded off to make the roping more effective.

(G.A. 7.—3/40. C.G.M.—W.T/X.14231.)

LOADING OF DEALS, BATTENS, ETC .- Continued.

#### GENERAL INSTRUCTIONS TO BE OBSERVED.

All loads which overhang the ends of Wagons must, when necessary, be firmly secured by roping, or other effective means, before being despatched.

When twin timber trucks ("Mites") are used the bolsters must be greased underneath in the centre before loading, and also the quadrants upon which they move, in order to give them free play. This duty rests with the Chief Mechanical Engineer's Department, but at stations where there are no Greasers or Examiners belonging to that Department, the greasing (or oiling) of the bolsters must be carried out by the Goods or Traffic Department staff who perform the loading. The Inspector, Foreman, or other person superintending the loading will be held responsible for seeing that the bolsters are properly greased or oiled before the wagons are loaded.

Loose chains must be only wound round the load and the ends fastened together as shown in the Diagrams, and not secured to the trucks in any way.

Before loading is commenced the "D" shackles and chains attached to the Wagons must, as far as practicable, be thoroughly examined, as the safety of the load is to a large extent dependent upon them.

Overhanging leads of Timber in Tip End Door Wagons must not be loaded with the low ends resting against the tip end door, because any defect in the fastenings, or neglect in properly securing them, may lead to an accident. The low ends of Timber must be inclined against the fast end of the Wagon.

LOADS MUST NOT EXCEED THE REGISTERED CARRYING CAPACITY OF WAGON.

# LOADING AND SECURING CRATEWOOD, CRATE HEADS AND CRATE RODS.

#### LOADING.

- 1. This class of traffic should not be loaded into wagons with less than "four plank" sides.
- 2. Six good poles, not exceeding 6 ft. 6 ins. in height, should be fixed as stanchions, three on each side of the wagon.
- 3. The wagon should then be filled up to a height of about three feet from the floor of wagon or about one foot above the sides of the truck, with the wood lying lengthwise and packed together as closely as possible, and tight with the stanchions.
- 4. The roping, as described in paragraph 6, should then be performed, after which the loading is to be continued to a point 9 inches below the top of the stanchions. The top of load may afterwards be rounded off in such a way that the centre or crown will not exceed the height of the stanchions.

#### ROPING.

- 5. Two ropes should be used to secure each load.
- 6. The first rope should be used when the load has reached the height referred to in paragraph 3, and should secure the stanchions themselves in position, to avoid spreading. The rope should be attached to the leading end of the wagon on the near side, secure the adjacent stanchion, be carried across the wagon to the opposite stanchion, and after making this stanchion secure it should be passed to the middle stanchion on the same side of the wagon, then back across the load to the middle stanchion on the near side, then to the third stanchion on the near side and once more across the load to the third stanchion on the off side.
- 7. This will leave a spare end of rope which must be brought tightly down and passed through the ropehook and secured.
- 8. The load must then be completed as described in paragraph 4; and the spare end of the rope that has already passed through the load must be thrown over the load so as to secure the completed load at the trailing end of the wagon.
- 9. The second rope should then be passed twice over the load, commencing at the leading end of the wagon on the off side, and the spare end utilised for bracing both ropes tightly. Each truck will in this way be roped over the load three times altogether.

# LOADING OF CHANNEL, BULB AND ANGLE IRON, TIN PLATE BARS, IRON AND STEEL BARS, BILLETS, PLATES, RAILS, GIRDERS AND OTHER SIMILAR TRAFFIC, ALSO SHEARINGS, SCRAP, ETC.

Key.

42 ..

Lengths (a

Lengths wagon b

more the 21 ft, in Exceeding 24 ft, 6

Mixed len

Exceeding
36 ft.

Rigid len not exc

46 ft.

Exceeding 21 ft., but not exceeding

Exceeding 36 ft., but not exceeding

Exceeding 49 ft., but not exceeding 70 ft.

Length.	Weight in Tons no exceeding	Class of Wagon.	Number of Instruction	Number of Diagram
	Tons.		1	
uniform or mixed) less than (	8	8 ton open wagons	1	NICESTED IN
ength of wagon	10	10 ,, ,, ,,	I	7
1		" " " " " " " " " " " " " " " " " " " "		1
not exceeding length of [	10	10 or 12 ton open wagons	2	1
by more than 1 ft	10	10 ,, 12 ,, ,, ,,	2	2
xceeding length of wagon by	6	10 ,, 12 ,, ,,	/3	3
han I ft. but not exceeding a length	10	10 ,, 12 ,, ,, ,,	3	4
g 21 ft., but not exceeding f	14	14 ton open wagons (Open C)	4	5
ins	14	"Macaw A" (Four Wheel Double Bolster Timber Truck)	4	6
ngths up to 24 ft. 6 ins	14	14 ton open wagons (Open C)	4	5
g 21 ft., but not exceeding	20	"Macaw H" (20-ton Bogie Rail and Timber Truck	5	8
ngths exceeding 21 ft., but seeding 36 ft.	21	"Mite or Mite B" (Twin Timber Trucks) with Check Wagons as		

required .. ..

Timber Trucks)

as required

Wagons as required

" Macaw B, D or E" (Bogie Rail and

"Macaw B, D or E" (Bogie Rail and

"Macaw B, D, E or J" (Bogie Rail and Timber Trucks) with Check

Timber Trucks) with Check Wagons

		(G.A. 7.—3/40. C.G.M.—W.T/X.14231.)
Exceeding 62 ft. 6 ins., but not exceeding 70 ft	30	"Macaw C" (30-ton Bogie Rail and Timber Truck) 8 8

30

30

The Authority of the Chief Goods Manager must be obtained for the conveyance of lengths of more than 60 feet.

For General Instructions see page 217, Instruction 9.

The following instructions are issued for the guidance of the Staff in loading and securing the above descriptions of Traffic, and must be strictly observed:---

ALL CONSIGNMENTS (OR ANY PART OF A MIXED CONSIGNMENT) OF TRAFFIC OF THESE CLASSES WHICH ARE CAPABLE OF LYING ON THE FLOOR AND ENTIRELY INSIDE THE BODY OF A WAGON, MUST BE SO LOADED FOR TRANSIT.

WAGONS WITH ROUNDED ENDS MUST NOT UNDER ANY CIRCUMSTANCES BE USED FOR THE CONVEYANCE OF OVERHANDING TRAFFIC.

9

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# LOADING OF CHANNEL, BULB AND ANGLE IRON, TINPLATE BARS, IRON AND STEEL BARS, BILLETS, PLATES, RAILS, GIRDERS AND OTHER SIMILAR TRAFFIC, ALSO SHEARINGS, SCRAP, ETC.—Pages 211 and 212.

The following to be substituted for the instructions issued for the guidance of staff in loading and securing the above descriptions of traffic: —

All consignments (or any part of a mixed consignment) of traffic of these classes which are capable of lying on the floor and entirely inside the body of the wagon, must be so loaded for transit, but end-door wagons must not be used for the conveyance of heavy flat steel plates. Light steel plates may be loaded in end-door wagons provided timber is secured to the floor of the wagon to prevent end-wise movement of the load towards the end door. Care must be taken to see that the end-door is fastened securely.

Bundles of black plates, tin plates and similar traffics of such length as can be accommodated in the bed of the wagon may be carried in end-door wagons provided that each bundle is securely fastened together, and a strip of packing is secured to the floor between the load and the end-door of the wagon. Care must be taken to see that the end-door is securely fastened.

Railway Companies' and Private Owners' end-door wagons with one end-door only may be used for over-hanging loads not exceeding 2 tons in weight provided the overhang does not exceed 2ft. One end of the load must be supported on the floor at the fixed end of the wagon and the other end of the load carried on the hinge bar of the end-door. Care must be taken to see that the end-door is securely fastened.

Wide plates must not be loaded in Open Goods Wagons with the plates resting against, on top, or overhanging the side of the vehicle.

Plates loaded on flat topped wagons without the use of stanchions must be chained laterally as well as longitudinally to avoid movement of the plates, even though this may involve a slight increase in the width of the load due to the binding chains.

Where it is not possible to load traffic between the stanchions, application should be made to the Chief Goods Manager for specially fitted wagons, full dimensions of the load being given when the application is made.

Wagons with rounded ends must not under any circumstances be used for the conveyance of overhanging traffic.

In selecting check wagons care must be taken to see that the sides of the check wagons are lower than the sides of the wagon carrying the overhanging load, and in no circumstances is a wagon with sides higher than the carrying truck to be used to check an overhanging load.

Authority has been given at certain works for the loading of steel bars, varying in length from 12ft. to 20ft. on single wagons with double bolsters in the manner shown on diagram 6, page 215, but the practice is not to be extended except by authority of the District Goods Manager or District Traffic Manager.

(G.A.12. 4/43. C.G.M.—W.T./X. 66224.)
The third lift to be placed overhanging the same end as the urst.

All three lifts must be placed close together in the middle of the wagon, and must not consist of more than about 20 bars each.

After loading, the top six bars from the first and third lifts are to be pulled over towards, the sides of the truck, making practically five rows of bars loaded in each vehicle.

#### Diagram No. 1.



LOAD

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# LOADING OF WOODEN SOLEBAR WAGONS WITH TINPLATE BARS, IRON AND STEEL BARS, CHANNEL, ANGLE, AND BULB IRON, BILLETS, PLATES, RAILS, GIRDERS, ROUND TIMBER BAULK TIMBER, PITWOOD, DEALS, BATTENS, BOARDS, SCANTLINGS, &c.

Railway Companies' wagons with wooden solebars may be loaded up to, but not beyond 75 per cent. of their marked carrying capacity when the loads overhang one end only, provided the overhang does not exceed 2 ft. 6 ins., but when loaded with traffic overhanging both ends and equally distributed as shewn in Diagram No. 4 they may be loaded up to full carrying capacity.

1. LENGTHS THAT CAN BE ACCOMMODATED IN THE BEDS OF OPEN WAGONS must be equally distributed over the floor, and the tops of loads must be kept at least 4 inches, below sides of

2. LENGTHS NOT EXCEEDING LENGTH OF WAGON BY MORE THAN 1 FOOT. When the bars cannot be laid on the floor of the wagon, they must be loaded in 12, 10 or 8 tons wagons with steel solebars up to the maximum weight as shown in Diagram No. 1. In loading flat bars, one bar must be placed on edge on each side of wagon as shown in Diagram No. 1, so as to form a sloping stanchion.

Note. In mixed loads (i.e., lengths less than inside length of wagon and lengths not exceeding outside length of wagon by more than I foot), short bars must be sorted out and laid on the floor

BARS, FLAT OR TINPLATE, exceeding inside length of wagon but not exceeding outside length of wagon by more than 1 ft., handled by crane, may be loaded in accordance with the following method, as shewn in Diagram No. 2 (as an alternative to Diagram No. 1). See note at foot of Diagram No. 2

The first lift of bars to be placed slightly overhanging the trailing end of the wagon.

The second lift to be placed in the centre of the wagon overhanging the other end.

The third lift to be placed overhanging the same end as the first.

All three lifts must be placed close together in the middle of the wagon, and must not consist of more than about 20 bars each.

After loading, the top six bars from the first and third lifts are to be pulled over towards, the sides of the truck, making practically five rows of bars loaded in each vehicle.

#### Diagram No. 1.

LOADING OF CHANNEL, BULB AND ANGLE IRON, TINPLATE BARS, IRON AND STEEL BARS, BILLETS, PLATES, RAILS, GIRDERS AND OTHER SIMILAR TRAFFIC, ALSO SHEARINGS, SCRAP, ETC.—page 211.

The following to be added at the end of the first paragraph of the instructions issued for the guidance of staff in loading and securing the above descriptions of traffic:

End door mineral wagons with steel floors are not to be used for the conveyance of this traffic.

(G.A. 18. 11/47. C.G.M.WT/X.1/39052.)

Lengths that can be accommodated in the beds of open wagons must be equally distributed over the floor and the tops of loads must be kept at least 4 inches below the rave of the wagon. The load must not be placed diagonally across the floor as this produces uneven loading at the corners of the wagons. In those cases where the use of end door wagons is authorised the doors must be properly secured.

(G.A.15-12/44 C.G.M.-W.T./X.76333)

The load must not be placed diagonally across the floor as this produces uneven loading at the corners of the wagon.

(G.A.12, 4/43, C.G.M.-W.T./X, 65759.)

2. LENGTHS OVERHANGING END OF WAGON BY NOT MORE THAN 1 FOOT may be loaded in ordinary 8, 10 or 12 ton open goods wagons not exceeding 3 feet 3 inches (five planks) deep with weights as shewn in table of Diagram "X", when loads overhang one end only. In loading flat bars, one bar must be placed on edge on each side of wagon as shewn in Diagram No. 1, to form a sloping stanchion.

Note.—In mixed loads (i.e. lengths less than the inside length of wagon and lengths overhanging end of wagons by not more than 1 foot) short bars must be sorted out and laid on the floor of the wagon.

(G.A. 7.-3/40. C.G.M.-W.T/X.14231.)

#### BARS, FLAT OR TINPLATE-page 212.

The following paragraph to be added at the foot of page 212 before Diagram No. 1.

The following method of loading lengths overhanging each end of the wagon by not more than 1 foot, when handled by crane, shewn on Diagram 2, page 213, with weights in accordance with table of Diagram "Y", is authorised at certain works, but must not be extended except by authority of the Chief Goods Manager. Instructions for loading remain unaltered.

(G.A. 7.—3/40. C.G.M.—W.T/X.14231.)

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The following to be added to paragraph 3 of the instructions relating to "Lengths exceeding outside length of wagon by more than 1 ft., but not exceeding 2! ft.":—

Wagons deeper than 3 ft. 3 in. inside must not be used for conveying overhanging loads of metal traffic, when loaded in accordance with Diagrams 1, 2, 3, and 4."

LOADING, ETC., OF MERCHANDISE TRAFFIC. LOADING OF CHANNEL IRON, ETC .- Continued.



### INSTRUCTION 3. LENGTHS EXCEEDING OUTSIDE LENGTH OF WAGON BY MORE THAN 1 FOOT BUT NOT EXCEEDING 21 FEET—page 213.

The following to be substituted for the existing instruction:

Loads overhanging one or both ends of the wagon by more than 1 footbut with overhangs not exceeding those laid down in the tables of Diagrams "X" or "Y", may be carried in ordinary 8, 10 or 12 ton open goods wagons not exceeding 3 feet 3 inches (five planks) deep, provided the applicable weights for the various overhangs and methods of loading are observed. Loads must be properly secured by ropes as shewn on Diagrams 3, "X" and "Y", also Diagram 4 on page 214. Check wagons to be used where necessary. (G.A. 7.-3/40. C.G.M.-W.T/X.14231.)

Diagram No. 3.



igth of traffic, LOADING, ETC., OF MERCHANDISE TRAFFIC. & LOADING OF CHANNEL IRON, ETC.—Continued.

Diagram No. 4.

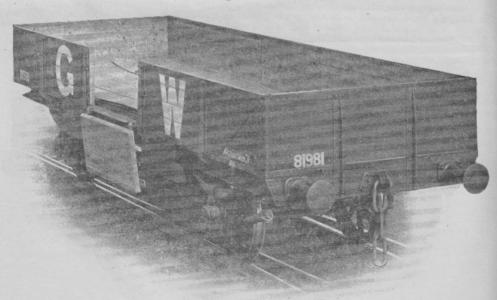


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Note. The method of loading and roping illustrated by Diagram No. 4 is authorised at certain Works for Billets exceeding 21 ft., but not exceeding 24 ft. 6 ins., but the practice is not to be extended except by authority of the District Goods Manager.

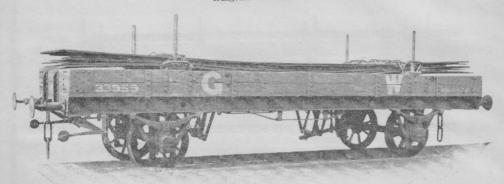
4. LENGTHS EXCEEDING 21 FEET BUT NOT EXCEEDING 24 FEET 6 INCHES should be loaded in 25 feet Open Wagons (Opens C) or on Double Bolster Timber Trucks (Macaws A) when available, as shewn in Diagrams Nos. 5 and 6. When "Opens C" or "Macaws A" are not available, the method of loading and securing, as set out in Instruction No. 5 and illustrated by Diagram No. 7, must be adhered to. Mixed lengths (i.e. some below 21 feet and some above 21 feet and not exceeding 24 feet 6 inches) must be loaded in "Opens C" wagons.

Diagram No. 5.



LOADING, ETC., OF MERCHANDISE TRAFFIC. LOADING OF CHANNEL IRON, ETC.-Continued.

Diagram No. 6.



LENGTHS EXCEEDING 21 FEET BUT NOT EXCEEDING 36 FEET AND NOT EXCEEDING 20 TONS IN WEIGHT must be conveyed on 20-ton Bogie Rail and Timber Trucks (Macaw H), as shewn in Diagram No. 8, when available. These wagons can be loaded to the full width of bolsters between stanchions. Care must be taken to distribute the load so that 20 tons weight is not loaded in the centre of the wagon. When 20-ton Bogie Rail Trucks (Macaw H) are not available, 10-ton Twin Timber Wagons (Mite or Mite B) may be used as shewn on Diagram No. 7, with check wagons where necessary, for loads not liable to sag on to the carrying wagons between the bolsters or on to the check wagons and the following instructions must be observed :-

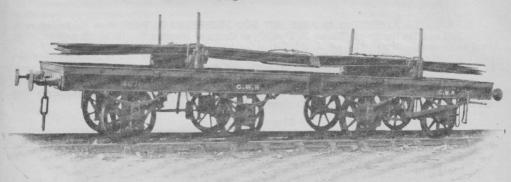
10 ton Twin Timber Trucks must be selected; the load must not exceed 20 tons and must extend fully 2 feet over the bolsters at each end. Loads requiring the full width of bolsters must be tightly chained at the bolsters, the chain being passed over and not round the loads to secure them to the wagon, and the stanchions must be fixed in the outer sockets of bolsters. A loose binding chain or rope (i.e., not secured to the wagon in any way) must be fastened round the centre of the load as shewn in Diagram No. 7. When the load is a narrow one, only requiring the width of the bolsters between the inner stanchion sockets, the same method of loading must be followed, the only alteration being that the stanchions must be fixed in inner stanchion sockets of the bolsters.

The "D" shackles must not be placed outside the stanchions unless the load occupies the whole

of the bolster between the stanchions.

Lengths liable to sag on to the carrying or check wagons must not be carried on Twin Timber Trucks but on a 20-ton Bogie Rail Wagon (Macaw H) if available, or alternatively on a Macaw B, D or E.

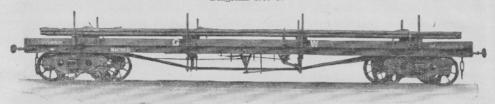
Diagram No. 7.



LENGTHS EXCEEDING 21 FEET BUT NOT EXCEEDING 46 FEET AND EXCEEDING 20 TONS BUT NOT EXCEEDING 30 TONS must be conveyed on Bogie Rail and Timber Trucks (Macaws B, D or E), as shewn in Diagram No. 8. These wagons can be loaded to the full width of bolsters between the stanchions. Care must be taken to distribute the load so that 30 tons weight is not loaded in the centre of the wagon.

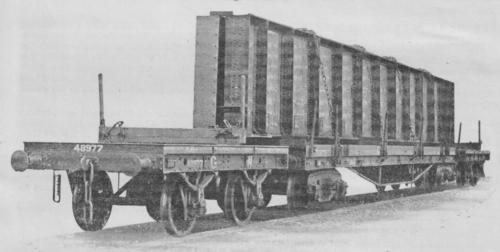
# LOADING, ETC., OF MERCHANDISE TRAFFIC. LOADING OF CHANNEL IRON, ETC.—Continued.

#### Diagram No. 8.



7. LENGTHS EXCEEDING 36 FEET BUT NOT EXCEEDING 49 FEET AND NOT EXCEEDING 30 TONS IN WEIGHT must be conveyed on Bogie Rail and Timber Trucks (Macaws B, D or E), as shewn in Diagram No. 9, with check wagons where necessary. These lengths can be loaded to the full width of the bolster between stanchions.

Diagram No. 9.

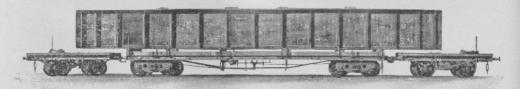


8. LENGTHS EXCEEDING 49 FEET BUT NOT EXCEEDING 70 FEET may be loaded on a Bogie Rail and Timber Truck (Macaw B, D or E), as shewn in Diagram No. 10, check wagons being used as required.

The load must be placed centrally on the bolsters and must not exceed 30 tons in weight nor 5 feet in width when load exceeds 62 feet 6 inches in length, and check wagons without bolsters must be used.

LENGTHS EXCEEDING 62 FEET 3 INCHES BUT NOT EXCEEDING 70 FEET may be loaded on a 30-ton Bogie Rail and Timber Truck (Macaw C), but not to any other Company's line without the authority of the Chief Goods Manager being first obtained.

Diagram No. 10



For loading of traffic over 46 feet in length which is flexible and liable to sag, see Special Instructions, pages 218 and 220.

#### LOADING, ETC., OF MERCHANDISE TRAFFIC. LOADING OF CHANNEL IRON, ETC .- Page 211.

The following to be inserted as the second and third paragraphs of Clause 9-General Instructions :-

Overhanging is permitted as follows :-

# (a) TRAFFIC LOADED IN PLATE AND BO-PLATE WAGONS OTHER THAN BO-PLATE "E."

Where width of wagon is		ermitted-	
From 6 ft. to 8 ft. ,, 4 ft. to 6 ft. ,, 2 ft. to 4 ft.	 	6 ft. 6 ft. 4 ft.	No securing required.  If adequately held in position by short plates on top, overhang need not secured.
Up to 2 ft	 	4 ft.	Overhang to be secured irrespective any shorter plates loaded on top.

From 6 ft. 4 ins. to 8 ft.	 4 ft.	No securing required.
,, 4 ft. to 6 ft. 3 ins.	 6 ft.	[ If adequately held in position by shorter
" 2 ft. to 4 ft	 4 ft.	plates on top overhang need not be secured.
Up to 2 ft	 4 ft.	Overhang to be secured irrespective of
		any shorter plates loaded on top.

Notes. - (i) Shorter plates loaded on top of overhanging plates, whether the overhang is secured or not, must be kept at least 4 ins. below the rave of the wagon.

> It is still the practice with some of the other Railway Companies to use single timber trucks for conveying traffic of this description, and in order to avoid difficulty at junctions with other Railways, it has been agreed to accept the traffic at such junctions when so loaded, provided the bolsters of each set of trucks are of equal height, the trucks short coupled, loads suitably secured and satisfactory in all other respects.

> The maximum movement allowed between any two of these short coupled trucks is 1 ft. 4 in. This movement being the total stroke of two buffers plus clearance between buffer faces with drawgear extended.

(ii) Overhanging plates in (a) and (b) above to be loaded centrally within the width of the wagon and the securing where necessary to be by means of ropes, suitably protected at the plate edges by means of old sheeting (to be requisitioned from the Stores Department) or chains, whichever are available.

Where there is an overhang, the maximum load to be conveyed to be as shewn below :-

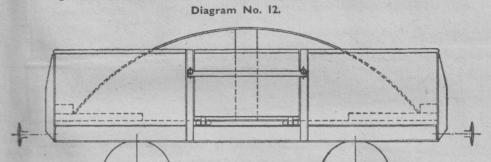
21 ton	plate	wagor	1					15 tons.
30 ,,	bogie	plate	wagon			***		26 tons.
40 ,,			"					33 tons.
				(G.A.2	3-7/49.	R.E.	. Stan	d:-C.SWT/X.1/39104.)

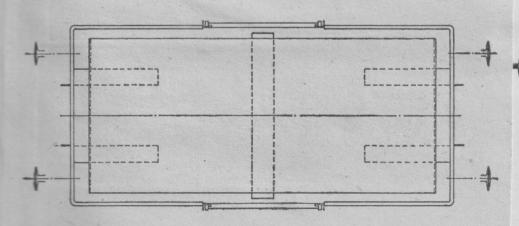
#### 10. LOADING OF CURVED PLATES.

Where a consignment of curved plates is of dimensions which permit it to be accommodated in a high-sided open goods wagon, such wagons of steel frame construction (other than end door wagons) can be employed, subject to the plates being nested with the curve uppermost and an adequate measure of packing afforded. The method of loading and standard of packing must be a matter for discretion according to curvature and length of plates, but generally in accordance with Diagram No. 12. It is essential that the centre packing supporting the curve of the plates should be solid.

The plates can also be loaded, self contained, curve downwards, upon double bolster wagons or bogie bolster wagons. There must be a minimum overhang of one foot over the outside edge of both bolsters upon which the load rests. Packing to be secured on top of the bolsters, if necessary, and the securing chains should encircle the load and be inside the stanchions and bolsters.

10. Loading of Curved Plates-continued.





11. LOADING OF SHEARINGS, SCRAP, ETC.—This clause to be re-numbered 11.

(G.A.25-1/50. R.E. Stand: -C.S.-W.T.X./1/46431.)

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### LOADING, ETC., OF MERCHANDISE TRAFFIC.

# LOADING OF CHANNEL IRON, ETC .- Continued.

9. General Instructions.—All loads which overhang the ends of wagons must, when necessary, be firmly secured by roping, or other effective means, before being despatched.

In loads of varying lengths a foundation must first be made with the greater lengths on the bolsters, the shorter pieces being loaded upon them. Iron loaded against iron is apt to slip, therefore, in loading Girders, Large Plates, or similar Traffic, close together, a piece of wood or old sacking must be placed between the binding chains and the load, and also between the Girders, Plates, etc., when this is necessary to prevent slipping during transit.

When Twin Timber Trucks ("Mites") are used, the bolsters must be greased underneath in the centre before loading, and also the quadrants upon which they move, in order to give them free play. This duty rests with the Chief Mechanical Engineer's Department, but at stations where there are no Greasers or Examiners belonging to that Department the greasing or (oiling) of the bolsters must be carried out by the Goods or Traffic Department staff who perform the loading. The Inspector, Foreman or other person superintending the loading will be held responsible for seeing that the bolsters re properly greased or oiled before the wagons are loaded.

Loose chains must be wound round the load and the ends fastened together, as shewn in the ligram, and not secured to the trucks in any way.

Before loading is commenced the "D" shackles and chains attached to the wagons must, as ar as practicable, be thoroughly examined, as the safety of the load is to a large extent dependent

It is still the practice with some of the other Railway Companies to use single timber trucks tor conveying traffic of this description, and in order to avoid difficultivate Junctions with other Railways, it has been agreed to except the traffid as of the functions when to loade the rucks are short coupled, the loads suitably secured to the trucks, and satisfactory in all other respects. apon them.

Diagram No. 11.



10. Loading of Shearings, Scrap, etc.—When it is necessary to load bundles of Shearings, Scrap, etc., above the sides or ends of Wagons, suitable bundles must be selected and placed on end round the sides and ends of Wagons, as shewn in Diagram No. 11, in order that such bundles may form stanchions over which those bundles loaded in the interior cannot slide. All loose or overhanging pieces must be removed before the loads are allowed to go forward. Wagons with sides of five or seven planks must be used for this traffic when available.

# LOADING OF TRAFFIC OVER 46 FEET IN LENGTH WHICH IS FLEXIBLE AND LIABLE TO SAC, SUCH AS POLES, BARS, TUBES, RAILS, &c.

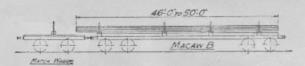
KEY.

Length.	Weight not exceeding.  Tons.	Class of Wagon.	Instruc-	Diagram.
Exceeding 46 feet but not exceeding 50 feet.	30	Macaw B with match truck at one end.	1	A
Exceeding 50 feet but not exceeding 58 feet.	30	Macaw B with match trucks at each end.	2	В
Exceeding 58 feet but not exceeding 64 feet.	30	Macaw B with match trucks at cach end (match trucks to have no bolsters).	3	С
Exceeding 62 feet 6 inches but not exceeding 70 feet.	30	Macaw C	4	D
Exceeding 60 feet but not exceeding 65 feet.	30	Two Macaws H with outer bolsters moved to extreme ends of pair of wagons.	5	E
Exceeding 65 feet but not exceeding 70 feet.	35	Two Macaws H with bolsters in position over bogie centres.	6	F
Rails 90 feet in length	30	Two Macaws B	7	l G

THE FOLLOWING INSTRUCTIONS AND DIAGRAMS ARE ISSUED FOR THE GUIDANCE OF THE STAFF IN LOADING AND SECURING THE ABOVE DESCRIPTIONS OF TRAFFIC, AND MUST BE STRICTLY OBSERVED.

1. Lengths exceeding 46 feet and not exceeding 50 feet may be loaded with load overhanging at one end on Macaw B and one match truck (with or without bolster) used (see Diagram A). The bolster over bogic centre of Macaw B at end where load is overhanging, to be placed in the bolster guides provided at the extreme end of bogic wagon, to reduce the amount of overhang beyond bolster as much as possible) Load to be secured to bogic wagon at each bolster and, in addition, loose chains or ropes to be wound round load to prevent spreading where considered necessary, according to the nature of the load. Width of load not to exceed 4 feet 6 inches.

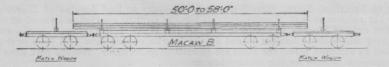
#### Diagram A.



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2. Lengths exceeding 50 feet and not exceeding 58 feet to be loaded centrally on Macaw B, i.e., with equal overhang at each end and two match trucks (with or without bolsters) used (see Diagram B). The end bolsters of Macaw B to be placed in the bolster guides provided at the extreme ends of bogic wagon.) Load to be secured to bogic wagon at each bolster and, in addition, loose chains or ropes to be wound round load to prevent spreading where considered necessary, according to the nature of the load. Width of load not to exceed 4 feet 6 inches.

#### Diagram B.





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Ref: - A1/59,504.

Dear Sir,

Standard Methods of Loading and Securing Long and Projecting Loads " Use of Plate Wagons. 

12 Va 1410 11

With reference to my circular letter of the 11th October, the Railway Executive have now modified the instructions regarding the loading of Plate and Bo-Plate wagons with overhanging plates at one end, as shown below:-

(1) That overhanging be permitted as follows:

# (a) TRAFFIC LOADED IN PLATE AND BO-PLATE WAGONS OTHER THAN BO-PLATE "E".

Where width of plates in wagon is-	Maximum overhang permitted -	Remarks.
From 6ft to 8 ft.	6 ft.	No securing required.
# 4ft to 6 ft. # 2ft to 4 ft.	6 ft. 4 ft.	(If adequately held in position by shorter plates on top, overhang part not be secured.
Up to 2 ft.	4 ft.	Overhang to be secured irrespective of any shorter plates loaded on
		top.

# --- # -# 111 00010

(b) TRAFFIC LOADED IN BO-PLAT	E "E" WAGONS.	
From 6 ft 4 ins to 8 ft.	4 ft.	No securing required.
" 4 ft to 6 ft 3 ins. " 2 ft to 4 ft.	6 ft. 4 ft.	(If adequately held in (position by shorter (plates on top overhang need not be secured.
Up to 2 ft.	4 ft.	Overhang to be secured irrespective of any shorter plates loaded on top.

NOTES - (i) Shorter plates loaded on top of overhanging plates, whether the overhang is secured or not, must be kept at least 4" below the rave of the wagon.

(ii) Overhanging plates in (a) and (b) a over to be loaded centrally within the width of the couring where necessary to be by means of a collection of the centrally protected at the plate edges by means of old sheeting (to be requisitioned from the Stores Dept) or chains, whichever are available.

(2) Where there is an overhang, the maximum load to be conveyed to be as shown below:-

07	ton	nlate	wagon.				 29	9	* TD	00115.
KT.	0011	practo	nlate	wagon.			 		.26	tons.
40	11	DOETE	11	wagon.			 		.33	tons.
1										

Please note and advise all concerned.

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Diagram.

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Macaw B, used (see e extreme ose chains ing to the

# LOADING OF TRAFFIC OVER 46 FEET IN LENGTH WHICH IS FLEXIBLE AND LIABLE TO SAG, SUCH AS POLES, BARS, TUBES, RAILS, ETC.

KEY-page 218.

The following additions to be made to the Class of Wagon column:

Items 1 and 2 add: "or Macaw J without match truck".

Item 3 add: "or Macaw J with match truck at one end for lengths exceeding 62 feet 6 inches".

(G.A. 7.-3/40. C.G.M.-W.T/X.14231.)

The following to be added after the second sentence:

Macaw J, loaded centrally, may also be used without match truck. The bolsters over the bogie centres at each end of Macaw J to be placed in the bolster guides provided 4 feet from bogie centres towards centre of wagon.
(G.A. 7.—3/40. C.G.M.—W.T/X.14231.)

### INSTRUCTION 2. LENGTHS EXCEEDING 50 FEET AND NOT EXCEEDING 58 FEET-page 218.

The following to be added after the second sentence:

Macaw J, loaded centrally, may also be used without match truck. The bolsters over the bogie centres at each end of Macaw J to be placed in the bolster guides provided 4 feet from bogie centres towards centre of wagon.

(G.A. 7.-3/40. C.G.M.-W.T/X.14231.)

The following to be added after the first sentence:

Macaw J may also be used. Lengths up to 62 feet 6 inches to be loaded centrally, with bolsters in positions over bogic centres of Macaw J, and no match trucks. Lengths over 62 feet 6 inches and not exceeding 64 feet to be loaded as in Diagram "A", page 218, with end bolster placed in outer brackets at overhanging end of load only, with match truck at that end.

(G.A. 7.-3/40. C.G.M.-W.T/X.14231.)

The following to be added to Instruction 3:-

Forty 60 ft. rails from the Port Talbot Steelworks to this Company's order may be loaded on Ganes with Match Trucks at each end (Match Trucks to have no bolsters) to an extreme width of 5 ft.

Similar loads on Macaws B. D and E. will still be confined to 35 rails giving a width less than 4 ft. 6 in.

### LOADING OF TRAFFIC OVER 46 FEET IN LENGTH, ETC.

With reference to the instruction contained in the General Appendix, page 219, in connection with the conveyance of lengths exceeding 58 feet and not exceeding 64 feet, there has been a difficulty in the Swansea Division in connection with the supply of stock, and to assist in overcoming this the Chief Mechanical Engineer has agreed, as a **temporary measure only**, to permit 60 feet 6 inch rails being loaded on a Macaw "B" with match trucks fitted with bolsters, but stipulates that the rails must be loaded in such a manner as to ensure that the clearances between the ends of rails and bolsters of match trucks (with buffer faces in contact) are not less than 2 feet. Care is to be taken to see that the load is efficiently secured, and arrangements are being made by the Goods Department, so far as loading is concerned, and for the wagons to be watched by the Traffic Department before they leave the Marshalling Yard in the Swansea Division.

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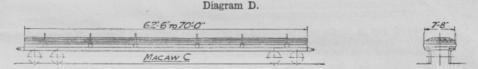
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#### LOADING OF TRAFFIC OVER 46 FEET IN LENGTH, ETC .- Continued,

3. Lengths exceeding 58 feet and not exceeding 64 feet to be loaded in the same manner as 50 feet to 58 feet lengths, except that match trucks without bolsters must be used (see Diagram C). Load to be secured to bogic wagon at each bolster and, in addition, loose chains or ropes to be wound round load to prevent spreading where considered necessary, according to the nature of the load. Width of load not to exceed 4 feet 6 inches.

# Diagram C. 58.0 TO 64.0 MACAW B March Wagon.

4. Lengths exceeding 62 feet 6 inches and not exceeding 70 feet may be loaded on Macaw C (see Diagram D), but not to another Company's line without the authority of the Chief Goods Manager being first obtained. End bolsters to be placed in the bolster guides provided over the bogic centres. Load to be secured to bogic wagon at each bolster and, in addition, loose chains or ropes to be wound round load to prevent spreading where considered necessary, according to the nature of the load. Wagon may be loaded to full width of bolsters between the stanchions.



5. Lengths exceeding 60 feet and not exceeding 65 feet may be loaded on two Macaws H (see Diagram E), coupled together with a special short three-link coupling. Bolsters at the ends of the pair of Macaws H to be placed in the bolster guides provided at the extreme ends of the bogie wagons. Load secured to bogie wagons at bolsters marked → on Diagram E, and, in addition, loose chains or ropes to be wound round load to prevent spreading where considered necessary, according to the nature of the load. The chains at securing bolsters → must be passed round the load. Suitable steel plates to act as skids to be fixed to tops of all bolsters except those marked → and top surfaces of plates well greased to ensure free movement of load when negotiating curves. Securing bolsters marked → not to be greased but packed up with timber approximately ½-inch thicker than the steel skid plates on the other bolsters.

Not to negotiate curves of less than 3-chain radius. Width of load not to exceed 4 feet 6 inches. Load to carry not to exceed 30 tons.

4 ft. 6 in.

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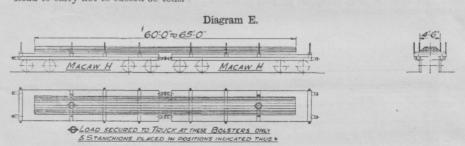
rails being

s must be

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6. Lengths exceeding 65 feet and not exceeding 70 feet may be loaded on two Macaws H (see Diagram F), coupled together with a special short three-link coupling, in the same manner as 60 feet to 65 feet lengths, except that the bolsters at ends of the pair of Macaws H must be placed in the bolster guides provided over the bogic centres. Load secured to bogic wagons at bolsters marked ↔ on Diagram F, and, in addition, loose chains or ropes to be wound round load to prevent spreading where considered necessary, according to the nature of the load. The chains at securing bolsters must be passed round the load. Suitable steel plates to act as skids to be fixed to tops of all bolsters except those marked ↔ and top surfaces of plates greased to ensure free movement of load when negotiating curves. Securing bolsters marked ↔ not to be greased but packed up with timber approximately ½-inch thicker than the steel skid plates on the other bolsters.

#### LOADING OF TRAFFIC OVER 46 FEET IN LENGTH, ETC .- Continued.

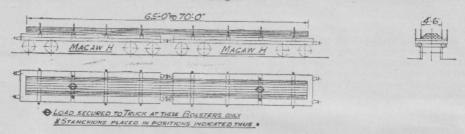
Not to negotiate curves of less than 3-chain radius.

Width of load not to exceed 4 feet 6 inches.

Load to carry not to exceed 35 tons.

Positions of stanchions and bolsters are shewn on Diagrams A to F. In all cases where loose chains or ropes are wound round loads to prevent spreading, the ends must be fastened together, but not secured to trucks in any way.

Diagram F.



7. Rails 90 feet in length may be loaded on two 30-ton Bogie Rail Wagons (Macaws B) coupled ogether with a special short three-link coupling.

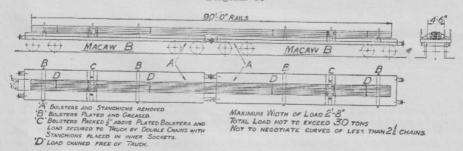
The following instructions must be observed—see Diagram G below:—

(a) Bolsters and stanchions to be removed at points "A."

(b) Suitable steel plates to act as skids to be fixed to tops of bolsters "B" only, and top surfaces of these plates well greased to ensure free movement of load when negotiating curves.

- (c) Rails to be loaded centrally on the two wagons and secured at bolsters "C" only, with stanchions placed in inner sockets of these bolsters. Two chains to be used at each securing bolster and each chain must pass round the load from one side and be secured on the other side, with the adjusting screws on opposite sides of the load. Securing bolsters "C" to be packed up with timber approximately  $\frac{1}{2}$  inch thicker than the steel skid plates used on Bolsters "B."
- (d) Load must also be chained free of the wagons at points marked "D" to prevent load spreading.
  - (e) Maximum weight of the load on the two wagons must not exceed 30 tons.
  - (f) Width of load must not exceed 2 feet 8 inches.
  - (g) Loads not to negotiate curves of less than 2½ chains radius.
- (h) Speed of train should be reduced to between 10 and 15 miles per hour when negotiating sharp curves, cross-overs, etc.

#### Diagram G.



Lengths exceeding 70 feet for G.W. Stations and 60 feet for stations on other Companies' lines, must be considered as loads of exceptional dimensions, and special authority for conveyance must be obtained from the Chief Goods Manager before being sent forward and, where considered necessary, one of the Chief Mechanical Engineer's Department Inspectors from Swindon or Wolverhampton should accompany the load.

AL

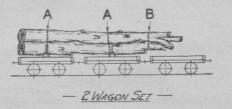
TH

# LOADING OF SINGLE BOLSTER WAGONS. EMERGENCY LOADING FOR WAR PERIOD ONLY.— Page 220.

The bolster on the fourth wagon in diagram 3 (see Circular G.A.10) to be removed, this to be illustrated by draw-

THE FOLLOWING TO BE INSERTED AFTER PAGE 220

# LOADING OF SINGLE BOLSTER WAGONS EMERGENCY METHODS FOR WAR PERIOD ONLY

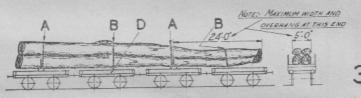


1

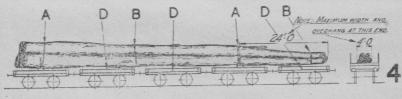
A DB A

2

-3 WAGON SET -



-4 WAGON SET-

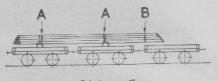


- 5 WAGON SET -

CARRYING WAGONS MUST NOT BE LOADED IN EXCESS
OF THEIR RESISTERED CARRYING CAPACITY

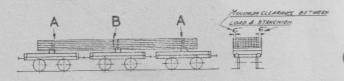
METHOD OF CARRYING RIGID ROUND TIMBER

NOTE: LOA WIDT STANC END



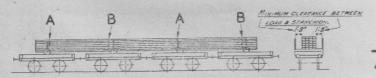
-2 WAGON SET -

CARRYING WAGONS MUST NOT BE LOADED IN EXCESS OF THEIR REGISTERED CARRYING CAPACITY



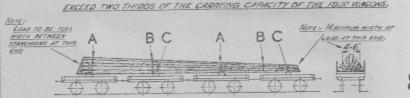
- 3 WAGON SET -

TOTAL WEIGHT OF LOAD NOT TO EXCEED CARRYING CAPACITY OF TWO WAGONS.



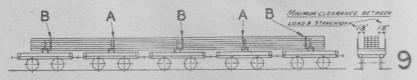
- 4 WAGON SET -

TOTAL WEIGHT OF LOAD NOT TO.
TED TWO THIRDS OF THE CARRYING CAPACITY OF THE FOUR WASONS.



- 4 WAGON SET -

TOTAL WEIGHT OF LOAD NOT TO EXCEED TWO THIRDS OF THE CARRYING CAPACITY OF THE FOUR WAGONS.



- 5 WAGON SET -

TOTAL WEIGHT OF LOAD NOT TO.
EXCEED TWO THIRDS OF THE CARRYING CAPACITY OF THE FIVE WAGONS.

METHOD OF CARRYING FLEXIBLE LOADS SUCH AS PAILS, POURT SECTIONS, POURS, PLATES, ETC.

Routo Sections, Poles, Plates, Etc. (excluding Telegraph Poles) GAI3
Instruction of Telegraph Poles see Page 194

where loose ogether, but



B) coupled

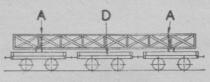
ly, and top ng curves.

' only, with ch securing cother side, be packed ers "B." LO

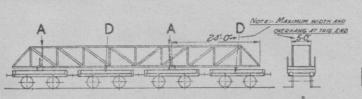
negotiating



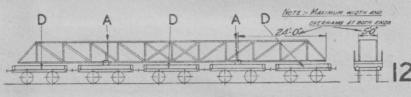
lines, must be obtained one of the old accom-



- 3WAGON SET-



- 4 WAGON SET -



- 5 WAGON SET -

CARRYING WAGONS MUST NOT BE LOADED IN EXCESS OF THEIR
REGISTERED CARRYING CAPACITY

METHOD OF CARRYING RIGID LOADS SUCH AS GIRDERS,
LARGE SQUARED TIMBER, ETC.

- A. LOAD CHAINED TO BOLSTEDS, STANCHIONS IN POSITION & SHACKLES OUTSIDE STANCHIONS.
- B. LOAD CHAINED FREE OF WAGON
- C. BOLSTERS TO BE RETAINED
- D. BOLSTERS TO BE DEMOVED

NOTE: LOADS EXCEEDING GOFEET IN LENGTH TO BE TREATED AS SPECIAL LOADS.

THE DIMENSIONS ON ALL DIAGRAMS HAVE BEEN BASED ON WAGONS 15 FEET LONG OVER HEADSTOCKS ON A MINIMUM CURVE OF GCHAINS

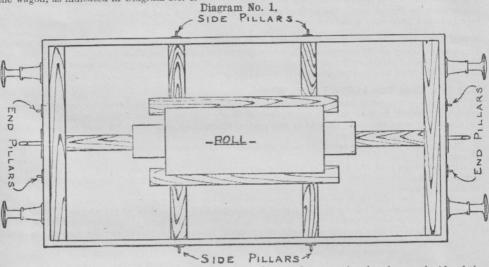
ALL LOADS SHOULD OVERHANG THE CARPTUNG BOLSTERS BY AT LEAST EFEET, 61NS.
THE MAXIMUM POSSIBLE MOVEMENT BETWEEN ANY TWO COUPLED MAGONS TO BE I FOOT, 41NS

G.A.10. C.G.M.S WT/X.56509

#### LOADING, SECURING AND CONVEYANCE OF IRON OR STEEL ROLLS.

Rolls One Ton and Under Three Tons in Weight.

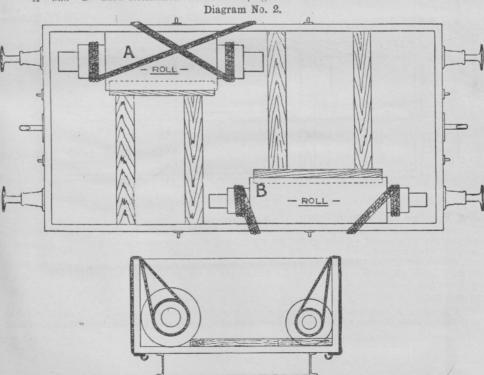
Rolls one ton and under three tons in weight, if carried singly, must be loaded in the centre of the wagon, as indicated in Diagram No. 1.



Scotches of suitable timber not less than 7 inches by 3 inches must be placed on each side of the roll, and struts be firmly fixed between the scotches and side pillars of the wagon, as indicated in the Diagram. A piece of suitable timber must be placed across the wagon at each end, and a strut of sufficient length and height to reach from the end timber to the neck of the roll must be used between this and the roll as indicated to prevent longitudinal movement.

Timber packing must be firmly nailed to the floor of the wagon.

Rolls not exceeding two tons in weight forwarded in pairs must be loaded in opposite corners , of the wagon, as indicated in Diagram No. 2, but this method must not be adopted when there is a greater difference than 10 cwts. in the weight of the respective rolls. "A" and "B" show alternative methods of roping.



#### LOADING, SECURING AND CONVEYANCE OF IRON OR STEEL ROLLS-Continued.

One roll must be placed at each end of the wagon on opposite sides with a scotch of suitable timber not less than 7 inches by 3 inches, with struts firmly fixed between the scotch and side of the wagon, as indicated in the Diagram, the timber packing being firmly nailed to the floor of the wagon. Ropes must be placed round the neck of each roll at either end and firmly secured by means of the rope fasteners provided for the purpose.

#### Heavy Rolls, e.g. any Roll Weighing Three Tons or Over.

NOTIFICATION BY SENDERS.—Senders are required to notify the Station Agent when they are about to forward this description of traffic so that special arrangements may be made for the examination of the load by the Company.

#### Rolls from Three Tons to Nine Tons in Weight.

Wagons permanently fitted with packing for the loading of rolls are available, and these must, whenever possible, be used for the conveyance of heavy rolls up to nine tons in weight. Under no circumstances must the loose bolsters or any part of the packing with which the wagons are equipped be removed from the vehicles.

Application for these wagons must be made to the Divisional Superintendent or District Traffic Manager.

In the absence of the specially fitted wagons, ordinary 10 ton Open Goods Wagons with sides not more than two feet four inches in height must in all cases be used, and the following method of loading be adopted:—

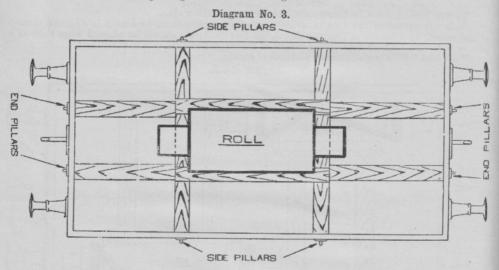
The rolls must be placed in position with the necks resting on baulks of timber of such dimensions as will assure the barrel of the roll clearing the floor of the wagon.

The baulks must extend across the wagon and be held in position by four struts of suitable timber (two at each end) firmly fixed between the baulks and the end pillars of the wagon to prevent longitudinal movement.

To prevent lateral movement a longitudinal tie of suitable timber must be placed at each side of the roll and flush with it and be firmly secured to the baulks, and the baulks must be sufficiently grooved in the centre for the neck of the roll to rest in.

The rolls, when loaded, must have the necks packed with wedges if the groove space is too large for the neck of the roll to fit tightly therein.

The method of packing is illustrated in Diagram No. 3.



#### Rolls Exceeding Nine Tons in Weight.

Rolls exceeding nine tons in weight must not be loaded in ordinary Open Goods Wagons, but on specially constructed vehicles under the supervision of an Inspector from the Chief Mechanical Engineer's Department.

Particulars of such rolls to be forwarded must be promptly submitted to the District Goods Manager, who will make arrangements for the supply of the special wagons and the attendance of an Inspector.

#### Broken Rolls.

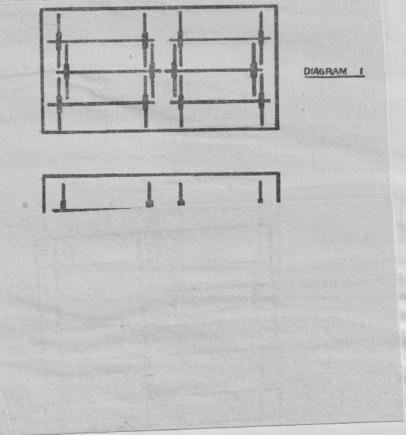
These must be embedded in sand or ashes to a depth of not less than three-fourths of the diameter of the roll, except in cases where a specially constructed wagon, or wagons, with special fittings are used.

#### LOADING OF RAILWAY CARRIAGE AND WAGON WHEELS.

The following instructions must be observed in the loading of railway carriage and wagon wheels:—

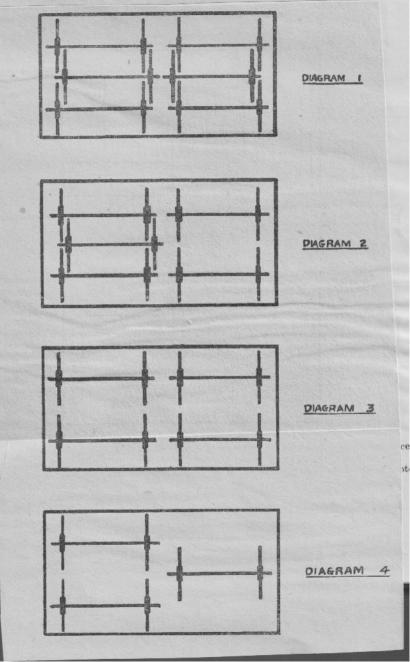
- 1. Wheels, when mounted on their own axles, to be loaded with axles lengthwise to the wagon withoutside faces of wheels pressed towards the end of the wagon in accordance with methods shown on diagrams 1, 2, 3 and 4. The practice of loading wheels crosswise on top of the axles of other wheels which are loaded lengthwise to be discontinued.
- 2. The most suitable wagons available must be selected for the conveyance of this traffic, but in no case must wagons having sides with a height inside of less than 22 inches be used.
- 3. The whole of the wheels must be properly scotched in order to prevent rolling or shifting during transit.
- 4. Station Masters and Goods Agents must see that these regulations are strictly carried out and that they are complied with by private Wagon Companies and others as well as by the Company's own staff.

Note.—Instances have occurred of carriage, wagon and engine wheels and axles being received from other Companies loaded in low-sided traffic wagons without being properly secured. Such traffic must not be accepted in low-sided wagons unless the wheels are properly scotched and securely roped to prevent movement in transit.



Instances have occurred of Carriage and Wagon and Engine Wheels and Axles being received from other Companies loaded in low-sided traffic wagons without being properly secured.

Such traffic must not be accepted in low-sided wagons unless the wheels are properly scotched and securely roped to prevent movement in transit.



# LOADING AND SECURING OF ENGINES, MACHINES, CONTAINERS, LIFT AND FURNITURE VANS.

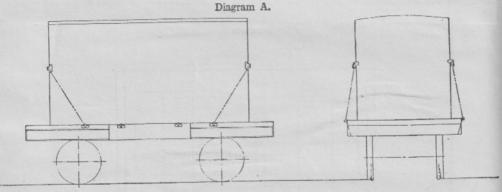
For loading Engines and Machines the running wheels must be secured by ropes as well as by scotches. This is necessary in the case of Second-hand Engines or Machines. Driving or Fly Wheels must also be properly secured by ropes.

Lift vans and containers, whether full or empty (except as otherwise provided below) must be securely chained or roped to flat wagons. When any other type of truck is used the lift vans and containers must

be securely roped.

with the method shewn on the diagrams A, B or C, and in the scale of the container Trucks (four cach side) to be used as shewn in Diagram B. Each securing chain on G.W.R. Container Trucks (Nos. 39XXX) is fitted with a cylinder containing a spring and it is essential that each chain should be tightened until there is a clearance of 3/8ths of an inch between the shoulder of the rod controlling the spring and the end of the cylinder.

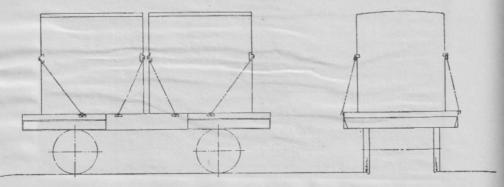
Open Containers (large or small) should be loaded centrally as shewn on Diagram E and secured by ropes in the following manner:—Rope A (Diagram E) should be secured on one side of the underframe, passed diagonally up and over wagon side, round end of container, over opposite side of wagon and down diagonally to corresponding position on underframe to that from which roping was commenced. Rope to be then secured and the process repeated in the reverse direction. Rope B, after being attached to the underframe on one side, to pass up and over one spreader bar of the container, down to underframe on opposite side and then secured. The process to be repeated in the reverse direction over the other spreader bar and rope secured.



METHOD OF LOADING LARGE CONTAINER

4 BINDING CHAINS IN USE

#### Diagram B.



METHOD OF LOADING & SMALL CONTAINERS

8 BINDING CHAINS IN USE

The following to be added to these instructions:

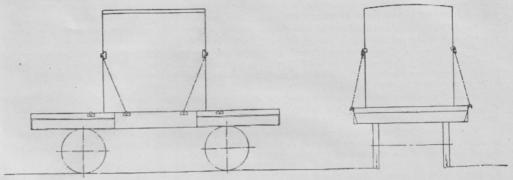
Containers loaded on Single Plank Wagons must only be conveyed in accordance with the instructions shewn on page 17th.

(G.A. 1. 3/37. E/71912 H.)

LOADING, ETC., OF MERCHANDISE TRAFFIC.

LOADING AND SECURING OF ENGINES, ETC.—Continued.

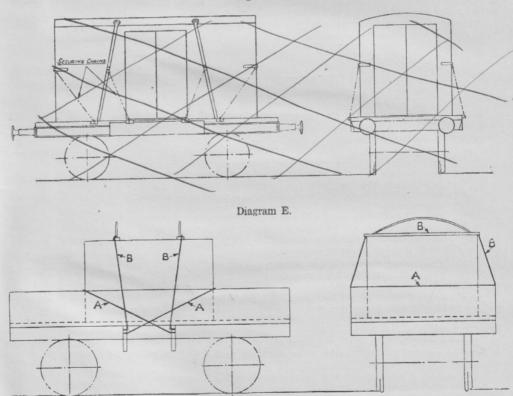
Diagram C.



METHOD OF LOADING 1 SMALL CONTAINER

4 BINDING CHAINS IN USE

Diagram D.



METHOD OF LOADING "C" & "D" TYPE OPEN CONTAINERS

#### LOADING AND SECURING OF ENGINES, ETC .- Continued.

To facilitate unloading at Receiving Stations, Lift Vans and Covered Containers mus the loaded, where possible, in container wagons, unless it is definitely known that destination stations can, without difficulty, unload from wagons of other types.

Small Open Containers must not be loaded upon wagons without sides and ends, and where possible wagons with bodies not less than three planks deep should be used.

The majority of Great Western covered containers are provided with holding down loops, rope brackets or rings, and these must be used in preference to roping over the roof. The ropes must be placed through the brackets both front and rear, and then braced at the ends of the containers, in order to prevent movement of the containers and damage to the roof canvas.

Other Companies' instructions governing the securing of containers do not in all cases coincide with those of the Great Western Co. In such instances it will be the duty of the Guards, Yard Staffs, Examiners, etc., to satisfy themselves that the vehicle is safe to travel.

Furniture vans, Showmen's vans, and similar read vehicles on their own wheels, whether loaded or empty, must have the wheels fastened to the trucks by ropes, straps or chains, and, in addition, the leading end of the van bodies must be secured to each side of the railway trucks by ropes passed over the cross-bar underneath the van bodies (generally known as the body bolster bar), the ropes being afterwards tightly braced. Particular care is necessary in dealing with Furniture Vans and other vans on springs to comply with the maximum load gauge, as when travelling empty the height of such vans is greater than when travelling loaded. There is no necessity for the wheels of Furniture vans, etc., to be scotched when loaded on Carriage Truck.

Roof boards of all Furniture Vans must be firmly fixed on their hinges, and the pins at either end used to keep the boards secure in their proper position.

The staff engaged in loading or unloading Lift Vans must satisfy themselves that all lifting eyes, binding straps and ropes are in a satisfactory condition.

The provisions of Rule 158 must also be complied with. These regulations also apply to similar traffic received from other Companies' Lines.

#### LOADING OF STEAM ROLLERS AND TRACTION ENGINES.

The following instructions must be observed:—Loading.

10-ton "Loriot K" to carry rollers and engines not exceeding 6 tons.

12-ton "Loriots" to earry rollers and engines not exceeding 8 tons.

15-ton "Loriots" to carry rollers and engines not exceeding 10 tons.

20-ton "Loriots" to carry rollers and engines not exceeding 16 tons.

25-ton "Loriots" to carry rollers and engines not exceeding 18 tons. 35-ton "Rectanks" to carry rollers and engines exceeding 10 tons.

Subject to examination by the Chief Mechanical Engineer's Inspector before dispatch, rollers and engines not exceeding 12 tons in weight may be loaded on the following 15-ton "Loriots":—

"Loriots D" Nos. 42138 to 42155.
"Loriots E" Nos. 42156 to 42164, 42202 to 42205.

Note.—The wheel base of the roller or engine must not exceed the length of the well of the vehicle upon which it is loaded.

In the event of the standard load gauge dimensions being exceeded, full particulars must be furnished to the District Goods Manager, in accordance with the standing instructions.

#### Securing Load.

Scotches must be fixed at the front and rear of the back wheels. The front wheels should not be scotched fore and aft, but should be substantially packed with 10 in. by 5 in. sleepers at the sides only.

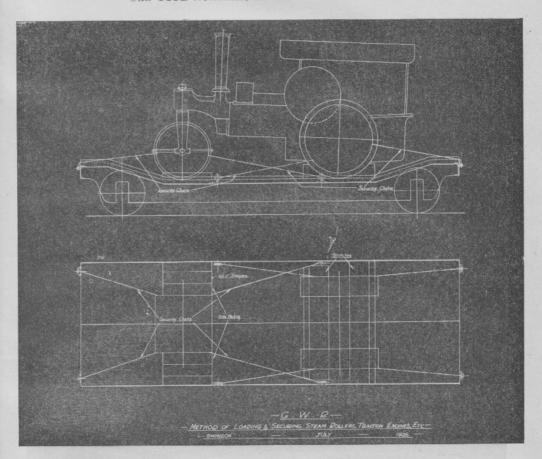
The method of securing with binding chains, illustrated below, must be employed.

The brakes of the rollers must be put on tightly before transit.

#### Securing Railway Vehicle.

Before unloading or loading a steam road roller or traction engine, the Foreman or person in charge must see that the brakes of the railway vehicle are securely applied and that scotches are placed behind all wheels of the vehicle.

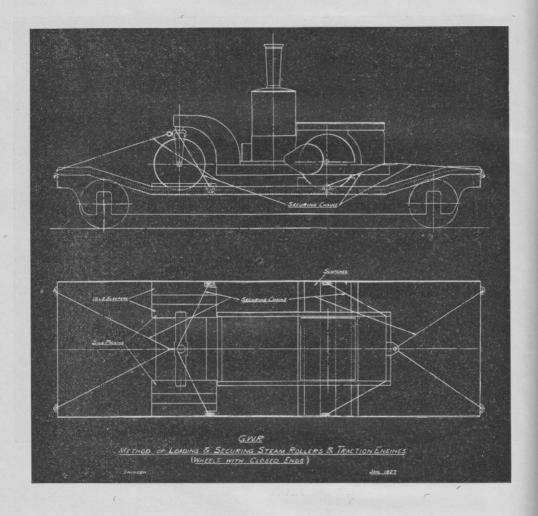
#### OLD TYPE ROLLERS, ALSO TRACTION ENGINES.



On no account may the chains be placed or bound over the front forks.

Driving or fly wheels must be thoroughly secured by ropes and specially examined to ensure that they are securely keyed on the shaft. If the fly wheel is the solid type and contains no holes through which a rope may be passed, strips of timber should be placed across it, one strip each side, these to be securely roped or bolted together and lashed back to the body of the machine. (G.A.3.—12/37. E.75868/H.)

NEW TYPE ROLLERS, WITH BOTH ROLLERS THE SAME SIZE, AND CLOSED AT ENDS.



# LOADING AND SECURING OF WINES AND SPIRITS IN PIPES, BUTTS, PUNCHEONS, AND SMALLER CASKS, ALSO BEER AND OIL, ETC., IN CASKS OR BARRELS.

Wines and Spirits in Pipes, Butts and Puncheons must be loaded on the bilge lengthwise in the wagon on rope rings of sufficient circumference and depth to permit of the casks being well nested and prevent contact with the floor of the truck, the pipes, &c., being securely roped to the wagon in such a way as completely to encircle each cask at both ends—see Diagrams Nos. 1 and 2. The use of straw rings must be discontinued.

Diagram No. 1.

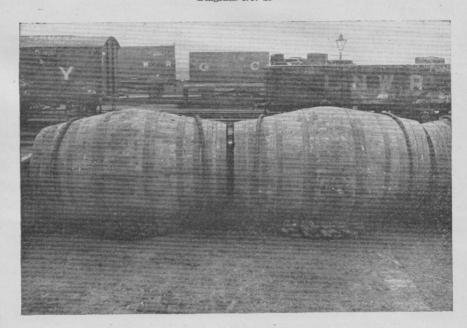
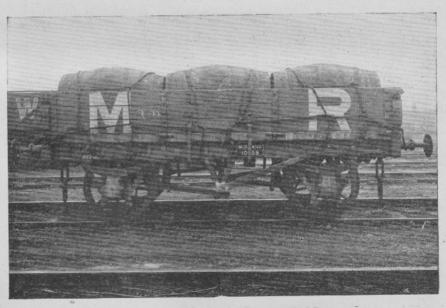


Diagram No. 2.



#### LOADING OF WINES AND SPIRITS, ETC .- Continued.

Wines and Spirits in Hogsheads and Smaller Sized Casks must be loaded on end and secured by suitable means within the space they occupy in the wagon if other traffic is not available to fill the vehicle.

Trucks with sides less than 21 inches high must not be used.

The use of wagons with single lever, or ratchet, or headstock brakes is prohibited for Wines and Spirits in Pipes, etc.

Beer.—Casks must be loaded on end in the case of full loads, either of casks, or partly of casks, and partly other goods, care being taken not to employ trucks having sides less than 21 inches high. When there is not a full load the casks must be loaded on the bilge lengthwise, and carefully scotched; or they may be loaded on end if so packed with other goods as to prevent risk of shifting in transit.

Petroleum and other Mineral Oils. Casks must be loaded in the same manner as Beer.

Pipes, Butts, Puncheons, Casks, etc., should not be loaded on the bilge in vans, but should be loaded on end and be so packed with other goods or seetched as to prevent risk of shifting in transit.

The bungs of all Pipes, Butts, Casks, etc., loaded on the bilge must be placed uppermost.

N.B.—All casks having a worn or bulged appearance must be loaded on the bilge.

Scotches.—Rope scotches must in all cases be used for Wines and Spirits in Pipes, Butts, or Puncheons. For other traffic circular straw scotches of sufficient depth to permit the casks being properly embedded may be used, and if they are not available, wood scotches may be used, or failing them, wads of twisted straw must be jammed firmly under the casks.

Stones, pieces of brick or other hard and rigid substances must not be used for the purpose of scotching casks, etc.

#### LOADING OF GRAIN, FLOUR AND OTHER SIMILAR TRAFFIC.

All concerned must see that this traffic is carefully loaded to prevent sacks of Grain, Flour, etc., falling from trucks during transit. The following instructions and diagrams must be adhered to:—

Wagons having sides 51 and 39 inches in height, i.e. 7 and 5 plank Wagons, must be loaded with Grain, Flour and similar heavy traffics in accordance with Diagrams 1 and 2. Light traffics such as Bran, Sharps, Dried Grains, Pollards, Offals, etc., may be loaded in these Wagons in accordance with Diagrams 3, 4, 6, 7, 9 and 10.

The method of loading to be adopted when Wagons which have sides of 28 inches in height, i.e. 4 plank Wagons are used, is that shewn in Diagrams Nos. 3 and 4. If it be found impracticable to load in accordance with Diagrams Nos. 3 and 4, the sacks may be loaded as shewn in Diagram No. 5.

When Wagons with sides of 21 inches, i.e. 3 plank Wagons are used, the sacks are to be loaded as shewn in Diagrams Nos. 6 and 7. If it be found impracticable to load in accordance with Diagrams Nos. 6 and 7, the sacks may be loaded as shewn in Diagram No. 8.

Wagons having sides less than 21 inches in height, i.e. 1 plank Wagons must be loaded in accordance with Diagrams Nos. 9 and 10.

Whenever a Truck of Grain, Flour, etc., is not loaded in accordance with these instructions, the Agent at the Receiving Station must report the fact to his District Goods Manager.

In the case of Diagrams Nos. 2, 5 or 8, where top loading upon sacks on end is permitted with Trucks having sides 39, 28 or 21 inches high, the top load must be placed towards the centre of the Truck, and the sacks must not extend further than covering the third row of sacks from each end.

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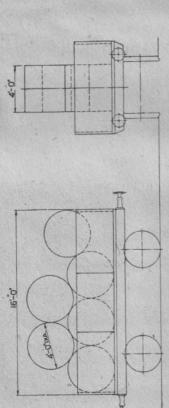
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FOLLOWING TO BE INSERTED OPPOSITE PAGE 230:-

# \_TIERCES OF TOBACCO LOADED IN OPEN WAGONS\_

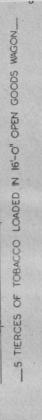
\_TIERCES TO BE PROPERLY SHEETED AND ROPED\_

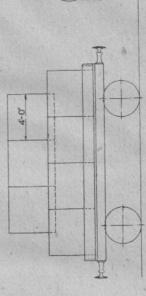
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\_\_ 6 TIERCES OF TOBACCO LOADED IN 16'-O" OPEN GOODS WAGON \_\_

17-6"





\_\_ 7 TIERCES OF TOBACCO LOADED IN 17-6" OPEN GOODS WAGON\_

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LOADING OF GRAIN, FLOUR, ETC .- Continued.

Diagrams Nos. 1 and 2 showing the mode in which sacks of Grain, Flour, &c., should be loaded when seven or five plank wagons are used.

Diagram No. 1.



The body of the wagon must be filled as tightly as possible with sacks placed on their ends, and upon them a layer of sacks must be placed lengthwise over the whole of the top. On this layer additional sacks may be placed lengthwise in the centre of the load.

Diagram No. 2.



The number of sacks which may be loaded in a 7 or 5 plank Wagon varies according to the description of traffic, but in no circumstances must such a number be placed in the Wagon as will bring the load beyond the carrying capacity.

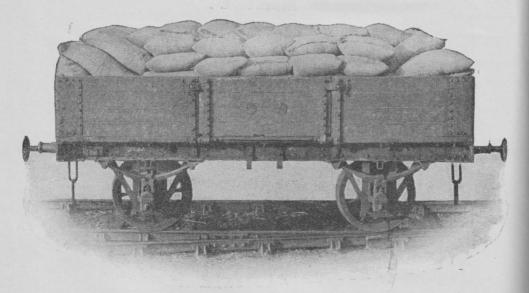
LOADING, ETC., OF MERCHANDISE TRAFFIC: LOADING OF GRAIN, FLOUR, ETC.—Continued.

Diagrams Nos. 3, 4 and 5 showing the mode when four plank wagons are used.

Diagram No. 3.



Diagram No. 4.



Two layers of sacks on their flat must be placed over the entire floor, and the third and subsequent tiers stowed in the same way as the first and subsequent tiers in Diagrams 9 and 10. The sides of the wagon must always be several inches above the level of the second layer of sacks in order to give the necessary cant inwards to the third tier.

LOADING, ETC., OF MERCHANDISE TRAFFIC.

LOADING OF GRAIN, FLOUR, ETC.—Continued.

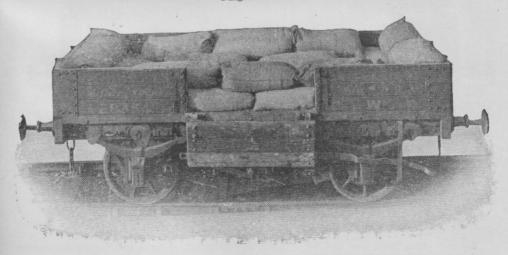
Diagram No. 5.



The body of the Truck must be filled as tightly as possible with sacks on end upon which may be placed a top load of Not more than 14 sacks.

Diagrams Nos. 6, 7 and 8 showing the mode when three plank wagons are used.

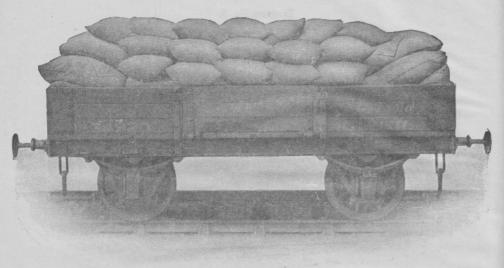
Diagram No. 6.



LOADING, ETC., OF MERCHANDISE TRAFFIC.

LOADING OF GRAIN, FLOUR, ETC.—Continued.

Diagram No. 7.



The entire floor of the Truck is to be first stowed with a layer of sacks on their flat, the second and subsequent layers being loaded in exactly the same manner as in Diagrams Nos. 9 and 10.



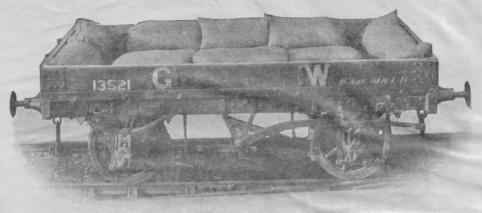
The body of the Truck must be filled as tightly as possible with sacks on end, upon which may be placed a top load of NOT MORE THAN  $14~{
m SACKS}$ .

LOADING, ETC., OF MERCHANDISE TRAFFIC.

LOADING OF GRAIN, FLOUR, ETC.—Continued.

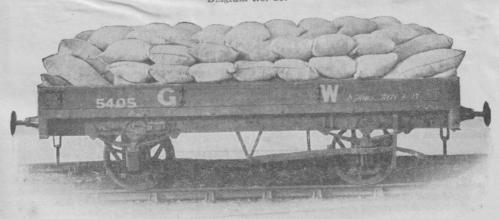
Diagrams Nos. 9 and 10 showing the mode when one plank wagons are used.

Diagram No. 9.



In the first tier (i.e. on the floor of the Truck) the sacks are loaded against the sides and ends only, leaving the centre of the floor clear, as in Diagram 9; three sacks are placed lengthwise on each side of the floor and two across each end. The three sacks at each side are loaded close together, and a space is left between them and the end sacks if the Truck is long enough to permit of it. The end sacks are loaded with the butts partly on the Truck side and partly on the Truck floor, with a corner of each butt resting on the end of the Truck and with the mouths meeting on the floor, thus giving the next layer at the end a tilt inwards.

Diagram No. 10.



In the second and third tiers, the sacks at the end of the Wagon are placed lengthwise; and those in the middle of the load are placed across the Wagon, and the fourth tier consists of sacks placed across the Wagon in the middle of the load.

In the second, third and fourth tiers, when more than four tiers are loaded on a Wagon, as in Diagrams 4 and 7, the mouths of the sacks are turned inward towards the centre of the Wagon and incline downwards, those in the second tier resting on the floor in the centre of the Wagon.

NOTE.—The above Diagrams are not intended to show the number of sacks to be placed upon trucks—which differ in capacity—but to indicate the method of stowing for the guidance of loaders.

#### LOADING OF HAY AND STRAW,

The existing instructions under this heading to be cancelled and the following substituted:-

#### LOADING OF HAND-PRESSED HAY AND STRAW.

1. Hay and Straw in hand-pressed trusses must always be sheeted and roped—as many sheets and ropes to be used as may be requisite in order to effectually cover and secure the load.

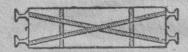
2. In loading it is desirable to somewhat reduce the width of each successive layer of trusses placed in a truck, thus throwing the centre of gravity into the middle of the load, and reducing the possibility of outside trusses becoming displaced. The trusses or bales should also interlock each other as far as practicable.

3. In loading Hay, additional security is given to the load by tying together the end trusses of the various

ayers with string or haybands.

4. The roping of the traffic must be performed as follows:

Hay-ropes to be placed over the sheets, twice across the load and twice diagonally thus:



and to be drawn as tightly as possible and firmly secured. For roping diagonally two ropes must be used to avoid passing the rope from buffer to buffer across the end of the wagon and making contact with the drawhook

Straw-ropes to be placed over the sheets three times across the loads only thus:



and to be drawn as tightly as possible and firmly secured. Straw has a tendency to settle down in the truck after loading and, when for any reason loaded consignments remain on hand at the forwarding station,

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the ropes should be tightened before the loads leave.

5. Trucks with "rounded" ends must not be used for the conveyance of Hay and Straw when this can be avoided, but, if it is absolutely necessary to use them, no portion of the load must project beyond or

rest upon the ends of the trucks.

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 Each truck of Hay and Straw must be carefully examined, prior to despatch, by a competent person
at the sending station, and it must not be sent forward unless it is securely loaded, sheeted and roped. Guards and also Yard Inspectors, Shunters and others concerned at Junction Stations, must examine trucks of Hay and Straw in transit, and see that any not securely loaded, sheeted and roped are put off for adjustment.

7. Hay and Straw received from other Companies' lines, although not loaded and roped strictly in

accordance with the above diagrams, may be allowed to travel if securely loaded and roped.

in accordance with the above diagrams, may be all wed to travel if secondly loaded and roped.

#### LOADING OF ESPARTO IN BALES, AND SIMILAR TRAFFIC.

Trucks containing bales of Esparto, Moss Litter, Pulp or Hemp, and traffic of a similar character, loaded higher than the sides of trucks, must be roped. Station Masters and Goods Agents must use discretion as to sheeting during dry weather as a protection against fire.

#### LOADING OF RAGS OR WASTE PAPER.

Trucks containing Rags or Waste Paper in Bags, Bales or Bundles must be sheeted and securely roped over the sheets. The class of truck best adapted for the conveyance of the traffic must be used, and under no circumstances is a larger consignment to be loaded upon a truck than it is capable of conveying without risk of displacement while running, or during shunting operations.

#### DEFECTIVE FLOORS OF RAILWAY COMPANIES' OR PRIVATE OWNERS' WAGONS.

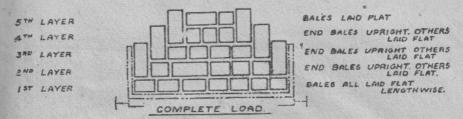
Careful examination must be made of floors of all wagons selected for the loading of Steel Billets, Forgings, or similar traffic, whether at Stations or in private sidings. The attention of the Chief Mechanical Engineer's Department should be called to any wagon the floor of which may be found to be in defective condition, and steps taken to prevent it being loaded or sent away empty, unless to a repairing depot, until the defects have been attended to.

#### GOODS DAMAGED BY PETROLEUM, CREOSOTE, &c.

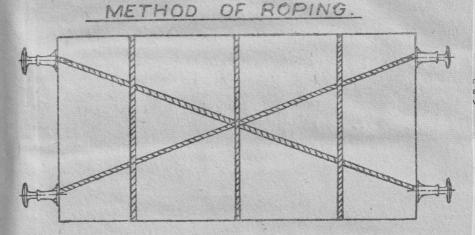
Grain, Flour, or other traffic likely to sustain damage must not be loaded in trucks which bear evidence of having been used for Petroleum, Paraffin, Creosoted Goods, or other similar traffic.

#### LOADING OF MACHINE-PRESSED HAY AND STRAW.

1. Machine-pressed baled Hay and Straw in 5-plank or more high-sided common user wagons to be loaded in accordance with the following diagram:



2. This traffic must be sheeted and roped in accordance with the following diagram:



Ropes to be placed over the sheets, twice diagonally from and round buffer to buffers of wagon and then three times across from side to side of load over diagonal ropes. Ropes to be braced together at each point of security—firm roping essential.

For roping diagonally two ropes must be used to avoid passing the rope from buffer to buffer across the end of the wagon and making contact with the drawhook.

- 3. Wagons with rounded ends must not be used for the conveyance of Hay and Straw when this can be avoided, but if it is absolutely necessary to use them, special care must be taken to ensure that diagonal ropes are secured in such a manner than they cannot become loose during transit.
- 4. Each truck of Hay and Straw must be carefully examined, prior to despatch, by a competent person at the sending station, and it must not be sent forward unless it is securely loaded, sheeted and roped. Guards, and also Yard Inspectors, Shunters and others concerned at Junction Stations must examine trucks of Hay and Straw in transit, and see that any not securely loaded, sheeted and roped are put off for adjustment.
- 5. Hay and Straw received from other Companies' lines, although not loaded and roped strictly in accordance with the above diagrams may be allowed to travel if securely loaded and roped.

(G.A. 18. 11/47. C.G.M.WT/X. 81531.)

#### LOADING OF DRUMS.

Drums, loaded or otherwise, must not be loaded with more than one-third of their diameters, or one-third of their lengths if loaded upright, above the sides of any wagon and those on the top layer which are next to the end of the wagon must be loaded longitudinally so that there is no risk of the drums rolling over the end of the wagon.

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#### PACKAGES CONTAINING POISONOUS GOODS.

Packages containing Poisonous Goods must not be accepted for transit if found to be leaking

or otherwise in a loose or bad condition. In the event of leakage occurring before or during the journey they must be immediately removed (together with any articles that may have been damaged by the leakage) to a place where they will not injure other traffic, and the circumstances must be promptly reported to the District Goods

Goods of a poisonous nature, such as Weed Killer, Sheep Dip. &c., must not be loaded in trucks Manager. containing articles for consumption, unless placed in one of the special iron containers provided for the purpose, nor placed upon platforms in positions where they will come in contact with such articles.

# CONVEYANCE OF ETHYL FLUID-THE ANGLO-AMERICAN OIL COMPANY.

Ethyl Fluid in drums, with screwed metal bungs, and marked "I.C.C.-5" may be accepted for conveyance, provided the drums bear the Dangerous, Corrosive and Poisonous Chemicals "A" label (see the Classification of Dangerous Goods by Merchandise Trains), a full indication of the contents, and the following notice:-

#### IMPORTANT NOTICE.

#### ETHYL FLUID-POISONOUS.

Do not let the contents of this package come into contact with your hands, body or clothes. If it does so by accident, deal with it at once as follows:-

Remove any clothes affected, and do not wear them again until they have been thoroughly washed with soap and water.

Wash the hands or affected part of the body at once with Petroleum (Lamp Oil), then

The loading regulations applicable to traffic bearing the special "A" Chemicals label must be strictly carried out and care exercised in handling the traffic.

Ethyl Fluid must not be confused with Ethyl Petrol, the former being the concentrated fluid before being mixed with the petrol.

## LOADING OF EMPTY MINERAL OIL CASKS, DRUMS, CARBOYS, &c.

Empty Petroleum or other Mineral Oil Casks, or any other Casks or Packages which, by leakage or otherwise, may be likely to leave a taint, must not be loaded in Box Vans, and any violation of this order must be reported at once to the District Goods Manager.

#### WAGONS LOADED WITH EXPLOSIVES OR DANGEROUS GOODS.

Traffic as above, upon arrival at the receiving station, must be separated immediately from other Wagons, and neither at the sending nor the receiving station must such traffic be allowed (unless otherwise provided in General Merchandise Classification) within any of the Company's enclosed sheds or warehouses.

#### CONVEYANCE OF RETURNED EMPTIES (AS PROVIDED IN THE YELLOW-COLOURED PAMPHLET ISSUED WITH THE GENERAL RAILWAY CLASSIFICATION) WHICH HAVE CONTAINED INFLAMMABLE LIQUIDS, &c., OR DANGEROUS, CORROSIVE OR POISONOUS CHEMICALS.

Returned empty Carboys, which have contained dangerous, corrosive, or poisonous chemicals, must not be accepted for conveyance unless they are securely stoppered or quite empty and clean. Other returned empties, as above, also must not be accepted for conveyance unless securely bunged or otherwise closed air-tight. Where it is so provided in the General Railway Classification these empties when handed to the Company must, in addition, be accompanied by the special consignment note provided for such traffic.

#### CONVEYANCE OF GAS PURIFYING REFUSE, ETC.

A number of iron-bodied vehicles have been allocated for the conveyance of Gas Purifying Refuse, Green Copperas and other traffic of a similarly objectionable nature. Should an enquiry be received in reference to the conveyance of such traffic, application for the supply of these wagons should be made to the Chief Goods Manager.

Ordinary Traffic Wagons must not be used without authority.

#### TRAVELLING GAS TANKS.

Rough shunting of travelling gas tanks causes the tank to shift on the bolsters and the joints of the fittings to become sprung, resulting in leakages. All concerned are cautioned to use more than ordinary care in handling these tanks, as the consequences of gas escaping and becoming ignited might be very serious.

Under no circumstances whatever may gas tanks be worked through the Severn Tunnel, neither Under no circumstances may gas tanks be sent on trains carrying "A" headlamps. (G.A. 10. 3/42. LK1/6185/2.)

#### EXAMINATION AND SECURITY OF LIDS OF TANK WAGONS CONTAINING ACIDS.

To obviate injury and damage to clothing by the splashing of acid from Tank Wagons, the lids must be properly secured.

Before Tank Wagons are removed from the sidings in which they are loaded the lids must be properly fastened by the Consignors and if any leakage be noticed by Guards, Shunters, or others, they must direct the attention of the senders to the fact, so that the lids may be made secure before the wagons are moved.

#### REMOVAL OF LITTER, &c., FROM TRAFFIC WAGONS.

All Railway Compenies' wagons, after unloading, must be properly swept out, or otherwise cleaned, as may be necessary. All wagons (whether Railway Companies' or Private Owners') must, after unloading, be cleared of straw or other ignitible matter.

Numerous cases of fire, resulting in damage to the Company's stock, have been reported, the outbreaks undoubtedly being due to hay or straw sweepings, or material used for packing damageable goods, such as pipes, &c., being left in the bottom of the vehicles after unloading.

goods, such as pipes, &c., being left in the bottom of the vehicles after unloading.

Before wagons are despatched in which straw or other similar packing is used, Station Masters and others concerned must see that all unnecessary packing on top of the contents of the wagon is removed.

# SHEETING OF LOADED WAGONS OF PIPES, &c., PACKED WITH IGNITIBLE MATERIAL.

Cases of fire have been reported in respect of open Wagons, loaded with such traffic as pipes, &c., where heather and similar packing has been used, and the vehicles have been despatched unsheeted. Discretion must be used as to the sheeting, during dry weather, of trucks containing traffic packed with material of an ignitible character.

#### GUNPOWDER VAN BOOTS AND LOCKS AND KEYS.

The Company's Gunpowder Vans are equipped with a pair of Gunpowder Boots, which must always, when not in use, be suspended from the hook provided for the purpose on the doors of the van, and the boots are to be returned in the van.

Should a pair of boots become separated from the van to which it is allotted, it must be immediately, sent to the Stores Superintendent, Swindon, and an advice also sent to the Chief Goods Manager, Paddington.

The whole of the Gunpowder Vans are fitted with the standard lock in use by all Railway Companies, and when forwarding consignments of explosives it is not necessary to send the keys of Gunpowder Vans to the receiving stations.

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At the stations where Gunpowder, &c., is regularly dealt with keys are provided, which will lock or unlock any Gunpowder Vans received (either G.W. or vans belonging to other Companies), and additional keys are in possession of the District Goods Managers. In the event of a consignment of explosives arriving at a station not provided with a key the District Goods Manager should be telegraphed at once.

Gunpowder Vans found only to have defective locks, and not in need of any other repairs, must not be sent to a repair depot, but application for a new lock should be made to the nearest of the undermentioned Carriage and Wagon Depots at which supplies are kept, when a man will be sent to the station to substitute a new one:—

Old Oak Common. Truro. Gloucester. Reading. Newport. Hereford. Bristol. Caerphilly. Worcester. Swindon. Barry. Wolverhampton. Newton Abbot. Cathays Yard. Oswestry. Neath.

#### DISCHARGING PETROLEUM FROM TANK CARS AT OIL AND SPIRIT DEPOTS.

Foremen and Shunters must avoid moving the Tank Cars while being discharged, and instructions have been given to the Oil Companies that a conspicuous red flag must be hoisted in a prominent position over the rail tank car to indicate that discharging is in progress. An Oil Company's representative must lose also be in attendance, whose duty it will be, upon completion of discharging, to see that the pipe is disc onnected and removed from the Tank Car.

(G.A. 18, 11/47, L.K.1/8596/16.)

Company's representative in charge must be required to first REMOVE THE DISCHARGE PIPE and give his assurance that the Tank Car can be moved with safety

Filling and emptying of Tank Cars must be performed only during daylight.

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#### TRANSIT OF PADLOCK KEYS.

Padlock keys sent to destination stations for unlocking covered vans containing general merchandise, and Keys for return to originating points must be sent forward in a sealed envelope by the "Booked Value" arrangements.

#### TRANSIT OF PADLOCKS.

Padlocks sent from one station to another should be locked together hasp to hasp and forwarded by the "Booked Value" arrangements using the special tie-on label No. 2670-8 which has been issued for this specific purpose.

Padlocks are not subject to common user arrangements and must be returned to the originating point without delay.

In no circumstances must locks and keys be returned together in one package.

(G.A.16, 5/46, F.2/84270 P.)

#### SECURING OF CHAINS AND ROPES ON TIMBER, ETC., WAGONS-Page 239.

The following to be substituted for the last paragraph of the instructions appearing under this heading:—

Wagons received from other Regions at Junctions must not be refused, neither should they be delayed on the grounds that the above instructions are not complied with, provided the chains, ropes or other appliances are so fastened as to render them safe to travel.

(G.A.23-7/49. L.K.1/7549/Gen.)

Private Owners' wagons must not be used for overhanging traffic, except in cases where Colliery wagons are required to be back loaded from Port to Colliery, when pit props may be conveyed.

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#### HANDLING OF WET PELT, HIDE AND SKIN TRAFFIC, &c.

In handling this traffic there is the risk of contracting the disease known as "Anthrax," i.e., a form of blood poisoning, unless the hands are protected.

To obviate this Stations and Depots where such traffic is dealt with are supplied with Oil Skin Aprons and India Rubber Gloves, which may be requisitioned as required through the District Goods Managers and must be used by the staff handling such traffic.

After use these articles must be thoroughly cleansed and kept in a place where they are accessible to members of the staff requiring to use them.

#### SECURING OF CHAINS AND ROPES ON TIMBER, &c., WAGONS.

Chains and ropes upon timber and other wagons must not be allowed to travel loose and unsecured and when not in use for fastening a load they must be stowed in the pockets or boxes, where these are provided, or, where there is no such provision, must be hooked and secured or fastened together across the vehicle in such a way as to prevent them moving or falling off during transit.

The attention of Goods Foremen, Loaders, Guards and Shunters is directed to Rule 157, which states that no timber truck, boiler wagon or other vehicle provided with chains or other appliances must be allowed to leave a station or siding without the chains and appliances being carefully examined and made perfectly safe and secure, and that Guards will be held responsible for seeing that they remain so during the journey.

Single or double bolster wagons must have the stanchions placed in the stanchion holes of the bolster, oth the chains being threaded through the "D" Shackles and secured tightly across the wagon.

So far as the fittings on specially constructed wagons, such as beams, etc., are concerned oncerneu, the securing pins or other appliances, where provided, must be used in all cases, and if further security is considered to be necessary ropes or chains must be employed.

Stanchions, when not in use, must not be left in the bolsters (when bolsters are loose on the truck), but must be properly placed in the stanchion sockets.

#### ADJUSTMENT OF TIMBER, &c. PRECAUTIONS TO BE TAKEN TO PREVENT ACCIDENT.

Before commencing work in connection with the adjustment, &c., of loads of timber, or other traffic, the man in charge of the loaders must communicate with the Foreman or Shunter in charge of the yard and advise him what is about to be done, and, in addition, he must take care that a red flag or red lamp is placed at both ends of the wagons in the Siding upon which the vehicles requiring adjustment stand.

#### LOADING OF CHAIR TRAFFIC.

Chairs in bundles, whether they are being loaded on Railway Trucks or on Road Vans, should always be placed upon their sides. If loaded otherwise they are liable to become strained and damaged.

#### LOADING OF WAGONS WITH TIP END DOORS.

Private Owners and Railway Companies' tip-end wagons may be used for the conveyance of bars, plates, etc., in accordance with the instructions on page 211.

Billets, blooms, ingots and moulds may also be loaded in tip-end wagons. Short billets, etc. not exceeding 2 cwts. in weight must be loaded across the wagon and large billets, etc. must be loaded longitudinally in the centre of the wagon over a length of not less than 5ft. The maximum load must not exceed 6 tons. End timbering and scotches must be used to secure the load in position.

(G.A.12. 4/43. C.G.M.—W.T./X. 66224.)
"Warning is given against the dangerous practice of propping up the doors of merchandisc trucks for the support of Coal Weighing Machines, for loading or unloading traffic, or for any

"The Great Western Railway Company give notice that such practice is prohibited, and any person disregarding this caution will be held responsible for injury or damage that may result." These notices may be requisitioned through the Stores Department in the usual way.

#### ADDRESSING OF GOODS WHEN CARRIED BY MERCHANDISE TRAIN.

1. Each article or package shall (except as hereinafter provided) bear:-

(a) The Consignee's full name and address in legible and durable characters, or

(b) A legible and durable distinguishing mark with a label (on the letter-card principle) stating on the outside the name of the station or place of destination, and on the inside the name and address of the Consignee.

(c) In the case of Wait Order traffic, a legible and durable distinguishing mark together with the name of the station or place of destination, and the full name and address of the person to whose order the article or package is sent.

(d) In the case of Export traffic, a legible and durable distinguishing mark together with the name of the port or dock of shipment, and the name of the ship or shipping agent.

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#### ADDRESSING OF GOODS-Continued.

2. Where a consignment consisting of more than ten articles or packages of the same or of a similar description of merchandise is forwarded to the same consignee the following provisions may be adopted :-

Number of articles or packages, 11 to 100:-

Not less than one article or package in every five shall be (a) addressed in accordance with Clause 1, and (b) marked to show the total number of articles or packages forming the consignment, provided that a minimum number of ten articles or packages shall be addressed and marked as aforesaid in each consignment.

Not less than one article or package in every ten shall be (a) addressed in accordance with Clause 1, and (b) marked to show the total number of articles or packages forming the consignment, provided that a minimum number of twenty articles or packages shall be addressed and

marked as atoresaid in each consignment.

Provided that where it is not possible for the Trader to indicate the total number of packages forming a consignment to be despatched by him, each part of the consignment when delivered to the Company must be labelled in accordance with this regulation as if the same were a separate consign-

- 3. Subject to Clause 2, Metal Bars, Rods, Tubes, Plates, Sheets, Forgings, Castings, Chains and any other similar merchandise shall have the addressing particulars as provided in Clause 1 conspicuously shown in legible and durable characters
  - (a) On wooden, metal or other durable tallies fastened to the merchandise by wire, or (b) Painted, stencilled or otherwise legibly and durably specified on the merchandise.
- Provided that Bars, Rods, Tubes and other articles which do not afford a suitable surface for painting or stencilling shall be securely bound into bundles convenient for handling, by wire, rope or other material to which the Company has given its approval in writing, and that such bundles shall have attached thereto tallies as provided by this condition.
- 4. Hides, Skins, Pelts or other merchandise carried loose shall have labels, or wooden, metal or other suitable tallies, affixed, and addressed in accordance with Clause 1 or 2 hereof.
- 5. Every label, tally, address or mark shall be securely fastened or affixed to the article or package.
- 6. All old or conflicting labels or addresses shall be removed or entirely obliterated before the article is tendered for carriage.

These regulations shall not apply to:-

(a) Returned empties, when legibly branded with the Owner's name and address. (b) Merchandise for which the exclusive use of a wagon is provided by the Company.

Articles identical in all respects, or packages of uniform description and size containing merchandise identical in all respects, when such articles or packages are forwarded in consignments of two tons and upwards from one sender to one station or place of destination.

(d) Export and import merchandise conveyed in through trucks direct to ship, and vice versa. (e) Merchandise forwarded to Ireland or the continent of Europe, which is carried subject to special addressing regulations.

 The Railway Companies do not undertake to label consignments on behalf of Traders except in the circumstances named hereunder, when, if requested, they will perform the service at the charges set out in Clause 9:-

(a) Goods stored with Railway Companies where it is impracticable for the Trader to arrange

- the necessary labelling.

  (b) "Third Party" Transactions, where goods are collected by the Railway Companies from a Trader, and the original Consignor has no knowledge of the ultimate Consignee or destina-
- (c) Goods sent to "Order" at destination station, where the Company is requested to remove the address labels and attach new labels giving the name and address of the ultimate Consignee.

(d) Goods stored in Public Warehouse, or Dock Warehouse, or on Quay, where the Railway Company holds the Bill of Lading for clearance.

9. When the labelling is performed by the Railway Company the following charges must be made for the service (except where other charges already apply):-

When label is supplied and addressed by Company When blank labels provided by the Trader are ½d. per label. Minimum 3d. for each day's forwardings. addressed by Company.

<sup>1</sup>/<sub>2</sub>d. per package. Minimum 3d. for each When Consignee's name and address is written on ) day's forwardings. goods by Company.

When addressed labels supplied by Traders are 1d. per label. Minimum 1d. for each day's forwardings. attached to goods.

In the event of the charges being paid by various Traders, the respective minima are to be maintained for each service.

Fractions of a penny must be charged as a penny when they amount to a half-penny or upwards, and fractions of less than a halfpenny must be dropped.

## STATION AND PICK-UP TRUCKS.

#### Station Trucks.

The Yard staff at Junctions or Marshalling Yards are responsible for ensuring that all station trucks which have been received and are available to go forward are despatched by their scheduled trains.

Guards must show on their journals when station trucks usually conveyed by such services are absent

Articles which from their number, weight or awkward nature are likely in handling to cause delay to the train on which the station truck is conveyed must not be loaded into station trucks, but be sent forward by other means, i.e., by being sent to some transfer point or in a direct wagon.

Care must be taken when loading station trucks that the goods are placed in accordance with the loading diagram provided for each station truck so as to be easily accessible for unloading at the various stations on the journey. Guards are responsible for seeing that at intermediate stations goods are loaded in the proper wagons and are put out at the proper stations. In taking out the goods at intermediate stations, the Guards must be careful to see that as far as possible they are securely placed in a safe position where they are not liable to fall or be damaged by wet or other means.

The number of packages for each destination loaded in station trucks must be entered on the station truck labels by the station starting the wagon and this information must be added to as necessary at intermediate stations.

Guards must record on their journals any case where they find a truck is habitually running with a light load, or which in their opinion can be dispensed with. The Divisional Superintendents must advise the District Goods Managers concerned of all cases reported on the journals of the Guards.

#### Pick-Up Trucks.

Guards must record on their journals any instances where pick-up wagons usually conveyed by certain services have not worked on these trains. The Divisional Superintendents must advise the District Goods Managers concerned of any such cases.

#### Invoices and Labels.

Except in certain cases (with which each Guard should make himself acquainted) where arrangements exist for Invoices to be despatched by Passenger Train, the Guards must see that they have Invoices for all the Wagons in their trains, and must report every instance in which this rule is departed from. They must use great care to prevent their being overcarried, or given out at any point other than the proper one.

Guards must see that Station and Pick-up Trucks which are in their trains are appropriately labelled. (G.A. 5.-2/39. C.G.M.-W.T.25073.)

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In order to avoid delay in transit, all wagon labels should be made out in block letters in *Thick Blacklead Pencil*, and not in ink or copying-ink pencil, which become indecipherable in wet weather. (G.A. 10. 3/42. E.78905/1.G.).

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#### LOADING, ETC., OF MERCHANDISE TRAFFIC.

#### ADDRESSING OF GOODS-Continued.

10. Where for purposes of trade secrecy the Company is requested to have the address labels removed at destination Station, arrangements must be made for this to be done before delivery is effected, and care must be taken that the point of origin is not improperly divulged.

11. Shipping Traffic.—It will meet the Railway Companies' requirements if particulars of the name of Ship, Dock, and Port through which the goods are to be shipped, and the total number of articles, or packages, in the consignment as prescribed in Clause 2 are given on the labels.

#### LABELLING OF WAGONS.

#### 1. Wagon Labels.

All loaded wagons, except those containing Mineral traffic in train loads for one destination, must be labelled on each side.

In order to avoid delay in transit all wagon labels should be made out in Writing Ink, and not in copying ink or pencil, which become indecipherable in wet weather.

#### 2. Full Routes must be shewn on Labels for Wagons going to other Companies' Lines.

Wagons for Stations on other Companies' lines must bear labels shewing the name of destination Company and the full route by which they are to travel, and, where the Great Western Company has no direct exchange with the destination Company, the following information must also be shown:—

Junction of exchange between C.W.R. and intermediate Company;
 Junction or Junctions (if any) between intermediate Companies; and

(3) Junction between intermediate and receiving Companies in accordance with the instructions in the Merchandise Route Book.

#### 3. Labels from Wagons received Loaded to be examined and kept.

Labels off ALL wagons (from Local or other Companies' Stations) must be collected, checked off with the invoices for the goods, and filed for reference.

In the event of full information of the points of exchange not being shown on the labels of wagons received from other Companies' Lines, a report of each case, accompanied by the defective label, must be promptly sent to the District Goods Manager.

#### LABELLING OF DEFECTIVE PRIVATELY OWNED WAGONS.

The Carriage and Wagon Examiner must inform the Traffic Department Inspector, Guard or Shunter where loaded wagons are marked off, and arrangements to be made immediately by the Traffic Department to berth the trucks in a Cripple Siding.

The Carriage and Wagon Examiner must place the red "Not to Go" card over the traffic label in the pocket or under the clip, but not to completely obscure the traffic label. If the Traffic label is tacked to the side of the wagon, the red card must be placed immediately by the side of it.

The Chief Mechanical Engineer's Department, after the necessary repairs have been carried out, to be responsible for intimating to the Traffic Department Staff that the wagons are available to continue their journey.

When a loaded wagon is marked off for repairs, particulars of the stoppage must be entered by the Examiner in the "949" book, which is to be handed to the Goods Agent. At stations where the Station Master also controls the Goods Department staff, the advising to be done by the Station Master.

When a wagon containing a through load is found during transit to be unfit to travel and Chief Mechanical Engineer's Department men with suitable material are available at or near the spot, and the necessary repairs to the vehicle can be carried out without causing greater delay than would otherwise occur owing to transhipment of the load, the vehicle should be repaired while under load. The local Goods and Chief Mechanical Engineer's Departments must retain close contact to avoid the transit of the Goods being unduly delayed. In all other circumstances the load must be transhipped, a record of the occurrence being kept at the transhipping station (at large stations in a separate book), and, in labelling the wagon into which the load is transhipped, such transhipping station must state on the labels the name of the original forwarding station, the date on which the disabled wagon left there, and its number.

The transhipping station, on the day of the occurrence, must advise the sending and receiving stations of the failure of the Wagon and consequent transhipment; also take the steps prescribed in such cases for its repair.

INVOICES AND STATION TRUCK AND PICK-UP LABELS.

Except in certain cases (with which each Guard should make himself acquainted) where arrangements exist for Invoices to be despatched by Passenger Train, the Guards must see that they have Invoices for all the Wagons in their trains, and must report every instance in which this rule is departed from. They must use great care to prevent their being overcarried, or given out at any point other than the proper one.

than the proper one.

Guards must see that Station and Pick-up Trucks which are in their trains are appropriately labelled.

The invoices should accompany the wagons.

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#### COVERED VANS FOR STATION TRUCK OR PICK-UP TRUCK TRAFFIC.

Covered vans must, whenever practicable, be used.

#### SHEETS.

The Company make and maintain their own Wagon, Cartage and Station Sheets. The principal part of this work is performed at Worcester, and the Sheet Works Manager who has charge of the Making and Repairing of Sheets is at that Station.

The attention of all concerned is directed to the following Regulations:

#### SHEETING OF TRAFFIC.

- 1. Every opportunity must be taken to examine sheets before they are utilised, and any discovered not waterproof must not be used to cover load but must be sent to the shops for attention.
- 2. Care must be taken, in sheeting wagons, that the loads do not project in such a way as to render the Sheets liable to be damaged on the journey.
- 3. When two Sheets are used to cover a load (and this must only be done when absolutely necessary), the Sheet placed on the end of the load nearest the Engine must over-lap the other Sheet. The name of the owner and numbers of such Sheets, as well as of those placed under the loads, must be shown on both the wagon labels.
- 4. Goods must be so loaded as to ensure water draining off the sheet properly. A large number of wagons are fitted with the Sheet Supporter, and where such wagons are used the appliance must, whenever practicable, be placed in an upright position and the sheet carefully spread over it in order that "hollow" sheeting may be entirely prevented. (See instructions on page 244 relating to the sheeting of wagons fitted with sheet-supporters.)
- 5. Every precaution must be taken in sheeting Goods (particularly bulky traffic, such as Wool, Hay, Straw, Esparto, Hops, Hemp, Flax, &c.) to ensure protection against inclement weather or fire.
- 6. Sheets must be properly secured by all their strings to the Sheet fasteners specially provided for the purpose on the wagons, and not to the buffers, brake handles, springs, guard plates, vacuum release valve cords, or drawbars. In the event of any of the Sheet ties being missing, or unfit for use, they must be replaced by new ones, supplies of which can be ordered from the Stores Department.

Sheet ties must not be fastened to crossbars or catches of tip end doors of wagons.

#### CARE OF SHEETS AT STATIONS.

7. Before proceeding to unload any sheeted wagon, the sheet or sheets covering it must be taken off, folded so that the number or numbers shall be visible, and removed to a place of safety, and, where practicable, under cover, but in all cases clear of rail or road traffic.

Sheets must not be stored in wagons, and any surplus for which no disposal instructions are held

must be sent to the Sheet Shops.

Care must be taken to see that sheets are not :-

(i) Run over by road vehicles, wagons or engines. (ii) Dragged along the permanent way or road way

(iii) Left in such a position when a wagon is partially uncovered as to render them liable to damage if the truck is moved.

(iv) Cut or torn by goods with sharp corners, or edges or projections when being placed over a load, during transit, of when the traffic is being uncovered at destination.

(v) Tied to buffer rods or castings, brake handles, springs or drawbars.

(vi) Used as packing. (vii) Sent to Private Sidings, Works, Collieries, Brickyards, &c., in otherwise empty wagons when not required for outward traffic.

(viii) Allowed to remain in Private Sidings, Works, Collieries, Brickyards, &c., where they may be misused and damaged.

(ix) Burnt by Acids or other Chemicals.

(x) Misappropriated, or misused in any way.

- (xi) Damaged as a result of hollow sheeting owing to the weight of water which may accumulate in the hollows bringing the sheets into contact with cases, &c., which have sharp corners or edges or projections.
- 8. When traffic is dealt with in the open, and it is necessary to use a Sheet to protect it pending the completion of unloading (or loading) operations, the Sheet must invariably be so secured to the wagon as to prevent it falling or being blown over the wagon side and trailing on the ground. Neglect in this particular must not occur, or serious loss will be entailed upon the Company.
- Wagon Sheets are supplied solely for the purpose of covering goods handed to the Company for transit, and they must not be allowed to be used by the Traders for their own purposes; nor must they, except when absolutely necessary, and then only for a limited period, be used to cover goods lying in warehouses or upon wharves at the Stations. If it is necessary to have Sheets on hand for this purpose, application is to be made to the District Goods Manager for "Station Sheets."

SHEETS-Continued.

#### SHEETS SENT OFF THE COMPANY'S PREMISES.

10. The numbers of Sheets covering traffic sent into Sidings, Works and Collieries situate off the Company's line must be recorded, and steps taken by the person appointed to look after the Sidings to ensure the Sheets being returned in due course, and in good condition. All cases of damage to and misuse of Sheets in such Sidings, Works, &c., must be reported; and the Sidings, Works, &c., must be visited frequently and regularly with the view to seeing that the Company's Sheets are properly treated in every respect.

11. All sheets, whether actually damaged or not, should be sent periodically to the Sheet Shops

to be overhauled. The red figures on the Sheets indicate the date on which they are due back at the Sheet Shops for examination, and a special look-out must be kept to ensure that out-of-date sheets are not used for traffic purposes but are sent to the Sheet Shops immediately.

#### SHEETS DAMAGED OR LOST.

- 12. If a Sheet be damaged during transit, the Guard, on arriving at the Station where the truck covered by the damaged Sheet is put off, must call the attention of the Station Master or Goods Agent or other responsible person to the damage, informing him of the circumstances under which it was sustained. He must also give particulars of the occurrence upon his journal, so that extracts from such reports can be sent by the Divisional Superintendents to the Sheet Works Manager, Worcester. Serious notice will be taken of any failure on the part of Guards to carry out these Instructions.
- 13. The receipt of a sheet in a damaged condition must be immediately reported by the Station Master or Goods Agent at the inwards station to the Sheet Works Manager at Worcester, and also to the sending station. The report must state the train by which the sheet was received, the number of the wagon, the Guard's name, and, when practicable, his explanation of the damage. In the event of a Sheet being destroyed or lost, the Staff at the Station to which it was last traced will be held responsible for such destruction or loss. Any Servant of the Company detected in wilfully damaging a Sheet will be severely dealt with.
- When Sheets, which are rendered unfit for use otherwise than by ordinary wear and tear, are sent for repairs, the invoices issued must, when possible, state when, where and how the damage occurred. The Station Masters or Goods Agents at the Stations where the Sheet Depots are situated must report to the Sheet Superintendent every case that comes under notice in which this rule is not carried out.

#### COLLECTION OF SHEETS FROM EMPTY WAGONS, &C., AND DISPOSAL OF SPARE SHEETS.

Sheets passing in otherwise empty wagons, or received at stations in wagons en route to Wagon Repairing Depots, must be collected and disposed of in accordance with the Divisional Superintendent's or District Traffic Manager's Instructions. The "Sheet Trucks" running on various parts of the line must be fully utilised. On Branch lines, served only by mixed trains, sheets may be placed in the Guard's van if no open truck is available. Sheets used to cover Live Stock or Cattle Trucks must be promptly removed when the trucks are released.

16. Sheets not required for immediate use must be shewn as spare upon the daily return to the Divisional Superintendent's or District Traffic Manager's Department, and disposed of in accordance with his instructions. Sheets must not be kept on hand in anticipation of traffic. When sheets are loaded up in wagons and sent for local distribution in accordance with instructions, the total number of sheets in the truck must be shewn on the wagon labels.

#### DISPOSAL OF SPARE AND DEFECTIVE SHEETS TO SHEET SHOPS.

17. Spare sheets other than those required for distribution (i.e. sheets on hand not ordered elsewhere by the Divisional Superintendent or District Traffic Manager the same day as they are shewn as spare), together with any defective sheets must be forwarded to the various Sheet Shops, indicated hereunder, in the authorised daily Sheet Trucks or by other means specially provided for :-

#### No. 1 Area for Saltney Sheet Shop.

From stations north of a line drawn from Bryn Teify through Llandilo, Torpantau, Craven Arms, Tenbury Wells, Stourport, Brettell Lane, Lye, Henley-in-Arden to Lapworth (inclusive).

#### No. 2 Area for Worcester Sheet Shop.

From stations south of No. 1 Area and east of a line drawn from Presteign, through Hay, Pontrilas and down the Golden Valley line to Little Mill Junction, all inclusive, and north of and including the main line from Paddington (embracing all London Depots) to Llanwern via Didcot, Swindon and Gloucester but excluding the section between Wantage Road and Swindon.

#### No. 3 Area for Cathays Sheet Shop.

From stations in South Wales, i.e. south of No. 1 Area and west of No. 2 Area.

## LOADING, ETC., OF MERCHANDISE TRAFFIC.

#### SHEETS-Continued.

### No. 4 Area for Bridgwater Sheet Shop.

From stations south of No. 2 Area and east of the River Severn.

18. Covered Goods Vans, except where specially authorised, must not be used for loading sheets, and all open wagons containing sheets sent from point to point for use, repairs, or any other purpose, must be sheeted.

## NEW AND REDRESSED SHEETS.

19. New and redressed sheets must be opened immediately on receipt at stations and used without delay.

Owing to the tackiness of the sheet dressing serious damage occurs by adhesion if this is not done.

20. Station Sheets must not be sent away from the station to which they belong, nor be used for any other purpose than that for which they are supplied. Station Masters or Goods Agents will be held personally responsible for the safe care and maintenance of these sheets, which must be sent to the Worcester Sheet Depot for overhauling at least once each year.

## SHEETING OF WAGONS FITTED WITH SHEET SUPPORTERS.

The following instructions must be observed :-

(1) Each sheet must be provided with its full complement of ties: if any of the ties are missing they must be replaced by new ones, supplies of which can be obtained from the Stores Department in the ordinary way.

(2) Each sheet must be placed centrally on the wagon, and the sheet ties secured to the fasteners provided on the truck.

## WAGONS AND SHEETS INFESTED BY WEEVIL.

Wagons and sheets infested by weevil must be sent to the Carriage and Wagon Works, Swindon, where a steam-heated insulated chamber has been installed for exterminating the pest, the label to be endorsed "WEEVILY-FOR CLEANSING" and an advice of despatch to be sent to the Chief Mechanical Engineer, Swindon.

Stations forwarding traffic known to be infested with weevil must endorse invoice "WEEVILY TRAFFIC-AFTER DISCHARGE TRUCKS TO BE SENT TO CARRIAGE & WAGON WORKS,

SWINDON, FOR CLEANSING.'

#### ROPE SCOTCHES.

Supplies of rope scotches are allocated to certain stations in each District.

Each scotch bears a round ferrule stamped with the letters "G.W.R.," number, and the name of the station to which it belongs.

Stations receiving these scotches must see that they are promptly returned to the forwarding point and properly labelled.

When a large number of scotches is being returned to one station, it will suffice if one in five is

Stations to which these articles are permanently allocated must check their stock once a month to ensure that they have their full complement.

The numbers of any missing scotches must immediately be reported to the District Goods Manager concerned.

Any found branded for another station must be immediately returned to the station to which they belong, except where otherwise authorised.

Any scotch which is found to be without a ferrule must be immediately labelled to the Sheet Department, Worcester.

## CARTAGE ROPES FOR ROAD VEHICLES.

Ropes for use on Road Vehicles are supplied to the Stations by the Sheet Department at Worcester and must be ordered through the Stores requisition.

When application is made for new cartage ropes, the old ones, with ferrules, must be sent to the Sheet Depôt, Worcester, and a note made on the order.

## WEIGHING OF TRACTION ENGINES AND OTHER HEAVY TRAFFIC.

Traction Engines and other traffic, which cannot be weighed at forwarding point, must be sent to the nearest station en route, having a weighbridge of sufficient capacity, with instructions to "Weigh and advise," and it will be the duty of the person performing the weighing to see that the wagon is not sent forward if overloaded, or otherwise unfit to travel.

Office District Piblive Tower Superintendent's stail and sent to the appropriate

#### LOADING, ETC., OF MERCHANDISE TRAFFIC.

REGULATIONS FOR THE CONVEYANCE OF PRIVATELY-OWNED LOCOMOTIVES RUNNING ON THEIR OWN WHEELS OVER G.W. OR OTHER COMPANIES' LINES—page 245.

The instructions under the above heading to be deleted and the following substituted:-

REGULATIONS IN REGARD TO THE ACCEPTANCE AND CONVEYANCE OF PRIVATELY-OWNED LOCOMOTIVES AND TRAVELLING CRANES RUNNING ON THEIR OWN WHEELS.

The following regulations must be observed by all concerned in the conveyance of this traffic. It should be understood that in these instructions the term privately-owned locomotive embraces privately-owned travelling cranes on their own wheels.

#### I. Examination

(a) On receipt of the application, the Goods Agent or Station Master who deals with the Senders in regard to the transit of locomotives must communicate with his appropriate District Officer, who will arrange for an examination to be made by the District Motive Power Superintendent: as much time as possible must be allowed for this examination. It must be clearly notified whether the locomotive is to be worked "dead" or under its own power.

(b) The District Motive Power Superintendent will arrange for the necessary examination to be carried out, and as soon as this is completed, for an advice by telephone or telegram to be given to the appropriate District Officer as to whether the locomotive is fit or unfit to travel on its own wheels. After the examination has been carried out, a report form—Diagram and Certificate of Examination of Privately-owned Locomotives running on own Wheels (B.R. 87262)—is to be filled in by the examiner and countersigned by a responsible member of the District Motive Power Superintendent's staff and sent to the appropriate District Officer.

(c) Before a steam locomotive is allowed to run under its own power, the Goods Agent or Station Master must inform the sender or owner that the examination made by the Railways does not include the firebox and boiler.

(d) Before a locomotive is allowed to run "dead", the coupling rods, connecting rods and valve rods, and eccentric straps where fitted, should be removed, the pistons secured to prevent movement and, if the material removed is of a suitable size to be conveyed on the locomotive, it must, in turn, be properly secured, otherwise it will be necessary for a wagon 1 to be attached to carry this material.

#### 2. Specification.

Privately-owned locomotives must comply with the following:-

(i) The minimum clearance above rail level of any part of the locomotive, either between the wheels or outside the wheels, must not be less than the measurements shewn in the table below confined to a width of 3 ft. 6 in. on either side of the centre of track, except that the clearance may be down to rail level over a width of  $2\frac{\pi}{8}$  in. on either side of centre of each rail, as shewn in sketch depicted on the Diagram and Certificate of Examination.

Any excess over this width of 3 ft. 6 in, to be within the loading gauges of the lines over which the locomotive is required to travel.

Minimum Underclearar					Between
Six inches		***	***	***	Centre Line of Track and 1 ft. 4 in. either side.
Five ,,			***		I ft. 4 in. and 2 ft. 15 in. either side.
Four "	***		***		2 ft. 1\frac{5}{8} in. and 2 ft. 2\frac{3}{4} in. either side.
Six "					2 ft. 8½ in. and 3 ft. 6 in. either side,
				EXC	CEPTIONS.

## Loftus & Whitby Line (N.E. Region)

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Nine inches ... ... 2 ft. 8½ in. and 3 ft. 6 in. either side.

## Manchester and Bury Line and Tottington Branch (L.M. Region)

Ten ... ... 3 ft. 2 in. and 3 ft. 6 in. either side.

(ii) The height of centre of buffers above rails should be not more than 3 ft. 6 in.

(ii) The height of centre of buffers above rails should be not more than 3 ft. 6 in. nor less than 3 ft. 4 in. unless the examiner considers the buffer faces are sufficiently large to prevent locking.

"Dead" buffered locomotives may be accepted for conveyance over all Regions;

such locomotives must be marshalled between spring-buffered vehicles.

The height of the centre of drawgear above rails should be not more than 3 ft. 6 in. nor less than 3 ft. 2 in.

(iii) The diameter of the wheels forming the rigid wheelbase should not be less than 2 ft. 6 in. In the case of a two-wheel or a four-wheel bogie in addition to at least four coupled wheels, the minimum diameter allowed for bogie wheels to be 2 ft.

(iv)	The minimum thickness of tyres to be as shewn below:—	
	For axle loads under 15 tons	11 in. on tread
	For axle loads 15 tons and under 18 tons	13 in. on tread
	For ayla lands 10 cans and above	II to an accord

In the case of axle loads under 10 tons, and the tyres formed solid with the rims of s. the wheels the minimum thickness on the tread may be 1 in.

Axle loads should be supplied by the Owner; if details are not available, an estimate is to be made by the examiner.

(v) The rigid wheelbase of any privately-owned locomotive to be conveyed by Freight train to be not less than 5 ft.

## LOADING, ETC., OF MERCHANDISE TRAFFIC.

## REGULATIONS FOR THE CONVEYANCE OF PRIVATELY OWNED LOCOMOTIVES RUNNING ON THEIR OWN WHEELS OVER G.W. OR OTHER COMPANIES' LINES.

1. When a privately owned locomotive running on its own wheels is offered for transit the District Goods Manager or District Traffic Manager will communicate with the Divisional Locomotive, Superintendent, who will arrange for a competent mechanic to examine the locomotive, noting the following details :-

(a) Maximum width and height of engine (including width over cab at the cornices and height of cornice), any abnormal projections to be noted.

(b) Height from rail level to: (i) Centre of buffers. (i) Centre of drawgear.

(iii) Centre of cylinders at front end. (iv) Bottom of buffer beam.

(v) Platform of engine.

Dimensions of buffer faces and whether "Spring" or "Dead."

Minimum clearance above rail level between wheels and outside wheels.

(On the Great Western Railway the minimum clearance above rail level need only be six inches. See diagram below).

Distance between axles.

Wheel arrangement. (q) Diameter of wheels.

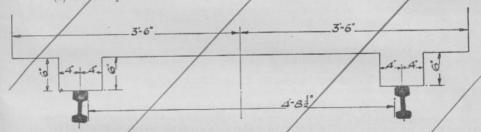
(h) Whether contour and thickness of tyres and distance between the backs of the tyres are satisfactory

Weight on each axle in working order if to be run in steam, and empty if to be rup" dead"

(senders declaration to be accepted).

(j) Whether fitted with ash pan.

(k) Whether provided with an efficient hapd brake.



THE MINIMUM CLEARANCE ABOVE RAIL LEVEL TO BE 6, BOTH OUTSIDE AND INSIDE THE RAILS FOR A DISTANCE OF 3-6 EACH SIDE OF THE CENTRE LINE EXCEPT FOR A ON EITHER SIDE OF THE RAIL CENTRES.

If the transit involves travelling over other Companies' lines, these details must be filled in on the diagram provided for the purpose and the Company on whose line the journey is commenced must make the necessary arrangements with the other Companies concerned for the acceptance of the engine, and whilst it is not necessary for them to send diagrams, they must give an assurance that the engine complies in every respect with these regulations. Should the engine not so comply with the Regulations, the procedure in respect to out-of-gauge loads must be followed, and, before the engine is allowed to go forward agreements must be obtained from all concerned in the throughout transit.

2. Providing the detail are satisfactory and the engine complies with the following, it may be accepted for running on its own wheels :-

(a) The height of centre of buffers above rail should not be more than 3 ft. 6 inc., nor less than 3 ft. 4 ins., unless the Examiner considers that the buffer face is sufficiently large to prevent locking. The height of centre of drawgear above rail should not be more than 3 ft. 6 ins., nor less than 3 ft. 2 ins.

(On the G.W. system, before a dead buffered engine is formed in a train for transit, it must

be coupled between two spring buffered wagons or between a spring buffered wagon and the

train brake van by means of emergency screw couplings.)

(b) The diameter of the wheels forming the rigid wheel base to be not less than 2 ft/6 ins. In cases where the engine has a two-wheeled or four-wheeled bogie, in addition to at least four driving wheels, the minimum diameter for bogie wheels to be 2 feet.

## LOADING, ETC. OF MERCHANDISE TRAFFIC.

## PRIVATELY OWNED LOCOMOTIVES-Continued.

(c) The minimum thicknesses of tyres for engines with the following axle loads to be as shewn below :-

11 inches Axle Loads under 15 tons . 15 tons and under 18 tons 13 18 tons and above 11

In the case of an engine having axle loads under 10 tons, and the tyres formed solid with the rims of the wheels, the minimum thickness on the tread may be inch.

(d) Rigid wheel-base to be not less than 5 ft. (See also Clause (e).)

An engine having a rigid wheel-base of not less than 4 ft. 9 ins. may be run light in steam or hauled by special engine. An engine with a rigid wheel-base of less than 4 ft. 9 ins. must be

If run in freight trains, an engine weighing 30 tons and over must be marshalled next the train engine; any engine below this weight must be marshalled next within the rear brake van

4. Each engine forwarded "dead" must be accompanied by a competent man in chapge, and in case such a man is not provided by the owner, a Railway Company's Fireman, or other competent person, appointed by the Locomotive Running Department, must travel with it. The man accompanying the engine must travel on its footplate.

(5) Each engine forwarded in steam must be accompanied by a competent man to be provided by the Owner. A Railway Company's Driver and Fireman must talk

engine and the Private Owner's man mu

6. An engine in steam must not be accepted for conveyance unless it is fitted with an ash pan

and an efficient hand brake, which must be in good working order.

If the hand brake of an engine to be forwarded "dead" is defective, suitable labels bearing in large letters "DEFECTIVE BRAKE" must be attached to either side of the engine near the footplate, and the engine must be coupled to a wagon fitted with "either-side" brakes by means of emergency screw couplings. It must not be detached from this wagon during its transit.

7. Private Owner's engines, if conveyed in freight trains on their own wheels, must be formed only in services carrying "J" or "K" headlamps, and which have a booked sectional speed not exceeding 25 miles per hour and do not travel more than 25 miles without stopping.

8. Should the axle load exceed 14 tons, the Engineer for the respective Railway, or Kailways,

over which the engine will pass must be consulted.

9. Before despatch, an engine, whether to be run "dead" of in steam, must first be examined by a representative of the Chief Mechanical Engineer's Department as to its fitness to run. The Owner must be given to understand that this examination does not include the firebox and boiler of the engine, and when an engine is to run in steam, a certificate must be obtained from the Owner, stating that the engine is in good condition and fit to be worked in steam, and indemnifying the Railway Company against any accident or injury which may occur. This will be arranged by the District Traffic Manager, or District Goods Manager, who will, where necessary, pass the certificate on to the Divisional Superintendent. The Divisional Superintendent, District Traffic Manager, or other officer concepted, will obtain from the Chief Mechanical Engineer's Department a certificate as to fitness to run, and will give the necessary instructions to the Station Masters, Signalmen and all concerned in the working of the traffic.

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(vii) Steam

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Civil Engineer Accompanying

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(b) Each priva nied by a Railway take charge thr be transferred mers must accor otplate. (See (c

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- (vi) A privately-owned locomotive having a wheelbase not less than 4 ft. 9 in. may be run light under own power or hauled by special engine. If the rigid wheelbase is less than 4 ft. 9 in. the locomotive must be loaded up.
  - (vii) Steam locomotives without ashpans must not be allowed to run in steam.

#### Acceptance and Conveyance.

(a) Locomotive within Gauge and Axle Weights not excessive.—The appropriate District Officer, on receipt of Diagram and Certificate of Examination (B.R. 87262), satisfactorily completed in all respects and being satisfied, through prescribed Regional channels, that the locomotive is suitable from a loading gauge aspect and axle load requirements to travel to its destination, must make the necessary arrangements for despatch through the

Operating Department, advising Goods Agent or Station Master accordingly.

In cases of interchange between Regions, the appropriate District Officer must make the necessary arrangements for the acceptance of the locomotive with the other Regions

concerned, through the prescribed Regional channels.

(b) Locomotive Out of Gauge and or Axle Weights excessive.—Should a locomotive not conform in all respects with the loading gauge and axle weight requirements, or present any other features not herein provided for, the procedure in respect of out-of-gauge loads must be followed, and before the locomotive is allowed to go forward, agreements must be obtained from all concerned in the throughout transit, through the prescribed Regional channels.

(c) General.—Privately-owned locomotives must not be allowed to work under their own power over lines maintained by the Railway Executive without at least one axle load

of 10 tons or over unless specially authorised by the appropriate Regional Headquarters.

Any number of privately-owned locomotives "dead" on own wheels may be conveyed by one freight train up to the loading capacity available, provided axle weights produced are not excessive, having regard to the route to be travelled, and that instructions are complied with respecting clearances, etc., as shewn on the Diagram and Certificate of Examination of Privately-owned Locomotives running on own Wheels (B.R. 87262). All other cases must

be referred to the Civil Engineer for consideration.

A privately-owned locomotive "dead" on its own wheels must not travel at a speed exceeding 25 m.p.h. at any point, and must stop for examination purposes at least once every

25 miles.

A locomotive weighing 30 tons and over must be marshalled next the train engine, and if more than one such locomotive is attached to a train they must be separated by at least

two ordinary wagons (more if the Engineer requires them).

A locomotive weighing below 30 tons must be marshalled next within the rear brake-van but not more than three such locomotives are to be grouped together; or if required by the Civil Engineer they must be separated by a stipulated number of ordinary wagons.

#### Accompanying and Indemnity.

(a) Each privately-owned locomotive forwarded "dead" must be accompanied by a competent caretaker, and in case such a man is not provided by the sender or owner, arrangements must be made by the appropriate District Officer with the District Motive Power Superintendent to provide a Fireman or other competent person to travel on the footplate.

The person in charge of the locomotive, whether provided by the owner or the Railways, must travel on the footplate of the locomotive and not in any other part of the train. If the destination is on another Region, the Railway caretaker will travel to the junction at which transfer is to be effected, unless otherwise arranged, and the appropriate District Officer must arrange in advance with the other Regions concerned to provide caretakers over their respective Regions. In every case possible, the caretaker should be provided by the sender or owner. (See (c) below regarding indemnity.)

(b) Each privately-owned locomotive forwarded under its own power must be accompanied by a Railway Driver and Fireman provided by the District Motive Power Superintendent to take charge through to destination, or if for another Region, to the junction at which it will be transferred, unless otherwise arranged. A competent man provided by senders or owners must accompany each locomotive forwarded under its own power and travel on the ootplate. (See (c) below regarding indemnity.)

(c) Indemnity Form (R.C.H. 60040), properly completed, must be obtained in respect of all journeys of locomotives travelling under own power and also caretakers in charge of ocomotives travelling under own power or "dead".

(d) A sender's or owner's man travelling with a locomotive must have a pass issued to him through to destination; this must be obtained from the appropriate District Officer by the most expeditious means. On the return journey the man must pay his fare.

(G.A.29.Op.-5/52. LK1/10310 (Gen. E.).)

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## SECTION III. (b).

# INSTRUCTIONS CONCERNING LOADING, CONVEYANCE, ETC., OF LIVE STOCK BY PASSENGER AND FREIGHT TRAINS.

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## LOADING AND UNLOADING HORSES, &c., CONVEYED BY PASSENGER TRAIN.

1. Before accepting a horse, pony, or other animal for conveyance by rail, the animal must be carefully inspected to see whether it is suffering from injury, wounds, or other defect, and if anything whatever be observed, the attention of the owner, consignor, owner's agent or representative must be drawn to it, and a note of the injury or defect must be made upon the way-bill.

2. The Traffic Staff at Stations where horse-boxes (both other Companies' and G.W.) are received, or loaded, must see that the fittings, etc., are intact, and any deficiencies reported at once.

Before any horse-box is used it must be thoroughly examined, and all partitions properly secured. No box should be loaded unless the whole of the fittings, ropes and head-stalls are in thorough working order.

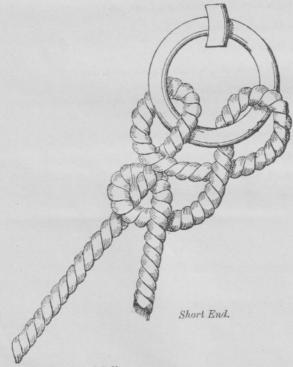
Bales and other fittings removed from horse-boxes for purposes of loading or other causes, must be replaced in position; and when vehicles are received without the necessary fittings, they must be returned to the loading Station to enable the parts to be replaced. Head-collars must not be placed in the  $coup\acute{e}$ , but hung in position in the stalls.

3. No horse, etc., must be accepted for conveyance unless provided with a hardr or bridle, and with a label attached giving name of consignee and destination station.

4. In tying up horses, or other animals, the length of the upper and lower ropes must be carefully regulated by the height of the animal, so as to keep its head in a natural position. The top tope should be of sufficient length to allow the animal's nose to reach the manger, but not longer; and the length of the bottom rope should be from twelve to eighteen inches according to the height of the horse or other animal, but in no cases should it exceed eighteen inches.

5. The head-stalls, which are adjustable both in the nose and neck straps, must be made to fit the head of the animal. When ponies, or horses with small heads, are being tied up, the nose and neck straps must be reduced to such a size as to avoid the head-stalls being slipped.

6. With a view to assist those engaged in fastening horses, etc., in a box, the following diagram illustrates the proper method of securing the ropes to the rings:—



End attached to Leather Head Collar.

#### LOADING AND UNLOADING HORSES, ETC., BY PASSENGER TRAIN-Continued.

In tying this knot the end of the rope should be taken in the right hand, and passed through the ring downwards, and brought out at the left-hand side of that part of the rope attached to the Head Stall, then carried over the rope towards the right, and passed again through the ring downwards, and brought out between the ring and the rope loop. The end rope must be tied again to complete the knot.

- 7. Horses fully harnessed should not be boxed except at the special request of the sender, when a remark must be made on the Live Stock Ticket and initialled by the sender. Harness in all other cases must be removed before the animal is loaded.
  - 8. Loose harness must be loaded in the coupé of the box, and a remark made on the horse ticket.
- 9. If a horse tendered for conveyance is considered too large to travel safely in one stall, it must not be loaded until the attention of the owner, consignor, or owner's agent or representative has been called to the fact and his instructions taken, and if two stalls are ordered the sender must endorse the live stock ticket accordingly.
- 10. If a horse is found to be restive from nervousness or any other cause, and there is a doubt of its travelling safely, the attention of the Station Master or other responsible person must be drawn to it, and he must use his discretion as to whether it is advisable to forward the animal or not.
- 11. In loading animals preference should, whenever possible, be given to boxes which will travel with their head-stall in the direction of the engine.
- 12. The windows of any unoccupied  $coup\acute{e}$  of a horse box must, in all cases, be kept closed on the journey.
- 13. The conditions of the live stock ticket must in all cases be signed and the rate at which the animal is to be charged must be specified in full, i.e. "Company's" or "Owner's" Risk.
- 14. Horse boxes containing animals likely to reach their destination after dark must be provided with Trimmed Lamps, placed in the lamp holes of the horse box at forwarding station, if possible; if, however, there are no lamps available a telegram must be sent to the first depot *en route* at which lamps are kept, and they must be lighted wherever the carriage lamps are lighted for passengers.

Horse boxes loaded with horses proceeding to race meetings should be fully gassed before commencing the forward journey. In the event of no gas-fitted boxes being available, oil lamps, trimmed and filled, must be provided.

- 15. Guards must, as far as possible at junction stations and on the journey, enter the  $coup\acute{e}$  of loaded horse boxes attached to their trains in which no attendant is travelling and satisfy themselves that the animal or animals are in the same position as when loaded and that they are properly secured, If a horse should be found injured in any way, down in the box, in an unusual position, or slipped its head-stall, the attention of the Station Master must at once be drawn to the fact in order that proper steps may be taken, and a note must be made on the Guard's journal.
- 16. In the event of an animal injuring itself in transit, the Station Master where the injury is discovered must carefully note how the animal is tied and measure and record in writing the length of the top and bottom ropes from rings and head-stall, and in case of an animal slipping its head-stall or getting down in a box the same information must be obtained, whether any injury is observable or not. The head-stall and ropes must be retained when either of these have to be cut or have been broken to release the animal, and the horse box, &c., must be thoroughly examined by the Carriage Department and the vehicle must not be used again until instructions are received from the Divisional Superintendent or District Traffic Manager. The manner in which the head-stall is buckled should be particularly noted, and a record kept of the number of holes from the rings in which the head-stall is fastened. If an animal has slipped its head-stall it should be replaced with the view of seeing if it had been properly fastened. Every case must be reported, on Form No. 1874, to the Divisional Superintendent or District Traffic Manager, giving date, number of box, particulars of consignment, train, Consignor, Consignee, Guard's name, &c., and if it is necessary for a Veterinary Surgeon to see the animal a telegram is to be sent to the Divisional Superintendent or District Traffic Manager stating nature of injury, and where the animal can be seen. If the case is very serious a local Veterinary Surgeon must be called in to examine the animal, and an advice (by wire in the case of an animal being dead) sent to the sending and receiving Stations in order that the Consignor and Consignee can immediately be acquainted with the facts, and may be present, or represented, at any post-mortem examination which may be held and which should be carried out by the Veterinary Surgeon who may examine the animal.
- 17. The staff at the destination Station must search the coupé and locker of every horse box immediately on arrival, and any property other than the Company's must be removed. Should anything be found not connected with the horse travelling in the box it must be dealt with as lost property.
- 18. On arrival of a loaded box at its destination, steps must be taken to see that any animal that may be in the box is in proper condition before the flap of the box is let down.
- 19. Immediately a horse is unloaded the box must be properly cleansed and disinfected, and the coupés must be swept out and the windows cleaned.
- 20. Horse box stall partitions when removed for the purpose of loose loading must be replaced by the receiving Station after unloading the traffic, excepting on those occasions when a special event, such as an Agricultural Show or Fair, is being held.

### LOADING AND UNLOADING HORSES, ETC., BY PASSENGER TRAIN-Continued.

21. If there is an animal in the near or platform side stall, the head-stall of such animal must be released by a man reaching into the stall from the coupé before the flap of box is let down, and the horse must then be held by the bridle or halter until the doors have been fully opened, when the animal must be carefully led from the box.

22. When an animal is loaded in the centre or off-side stall it is necessary, before opening the partitions, that the head-stall should be released and a man stationed at the animal's head to hold

it until the partitions have been fully opened.

- 23. Should by any chance a horse be found in a box without its owner's bridle or halter, the ropes of the Company's head-stall must be released and the animal can then be led from the box by the head-stall and taken to a stable or shed before removing the head-stall and putting on the bridle. In that case a report of the circumstances must be made to the Station Master and care must be taken to see that the head-stall is replaced in the box.
- 24. In attaching or detaching horse boxes from a train, special care must be taken to prevent any animals which may be loaded therein being shaken or startled, and this instruction must be particularly observed at Junctions and Stations in transferring horse boxes from one train to another and in shunting other vehicles on to the train.
- 25. Whenever loaded horse boxes have to be shunted or moved by an engine, the Engine Driver must be informed that the vehicles are loaded, and warned to be careful in starting and stopping in order to avoid any sudden jerk.

#### TRANSIT OF LIVESTOCK TRAFFIC-FEEDING AND WATERING.

Attention is directed to the instructions in regard to live stock in transit contained in General Manager's Circular No. L.S.D. 1747, September, 1927.

The following instructions must be observed :-

- 1. It is essential that cattle traffic should receive expeditious transit, and the staff are requested to co-operate in effecting this. The loading station must ascertain the train which will afford the best throughout service to destination, or junction with other Company, and when opportunity presents itself, intimate to Senders that it is to their interest to have the animals at the station in time for this train. In this connection consideration should be given as to whether it will be necessary to feed and water en route.
- 2. It is important that live stock labels show the time of loading and also when and where the watering and feeding takes place on the journey.
- 3. Guards will be held responsible for calling the attention of the Yard staff to animals conveyed by their trains which have been, or will be, in transit longer than the periods within which the various description of animals must be watered before the next station which is in a position to water, will be reached.
- 4. If the hours within which animals must be watered after commencement of journey, or after feeding and watering will, or is likely to, elapse before animals can again be conveniently fed and watered, having regard to services available, attention must be given before sending forward from intermediate junctions, or recognised feeding stations.

Delete clause 5 and substitute the following:

- 6. Guards must record on their journals particulars of all cattle conveyed. The information must be shown in the general remarks, etc., column and include:—
  - 1. Truck Nos.
  - 2. Forwarding station.
  - 3. Destination.

DISEASES OF ANIMALS ACTS.—All concerned must pay particular attention to the complete Circular issued, from time to time, by the General Manager, and also to the supplementary Circulars of the Chief Goods Manager. They must carefully check the regulations respecting movements of animals which affects their stations and, in the event of any order applying to a station not included in the list of stations shewn in the Circulars, a notification must be given to the Chief Goods Manager at once; if necessary by telegram. Immediately on receipt of a Circular from the Chief Goods Manager, announcing a revocation of a Regulation, or the issue of a new one, the General Manager's Circular must be amended accordingly.

Catt labels by due for

loading, understo attention

Delete clause 5 and substitute the following:-A telegraphic or telephonic advice must be despatched by the sending station to the originating Control, giving number of wagons, destination and route, description of animals (specially disclosing if milking cows, etc., and if attention is required in this con-

nection en route), time of loading, and service by which being despatched. The originating Control to include these particulars in all train advices, and the information to be included (G.A.27.Op.-1/51. E.78511.H.)

in any advices sent by subsequent Controls or Yards. Where attention is given in transit this information to be included in the advices. Where train advices are sent from Marshalling Yards, etc. to points other than or in addition to originating Control, full details to also be included in these advices.

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Cattle are usually fed and watered immediately prior to loading and the loading time shown on wagon labels by the forwarding stations is generally the time taken from which to calculate when animals are next due for attention.

At certain stations, however, a considerable period, elapses between feeding and watering and the loading, and two separate times are accordingly inserted on the labels. When this occurs, it must be clearly understood that the time of feeding and watering shown is the one on which calculations for subsequent attention must be based and not the loading time.

(G.A.12. 4/43. C.G.M.-W.T.64311.)

## LOADING, FEEDING, AND WATERING OF LIVESTOCK, MILKING OF DAIRY COWS, ETC.

In dealing with Live Stock, including horses, cattle, sheep, pigs and goats, care and patience must be shown, not only in loading and unloading, but also in their treatment during transit, and ir and about the yards, pens, sheds and stations, in order to avoid fright or injury, and consequent suffering on the part of the animals.

If, upon arrival at a station, an animal is found to be injured, the agent must be at once informed. Special steps are to be taken to immediately advise the owner, and everything possible done to alleviate its suffering. A veterinary surgeon should, if necessary, be called in, and, if he advises that the animal is injured beyond recovery, it must be properly slaughtered.

In the case of animals arriving during the night, the person in charge of the station is responsible for

seeing that no unnecessary suffering takes place.

When Live Stock by freight train is to be conveyed long distances, or from any cause is detained at an intermediate or receiving station, such stock must, where necessary, be supplied with water as well as food, and any expense incurred be charged forward to destination and collected from the consignee. The stock must be frequently examined, and the periods within which various descriptions of animals must be watered are as follows :-

.. 24 hours .. 24 hours if Horses, Asses and Mules .. .. Cattle "destination can be Sheep and Lambs .. 36 .. 27 ,, .. .. ..

There is no specific regulation which requires Railway Companies to feed animals after they have been in transit any particular number of hours. The Protection of Animals Act of 1911, however, makes it incumbent upon the Companies to avoid undue suffering to animals in transit, and as such suffering would be caused if the animals were kept without food for a prolonged period, the Railway Companies generally as a matter of convenience have adopted the practice of feeding animals at the same time as they are watered. This practice must be continued.

The Station Masters and Agents must clearly understand that they are held personally responsible

for seeing that the instructions are adhered to at the expense of the owners.

When milking cows are tendered for conveyance, the sender or his representative must be requested to state how long the animals will go without being milked to avoid suffering pain or discomfort, and should be asked to make arrangements to meet the requirements.

Care should be taken to see that milking cows are milked in transit if it appears they are suffering

any pain or discomfort.

Young calves travelling with the mother should not be loaded with other cattle, but put behind partitions by themselves, unless the number is sufficient to warrant a truck being used exclusively

A cow shall not be permitted by the Owner thereof, or his agent, or any person in charge thereof, to be carried by railway, if the calving of the cow during transit by railway is reasonably probable.

When cows, which are apparently near their calving time, are tendered for conveyance, the attention of the sender or his representative must be called to this regulation, and (before the cows are accepted) his assurance obtained that they are not likely to calve on the journey.

An assurance in writing on the consignment note, signed by the sender or his representative,

should be obtained where practicable.

Live Stock labels, including those for Horses, should show the time of loading, and also when and where the watering and (or) feeding takes place on the journey.

## SUPPLY OF FOOD AND WATER TO ANIMALS IN TRANSIT

At every railway station at which animals are habitually loaded, unloaded or detained during transit, the railway company or companies concerned shall make a provision of water to the satisfaction of the Ministry of Agriculture for the animals carried, or about to be or having been carried on the railway, and such provision of water shall be easily accessible to all animals so carried.

The water and food, if provided, or either of them, shall be supplied to any such animal by the

company carrying it, on the request of the consignor, or of any person in charge thereof.

An obligation is placed upon the Railway Companies under various Orders of the Ministry of Agriculture to water animals within a certain length of time after the commencement of a rail journey. The periods within which various descriptions of animals must be watered are as follows:-

Horses, Asses and Mules	 	-	24 hours 24 hours if
Cattle	 		27 , all of cotton can be
Sheep and Lambs	 		36 " destination can be
Pigs	 		27 " reached. CHIO

The consignor and person in charge of the animal shall each be guilty of an offence against the Act, and it shall lie on the person charged to prove such a request, and the time within which the animal had a supply of water.

## SUPPLY OF FOOD AND WATER TO ANIMALS IN TRANSIT-Continued.

The Company supplying water or food may make, in respect thereof, such reasonable charges as the Ministry of Agriculture by Order approve, over and above the rates for carriage, and the amount of such additional charges shall be a debt from the consignor and/or from the consignee to the Company, and can be recoverable by the Company from either of them, with costs, by proceedings, in any Court of competent jurisdiction.

The Company shall have a lien for the amount of such additional charges on the animal in respect whereof the same accrued due, and on any other animal at any time consigned by or to the same con-

signor or consignee to be carried by the Company.

When animals are fed and watered in transit, a note must be made on the labels that this has been done,

particular care to be taken that the station at which they have been attended to is shewn.

In the case of Irish stock travelling any long distance from Fishguard Harbour, it can be taken for granted that the animals have been fed and watered at that port.

A list of the Stations where the Company must provide a supply of water for animals is shewn on page 16 of the General Manager's Circular, No. L.S.D. 1747, dated September, 1927, but if a request is made at any other Station for water to be supplied to animals, the Goods Agent or Station Master should, if possible, comply with such request.

## Food and Water for Detained Animals.

An inspector, officer, or constable who detains an animal, horse, ass, or mule must supply it with requisite food and water during its detention, and the expenses incurred by him may be recovered summarily from the person in charge of the animal, horse, ass, or mule, or from its owner.

If there is no person travelling in charge of the consignment, Goods Agents and Station Masters must

refer the inspector, officer, or constable to its owner for the expenses.

#### ANIMALS (TRANSIT AND GENERAL) ORDER OF 1927.

Attention is directed to the following instructions issued by the Ministry of Agriculture:-

## Construction of Trucks, etc., Used for Carriage of Animals.

- 1. No animal shall be carried by railway in a truck or other vehicle which is not in accordance with the provisions of this Article, unless the use of such truck or other vehicle, either generally or in specific cases, or in respect of a particular type of truck or vehicle or in respect of the carriage of a particular class of animal, is authorised by an exemption granted by the Minister.
- 2. Every truck or other vehicle shall be provided at each end with spring buffers, and the floor thereof, in order to prevent slipping, shall, in the case of a truck used for the carriage of cattle, be fitted with battens or other proper foot-holds, and in any other case either be so fitted or be strewn with a proper quantity of litter or sand or other proper substance.
- 3. The battens in a truck or other railway vehicle shall be placed across the vehicle, except between the doorways, where they shall be placed lengthways.
- 4. The interior of every truck or other vehicle shall be free from any boltheads, angles or other projections likely to cause suffering to animals carried therein.
- 5. Every truck for carrying animals shall be fitted with a roof, and with falling loading doors of a pattern approved by the Minister, and all internal projections shall be rounded.
- 6. Every falling loading door and every gangway, passage-way, loading or unloading board, shall be fitted with longitudinal battens or other proper footholds.
- 7. Every truck or other vehicle shall be so constructed as to admit of ventilation and inspection at the floor level.
- 8. Every truck or other vehicle shall be so constructed as to permit of its being cleansed and disinfected in the manner prescribed by this Order.

Securing of Cattle in Railway Trucks.—All bulls, whether polled or not, shall, while being carried in a railway truck or other railway vehicle, be securely tied by the head or neck.

All horned stock carried in the same railway truck or other railway vehicle with a bull shall, unless

separated therefrom by a suitable partition, be securely tied by the head or neck.

Trucks must not be supplied unless senders bring ropes of sufficient strength when tendering bulls or horned stock, if the latter are to be loaded with a bull in the same vehicle without being separated by means of a suitable partition.

All the Railway Companies have agreed that senders are to provide the ropes, excepting only for bulls forwarded by rail from the ports. Other animals must not be loaded with bulls, if the animals are likely to suffer injury through being so conveyed.

Carriage by Railway of Unfit Animals.-No animal shall be permitted by the owner thereof, or his agent, or any person in charge thereof, to be carried by railway if, owing to infirmity, illness, injury, fatigue, or any other cause, it cannot be carried without unnecessary suffering during the intended transit by railway.

## ANIMALS (TRANSIT AND GENERAL) ORDER OF 1927-Continued.

Instances have come under notice, from time to time, where animals quite unfit to bear a railway journey

have been accepted for conveyance. Every precaution must be taken to prevent such cases arising.

Although the senders are primarily responsible for tendering to the Company animals not fit to travel, it must always be borne in mind that the Company are subject to prosecution in the event of their conveying animals contrary to the Orders issued by the Ministry.

The Station Masters and Goods Agents are reminded of their responsibility for supervising the carrying

out of the foregoing instructions.

Separation of Mixed Consignments.—Calves, sheep, goats and swine, if carried in the same railway truck or other railway vehicles with any head of cattle (other than a calf), or a horse, ass or mule, shall be separated therefrom by a suitable partition, but this provision shall not apply to the conveyance of a cow with its unweaned calf, if they are separated from other animals.

A consignment consisting of only one cow and its unweaned calf may be loaded together, or may be

separated, if sender so desires.

A consignment, consisting of, say, four cows and four unweaned calves, may be loaded (if the space permits) in the same truck, by the larger animals being put together in the main part of the vehicle, and the smaller animals being loaded together on the other side of the partition.

A consignment consisting of two cows and two unweaned calves must not be loaded together-either one cow must travel with its own calf, or the two cows must be on one side of the partition, and the two

calves on the other, according to senders' instructions.

Care must be taken to see that the correct charges are paid for the truck spaces actually utilised, unless the sums of the different head rates are cheaper.

Overcrowding .- A railway company shall not allow any railway truck or other railway vehicle used for carrying animals on the railway or any compartment thereof to be overcrowded so as to cause injury or unnecessary suffering to the animals therein.

Station Masters and Goods Agents must give effect to this Order to the best of their ability, preventing, on the one hand, such overcrowding of animals as would cause suffering and, on the other hand, avoiding

vexatious and unnecessary interference with the trade.

When it appears that interference is required, they must, in the first instance, call the attention of the owner or his agent in charge of the animals to the necessity of reducing the load; and it is hoped that, in most cases, the owner or his agent (who may be assumed to be experienced persons) will be able to show that the animals he proposes to put in the vehicles would not, by competent judges, be deemed "overcrowded so as to cause suffering."

If, however, after calling attention, the Station Master or Goods Agent is still clearly of opinion that the Order is being infringed, he must endeavour to get some one unconnected with the Company to take note of the facts, and, in default of such persons, one or more of the servants of the Company, who must be prepared to support his opinion by giving evidence of the overcrowding before a Court of Law; and he must then refuse to allow the surplus animals to be put into the vehicle, or must remove them from the vehicle if they have already been loaded, immediately reporting the circumstances of the case to the District Goods or Traffic Manager, Divisional Superintendent or Docks Manager, as the case may be.

If there is any suspicion of overcrowding the precautionary instructions contained herein must be

strictly earried out.

Shorn Sheep.—Between each 1st day of November and the next following 30th day of April (both days inclusive) the sides of every railway truck or other railway vehicle in which shorn sheep are being carried must be covered with tarpaulins or other suitable material in such a manner as to protect the sheep from the weather, without obstruction to proper ventilation. The Order does not apply to sheep last shorn more than 60 days before being carried.

## CLEANSING AND DISINFECTION OF HORSE BOXES AND SPECIALLY CONSTRUCTED CATTLE VEHICLES WORKED IN PASSENGER TRAINS.

Horse Boxes and Passenger-fitted Cattle Vans must be cleansed and disinfected in the manner shown below immediately they have been unloaded :-

(i.) By scraping and sweeping the floor and other parts with which the animal or its droppings have come in contact.

(ii.) By effectually removing therefrom, and forthwith well mixing with quicklime, the scrapings, sweepings, dung, sawdust, fodder, litter and other matter; and

(iii.) By thoroughly washing and disinfecting all the interior parts of the box with which the animal or its droppings have come in contact. A fluid disinfectant is provided specially for this

purpose, and it must be applied by means of a sponge, brush, swab or spray.

Where facilities for doing this work do not exist at the unloading station, the instructions of the Divisional Superintendent or District Traffic Manager must be obtained as to the action to be taken.

These Instructions apply in respect of Horse Boxes and Passenger-fitted Cattle Vans belonging to other Companies as well as the Great Western Railway.

#### CLEANSING AND DISINFECTION OF HORSE BOXES AND SPECIALLY CONSTRUCTED CATTLE VEHICLES WORKED IN PASSENGER TRAINS—Continued.

Attention should be given to disinfection of the head collars in horse boxes, and inside and outside of the mangers. After disinfection these parts must be thoroughly dried and if possible the interior of the box aired. It is not sufficient for the mangers to be sprayed with a disinfectant solution.

## Cleaning of Coupe Compartments and Windows.

The windows and coupé compartments of horse boxes and specially constructed cattle vehicles worked in passenger trains must be cleaned immediately the vehicles have been unloaded. At stations and depots where Carriage Cleaning staff are employed the work will be done by such staff. At other stations and depots the work will be done by the Traffic Department staff.

### CLEANSING AND DISINFECTION OF CATTLE WAGONS, MOVABLE GANGWAYS AND OTHER APPARATUS.

#### Cleansing and Disinfection of Wagons.

Wagons after being used for animals; and vans, on every occasion on which animals, horses, asses, or mules, are taken out of them, must before any other animal, or any horse, ass or mule, or any fodder, or litter, or anything intended to be used for or about animals, is placed in them, be cleansed and disinfected by :-

(1) Scraping and sweeping the floor, and other parts, with which animals, or their droppings,

have come in contact;

(2) Effectively removing therefrom and from all contact with the animals, the scrapings, dung, sawdust, litter and other matter, and forthwith well mixing with quicklime, or effectively

destroying by fire;

(3) Effectively cleansing by scraping and washing or scouring with water, and after such cleansing thoroughly coating, washing or saturating with the approved disinfectant supplied by the Stores Superintendent, Swindon, in the proportion of 1 per cent., or quarter of a pint to three gallons of water by means of a sponge, brush, swab or spray.
When cleansing, care must be taken to see that the springs and axle boxes of the vehicles are kept free

from accumulations, particular attention being paid to the cleansing and disinfecting of the upper iron

head bars of cattle wagons.

## Cleansing and Disinfection of Pens, Movable Gangways, etc.

The Ministry of Agriculture require all Cattle Loading Places, including any portion of the other premises at railway stations, on which animals may stand, to be cleansed and disinfected as "Railway Pens" in the manner stated below. It is not, however, necessary to perform the cleansing and disinfecting if the animals are not allowed to stand on, but are merely driven over, the Company's premises, as, in that case, such premises do not come under the term "Railway Pen." Care must, therefore, be taken not to allow animals to come on to the station premises from the high road or from the wagons, or other vehicles, unless there are means of immediately loading or removing them. This prohibition of the standing of animals on the Company's premises does not, of course, apply to the cattle pens, which admit of being cleansed, and disinfected, according to the Ministry's Order. The droppings of animals must be taken away, well mixed with quicklime, and effectually removed from contact with animals.

Railway Pens must be cleansed and disinfected either on each day on which they are used, and after they are used, or not later than twelve o'clock at noon of the next day, unless the next day is Sunday, and then of the Monday next following, and in any case before they are again used, by :-

(1) Scraping, and sweeping, the parts of the Pen with which animals, or their droppings,

have come in contact;

(2) Effectually removing therefrom, forthwith well mixing with quicklime, and effectually removing from contact with animals, the scrapings, sweepings, dung, sawdust, litter and other

matter;
(3) Thoroughly washing, scrubbing, or scouring the same parts with water; and (4) Applying to the same parts a solution of the approved disinfectant supplied by the Stores Superintendent, Swindon, in the proportion of 1 per cent., or quarter of a pint to three gallons of water by means of a sponge, brush, swab or spray.

It is to be understood that the above method of cleansing and disinfecting applies also to movable gangways or passage-ways, cages, hurdles, or other apparatus used for the loading, unloading or otherwise in connection with the transit of animals, which must be cleansed as soon as practicable after being used.

Theses intructions must be rigidly observed, as any failure to do so is likely to involve the Company in

heavy penalties.

The Local Authorities have been ordered to ascertain, and periodically report to the Ministry of Agriculture, whether their regulations as to the cleansing and disinfection of railway pens, trucks and other vehicles, movable gangways, or passage-ways and other apparatus, are strictly carried out.

## SUPPLEMENTARY INSTRUCTIONS IN REGARD TO THE CONVEYANCE OF LIVESTOCK BY PASSENGER TRAIN.

## (GUARDS' VAN TRAFFIC.)

The special attention of Station Masters, Inspectors, Foremen, Guards and all concerned is directed to the necessity for giving such attention to livestock during conveyance by rail or whilst at stations as will secure the birds or animals from the risk of injury or suffering. To attain this object it is essential that the following instructions should be strictly observed:—

- (1) Before a package containing livestock, dogs, pigeons, poultry, etc., is accepted, the staff must be satisfied that it is suitable in all respects, i.e. large enough to give the animal, etc., room to stand or move about and that ample ventilation is afforded.
- (2) Each package must have attached thereto a label giving the full name and address of consignee, in addition to which a Company's "Livestock" label must be affixed. Poultry, pigeons, etc., consigned to a show and intended to be returned unsold to the original sending station must have labels showing in addition the full name and address of the sender and the name of station to which the traffic is to be returned.
- (3) Dogs unaccompanied by passengers (and not packed in boxes, etc.) must not be accepted for conveyance unless secured by chain and collar, and muzzled so as to render it impossible for them to bite any person or animal—care, however, should be taken to see that the muzzles do not prevent dogs from breathing freely or drinking.
- (4) Turkeys, geese and ducks placed in the same receptacle with other poultry must not be accepted unless the turkeys, geese or ducks, as the case may be, are in a separate compartment.
- (5) When packages are tendered for long journeys it should be ascertained what attention, if any, is required en route, and when necessary a telegram sent to the point concerned. Where possible, food should be placed in the receptacle by the sender. Guards should also see that water is provided if necessary, irrespective of any arrangement made at forwarding station.
- (6) A signature to the conditions on Form 281 must be obtained for all livestock, and when senders desire the traffic forwarded at the reduced rate at the owner's risk, the Clause on the Form The livestock ticket must be handed to the guard of the train must be completed accordingly. by which the traffic is forwarded.
- (7) When senders desire to pay the additional premium (1 per cent.), the declaration on the livestock ticket must be filled up, but this should not be referred to as "insurance," as the Company's liability is not affected, except in cases of proved negligence. The arrangements under which livestock may be insured through the Railway Passengers Assurance Company are set out in General Manager's Circular No. 2912/30996 E., of May 14th, 1924.
- (8) Label 5712 must be affixed to all packages containing livestock upon which the additional premium has been paid, and they must be treated as "values" and signed for when transferred or handed over at the destination station.
- (9) When it is known that a large number of consignments of livestock is likely to be forwarded by any particular train, an advice must be sent to the Divisional Superintendent or District Traffic Manager in order that special accommodation may be provided as necessary.
- (10) Guards and other staff engaged in stowing traffic must take care that mail bags, luggage and parcels are not stowed near packages containing livestock in such a manner as to obstruct the free passage of air to the livestock.

Luggage and parcels should not be placed near animals not enclosed in receptacles.

(11) Where the conveyance of livestock has been so protracted by exceptional causes as to render it necessary to supply food or water in order to avoid unnecessary suffering, the Station Master at the first possible station must make arrangements for this to be done.

When livestock has to be kept all night at stations (destination, or otherwise) where no special accommodation is provided, the packages must be placed in a suitable position, and dogs not in boxes, etc., properly secured away from other traffic, food and water being provided.

- (12) On arrival at destination station the staff must satisfy themselves that the animals, birds, etc., are in good condition, and if it is not possible to effect delivery within a short time food and water must be provided. Where delivery is not undertaken, or consignee resides outside the cartage area, an advice must be sent by the most convenient means-telephone or telegram, if necessary.
- (13) Any cost incurred in feeding must be charged to consignee and collected on delivery, a special R/c waybill being sent with the original entry from any intermediate station where food is provided, and a "To-pay" docket affixed to the package or label attached to the animal.
- (14) For the regulations respecting the conveyance of horses and livestock in truck loads, see pages 248 to 250.

#### HOMING PIGEON TRAFFIC.

## Instructions to Stations from which Homing Pigeons are forwarded.

- (1) The Secretaries of County or Town Federations of Pigeon Flying Clubs can be informed of the arrangements outlined in the Coaching Arrangements Book, and an advice should be sent to the Divisional Superintendent or District Traffic Manager immediately any information regarding a Federation flight is available.
- (2) Senders have the option of consigning at "Owner's Risk," and where this is taken advantage of the staff should see that signature is held to the O.R. conditions.
- (3) Birds should be despatched by the train specified on the label; where no train is shown, by the first available service.
  - (4) To facilitate transit, pigeons must be forwarded by through trains whenever possible.
- (5) Baskets containing birds must never be thrown down. If this is done, the pigeons are likely to receive injury by concussion. The staff should be trained to treat the birds with kindness and consideration. Every care must be taken in loading baskets into and out of vans. The baskets should be placed flat on platform trolleys and not overhanging the raised sides, which causes damage. No heavy packages should be placed on top of the baskets.
- (6) When empty baskets are returned to forwarding station, latter should place in safe custody, also examine the labels to see that no baskets belonging to any other station are kept on hand. Empty baskets received without address labels must be immediately reported to L.P.O. and Divisional Superintendent or District Traffic Manager. Description and size, also date and train received to be given in all cases. Loss of or delay to empty baskets not only gives rise to claims, but involves the Company in loss of revenue, and seriously inconveniences the owners, as they are not able to utilise them for training purposes.
- (7) Any instances of overcrowding birds in baskets or failure to collect empty baskets promptly must be reported.

## Instructions to Stations at which Homing Pigeons are liberated.

- (8) Where practicable, Station Masters should select one or more members of their staff to specially attend to the liberation of birds. If any of the staff are interested in "Pigeon Flying," obviously such men are most suited to this purpose. A site adjacent to the station, clear of buildings, telegraph wires, moving or standing vehicles should be selected, and all birds to be liberated should be released from this spot. Failure to act in accordance with these instructions may result in valuable birds being maimed or killed.
- (9) Birds going in opposite directions must not be liberated within several minutes of each other or until the released birds have got clear away, otherwise large numbers of birds are diverted from their course and in the case of young untrained birds, many are lost by being carried off by birds flying a different course.
- (10) Guards and others concerned should give special attention to this class of traffic to avoid overcarrying. In the event, however, of any birds being overcarried a telephone or telegraphic advice must immediately be sent to the correct destination and the consignment returned by the first available service; a report must at once be submitted to the Divisional Superintendent or District Traffic Manager. This particularly applies in the case of young birds during the months of July, August and September.
- (11) If weather is unfavourable for flying, birds should not be liberated. If necessary, they should be held until the following morning and sending station advised; it should also be seen that the birds are supplied with water.
- (12) Every endeavour should be made to water all birds upon arrival and before liberation, especially those received from long distances, and particularly in hot weather. Baskets sent long distances are usually equipped with troughs for this purpose. It is not necessary to feed the birds unless they have suffered exceptional delay, even then one, or at the most two, handfuls of corn per basket will suffice. If it is found expedient to hold birds overnight, it should be seen that they are stored out of reach of cats or rats.
- (13) Labels should always be examined carefully before birds are released to see that sender's instructions are complied with, and the name of the station at which the birds are liberated must be stamped on the address label, and the time and date of liberation inserted and initialled by the person effecting the liberation. It should also be observed from what station forwarded, and if any delay has occurred en route. Pigeons cannot "home" in the dark, therefore they should not be liberated at such a time that it would be impossible for them to reach their lofts the same day.
- (14) After liberation, the baskets should be emptied of chaff; this, if not done, causes untidy platforms and entails extra work for the staff. Empty baskets must be returned to home stations as promptly as possible.

## CONVEYANCE OF LIVE POULTRY ORDER OF 1919.

The following instructions issued by the Ministry of Agriculture must be carefully observed:

PROTECTION OF POULTRY DURING CONVEYANCE BY WATER.

1.—(1) The conveyance of poultry (domestic fowls, turkeys, geese, ducks, guinea-fowls, and pigeons) on a vessel to or from a port in Great Britain shall comply with the following conditions:—

(i.) The poultry shall while on board or on premises of a shipping company in connection with their conveyance by water be protected as far as practicable from exposure to bad weather

or sea water, or excessive heat:

(ii.) The poultry shall be carried only in such parts of the vessel as are sufficiently ventilated, and receptacles containing the poultry shall be disposed so as to allow sufficient ventilation to reach each receptacle, and so as to afford access to each receptacle for inspection of the poultry as occasion may require:

(iii.) Receptacles containing poultry shall as far as practicable be secured so as not to be

liable to be shifted by the motion of the vessel:

(iv.) A receptacle containing poultry may be placed on another such receptacle only if sufficient ventilation is left for each receptacle:

(v.) The poultry, whether carried in receptacles or otherwise, shall not be so overcrowded

as to cause injury or unnecessary suffering to the poultry.

(2) If any head of poultry while being conveyed as aforesaid on a vessel or while on premises of a shipping company in connection with their conveyance by water is found to be so injured that, in the opinion of the master of the vessel or a superior officer of the shipping company, the destruction of the bird is desirable to prevent unnecessary suffering, the master or officer, as the case may be, may cause it to be killed.

(3) If any poultry are conveyed in contravention of this Article the shipping company and the master of the vessel shall, each according to and in respect of their or his own acts or omissions, be

deemed guilty of an offence against the Act of 1894.

PROTECTION OF POULTRY DURING CONVEYANCE BY RAILWAY.

2.—(1) The conveyance of poultry by railway in Great Britain shall comply with the following conditions:—

(i.) The poultry shall, while conveyed in a railway truck or other vehicle, or while on premises of a railway company in connection with such conveyance, be protected as far as practicable from exposure to bad weather or excessive heat:

(ii.) Every railway truck or other vehicle in which poultry are conveyed shall be sufficiently

ventilated:

(iii.) Receptacles containing poultry shall be so disposed as to allow sufficient ventilation to reach each receptacle, and so as to afford access to each receptacle for inspection of the poultry as occasion may require:

(iv.) Receptacles containing poultry shall, as far as practicable, be secured so as not to be

liable to be shifted during transit :

(v.) A receptacle containing poultry may be placed on another such receptacle only if sufficient ventilation is left for each receptacle:

(vi.) The poultry, whether carried in receptacles or otherwise, shall not be so overcrowded

as to cause injury or unnecessary suffering to the poultry.

(2) If any head of poultry while being conveyed by railway in Great Britain or while on the premises of the railway company in connection with such conveyance is found to be so injured that, in the opinion of the station master or other superior officer of the railway company, the destruction of the bird is desirable to prevent unnecessary suffering, the officer may cause it to be killed.

(3) If any poultry are conveyed in contravention of this Article the railway company so conveying the poultry or causing or permitting them to be so conveyed shall, according to and in respect of the

acts or omissions of the company, be deemed guilty of an offence against the Act of 1894.

(4) Poultry traffic must not be accepted for conveyance by Freight train when a service is involved of more than 30 hours.

PROTECTION OF POULTRY DURING CONVEYANCE BY ROAD OR EXPOSURE FOR SALE.

3.—Foultry, while being conveyed by road or exposed for sale or while in a market or other place, where poultry are habitually exposed for sale, shall in Great Britain

(i.) be protected as far as practicable from exposure to bad weather or excessive heat; and (ii.) not be confined in a receptacle which is not of a height and size reasonably sufficient for the poultry and the number contained therein, or which is so constructed as to be likely to cause injury or unnecessary suffering to poultry confined therein or which does not allow sufficient ventilation, but this shall not be deemed to prohibit the conveyance of poultry in "swills" or shallow baskets having a net over the top with mesh sufficiently small to prevent protrusion of the heads of the poultry; and

(iii.) not unnecessarily be tied by the legs or be allowed to remain so tied for a longer period

than is necessary; or unnecessarily be carried head downwards;

and in the event of any failure to conform to these provisions, the owner and the person in charge of the poultry in relation to which such failure occurs shall, each according to and in respect of his own acts or omissions, be deemed guilty of an offence against the Act of 1894.

## CONVEYANCE OF LIVE POULTRY ORDER OF 1919-Continued.

#### CONVEYANCE OF UNFIT POULTRY.

4.—Poultry shall not be permitted, by the owner or person in charge thereof, to be conveyed on a vessel from a port in Great Britain to any other port or by railway or by road in Great Britain, if from injury or any other cause such conveyance of the poultry may reasonably cause unnecessary suffering to them.

USE OF UNSUITABLE RECEPTACLES.

5.—(1) Any person who in Great Britain, with a view to the conveyance of poultry by railway water or road, shall place poultry in a receptacle, or portion of a receptacle, which is not of a height and size reasonably sufficient for the poultry and the number placed therein, or which is so constructed as to be likely to cause injury or unnecessary suffering to poultry carried therein, or which does not allow sufficient ventilation, or, in the case of conveyance by vessel or by railway, or by a public carrier, which is not so constructed as to protect the poultry from injury by protrusion of the head, legs or wings through the top or bottom or sides of the receptacle, shall be deemed guilty of an offence against the Act of 1894, and any person who, in Great Britain, being the owner or person in charge of poultry, shall put or cause to be put on board any vessel or consign by railway or by a public carrier the poultry while contained in any such receptacle, or portion of such receptacle, shall be deemed guilty of an offence against the Act of 1894.

(2) The provisions of this Article relating to the height of receptacles shall not be deemed to prohibit the use for the conveyance of poultry of "swills" or shallow baskets having net over the top

with mesh sufficiently small to prevent protrusion of the heads of the poultry.

(3) A receptacle shall be deemed to be so constructed as to be likely to cause injury or unnecessary suffering, for the purpose of this Article, if it measures more than twenty-four square feet, or if any compartment measures more than ten square feet, or if the receptacle is not sufficiently strong and rigid for the purpose for which it is intended to be used.

#### MIXED CONSIGNMENTS.

6.—Turkeys, geese and ducks, which are placed in the same receptacle with other poultry, shall not be conveyed by a vessel from a port in Great Britain or by railway in Great Britain, unless the turkeys or geese or ducks, as the case may be, are in a separate compartment, and if any poultry are conveyed in contravention of this Article, the owner and consignor of the poultry, and the master of the vessel or the railway company, as the case may be, shall, each according to and in respect of his or their own acts or omissions, be deemed guilty of an offence against the Act of 1894.

#### HANDLING OF RECEPTACLES CONTAINING POULTRY.

7.—Receptacles containing poultry shall, during conveyance on a vessel to or from a port in Great Britain, or in a railway truck or other vehicle in Great Britain, or while being loaded or unloaded thereon or therefrom, be lifted, carried and deposited, with care and in such manner as to avoid injury or unnecessary suffering being caused to the poultry, and any person lifting, carrying or depositing a receptacle who fails to comply with this Article shall be deemed guilty of an offence against the Act of 1894.

## INFORMATION TO BE FURNISHED TO INSPECTORS.

8.—A carrier of poultry by railway, water or road in Great Britain shall, if so required by an Inspector of the Ministry, furnish him with the names and addresses of the consignors and consignees of the poultry so far as they are known to the carrier.

## CONFINING OF POULTRY IN RECEPTACLES FOR UNNECESSARY TIME.

9.—Any person who in Great Britain, in connection with the conveyance of poultry belonging to him or being under his charge, shall cause or permit the same to be confined in a receptacle for a time longer than is reasonably necessary, shall be deemed guilty of an offence against the Act of 1894.

## CLEANSING OF RECEPTACLES.

10.—A receptacle which has been used for the conveyance of poultry by land or water to any place in Great Britain shall be thoroughly cleansed by the owner or person in charge thereof, before being again so used, and if sent by railway or vessel before being so sent.

## FEEDING, ETC., OF POULTRY IN EXCEPTIONAL CIRCUMSTANCES.

11 —Where the conveyance of poultry by a vessel or by railway has been so protracted by except tional causes as to render it necessary to supply the poultry with food or water in order to protect the poultry from unnecessary suffering, the master of the vessel or the railway company, as the case may be, shall cause the poultry to be supplied with sufficient food and water.

#### COMMENCEMENT.

12.--This Order came into operation on the sixteenth day of September, nineteen hundred and nineteen.

## SECTION IV.

INSTRUCTIONS CONCERNING STATION WORK.

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## INSTRUCTIONS CONCERNING STATION WORK.

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## INSTRUCTIONS CONCERNING STATION WORK.

## PROMPTITUDE IN DEALING WITH ACCIDENTS AND OTHER EMERGENCIES.

- 1.-In the unfortunate event of an accident to a train conveying passengers it is of the first importance that immediate steps should be taken by Station Masters, Inspectors, Foremen, Guards and other servants of the Company who may be concerned to relieve any who may have sustained injury, as well as to adopt all precautionary measures which the circumstances may render desirable, in the direction of limiting the effects of the occurrence. The following important points are therefore set out as being necessary to be borne in mind :-
  - (a) Examine train to see if there is any sign of fire from live engine coal or broken gas cylinders or connections, and take what steps are possible to extinguish it.
  - (b) Ascertain where the greatest damage has occurred, and take instant steps to release any passengers who may be entangled in the wreckage.
  - (c) Ensure prompt telegraph or telephone communication with the nearest likely places for Doctors, Nurses, Ambulances, Refreshments, etc., according to circumstances.
    - At stations where Ambulance Sections are formed, in the event of a train accident in the vicinity, the Station Master must arrange immediately to despatch one or more of the organised Ambulance Sections, with their equipment, to the scene of the accident.
  - (d) Enlist the services of any uninjured and willing passengers or other persons to convey messages, and to assist generally, under the direction of the Railway Staff.
  - (e) Use cushions taken from the coaches in such a manner as to form comfortable couches for any injured persons, until such persons can be removed.
  - (f) Advise the nearest Station Master, and other officials, and also the Permanent Way men, at the earliest possible moment.
- 2. The attention of Station Masters is specially directed to the importance of being at all times prepared to deal with the various emergencies incidental to railway work, and especially those which are most likely to arise at their own Stations, or on the section of Line for the working of which they are directly responsible. Station Masters are reminded that not only is it necessary that they should themselves give such attention to the whole subject as will give them confidence in dealing with emergencies when they arise, but they should also take steps to satisfy themselves that the member of their Staff, who is in charge of the Station during their absence, has an intelligent appreciation of the character of the duties which in certain eventualities would devolve upon him, and particularly as to the course of action which should properly be taken in the following circumstances:-
  - (a) Working the Traffic of a Double Line over a Single Line of rails. See rules 189 to 208 in the Rule Book.
  - (b) Relieving a broken-down Train or Engine from the front by Engine or Break-down Train running on the wrong road. See Rule 183.
  - (c) Drawing or pushing an Engine, Train, or portion of Train on the wrong Line back to the next Station or Signal Box in the rear. See Rule 184.
  - (d) Working a Single Line by Pilotman owing to the failure of the Electric Token apparatus. Clause 25 of the Electric Token Block Regulations.
  - (e) Working a Single Line by Pilotman on one side of an obstruction and by Train Staff on the other.

See Clause 14c of the Electric Train Token Block Regulations.

- 20 Train Staff and Ticket Regulations. ,,
- 13 One Engine in Steam Regulations
- (f) Transferring passengers from train to train at an obstruction. No specific instructions are, or can be, laid down for guidance in cases of this kind, beyond the general principles for work. ing by Pilotman on the wrong line in each direction back to the nearest cross-over road after transferring passengers from train to train, provided for in Rules 189 to 208 of the Rule Book, and in the general Instructions for working Single Lines to be found in this Book.
- Each Station Master, with his principal assistant (should he not have done so already), is asked to thoroughly work out on the principles laid down in the Regulations referred to, the deta ls of the action he would be required to take were any of these emergencies to occur at, or near his station. He should thoroughly rehearse in his own mind the exact course to be followed in the various forms

Reference to the following to be made on page 263:-

## INSTRUCTIONS TO BE OBSERVED WHEN ASSISTANCE OF FIRE BRIGADES IS REQUIRED IN CONNECTION WITH TRAIN ACCIDENTS.

- I. In any serious train accident where passengers or railway staff may be trapped in the wreckage and in any accident where fire has broken out or would appear to be imminent, the nearest Fire Brigade must be immediately advised by the Operating Department Official on duty in charge of the station or section of the line concerned.
- 2. The fullest available information must be given to the Fire Brigade including the precise site, the best means of access to, and the extent of the accident.
- 3. The telephone number of the nearest Fire Brigade must be recorded at all stations (on Fire Notice "B") and in all Signal Boxes.
- 4. The Officer in charge of the Fire Brigade must be requested to report to the Railway Official in charge of the incident and must be advised where to find him.
- 5. When the railway breakdown vans arrive, the Official in charge of the vans must contact the Officer in charge of the Fire Brigade and co-ordinate the efforts of the rescue squads.
- 6. It will be the responsibility of the Operating Department Official in charge to ensure the safety of men working at the site from the movement of traffic on adjoining lines.
- 7. Divisional Operating Officers should ensure that all stations are advised of these instructions and that the Fire Brigades in their district are acquainted with the instructions issued to the railway staff.
- 8. The Market Fighter will satisfy himself that the Officials in charge of breakdown vans are advised of these instructions and will be prepared to direct the efforts of railway and non-railway personnel at the site of the accident.

  (G.A.23—7/49. L.K.1/8669/33.)

## PROTECTION OF GOODS IN TRANSIT-MISHAPS.

In the event of mishap involving derailment of loaded wagons where their contents are liable to misappropriation, immediate advice must be given to the Divisional Superintendent or District Traffic Manager, who will arrange with the Divisional Police Officer for police protection of the goods to be provided.

(G.A. 18. 11/47. L.K.2/15868/3.)

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Reference to the following to appear on page 263:-

#### EMERGENCY ARRANGEMENTS.

In an emergency, necessitating speedy transit of materials, etc., on behalf of the Engineering Department, arrangements for the transport of materials and plant as necessary, either (a) by special train; (b) by prompt despatch on ordinary freight train; or (c) by passenger train, according to circumstances, will be made with the Superintendent of the Line by the Chief Engineer or Stores Superintendent, who will supply all requisite information such as source, wagon numbers, time available, etc. In the case of materials and plant from local sources, Divisional Engineers should keep in touch with Divisional Superintendents (or District Traffic Managers) or the Control Office, who will require similar information of the traffic expected to pass. Local arrangements for delivery to site of materials coming from a distance should also be made between the Divisional Engineers and Divisional Superintendents (or District Traffic Managers) or the Control Office.

The special attention of all concerned is directed to the importance of ensuring prompt transit of materials and plant, as ordered.

G.A. 1. 3/37. E/74645/H.)



## PROMPTITUDE IN DEALING WITH ACCIDENTS, ETC.-Continued.

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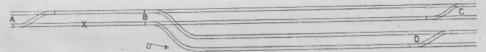
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sional ontrol to site perinterials in which an accident or casualty may be likely to present itself, so that if the need should arise, he may be quite familiar with the details of the exceptional arrangements to be made, and may act with that promptitude and adherence to established Regulations which it is so necessary should be observed; bearing in mind, however, that it is preferable to set about the adoption of the special working with such deliberation as will ensure the best arrangements being made, rather than to err from want of sufficient presence of mind or calm judgment. The necessary materials in the shape of Pilot Forms for single line working, the forms shown in Rules 183 and 184, and the Pilotman's badge, should be kept together in a conspicuous place in the Signal Box, and it is the Station Master's duty to satisfy himself from time to time that they are ready for immediate use. Attention is also called to Rules 183, 184, 195 and 196 of the Rule Book; and on page 265 of this Book with respect to the fastening of loose Runaway Catch Points before commencing to work Single Line.

4. The ramps provided at some Stations, and shown in the Appendices to the Working Time Books, for the purpose of re-railing wagons or other vehicles which may get off the Line, should also be kept in a convenient position well known to the staff at the Station, preferably near to where most of the shunting operations take place.

5. The proper method of making the Pilot arrangements, in the circumstances shown in the diagram below, there being no cross-over road at the Junction, or the cross-over road at the Junction being blocked by the accident, is as follows:—



The Line is supposed to be blocked at the point X, and in such a case two Pilotmen should be appointed—one to work between A and C, and the other between B and D, the latter to be instructed in writing on the Single Line Form not to take a train intended for A further than the home signal at the junction B, from which point it must be taken onward to A by the Pilotman working between A and C.

Where there is a frequent service of trains, the Pilotman working between A and C may find it of advantage, when taking a train from A to C, to alight and send the train on by verbal order if on arriving at B he finds a train either there or approaching that point from D, having to go to A. If when bringing a train from C he should find at or approaching B a train from D for A, he must determine according to the circumstances of the case which of the two trains he shall send on first on a verbal order, and afterwards follow himself.

In some instances it may be beneficial to appoint three Pilotmen—one between A and B, another between B and C, and a third between B and D. In such cases the Pilotman may take a train to the home signal at the junction B, where it must be handed over to the Pilotman of the section in advance.

6. Should the mishap involve the derailment of an Engine or Vehicles, and any of the running lines be thereby fouled, the breakdown gang, if required, must be wired for at once. When doing so it must be stated which line is blocked, and such other information given as may be useful. Single line Pilot arrangements should then be put into force, and if the breakdown gang has not arrived, an endeavour should, if practicable, be made by the Traffic Staff to clear the line by any means at their disposal. In no case should the running roads be made use of for traffic purposes after a vehicle has been off the line until the Permanent Way Inspector or Ganger or other competent member of the Engineer's Staff has certified that it is fit for use, nor should a vehicle which has been off the rails be allowed to run in a train until it has been examined and certified as fit to do so by a Carriage and Wagon Inspector or Examiner. The locking should also be carefully examined if it has been interfered with in any way through the accident.

7. Each Station Master must make himself acquainted with the facilities for working the traffic of a double line over a single line, not only at his own station, but at the stations on each side of him, and he must keep exhibited in a conspicuous place at the station, accessible to all the Traffic Staff, a list showing the depot for the nearest breakdown gang, and the names and addresses of the linemen for the Signal and Engineering Departments. Special attention is also directed to the instructions contained in Rule 177 of the Rule Book as to the Officers to whom information is to be telegraphed.

8. Should an accident happen of a character to cause a total stoppage of traffic by the usual routes for any considerable time, prompt arrangements should be made for diverting the trains by any other route that may be practicable, care being taken in such cases that all needful precautions are taken, and clear advices sent to all persons concerned. In all cases the best arrangements practicable must be made for the comfort and general convenience of passengers who are detained, and the best information obtainable afforded in order to relieve the natural anxieties of the public who may be awaiting the arrival of their friends.

In the case of a mishap to a freight train involving damage to Goods Rolling Stock or contents, or delay, an advice must be sent promptly to the District Goods Manager, and Local Goods Agents, so that steps may be taken to deal with any urgent or perishable traffic.

### CROSSING OF RAILWAY BY OVERHEAD ELECTRIC POWER LINES.

In connection with the Electricity Grid System, arrangements are being made by the Central Electricity Board to fix at towers on each side of the railway an additional label giving the telephone number of the Board's Control Room under whose jurisdiction each particular line comes, and particulars are also shown in the Signal Box and/or Station on each side of the power cables crossing the lines. Where the Signal Box on either side is switched out, the particulars are also recorded in the nearest Signal Box open.

The labels will be lettered in white on red background, and the following is a specimen of the

wording :-

CONTROLLED
FROM TELEPHONE NO.
BRISTOL 35055
BRISTOL.

The number and town applicable will be shewn.

For installations of Electricity Power lines, other than those of the Central Electricity Board, labels are not in all cases fixed on the pylons, but particulars of the Electricity Power Company to be advised are provided at the Signal Box and/or Station on each side of the power cables crossing the

lines.

In the event of wires breaking or sagging and fouling any of the lines, sidings, or other parts of the Company's property, the person becoming aware of the mishap must immediately advise the nearest Signalman on duty, or Station Master, stating if possible the name of the Company or Authority supplying the power. The Station Master or Signalman, who will be furnished with the address of the Central Electricity Board's Control Room, or Power Company's responsible officer, must communicate at once with the owning Company's Engineer on duty, giving particulars with the appropriate identification letters and numbers and obtain instructions. The obstruction must be protected as soon as practicable in accordance with Rule 217.

THE STAFF ARE SPECIALLY REMINDED THAT NO ATTEMPT MUST BE MADE TO INTERFERE WITH OR TOUCH A BROKEN WIRE OR ANY RAIL OR OTHER METAL STRUCTURE IN CONTACT WITH THE BROKEN WIRE UNTIL NOTIFICATION HAS BEEN RECEIVED FROM THE ELECTRICITY COMPANY'S ENGINEER THAT THE CURRENT HAS BEEN CUT OFF AT THE POWER HOUSE, AND THE WIRE IS SAFE TO HANDLE.

In addition to the steps mentioned above, an advice must immediately be sent to all concerned in accordance with Rule 177, and if the section of line on to which the electric power line falls is track circuited or the telegraph wires alongside the line are fouled an advice must be despatched promptly to the Telegraph Lineman.

#### REPORTING OF ACCIDENTS.

Injury to employees.

Employees sustaining any injury in the course of their work must make a written report of the circumstances to their superior officers.

Witnesses.

Any employee concerned in, or who may have witnessed an accident, must make a written report giving full particulars of his knowledge of the occurrence and hand

Trains on other

it to his superior officer.

If an accident happens to a train belonging to the Company whilst running upon the line of another Company, Guards must report the occurrence to their Superintendents by telegraph if necessary.

Accidents to passengers, etc.

In the case of accidents to passengers and other members of the public whilst on station premises, full particulars of the ticket held (if any) must be obtained.

Cause of accident,

The cause of the accident must, if possible, be obtained from the injured person, and dealt with in the statement of the members of the Staff who have knowledge of the case.

Wagon Brakes.

If an accident occurs to an employee whilst applying or releasing a wagon brake, the type of brake and owner of the vehicle must be given in the statement. Any defect in the brake must be described.

Examination of site of accident.

If a passenger sustains an injury by falling on steps or platform, or when joining or leaving trains or approach roads, the place where the mishap occurred (including the footboard of the coach if the injured person was boarding or alighting from a train) must be noted at the time and an examination made, so that the cause of the accident may be ascertained definitely. Similar action must be taken by the Guards and Travelling Ticket Collectors if a passenger sustains injury due to any alleged defect in carriages, etc.

Independent witnesses.

In all cases of accident causing injury to members of the public or to Company's employees, or damage to property either on the Company's premises, or in public thoroughfares or on private premises in connection with the Company's road vehicles, it is important that the names and addresses of any independent witnesses to the occurrence should be taken.

## SECURING SELF-ACTING LOOSE RUNAWAY CATCH-POINTS.

1. Referring to Rules 195 and 196 in the Rule Book, when it becomes necessary to work the traffic of a double line over one pair of rails on which there is a self-acting loose runaway catch-point, one of the two following appliances must be used to fasten the loose point against the stock rail so as to make the line continuous.

1st.—A bolt passed through the switch tongue and through the stock rail to which is attached a padlock.

2nd.—A clip passed under the stock rail and the switch tongue so as to embrace them and bring them close together by means of a screw.

These appliances, which will be kept in the Signal Boxes, are designed for the purpose of firmly securing the switch tongue so as to make it safe for trains passing along the running line in the facing direction.

- 2. To enable the switch tongue to be closed against the stock rail, a loose lever for insertion in the frame opposite the catch-point is kept in the signal box.
- 3. Before single line working is commenced, a suitable man, provided with hand signals and detonators, must be placed at loose runaway catch-points not worked from signal boxes, with special orders to secure them for the safe passage of trains in both directions, and to keep them in that position, and he must remain at his post until the order for single line working has been withdrawn, and the double line working resumed. Before taking the first train over the catch-point, the Pilotman must assure himself that the points are properly bolted, or clipped and padlocked, and when he has done so, he must take the key of the padlock from the man at the points must remain at them until the single line working has ceased. The man in charge of the points must remain at them until the pilotman returns the key, and when the single line working has ceased, he must take the loose lever and the bolt and padlock, or the clip and padlock, as the case may be, to the signal box to which they belong, where they must be carefully kept under lock and key until again required for single line working. Care should also be taken to ensure the appliances being always available for instant use.
- 4. Boards denoting the position of catch points are provided fixed on the skew so as to face Drivers and worded "Catch Points."

  "The yards."

## ACCIDENTS TO COMPANY'S SERVANTS.

Accidents occur from time to time which are clearly caused by disregard to those Rules and Regulations which have been framed in the interests of safety, and the Staff generally must use their influence and authority in order that unnecessary risks may be avoided.

Accidents also occur to employees as a result of their using unauthorised, and in many cases, dangerous routes when going to and from work, and also in irregularly crossing the line when passing from one platform to another which may result in serious and permanent incapacity to the individual concerned. Employees are, therefore, warned against the use of an unauthorised route when going to or from work or whilst on duty.

Attention is particularly directed to-

- Rule 12. As to coupling and uncoupling of Vehicles, and enjoining the use of coupling poles wherever practicable.
- Rule 11. Warning servants of the Company not to expose themselves to danger.
- Rules 14 and 15. As to trespassing and walking on the Railway.
- Rule 92 (a). As to Fog-signalman standing in a position of safety when giving hand signals to Drivers.
- Rules 110, 111 and 112. As to the precautions to be taken before vehicles are moved or shunted.
- Rules 234 to 236 inclusive, which deal with the precautions to be observed by Lengthmen and others engaged upon the permanent way.
- Rules 25, 126, 163 and 185, also specially relate to matters of safety.

The use of shunting poles for any other purpose than that of coupling or uncoupling vehicles is strictly forbidden. Shunters and others are specially warned against the dangerous practice of riding on shunting poles or brake sticks.

#### ACCIDENTS TO COMPANY'S SERVANTS-Continued.

Treatment of minor injuries.

Station Masters, Goods Agents, and others concerned are required to have the following notice exhibited in a prominent place for the information of the staff :-

#### NOTICE TO THE STAFF.

## MINOR INJURIES.

Personal injuries are sometimes sustained by the Staff, particularly those engaged in handling traffic, which, whilst appearing to be slight and not needing attention at the time, subsequently develop into blood poisoning, owing to the wound becoming dirty.

Much suffering might be avoided if members of the Staff who meet with slight injuries such as cuts or abrasions sought treatment from a qualified ambulance worker without delay.

Persons qualified to render "First Aid":-



Supplies of the notice, if required, can be obtained on application to the Divisional Superintendent, District Goods Manager or District Traffic Manager.

#### PREVENTION OF ACCIDENTS.

Trains must not be shunted about station yards with the doors of wagons left open, either hanging down or swinging about.

The attention of all concerned is directed to Clause (b) of Rules 112 and 115a, which states that vehicles must not be moved unless the doors are properly closed and fastened or in such a position that they will not come into contact with any obstruction when the wagons are moved.

Level Crossings

At stations where passengers have to cross over the rails from one platform to between platforms another, the Staff must exercise the utmost possible supervision to prevent the tisk of accident.

> At all stations where foot-bridges are provided special steps should be taken to prevent passengers using the level crossings.

Movement of Trolleys.

Porters and others wheeling barrows and trolleys or moving articles of luggage must be very careful to avoid injuring Passengers on platforms and must in all cases give timely and efficient warning of their approach.

Closing Carriage Doors.

To avoid injuries to passengers by reason of their fingers being caught in the process of closing carriage doors, Guards, Porters and others whose duties require them to close carriage doors should exercise every reasonable care to avoid inflicting personal injury.

Securing of Platform Seats

Platform seats in exposed places must be secured to prevent them from being blown off the platform.

Insecure Roof A

To avoid the possibility of roof label boards falling from express trains, the boards must be properly placed in the slots provided for their reception before the coaches start on their journey, and the attention of Station Masters, Inspectors and others concerned is specially directed to the importance of seeing that this is done before despatching the trains.

Guards' Brake Van Doors.

Guards working Passenger Trains must not allow their van doors to be open in such a way as to incur danger to persons on the platform while their trains are running into and out of stations.

#### PLATFORMS AND STEPS IN FROSTY WEATHER.

In frosty weather the Station Master, or person in charge, must, the first thing in the morning, or at other times, see that either ashes, sand or fine gravel are sprinkled on all parts of platforms, stairs, foot-crossings between platforms, or other necessary places used by passengers.

This must also be done with regard to the steps of carriages.

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## PREVENTION OF ACCIDENTS-Page 266.

The following to be inserted as paragraph 5:-

Whitening of Platform

load gauges.

The edge of platforms must be whitened for a width of approximately 5 inches. The edge of platforms must be writtened for a width of approximate.

The white lines must be renewed not less than once a month in Summer and weekly in Winter.

(G.A.23—7/49. L.K.1/9678/Gen.) The edges of

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PREVENTION OF ACCIDENTS.-Page 266.

The following to be added to these instructions :-

Maximum

Load gauges having adjustable ends must be left in the normal or upright position when not in use. (G.A. 1. 3/37. LK.1/3948/14.)

## SECURING AND HANDLING OF PLATFORM TROLLIES, BARROWS, &c.

To prevent mishaps through platform trollies or barrows falling on to or fouling running lines, the special attention of all members of the staff concerned is directed to the following instructions:-

Securing Trollies, Barrows, &c., on Platforms.—Luggage trollies, barrows, &c., when left standing, whether loaded or empty, must be properly secured by brake or chain, and left in a safe position. They should be placed well away from the edges of platforms, and, when not in use, near fences or the walls of buildings, or, in the case of an island platform, against a column supporting the roof or by a lamp post. They should always be left with their sides parallel to the line, so that in the event of any movement taking place, it would not be towards the running line. The handles of trollies and barrows left unattended must, when practicable, be left in the upright position in order to avoid risk of injury by persons falling over them.

Trollies fitted with automatic brakes must, before being left, have the handles raised so as to apply the brakes, and must not be loaded in such a way as to prevent the handle being placed in this

These instructions must be observed in the case of Post Office and other privately owned trollies or barrows used on station platforms, and the men concerned must be instructed definitely accordingly.

The unauthorised use of platform trollies must be prevented, and the utmost vigilance should be exercised by the staff generally in preventing any unauthorised persons, especially school children, newspaper, refreshment and telegraph boys or girls from interfering with or wheeling them.

MOVEMENT OF TROLLIES.—Before attempting to cross a line or lines, the man in charge of a trolley or barrow must stop and look both ways to assure himself that no train or engine is approaching. Care must be taken to load the trolley or barrow so that nothing is likely to fall off, and that the load is not too heavy to be taken across the line in safety.

Platform trollies and barrows should not be wheeled near the edge of a platform at any time, and care should be exercised, when wheeling trollies or barrows along platforms while trains are passing

through the station, to keep them as far from the edge of the platform as possible.

Unless a trolley or barrow can be kept at a distance of 4 ft. or more away from the edge of the platform, such trolley or barrow must be stopped while a train is passing within that distance.

In no circumstances must persons outside the Company's Service, except duly authorised Post Office employees, be allowed at any time to take trolleys or barrows over running lines, unless under the supervision of a Company's servant.

## OPENING GOODS TRUCK DOORS.

When a freight train in which a station truck is formed is standing on a running road, the Signalman must satisfy himself that the station truck doors have been closed and secured before accepting a train which will have to pass through without stopping on the opposite line to that on which the freight train is standing.

Guards and others must not open any doors on the off side of the station truck without first obtaining the sanction of the Signalman, and the Signalman must not give his sanction if he has given Line Clear for a non-stopping train on the opposite Line, and has lowered his Signals for it.

## ROAD VEHICLES IN STATION YARDS.

Road vehicles, when not in process of loading or unloading, must be so placed as to ensure a clear-

ance of not less than six feet from the nearest line or siding.

The attention of the supervisory staff must be directed immediately to any disregard of this instruction and steps taken to remove the vehicles to a safe position, or, if there is an unsurmountable difficulty in doing so, to warn members of the Staff concerned of the danger.

#### ACCIDENTS TO CHILDREN.

Company's servants are warned that young children must not be allowed to walk on or cross the permanent way for the purpose of bringing meals to their father or any other persons.

## HOW TO STOP BLEEDING, WITH OR WITHOUT TOURNIQUETS.

1. When a leg or arm is severely wounded, there may be no bleeding; in this case raise the limb on cushions above the level of the body, and carefully watch the wounded part, so that the first bleeding may be seen.

2. Should there be much bleeding raise the limb as high as you can above the body. To control bleeding without a tourniquet compress the main artery with the hands at the pressure point next above the wound. To control bleeding by means of a tourniquet (see Clause 4), apply the tourniquet on the pressure point next above the wound. For pressure point, see Chart on lid of Ambulance cabinet, or pages 98 to 112 of First Aid Manual, or chart attached to inside of front cover thereof.

If you cannot find the pressure point, put a tourniquet (or strap) tightly round the limb above the wound. A tourniquet (or strap) should not be applied to the bare limb, but over the clothing.

### HOW TO STOP BLEEDING, ETC .- Continued,

Then dress the wound with clean dressings and bandage with firm pressure. Having done this, slack off the strap or tourniquet, leave it loose on the limb during transport, ready to tighten again in case bleeding re-starts in spite of the dressing.

A tourniquet, to be effective, must for the time cut off all blood supply to the limb. Therefore

it must not be kept tight one second longer than is vitally necessary

Fingers, handkerchiefs, caps, or any foreign bodies not surgically clean, should not on any account be introduced into a wound.

3. If the limb be wounded so near the trunk that you cannot put on a tourniquet, raise the limb as high as you can above the body and control bleeding by digital pressure—see Clause 2.

4. If the injured man has to be carried far, either to a hospital or his home, bear in mind—(a) to keep him warm with clothing; (b) to keep the limb continuously raised on cushions; (c) to look out for bleeding; (d) no brandy or other stimulants to be given unless bleeding is controlled, and then only if the patient's condition requires it.

5. Tourniquets.—In addition to a rubber tourniquet, St. John pattern tourniquets are being provided in all the Company's first aid cabinets, together with instructions and a chart shewing pressure points. The St. John tourniquet, which is of the pad and strap type, is intended for use by men who have a knowledge of first aid and can readily locate pressure points.

The efficiency of the rubber tourniquet depends on the tightness of the first turn.

An improvised tourniquet may be made of the "Swansdown" bandage supplied in Ambulance cab nets in the following manner: Pass bandage round limb and tie two knots, insert pencil (or anything similar) between knots, and twist to tighten. Secure pencil with piece of roller bandage.

An improvised tourniquet may also be made from a bandage, or even from a piece of rope; or an ordinary strap may be used if care is taken not to pucker the skin under the buckle in pulling tight.

6. Testing Rubber Tourniquets.—Rubber tourniquets must be stretched within reasonable limits at least once a month, and if any cracks or defects are visible when stretched, new tourniquets must be ordered, the old ones being returned to the Stores Department, Swindon.

When stretching a tourniquet for testing purposes the ends must not be held. The tourniquet must be grasped in the middle by one hand, and as near to an end as possible by the other. While the hands are in these positions the tourniquet must be stretched. Each half of a tourniquet must be treated in a similar manner.

The rubber tourniquet must be kept in the tin box provided in the Ambulance cabinet, not exposed to the light.

## INTRODUCTION OF ROAD MOTOR SERVICES IN LIEU OF TRAIN SERVICE. APPLICATIONS FOR SHORT PERIOD ROAD SERVICE LICENCES. ROAD TRAFFIC ACT, 1930.

Application has to be made to the Traffic Commissioners for Short Period Road Service Licences whenever Road Services have to be arranged in consequence of the temporary withdrawal of passenger train facilities owing to Engineering occupation or in emergency. Such application will be made by the Road Transport Superintendent, and it is necessary to provide the Commissioners with the following particulars :-

1. Date or dates service is required.

 Name of Omnibus Company to
 Details of route to be followed. Name of Omnibus Company to operate the road service.

4. Fares to be charged (i.e. passengers to be booked on the bus or to hold rail tickets throughout).

Any picking up or setting down points other than the Railway Stations.

6. Details of service (times, etc.).

7. Seating capacity of vehicles to operate the service.

When making application for authority to curtail the Train Service in connection with permanent way work and operate Road Motor Services in lieu thereof the particulars asked for above are to be furnished by the Divisional Superintendent or District Traffic Manager and as long a notice as possible

must be given of the proposed arrangements.

When, however, it is necessary to provide Road Motor Services in case of emergency, these may be arranged without prior sanction being obtained from the Superintendent of the Line, but telegraphic advice of the arrangements made must be immediately dispatched to "Traffic L.K., Paddington," also to the Road Transport Superintendent, addressed "Roads, L., Paddington," and the particulars required in connection with the application to the Traffic Commissioners for licences must be forwarded the same day to the Road Transport Superintendent, Paddington.

The person arranging the service must satisfy himself that the vehicle to be provided for the emergency service is properly licensed as a Public Service Vehicle. The telegraphic advice should state the circumstances necessitating the introduction of the road service, the service operated, from whom

the vehicle is hired, type of vehicle, and probable duration of the service.

# D. C. S. O. BRISTON

# THE RAILWAY EXECUTIVE (WESTERN REGION).

OFFICE OF COMMERCIAL SUPERINTENDENT,
PADDINGTON.

Office of Operating Superintendent, Paddington,

## INSTRUCTIONS TO STATION MASTERS AND OTHERS CONCERNED.

Provision of emergency road services when rail services are interrupted in consequence of accident, flood, subsidence, or other unforeseen cause.

The following revised information is given for the guidance of Station Masters and others who are concerned with the provision, in emergencies, of arrangements for the conveyance of railway passengers by road and will supersede the instructions contained in page 268 of the General Appendix to the Rule Book.

## AUTHORITY FOR THE OPERATION OF ROAD SERVICES.

Under the provisions of the Transport Act, 1947, the British Transport Commission has obtained from the Licensing Authorities a general approval of the operation, in cases of emergency, of passenger road services in lieu of rail or boat services operated by the Railway Executive. This approval is subject to compliance with certain conditions which include the following:—

## (1) NOTICE TO BE GIVEN TO THE LICENSING AUTHORITY AND POLICE.

The Licensing Authority and the Police are to be advised of all emergency road services operated, the advice to state the points between which the service will be operated and the routes to be followed, if possible before commencing to operate the road service, and if not so possible assoon as practicable thereafter.

## (2) ROUTES TO BE FOLLOWED.

It will be necessary to comply with any Police instructions regarding routes to be followed, or otherwise, for the direction or control of traffic.

## (3) NOTICE OF CESSATION OF ROAD OPERATIONS.

The Licensing Authority and the Police are to be advised immediately the road service is withdrawn.

## PROCEDURE TO BE FOLLOWED.

In the normal course there will be consultation with the Office of the District Operating Officer prior to the introduction of road services, but if for any reason that Office cannot be contacted, it will be necessary for Station Masters to act on their own initiative.

To comply with clauses (1) to (3) above, it will be the responsibility of the person ordering and terminating the road transport arrangements:—

- (a) to advise, by telephone, the local police officer responsible for the area in which the service is to be operated, conferring, if necessary, with the operator engaged to provide the service as to the route which will be followed by the vehicles;
- (b) to advise, by telephone or telegram, the District Commercial Officer of the arrangements, including the route to be followed in order that the District Commercial Officer can give the required advice to the Licensing Authority concerned;
- (c) to advise the local police officer and the Distict Commercial Officer of the termination of the road service and to furnish to the latter a report of the arrangements, giving the following information:—
  - (i) Name of operator engaged;
  - (ii) Dates on which the road service was operated;
  - (iii) Number of trips run between the various points each day;
  - (iv) Number of passengers carried each day;
  - (v) Number of vehicles hired each day (stating whether any vehicles hired but not utilised for any reason);
  - (vi) The vehicle mileage each day.

Wherever possible, with reasonable convenience, an associated undertaking should be called upon to provide road services in preference to a non-associated operator.

If there is doubt as to the operator to be approached in an emergency, guidance should be sought forthwith from the District Commercial Officer concerned.

If questions are raised by operators as to the legality of the services they are asked to provide, they should be informed that the necessary authority is held by the British Transport Commission.

It is the intention that vehicles engaged on these services shall bear a notice "Emergency Service for Rail Passengers." These labels are provided by the Railway Executive and arrangements have been made for Associated Omnibus Companies to hold supplies at their various garages and to affix a label to each vehicle hired to the Railway Executive. Stations should, however, hold a small supply (obtainable from the District Commercial Officer) for fixing to vehicles hired from other operators.

All classes of railway tickets will be honoured on the special road services between the points for which they are available and only passengers holding rail tickets are to be allowed to travel on those services.

Rail tickets will not be collected on the road services except when the stage travelled by road represents completion of the journey covered by the rail ticket. It is desirable, where practicable, that railway staff should examine and/or collect the tickets, but when the collection is undertaken by the bus staff, such tickets should be handed to the responsible railway official at the end of the road journey.

Accounts subsequently received for the operation of road services under these arrangements should be certified, if correct, and passed to the District Commercial Officer.

The above instructions relate to instances where special road services are necessary. If there is in existence a convenient road service linking up stations between which it is required to provide for the conveyance of railway passengers by road, it may be possible to arrange for the passengers to be conveyed on this ordinary service, the railway tickets being honoured by the operator and an account (accompanied by any such tickets collected) rendered later by him to the Railway for the payment of the appropriate fares. The Associated Omnibus Companies have agreed that such arrangements will be brought into operation on the written authority of the Station Master or other responsible official. In such cases it will not be necessary to advise the Licensing Authority or Police of the arrangements, but a full report of the circumstances must be made to the District Commercial Officer.

C. FURBER.

COMMERCIAL SUPERINTENDENT. (B7/164/G.)

April, 1951.

GILBERT MATTHEWS.

Operating Superintendent. (A2/71106/22.)

## MUTUAL ASSISTANCE AS BETWEEN RAILWAY COMPANIES AND ASSOCIATED ROAD COMPANIES IN CASES OF EMERGENCY.

Arrangements have been made between the Railway Companies and their Associated Road Companies, under which mutual assistance will be rendered in case of emergency.

The following formula gives particulars of the understanding come to as far as the Great Western

Railway is concerned :-

"On the breakdown of any vehicle of the......Omnibus Company the passengers should be sent forward by train to their destination at the request of any representative of the Omnibus Company and without immediate charge, third class single tickets being issued. The Station to render promptly the Account to the Omnibus Company for tickets issued (with a request to remit amount to Chief Cashier, G.W.R., Paddington) and to take credit for the amount on Daily Cash Account '110' as 'Breakdown ....... Omnibus Company,' attaching copy of Account in support.

"The ...... Omnibus Company will provide, whenever possible, on intimation from a Great Western Station Master or other responsible Official, vehicles to convey railway passengers in the event of a railway breakdown or interruption of service on the Great Western system; accounts for the services performed to be rendered to the ordering Official."

As the journeys made by the vehicles of Associated Omnibus Companies are not always limited to the territory served by the Railway Company with whom they are associated, these arrangements will apply in the event of a road vehicle of a Company associated with any of the Railways being unable to complete its journey through breakdown or other emergency.

The following list shows the Road Transport Compaines in the United Kingdom recognised by the Railway Companies :-

Aldershot and District Traction Co., Ltd., Halimote Road, Aldershot. Alexander, Walter & Sons, Ltd., Brown Street, Camelon, Falkirk.

\*Birmingham and Midland Motor Omnibus Co., Ltd. (Midland Red), Bearwood, Birmingham.
\*Black and White Motorways, Ltd., Motor Coach Station, Cheltenham Spa. \*Bristol Tramways and Carriage Co., Ltd., Tramways Centre, Bristol.

Caledonian Omnibus Co., Ltd., Loreburn Chambers, King Street, Dumfries. Central S.M.T. Co., Ltd., Traction House, Hamilton Road, Motherwell.

Chatham and District Traction Co., Ltd., Knightrider House, Knightrider Street, Maidstone.

\*City of Oxford Motor Services, Ltd., The Garage, Cowley Road, Oxford.

\*Crosville Motor Services, Ltd., Crane Wharf, Chester. Cumberland Motor Services, Ltd., Tangier Street, Whitehaven. \*Devon General Omnibus and Touring Co., Ltd., Newton Road Garage, Torquay.

East Kent Road Car Co., Ltd., Station Road West, Canterbury. East Midland Motor Services, Ltd., New Street, Chesterfield. East Yorkshire Motor Services, Ltd., 252, Anlaby Road, Hull.
Eastern Counties Omnibus Co., Ltd., Thorpe Road, Norwich.
Eastern National Omnibus Co., Ltd., New Writtle Street, Chelmsford.
General County Omnibus Co., Ltd., Picktree Lane, Chester-le-Street, Co. Durham.

General Motor Carrying Co., Ltd., Esplanade, Kirkaldy.

Greenock Motor Services, Dellingburn, Greenock.
\*Greyhound Motors Ltd., 5, St. Augustine's Place, Colston Street, Bristol.

Halifax Corporation and Railways Joint Omnibus Committee (L. & N.E., L.M.S.), Skircoat Road,

Halifax. Hants and Dorset Motor Services, Ltd., Royal Mews, Norwich Avenue, Bournemouth.

Hebble Motor Services, Ltd., Walnut Street, Halifax. Highland Transport Co., Ltd., 29, Bank Street, Inverness.

Huddersfield Corporation and L.M.S. Railway Joint Omnibus Committee, John William Street, Huddersfield.

Lanarkshire Traction Co., Traction House, Hamilton Road, Motherwell.

Lincolnshire Road Car Co., Ltd., Bracebridge Heath, Lincoln.

Majestic Saloon Coaches (Newcastle & London) Ltd., United House, Grange Road, Darlington. Maidstone and District Motor Services Ltd., Knightrider House, Knightrider Street, Maidstone.

Northern General Transport Co, Ltd., Queen Street, Bensham, Co. Durham.

North Western Road Car Co., Ltd., 120, Charles Street, Stockport.
Orange Bros., Ltd., United House, Grange Road, Darlington.
Orange Bros., Ltd., Haymarket, Newcastle-on-Tyne.
Phillipson's Motor Coaches, Ltd., United House, Grange Road, Darlington.

Pitlochry Motor Co., Ltd., Main Street, Pitlochry.

Ribble Motor Services, Ltd., Frenchwood Avenue, Preston.

Rothesay Tramways Co., Ltd., Point House, Port Bannatyne, Rothesay.

\*Royal Blue Services, 48-50, Queen Street, Exeter. Scottish Motor Traction Co., Ltd., 29, East Fountainbridge, Edinburgh.

Sheffield Corporation and Railways Joint Omnibus Committee (L. & N.E., L.M.S.), Division Street, Sheffield.

## MUTUAL ASSISTANCE IN CASES OF EMERGENCY-Continued.

Sheffield United Tours, Ltd., 88, Kingsway, London, W.C.2.
Simpsons and Forresters, Ltd., 1, Market Street, Dunfermline.
Southdown Motor Services, Ltd., 5, Steine Street, Brighton.
Southern National Omnibus Co., Ltd., 48-50, Queen Street, Exeter.
Southern Vectis Omnibus Co., Ltd., Nelson Road, Newport (I. of W.).
Sunderland District Omnibus Co., Ltd., Philadelphia, Co. Durham.
\*Thames Valley Traction Co., Ltd., Lower Thorn Street, Reading.
Todmorden Corporation & L.M.S. Railway Joint Omnibus Committee, Millwood, Todmorden.
Trent Motor Traction Co., Ltd., Uttoxeter New Road, Derby.
Tynemouth and District Electric Traction Co., Ltd., John Street, Cullercoats, Whitley Bay.
United Automobile Services, Ltd., United House, Grange Road, Darlington.
Wakefield Motors, Ltd., John Street, Cullercoats, Whitley Bay.
West Yorkshire Road Car Co., Ltd., East Parade, Harrogate.
\*Western National Omnibus Co., Ltd., 48-50, Queen Street, Exeter.
Western S.M.T. Co., Ltd., Bas Station, Portland Street, Kilmarnock.
\*Western Welsh Omnibus Co., Ltd., Cowbridge Road, Ely, Cardiff.
Wilts and Dorset Motor Services, Ltd., 2, St. Thomas's Square, Salisbury.
Yorkshire Traction Co., Ltd., Upper Sheffield Road, Barnsley.
Yorkshire (Woollen District) Electric Tramways, Ltd., Savile Town, Dewsbury.

#### \* Associated with Great Western Railway.

Note.—The arrangements for emergency use of the road services (omnibuses and trams) of the London Passenger Transport Board in Great Western territory in case of interruption of train services are detailed in Superintendent of the Line's Circular No. 4838, dated January 28th, 1935.

## DAMAGE TO PRIVATE OWNERS' WAGONS.

Damage to Private Owners' Wagons has occurred owing to vehicles having been shunted after being marked off for defective draw-gear, with serious results to the main members of the underframes. The damage has been due to wagons being placed in an occupied siding and afterwards coupled to an engine during shunting operations. When wagons have been marked off for repairs to defective draw-gear, they must be placed in a position of safety until such time as the draw-gear is repaired.

#### DAMAGE TO VEHICLES.

To avoid damaged vehicles proceeding into traffic in an unsafe condition, the special attention of the staff is directed to the importance of all cases of accidental rough shunting, collision with buffer stops, etc., involving damage to rolling stock, being immediately reported and the vehicles specially examined before again being put into service.

## HEAVY WEIGHTS OVER BRIDGES.

The Divisional Engineers will be advised by the District Goods or Traffic Managers of cases in which traffic of an exceptionally heavy nature, arriving or to be despatched by railway, is to be conveyed over the Company's road bridges, and, if necessary, the bridges affected must be temporarily supported. The notification should take the following form:—

NOTICI	E OF HEAVY TRAFF	TIC T	O BE	CAR	TED OV	ER ROAD-BRIDGES.
The following	heavy traffic will be c	arted	from.			to
on	19 , at or ab	out		o'elocl	kn	1.
						be inspected and (if necessary
supported, to allow	v of the traffic passing of	over it				
	Description of traffic					
					Tons.	Cwts.
	Weight of traffic					
	Tare of road vehicle					
	TOTAL					

DEFECTIVE DOOR LOCKS ON PASSENGER COACHES.

Should a defect in the door handle lock of a corridor coach come under notice, the vehicle must be labelled for repairs, but the coach may be allowed to continue temporarily in traffic if the door handle is tied, the door locked with the carriage key and the hole of the carriage key lock plugged with timber.

In the event of a defect being observed in the door handle lock of a non-corridor coach, the passengers must be requested to leave the compartment, after which the blinds must be drawn, door handles tied, both doors locked with the carriage key and the holes of the carriage key locks plugged with timber. The coach must be labelled for repairs.

If both the door handle lock and the carriage key lock are defective, the coach must not be allowed to go forward.

(G.A.16. 5/46. L.K.2/10462/N.)

The following instruction to be inserted on page 270:-

MISSING WINDOW STRAPS IN NON-CORRIDOR STOCK.

Where window straps are missing in compartments of non-corridor stock and there is no means of operating the door handle from the inside, the Guard or other staff concerned must draw the blinds, close the windows and lock up the compartment.

The attention of a carriage examiner must be drawn to the defects and the Guard must record particulars on his train journal.

If the vehicle is to remain in service until replace straps are available, the key-holes must be plugged immediately after the vehicle has completed the journey on which the defect was noticed, in order to prevent (G.A.16. 5/46. L.K.2/1437 L.P.) irregular entry.

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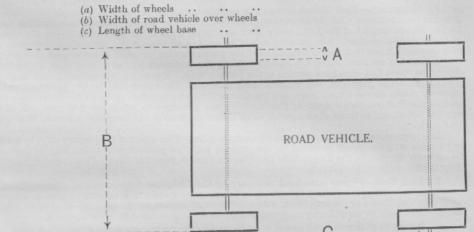
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### HEAVY WEIGHTS OVER BRIDGES-Continued.

The following are particulars, as per diagram below, of the road vehicle which will be used :-



Note.—Whenever possible, three days' notice must be given to the Engineering Department, in order that proper arrangements may be made for inspecting and supporting the bridge.

When it is not possible to give three days' notice, owing to urgent or other circumstances, the District Goods Manager or Dock Manager, or their representatives, must see the Divisional Engineer, or his representative, personally, in order that special arrangements may be made.

#### TRESPASSING ON THE RAILWAY.

When reporting to the Head of Department cases of trespassing upon the line, the following particulars must be given on the standard form :-

- 1. Christian and Surname of Offender.
- 2. Address and Occupation.
- Whether address has been verified.
- 4. Date and place of offence.
- Whether Trespass Notices were exhibited at the nearest station or public level crossing near the spot, and if so to what Act of Parliament they refer.
- 6. Whether any trains were due. If so, name them.
- 7. If not, state if the trespasser was exposing himself to danger, and, if so, how.
- 8. State particulars of refusal to quit, if any.
- Particulars of any previous irregularities.
   If offenders are young, state age.
- 11. General observations on facts of case.

The Company and amalgamated constituent and subsidiary Companies have had special powers conferred upon them by Acts of Parliament in regard to trespassing, by which it is enacted that any person who shall trespass upon any of the lines of railway now or hereafter belonging or leased to or worked by the Company, solely or in conjunction with any other Company or Companies, or in the case of the Rhymney Railway Act, 1887, or the Great Western Railway Act (No. 1), 1882, any person who shall trespass upon any of the railways and stations of the Company in such a manner as to expose himself to danger, or risk of danger, shall without previous warning be liable to a penalty of 40 shillings, provided that a Notice Board is exhibited at the nearest station and at the public road level crossing (if any).

The prevention of trespassing upon or other improper use of the Railway by the Public is a matter to which the Staff of all Departments are required to give continuous attention. Any permatter to which the Staff of all Departments are required to give continuous attention. Any person passing over, along, or on to any part of the line or premises not ordinarily open to the public, must be warned and required to withdraw. If the trespass is persisted in, or repeated, the name and address of the offender must be taken and the matter reported; great care must be taken to avoid the possibility of rights of way over the line at accommodation crossings becoming extended by continuous user on the part of persons not entitled to it, and in no case, either by acquiescence or otherwise, must a new right of way be conceded without authority. In all cases in which application is made to cross or walk along the line or use the railway, or any part of the Company's land or premises as a means of access to or exit from other preperty, the circumstances must be land or premises, as a means of access to, or exit from other property, the circumstances must be referred to the General Manager through the Heads of Departments. (Vide Rule 14 of the Rule Book.)

#### INSTRUCTIONS TO TICKET COLLECTORS AND EXAMINERS.

April 1938 5

1. Equipment.—Each Ticket Collector is equipped with an excess pad, a supply of examining punches (in accordance with the arrangements set out in Circular No. 1897, dated October, 1923, issued by the Superintendent of the Line) and a book containing a list of the fares most frequently required at the particular station. The punches should be used in all cases where tickets are examined, and care exercised to see that a clear impression of the punch number (or mark) is made upon the ticket.

A case has occurred where a passenger holding the backward half of a third class return ticket desired to travel first-class for a portion of the journey only. The excess was collected, and the ticket given back to the passenger, but without being punched. The ticket held by the passenger was incorrectly shewn on the Excess Note, and the Excess Note was therefore accepted for the journey from and to the Stations shewn upon it, thus enabling the passenger to retain the backward half of the original ticket, and as there was no punch mark to indicate that it had been used a Return Fare Claim was made.

It is imperative that tickets upon which excess is collected for a part of the journey only, be marked in all cases with the punch provided for the purpose, and that particulars of ticket held by passenger be correctly inserted on the Excess Note (See Clauses 8 and 9, page 2 of Excess Fare Circular, No. 3206, July, 1929).

2. Petty Cash.—Where considered necessary by the Divisional Superintendent, Ticket Collectors or other persons who are regularly occupied in collecting or examining tickets in trains must be supplied, by the Station Master or Booking Clerk, with a sufficient amount of petty cash made up of small change, so that there may be no difficulty in giving change to passengers when they are excessed.

3. Paying in of Excess.—All excess must be paid to the person appointed to receive it as soon as possible after it has been collected, but in no case less often than twice during the day, a final settlement being made before going off duty. An Excess Note must be made out by the person collecting the excess for each separate amount collected, except where the Docket System is in force.

4. Receipts for Excess Fares.—Numerous cases have occurred where the Collecting Staff have received fares at exits, etc., but have failed to comply with the instructions which require a receipt to be given to the passenger at the time. The importance of this phase of the arrangements for the collection of excess fares cannot be too strongly emphasised, and Station Masters, Inspectors and other members of the Supervisory Staff should see that this instruction is strictly carried out.

Station Masters will be held personally responsible for seeing that the Staff under their supervision pay due regard to the provisions of this instruction, and the opportunity is taken of reminding the Staff of the importance of distinctly punching Tickets NOT COLLECTED, in accordance with the instructions contained in Circular 1897, dated October, 1923.

5. Cancellation of Tickets.—All tickets collected must be cancelled at once, except in the circumstances set out in the following paragraph. Tickets upon which excess has been paid must be made up as soon as collected from each Train, and particulars entered upon the "Excess Return" (Form 305-1), and dealt with in accordance with Clause 198c of Station Accounts Instruction Book.

Where it has been arranged in accordance with instructions issued by the Divisional Superintendents for tickets collected for "Open" stations to be sent to such "Open" stations by the Trains collected from, such tickets are at the time of collection from holders to be cancelled or to be impressed with the punch in use at the time for punching through tickets and cancelled on receipt at destination stations. The special envelope provided for the conveyance of these tickets must be used in all cases.

6. Examination and Collection of Tickets.—Upon the arrival of a train at a Station where the Tickets have to be examined or collected, the Ticket Collectors must be on the platform awaiting it, and as soon as possible after the arrival of the train, the work of examination must be commenced from both ends simultaneously, and the door of each compartment must be locked until the train is nearly ready to start.

Special efforts must be made to ensure that persons over 14 years of age do not travel with half tickets, nor children over three and under 14 without any ticket at all.

- 7. Collecting Stations.—Under no circumstances may card tickets be issued to cover excess received at collecting stations either in the train or at the exit doors.
- 8. Intent to Defraud.—When cases arise of excess having to be collected, the Ticket Collector must deal with the matter in such a way as to cause the least possible delay to the train. If he has good reason to believe that the passenger is committing an irregularity, with intent to defraud the Company, the excess fare should not be demanded, but the attention of the Station Master or other person in charge must be called to the matter immediately.
- 9. Collection of Tickets at "Way Out" Gates.—The Ticket Collector or Porter collecting tickets at the "Way Out" Gates must observe as far as possible the class of carriage from which passengers alight, so that he may have some check upon the tickets given up.
- 10. Necessity of Ticket Collector travelling to next stopping Station.—It may be desirable under certain circumstances (such as the passenger being unable to find his ticket, or making some attempt to avoid payment of the excess), rather than delay the train, for a Ticket Collector to proceed with the train to the next stopping station, but this must not be done without the concurrence of the Station Master. If the Ticket Collector cannot accompany the train the Guard must be informed and a

# INSTRUCTIONS TO TICKET COLLECTORS AND EXAMINERS -Continued.

telegram must if possible be despatched to the next stopping station, giving particulars of the case, and number of compartment in which passenger is riding, so that he may be dealt with there rather than cause further delay to the train at the examining station.

11. Sailors without Tickets.—When reporting these on Form 1064 the sailor should be asked to produce his liberty ticket, and particulars as to the number and date of the liberty ticket, Ship No., Mess No., and any further details likely to assist in tracing the man, when the Authorities are written for the fare due, should be taken.

For full instructions in regard to Examination and Collection of Tickets and charging and accounting for Excess Fare, see re-issue of Circular 1897, dated October, 1923.

Note.—The special attention of all concerned is directed to Superintendent of Line's Circular 3206, July, 1919. August 1938

# EXAMINATION AND COLLECTION OF TICKETS.

1. Examination of Lavatories in Trains.—Sufficient importance is not attached to the examination of Lavatories in trains when tickets are being examined, or collected for open stations.

The Staff are particularly requested to bear in mind that, for Ticket Collecting and Examining purposes, a Lavatory is a Compartment, and the following points must receive special attention:

(a) Lavatories in Corridor Coaches must be examined FROM THE CORRIDOR, whether on the "off" or on the "platform" side of the train.

(b) Lavatories in Non-Corridor Coaches must be systematically examined.

(c) Under no circumstances is a Lavatory to be examined by lowering the window from the outside.

(d) In the event of difficulty owing to a Lavatory being "engaged," the Station Master, or person in charge, is to be called, in order that his instructions may be obtained for dealing with the case at his discretion.

2. Entrances and Exits at Stations Left Open Unnecessarily.—Numerous instances have been brought to notice where gates giving access to, or exit from, stations have been left open and free from supervision when trains arrive, the result being that passengers are enabled to alight from trains, use these gates, and retain their tickets, or leave without paying their fares. Where return tickets are held passengers are able to retain them for use on a future occasion, and passengers joining the trains are given the opportunity for doing so without booking. Station Masters and the Staff generally are required to take steps to see that all such means of entrance and exit are kept locked when not actually in use for the purpose intended.

3. Scrutiny of Tickets at the Time of Collection, or when Examinations are being made.—
Considerable importance is attached to the careful scrutiny of tickets at the time of collection or examination. Many instances have been brought to notice where through want of care difficulties have been created, or the Company has been defrauded by passengers who have taken advantage of the inadequate scrutiny, and Station Masters are requested to take steps to impress the importance of the subject on their Staff, and to impart to them such information as will enable them to readily detect irregularities.

(S. of L. Cir. 3231—7114.)

# STARTING TRAINS AT STATIONS WHERE TICKETS ARE EXAMINED.

Station Masters, Inspectors and others in charge of Stations are instructed to take care that when Tickets are being collected or examined in the Trains, the signal to start is not given while any of the Ticket Examiners or Collectors are in the carriages or on the carriage steps.

At places where the special Ticket Inspectors are at work, the special examination must be stopped (unless specific instructions have been received to the contrary) as soon as the other work of the train is completed, in order that no additional delay may be caused, but the Station Master or person in charge of the Station, before giving the signal to start, must have a signal from the Chief Inspector in charge of the Ticket Inspectors to indicate that the whole of his men have left the train, and the Chief Inspector will be responsible for seeing that none of his men are in the carriages when the train is started.

Where the Tickets are examined at the starting point, the examination must be completed before the train is due to leave.

# COLLECTION OF TICKETS FOR DOGS, BICYCLES, &c.

Tickets for Dogs, Bicycles, etc., conveyed in the Guard's van are not to be collected until the Passenger has arrived at his destination, and no Dog, Bicycle, or other article for which a ticket should be held is to be delivered to a Passenger until he produces such ticket, or pays the correct fare for such Dog, Bicycle, or other article, whether the Station be an open one or not.

#### COLLECTION OF TICKETS FOR DOGS, BICYCLES, ETC .- Continued.

It must be clearly understood that although these tickets are not to be collected until passengers claim the articles from the Guard's van in the case of an "open" station, or until they are leaving the gates at a "closed" station, they must be examined and punched on each occasion that the passengers' tickets are so dealt with en route, or when collection is being made for the destination station, if an "open" one. The attention of Train Ticket Collectors is specially directed to this requirement.

Great care must be taken to ensure that Dogs, Bicycles, and other articles for which tickets are

required, are properly booked at the starting-point.

# INSTRUCTIONS REGARDING THE ISSUE OF PLATFORM TICKETS AT CLOSED STATIONS.

General.

1. A charge of one penny is made for admission to the platforms to all persons, including children over three years of age, who are not in possession of a railway ticket, or who are not otherwise authorised to be thereon; and the following instructions are to be imparted to all concerned, the Station Master and Chief Booking Clerk seeing that they are carried out properly.

Tickets.

2. A specimen is given below of the tickets, which will be issued from Automatic Machines in numerical order, and dated on the back.

ACTUAL SIZE.

FRONT OF TICKET.

BACK OF TICKET.

GREAT WESTERN RAILWAY.

The holder is prohibited from entering the Companys Trains. NOT TRANSFERABLE. Admit ONE to PLATFORM 1D AVAILABLE ONLY 1D This Ticket must be given up on leaving Platform FOR CONDITIONS SEE BACK

1 2 3 4 5 6

Punching.

3. The numbers printed on the margin of the tickets indicate the hours of the day. The hour at which a person holding a ticket is admitted to the platform must be indicated by the Ticket Examiner punching out, with the ticket nippers provided, the hour at which the holder is admitted to the platform, and the ticket is available for one hour only.

For example, a ticket presented at any time between 10 o'clock and 10.59 is to be punched in the space marked 10, and a person in possession of a ticket so punched

is not to be challenged until 12 o'clock.

Collection.

4. From each person leaving the platforms who does not surrender or produce a railway ticket, hold a pass, pay an excess fare, or is not otherwise authorised to be upon the platform, a platform ticket is to be demanded. Particular attention is to be paid to the following:—

(a) That the ticket bears the current day's date.

- (b) That the ticket is properly punched. If a platform ticket be collected not punched, an explanation of how the ticket was obtained must be asked for.
- (c) A ticket punched in the space marked 10, for example, indicates that the holder passed the barrier some time between 10 o'clock and 10.59, and should such a ticket be presented after 1 o'clock an explanation must be obtained as to how the holder has occupied the interval, on the assumption that a journey may have been made to an adjacent station and back without paying the fare.

(d) When platform or other tickets are not produced or surrendered, or the former are held for more than the stipulated period, the necessary excess

fares are to be collected.

5. Collected platform tickets are to be cancelled, sorted into numerical order, and sent to the Audit Office daily, in the same way as collected railway tickets. Under no circumstances must a collected platform ticket be re-issued.

6. A monthly return of platform tickets issued must be rendered to the Superintendent of the Line.

7. In cases where the Station Master's Office, or the Cloak Room, is on the platform there will be a certain number of people requiring to make enquiries at these Offices, and before admitting them through the barrier without a ticket it will be the duty of the Examiners to satisfy themselves that such persons are on legitimate business.

Disposal of collected Platform Tickets.

Monthly Return.

Persons admitted free to platforms.

# ISSUE OF PLATFORM TICKETS AT CLOSED STATIONS-Continuea.

Where there is not an Enquiry Office outside the ticket barrier all enquiries regarding train services, excursion arrangements, &c., must be dealt with in the first instance at the Booking Office, and only be referred to the Station Master's Office if it be impossible to deal with them at the former place.

The following may also be admitted to the platforms without a platform

ticket :-

(a) Members of Messrs. Wyman's library wishing to exchange their books, on production of their library ticket.

(b) Messrs. Wyman's staff employed at the bookstalls, when on duty. (c) Policemen and Postmen in uniform, in the execution of their duties.
 (d) Wives and children of Company's servants, employed at the station, when bringing meals.

(e) "Boots" from the principal Hotels, when in livery.

Such persons, when admitted, must be asked to leave the station through the barrier by which they entered, and it should be an easy matter to confirm any statement made by a person leaving the platforms without a platform ticket if the Ticket Collector does not recollect admitting him, and this course must be adopted if neces-SATY; BUT AT ALL TIMES THE COLLECTOR MUST BE TACTFUL, AND NOT GIVE CAUSE FOR

8. Passengers who allege that they have travelled from Platform Ticket Passengers Stations without railway tickets are to be asked for their platform tickets, and if from Platform surrendered, excess fares are to be charged accordingly; if not surrendered, and in Ticket Stations. the absence of satisfactory explanation, excess fares are to be charged from the station at which the tickets were examined prior to the arrival of the passengers at the station they state they have travelled from.

The number of the platform ticket when surrendered must be shown against the Excess Fare entry, and the ticket is to be sent to the Audit Office with the other

collected tickets for the day.

9. Automatic Machines, actuated by means of a spring motor put into motion Description of 9. Automatic Machines, actuated by means of a spring motor put into notion Automatic by the insertion of a penny in a slot, will be supplied for issuing the necessary tickets, Ticket-issuing and will be placed in conspicuous positions near the entrance to the platforms, if Machines. possible at a point where the Ticket Collectors can see that they are not tampered

10. The tickets are supplied in rolls of 3,000; a roll of tickets is to be placed Charging in the machine on the spindle provided, with the backs of the tickets at the loose Machines with Tickets. end of the roll towards the attendant, and the loose end is to be inserted in the feeding device until it touches the knife. Metal prongs are fitted in the machine, and the loose end of the roll of tickets must be so placed that these prongs grip into the perforation of the second ticket.

11. The Clerk in charge is to arrange for the machines to be kept replenished Replenishing Machines. with tickets. Further supplies are to be requisitioned from the Audit Office on a separate Passenger Ticket Requisition Form, No. 143, AT LEAST THREE WEEKS BEFORE RENEWALS ARE REQUIRED.

12. The spring motor operates about 500 times before it needs re-winding. A Winding. large key is provided, and this must be turned to the LEFT in winding, care being taken not to over-wind the motor. A bell is provided to strike when the winding of the machine is complete, and the operation is to be stopped as soon as an indication of the bell striking is noticed. The duty of winding the machine will devolve upon the Chief Booking Clerk, who is to attend to it DAILY.

13. If coins stick in the coin detector, they can be released by pressing a button Coin Detector. which is fitted on the outside of the machine. The coin detector is to be handled

carefully, and must not be dropped or roughly used. 14. A screw is fixed in the machine for altering the date type. At the close Dating Device of each day the date is to be altered to that of the following day (see Clause 19). The dating device must be inked periodically, AT LEAST ONCE A MONTH.

15. EXTERIOR.

(a) The nickelled parts are to be rubbed occasionally with a suitable metal Cleaning and Oiling.

polish. It is important that the working parts of the machine should be kept CLEAN, AND THE FOLLOWING INSTRUCTIONS MUST BE CABRIED OUT ON THE FIRST DAY OF EACH MONTH.

(b) Feeding device.—This is to be dusted with the soft brush provided, care being taken not to damage the metal prongs.

(Note.—Omission to do this is liable to cause failure of the machine.)

#### ISSUE OF PLATFORM TICKETS AT CLOSED STATIONS—Continued.

- (c) Coin detector.—This can be taken off by unscrewing the nut at the back. and opened out by loosening a red screw; this will then permit of the removal of dust and dirt.
- (d) Chutes.—These and the coin detector are best cleaned by means of sticks with linen wrapped upon them.

All the working parts of the mechanism should be oiled with pure machine oil, except the metal prongs in the feeding device, which must be cleaned but not oiled.

16. Any defects in the machine which cannot be remedied by the Station staff are to be reported to the Divisional Superintendent or District Traffic Manager at once, and by wire if necessary. In the meantime the tickets are to be transferred to the Booking Office, and issued therefrom if the machine will not work. In cases of breakdown a telegraphic advice to be sent to the Stores Superintendent, Swindon, who will arrange for a machine to be sent by return to take the place of the machine requiring repairs, the latter on return to Swindon being held for subsequent re-issue.

17. The cash receptacle is to be cleared of cash at the close of each day, or more often if the circumstances require it, by the Chief Booking Clerk.

18. The platform ticket issues are to be recorded in the Booking Clerk's Train Book similarly to railway tickets, and accounted for monthly in the space provided for the purpose on the Passenger General Account (874).

19. As the machine on issuing a ticket also dates the next ticket to it, at the close of each day the ticket next to be issued is to be obtained by the Booking Clerk after the date has been altered (see Clause 14), treated as non-issued, and sent to the Audit Office with the Passenger General Account 874.

20. At some Stations Machines supplied by the British Automatic Machine Co. are in use, and the paragraphs 21-28 take the place of paragraphs 9-15.

21. Card tickets of different designs for adjacent stations are issued.

22. To open machine, unlock and pull side eash drawer half way out; then remove the front plate. The latter should always be pushed right home before closing cash drawer.

23. All tickets placed in machine must be of uniform thickness and absolutely flat.

Care must be taken that the edges of tickets are not damaged or frayed out in any way. The marks left by string when tickets are tied up in bundles should specially be looked for.

The weight should always be left on top of tickets in column.

24. To re-fill machine with tickets, open the front door; then remove front plate and weight on top of the tickets. See that the tickets are put in the column squarely and flat.

25. When changing date in type cylinder, remove inking roller, then pull the drawer right out, and allow the pins in side pawl to rest in the last tooth in the top rack; then push fork in the holes of the locking blade of type cylinder with the right hand, at the same time gently pressing on the type to prevent it jumping out with the left hand; then change type as required by means of tweezers supplied with the outfit; then remove fork and press down with the wooden block also provided for this purpose. To renew ink on roller, a very small portion of ink supplied should be spread on the glass slab with the palette knife and carefully rolled with hand roller; then roll type roller over surface of slab, and this to be repeated as required.

The ink must in no case be allowed to get between the type in cylinder, and must never be allowed to dry on same. This can be prevented by occasionally removing type and cleaning it with turpentine.

26. To put machine out of use, pull handle slightly forward and turn key in back of machine.

27. If the coin hopper should become blocked, the obstruction can be removed by taking out the two screws and lifting front of hopper; but care must be taken that screws are put back and screwed up tightly.

28. All the working parts of the machine should be oiled, and a good sperm or lard oil must be used. Care, however, should be taken not to let the oil go near the tickets in column of dating cylinder.

All screws should be examined from time to time to see that they are tight. The inking roller will probably require rolling on the slab after the issue of each This will, however, depend on the absorbent character of the tickets issued through the machine.

Defects.

Cash.

Record of Issues and Passenger General

Account. Non-issued

Tickets. Opening

Machines

Tickets.

Refilling Machine.

Dating Device.

Putting Machine out of use.

Coin Hopper

Care of Machine.

# REGULATIONS FOR WORKING ELECTRIC AND PETROL PLATFORM TROLLIES AND TRAILERS-page 277.

The following to be inserted as Clause 16 A:-

Except in cases where an attendant is returning to the point from which loaded trailers have been taken, MEMBERS OF THE STAFF ARE FORBIDDEN TO RIDE WITH DRIVERS ON THE TROLLIES.

(G.A.29.Op.-5/52. LKI/10088/1.)

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# REGULATIONS FOR WORKING ELECTRIC AND PETROL PLATFORM TROLLIES AND TRAILERS.

It is of the greatest importance that mechanical trollies should be moved about with every care at a speed never exceeding eight m.p.h., so as to prevent inconvenience, annoyance or possible injury to passengers on the platforms and the special attention of all concerned is directed to the following:—

- 1. The method of charging electric-driven trollies varies according to the type of trolley and system of electrical supply. Special instructions will be issued to drivers to suit the local conditions.
- No person must be allowed to drive any trolley unless he has been passed as competent to do so by the Road Transport Department. (Or by the responsible official in the case of the Docks Department Warehouses).
- 3. Each driver must see that the Log is on the vehicle, and that the instructions relative thereto are carried out. (Not applicable at Dock Warehouses).
- 4 Drivers must immediately enter in the Vehicle Log any defect they may notice, stating if possible whether the defect is mechanical or electrical, and present same to the Station Master, or to the officer under whom the driver is directly employed. (At the Docks the defects must be reported by the drivers to the responsible Foremen, who will draw the attention of the Electrical Department to the matter).
- 5. The driver will be responsible for oiling and greasing the machine and for keeping it clean. (Except at Dock Warehouses, where the maintenance of electrical trollies is attended to by the Electrical Assistant.)
- 6. Each driver must see that a Fire Extinguisher is on his vehicle, that it is in good working order, and must make himself familiar with the use of same. (Not applicable at Dock Warehouses where extinguishers are placed at certain points in the Warehouses.)
- No petrol to be carried other than that in the tank, and the tank to be filled at authorised places only.
- 8. These trollies must not be used to pull any vehicle or trailer unless it is fitted with the special coupling provided for the purpose.
- 9. Every trolley must have a warning signal, which must be in good order and give a loud sound. Trollies must not be run unless this is in working order. Horns and bells must not be sounded more than is necessary, but sufficient audible warning must be given.
- 10. The maximum number of trailers that may be hauled at one time during the day is set out below. During quiet intervals at night the number may be increased, at the discretion of the supervisory staff, within the limits shown:—

		On Level	L.	ON RISING GRADIENT.			
TYPE OF TROLLEY.	General parcels.	News- paper.	Empty.	General parcels.	News-	Empty.	
During the day.  Platform trolley Cob tractor		4 4	4 4	6 6	4 4	3 4	6 6
At night.  Platform trolley Cob tractor		6 10	4 8	12 12	4 8	3 6	12 12

- These trollies must be driven at reasonable speed and drivers must always have the vehicles completely under control, and be prepared to stop immediately, without warning, if necessary.
- 12. Trollies must not be run about unnecessarily.

1.)

- 13. Trollies must not be run alongside loaded trains whilst they are drawing in to the platform and are still on the move. As far as possible care must be taken to avoid moving trollies along the platform just before the train is starting.
- 14. Opportunities should be taken to move trollies when platforms are not crowded with passengers.

  When it is seen that there is a number of passengers at one particular point on the platform, time should be given for these to disperse before attempting to move through them.
- 15. A second man must always be in attendance when more than two loaded trailers are attached to a trolley (not applicable at Dock Warehouses).
- 16. The second man accompanying must ride on, or walk behind, the last vehicle and keep a good look-out forward to prevent passengers coming into contact with the moving vehicles and see that packages do not fall off.

# WORKING ELECTRIC AND PETROL PLATFORM TROLLEYS AND TRAILERS-Continued.

- 17. Trollies must always have a driver in attendance unless put away in their recognised place. The trollies must be driven only by authorised men, and the driver must not on any occasion leave the trolley with the starting handle or connecting plug with the trolley unattended. If, for any reason, it is necessary to leave the trolley, the driver must, on all occasions, disconnect the handle or plug and place it in the receptacle on the vehicle provided for the purpose, so that no unauthorised person can start the trolley or engine in his absence.
- 18 Recognised parking places must be kept clean by the Department working the trollies. (Except at Dock Warehouses, where the Maintaining Department does this work.)
- 19. Trollies must not be left standing in the proximity of lighted lamps unless the petrol is turned off.
- 20. Trollies must not be run on the public highway unless provided with a registration number on the forepart and rear part of the machine, and carry the Road Fund Licence and Certificate of Deposit (not applicable to trollies running light between Docks Warehouses or between Docks Warehouses and charging or maintenance point).
- 21. When trollies are run on the public highway, only one trailer, whether loaded or empty, may be hauled at one time.
- 22. Engines must be run as quietly as possible, and not left running while trollies are standing unattended. When the engine is stopped, the petrol and oil must be turned off.
- 23. The Area Assistants will arrange for the trollies to be mechanically inspected each week.
- 24. Supplies of oil, waste, grease, etc., can be obtained from the Stores Department, Swindon.
- 25. Inspectors and Foremen must give special attention to, and supervise the movement of all trollies, and see that every care is exercised by the men in charge of them, irrespective of the Department to which the men are attached.

# AUTOMATIC BRAKES ON PLATFORM TROLLIES.

All platform trollies are fitted with the automatic brake and in order that the brake apparatus should be maintained in an efficient condition, the following arrangements must be carried out:—

The Station Master must depute a suitable member of his Staff to be responsible for maintaining the Brakes in an efficient condition.

The man appointed must test the brake on each trolley twice a week, and make any adjustment that may be necessary

If any defect in the brake gear cannot be remedied by the man appointed, the Station Master must at once apply to the nearest Carriage and Wagon Department Depot and a mechanic will be despatched to do what is necessary.

be despatched to do what is necessary.

Travelling artisans are available for the purpose of instructing the specially selected men at the stations in the adjustment of the brake, and for carrying out any minor repairs. The services of the travelling artisans can be obtained on application to the Divisional Superintendent or District Traffic Manager.

Stations where trollies are in use are supplied with two double-ended spanners ( $\frac{3}{8}$  inch and  $\frac{1}{2}$  inch) for the adjustment of the brake gear.

The following instructions must be observed by the men engaged in the repair and adjustment of the brake:—

(a) The brake must be adjusted so that when the handle is upright the blocks are sufficiently tight on the wheels to prevent the trolley running away when standing unloaded on an average station platform ramp, but when the handle is in a "pulling" position the blocks must be clear of the wheels.

(b) Adjustment must be made principally by means of the eyebolt between the double and single chains.

(c) Should the springs be loose, further adjustment can be made by means of the eyebolts to which they are attached.

(d) Particular care must be taken that the nuts are not left slack.

(e) Worn out or broken parts must be changed for new ones before the trolley is allowed to be used again.

Trollies must be kept under close observation by Station Masters, Inspectors and Foremen, and special care must be taken to ensure that there is always at least one man at the station who is competent to adjust the brake. The brakes should be tested at frequent intervals by the person in charge, who will be responsible for seeing that these instructions are carried out.

If it is found that the brakes are not being maintained in efficient condition, particulars must at once be reported to the Divisional Superintendent or District Traffic Manager.

Trollies must not be used while the brakes are defective.

Rubber tyres on trollies so fitted must on no account be permitted to wear down to such an extent that the rims of the wheels run on the platform paving.

# CONSUMPTION OF GAS AND WATER AT STATIONS, DEPOTS, &c.

All gas and water meters should be read daily in order that leakages may be detected and remedied with as little delay as possible. The reading should be recorded so that a comparison can be made of the consumption of gas and water one day with another, and increases should at once be enquired into

#### PLATFORM OIL LAMPS.

Whenever it is necessary for any of the outer cases or interiors of platform oil lamps to be sent for repairs, others must be obtained in their place before they are sent away, and the following instructions must be strictly observed:—

#### OUTER CASES.

These will be dealt with by the Engineering Department and the attention of the Permanent Way Inspector must be drawn immediately to any case of disrepair.

#### INTERIORS.

These will be dealt with by the Stores Department, Swindon. Immediately an interior is found to be defective, application must be made, by wire if necessary, to the General Storekeeper, Swindon for another to take its place, and on receipt of this the defective interior must be sent to the General Stores, Swindon, where it will be repaired and retained in place of that sent to the station.

#### SPARE CASES OR INTERIORS.

Any spare cases or interiors on hand at Stations must be sent to the Local Engineering Department Depot, or to the General Stores, respectively. Whenever cases or interiors are sent away they must be labelled, and the labels must bear the name of Station and date despatched, and an advice must be sent to the Engineering Department or Stores Department, as the case may be, stating by what train and on what day they are dispatched.

Care must be taken by all concerned in dealing with the cases or interiors to ensure their not being

damaged during their transit to or from the Depôt or Stores Department.

#### LAMPS SENT FOR REPAIRS.

To avoid damage to parcels, &c., in consequence of oil running out of the reservoir of lamps which are on the way to the shops for repairs, the staff must, in all cases, see that the reservoirs are emptied before lamps are put into a train.

#### STORAGE AND CONSUMPTION OF PETROLEUM.

The special attention of Station Masters, Goods Agents and other members of the Staff is directed to the necessity for strict economy in the use of Petroleum. The following Regulations must be strictly observed:—

- 1. Barrels and cans containing petroleum must never be placed in a room where there is a fire and, wherever possible, they should be placed in a building, shed or locker apart from the Station buildings.
- 2. Lamp huts should be isolated from all buildings and lines; at least 12 feet away, and further if practicable.
- 3. The quantity of petroleum required for daily use should be carefully estimated, so that a larger quantity than is actually required may not be taken from the place of storage, but, if after all the lamps have been trimmed any oil should remain over it must at once be returned to the store.
- 4. To avoid any risk of accident or failure, immediate application should be made to the General Stores, Swindon, to replace any cask or can found to be defective.
- 5. In the event of a complaint of the failure of petroleum supplied for long burning lamps a report should be made on the printed form provided for the purpose. (See Clause 14, page 284.) In other cases where the quality of petroleum supplied is found to be in any way inferior or the oil to be of bad colour, or to contain water or sediment, the matter to be at once reported to the Stores Superintendent, Swindon, to whom a sample of the petroleum should be sent, giving the number and brand marks of the barrel, together with the date of receipt.
- 6. At places where the consumption of petroleum is small and does not warrant the supply of a barrel at a time, the issue will be made from local centres as arranged by the Stores Department.
- 7. All petroleum should be drawn direct from the casks as required, and not in any case put into oil tanks. A tally must be kept of the quantity drawn from each cask, and the total compared with the quantity charged in respect of the cask. Any deficiencies should be promptly reported to the Stores Superintendent, Swindon. The petroleum should be used in the order in which it is received, and to ensure that this is done the date of receipt should be chalked upon the barrels; the chalk date being rubbed off when the empty barrels are returned.

#### STORAGE AND CONSUMPTION OF PETROLEUM-Continued.

8. Care should be taken to avoid damage to the barrels. Cases have occurred in which barrels have become dented by being carelessly handled, such as allowing a full cask to drop on to the ballast from the truck instead of using the skids that are supplied.

No unauthorised person must be allowed to have access to the lamp rocms or lamp huts, which must be kept locked when unattended, and the key placed in charge of a responsible person.

10. The special instructions in the petroleum folder as to the barrel account must be carefully adhered to. Barrels must be returned when empty in accordance with the directions issued by the Stores Department, and in no case must an empty barrel be appropriated for station use unless a Stores Order has been given for it and the Stores Superintendent has agreed to its retention.

# INSTRUCTIONS FOR REQUISITIONING, STORAGE AND DISTRIBUTION OF PETROLEUM FOR TRAFFIC AND GOODS DEPARTMENT—Page 280.

The existing instructions on page 280 to be cancelled and the following substituted :-

# INSTRUCTIONS FOR REQUISITIONING, STORAGE AND DISTRIBUTION OF PETROLEUM FOR TRAFFIC AND GOODS DEPARTMENTS.

Consumption data.

Petroleum is to be ordered on Requisition Form 6806 by the local Officer on the basis of data entered by the Station Master or Goods Agent in Book No. 8288.

Record of estimated requirements.

The book must be made up and forwarded to the local Officer before the 12th of the month preceding the quarter for which supplies are needed. The Station Master or Goods Agent will enter in this book the number of every type of lamp which will be IN USE during each month of the quarter against the "hours burning daily" figure which applies in each case. He must not enter any figures in the quantity columns. The local Officer, after reviewing the data in the light of Traffic requirements, will fill in the quantity columns and summary page, certify the book and despatch it to the Storekeeper, General Stores, Swindon, in time to arrive on or before the 24th of that month.

Form 6806.

On the first day of each month Station Masters—and Goods Agents also where barrels are supplied to their Depots—must send an advice on Form 6806 of stocks of petroleum on hand at all their storage points to the local Officer, who will complete and certify the returns for his district and despatch them to the Storekeeper, General Stores, Swindon, in time to arrive on or before the 4th day of the month. To obviate delays in the issue of petroleum these returns must be rendered punctually.

Petroleum will as a rule be supplied in accordance with the total gallonage entered by the local Officer, but the requisition will be reviewed in the office of the Storekeeper, General Stores, Swindon. Any differences between the quantities recommended for supply and those ordered by the Stores Department will be notified to the local Officer

in order that his records may be amended.

Local Officers'

The local Officer must keep a record of requirements at each station in his district for the purposes of compiling his requisitions and controlling consumption.

Supply of petroleum.

In order to be in a position to accept delivery when made by road, the Station Master or Goods Agent must ensure that sufficient empty drums are available to take the quantity ordered. Also, drums must be empty and free from water before they are refilled. The drums are of 40-gallon capacity and supplies are made in units of 40 gallons as far as possible.

Road delivery.

The Firm's delivery notes must be signed by the Station Master or Goods Agent and sent to Swindon immediately to enable invoices to be released.

Rail delivery.

When oil is delivered by rail, the charge notes (Form 4943A) for full barrels must be signed and sent to Swindon on the day of arrival and empty barrels forwarded by the first possible freight train, to the Contractor's Depot mentioned in the package note (Form 4943C). Delay in this respect increases the hire charges payable by the railways. Care must be taken to drain the petroleum from the barrels before being returned as empty, and to ensure that the correct inividual barrel identification numbers are entered on Forms 4943A and 4943C before they are forwarded to Swindon.

Storage.

Wherever possible oil must be stored in oil huts, the keys of which must be in the charge of a responsible person who, before going off duty, must deposit them in the Station Master's (or Goods Agent's) Office. Any issue of oil, in the absence of the person normally responsible for issues (whether the oil be stored in an oil hut or not) must be made in the presence of an Inspector or other authorised person, who, where an oil hut is in use, will see that the keys are replaced in the Office.

Where huts are not provided, barrels must be placed on timber supports or concrete slabs. On no account must they be stored in ashes as the sulphur content eats into the metal. Attention is also directed to the instructions as to storage of petroleum spirit contained in Schedule 4, page 11, of Circular No. 3678, Regulations for the Prevention and Extension of Fire.

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Records of subsupplies.

When petroleum is sent to a station for redistribution-to other stations where the consumption is too small for direct issue to be made, the Stores Department will notify the issuing and receiving stations on Forms 6840 and 8064 respectively. A record of these supplies must be kept by the issuing station in a book available for inspection by the Stores Department, indicating the number of gallons supplied to each station, and the date and train on which the oil is despatched. Similar records must be kept by the receiving stations.

If for any reason the full supply of oil authorised by the Stores Department to be issued during any month is not taken, or if, through emergency, more is supplied than authorised, an advice must be forwarded to the Storekeeper, General Stores, Swindon, by both the issuing and receiving stations, stating the quantity actually issued and received respectively for that month. These advices must be dispatched in time to reach Swindon not later than the 4th day of the following month, and will be the basis for adjusting the respective credits and debits (in gallons) for the stations concerned.

Standard receptacle.

No method of storage and issue other than the standard combination of drum, barrel stand and tap may be adopted without the local Officer's authority. The use of cans must be kept down to a minimum.

Precautions against leakage.

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and tap-plus Full barrels must always be laid with the bung uppermost. Bungs and tap-plugs are to be tightened up before the drums are left in case they may have been loosened during transit. All drums must be carefully watched for leakage and receive daily attention before and after tapping. (Any found to be in leaking condition to be emptied and returned with an advice of despatch to General Stores, Swindon, for repair or replacement.)

Barrel stands, spanners, washers, etc. (for releasing and tightening bungs) and tap-plugs, for use with steel drums, if authorised by the local Officer, are to be ordered on Requisition Form 224.

(G.A.25—1/50. S.S.—C/R.P.P./39242.)

Traffic manager in the ordinary way). In the case of Stations not served by a Stores van, the matches required should be purchased locally, particulars being included on the petty disbursement accounts forwarded to the Divisional Superintendent or District Traffic Manager for certification.

#### CLEANING PETROLEUM LAMPS.

All petroleum lamps, other than long burning lamps, must be cleaned, trimmed, and the tanks filled with the requisite quantity of oil at the Lamp Room Petroleum Lamps. The very day during daylight; are being taken to remove any sediment or water from the tanks and, after fillips, to wipe every part of the lamp perfectly dry, the cotton waste used for the purpose being renewed frequently.

Signal lamps, other than long burning lamps, must be fetched from the signal Petroleum posts daily and lighted on the cleaning bench a sufficient time before being taken from the lamp room to the signal posts to enable the flame to be properly regulated. In the case of signal lamps which are electrically repeated to the Signal Box, special care must be taken to adjust the flame so as to properly operate the repeater.

The wick when lighted should be regulated until it times steadily without smoking. The person lighting the signal lamps must on each occasion see that the spectacles and lenses of the signal are perfectly clean and have with him suitable waste for cleaning purposes.

cleaning purposes.

cleaning purposes.

Lamp wick should be stored in a perfectly dry place and care should be taken. Storage and to see that the wick is quite dry before it is placed in the lamps. The wick must be sufficiently long to reach the bottom of the tank. When a new piece of wick is required it should be atturated with oil before it is used, and the wick should be inserted at the top of the burner; the burner should then be placed in the tank and the wick finally lowered into it, care being taken that the wick is not traited. Wicks must be repowed when drity, and when too stort to reach the bottom of the tank.

The charred or burnt portion of the wick must be reproved each time the lamp is relighted, and the wick left clean and even.

In trimming flat wick burners the wick should be out level with the wick tule, and the corpers should be slightly clipped. With the Barton Burner the wick should be about level with the hollow of the burner.

Lamp cleaners and others who may have to clean and light lamps at or near careful not to leave oily waste or other inflammable material lying about, and, upon leaving, to make quite sure there is no smouldering or lighted matter left in or near the structure. The lamp huts, cleaning benches, are must be kept perfectly clean and no oil or dirty waste must be allowed to lie about on the benches or in the huts. Dry grass or any inflammable material adjoining the outside of lamp huts should be removed.

Special care must be used to avoid dropping burning matches on the floor or cleaning bench of the lamp hut. Only safety matches must be used.

GA31

#### SUPPLY OF METHYLATED SPIRIT AND BENZOLINE.

Small quantities of these spirits required for torch lamps and cleaning long burning signal lamps may be purchased locally, accounts to be sent to Divisional or District Officer for certification, and accounted for through the pay sheets. The quantity used and on hand should be kept at the lowest possible point.

### MATCHES FOR STATION USE.

Supplies of matches for Stations served direct by Stores vans from Swindon must be requisitioned from the Stores Department (on Forms No. 224, to be passed to the Divisional Superintendent or District Traffic Manager in the ordinary way). In the case of Stations not served by a Stores van, the matches required should be purchased locally, particulars being included on the petty disbursement accounts forwarded to the Divisional Superintendent or District Traffic Manager for certification.

# LIGHTING AND EXTINGUISHING SIGNAL LAMPS.

At places where signal lamps are not lighted during the summer months, the interiors must be removed from the lamp cases, and taken to the lamp huts where they must be emptied, cleaned and

placed on a shelf.

Referring to Rule 73, where there is more than one Signal on a post, or where there are two or more posts side by side, or quite near to each other, all the signal lamps must be kept burning during the whole of the time it is necessary to keep any one of them burning, in order that a Driver may, by its relative position, as accurately select the Signal which applies to him during the night as he can when the whole of the arms are visible during the day.

The lighting and extinguishing of signal lamps at Public Level Crossings must be dealt with in accordance with Rule 102 of the Book of Rules and Regulations.

At intermediate Sidings which are not block posts, the signal lights must be dealt with as follows:—

(a) Places at which a man is stationed.

Signals must be lighted in accordance with Rule 73 and worked in accordance with the Regulations for working Goods Lines, where the absolute Block Telegraph is not in operation, shown in this Appendix. On single lines worked by Train Staff and Ticket the train calling at such places may carry either a Train Staff, or a Ticket if Tickets are in use.

(b) Places to which a man is sent with the train, or meets it at the Siding.

(c) Places at which there is no man stationed at the siding, or

the Guard of the

train.

Signals must be lighted in accordance with Rule 73, and worked in accordance with the Regulations for working Goods Lines, where the absolute Block Telegraph is not in operation, shown in this Appendix, except where there is any practical difficulty in lighting, or putting them out, in which case they need not be lighted if so authorised by the Divisional Superintendent or District Traffic Manager, who will make special arrangements for the protection of the train whilst it is occupied at the

Except in the case of sections worked by Pilotman, trains calling at such sidings must in all cases stop dead at the Signal Box at the commencement of the section in which the siding is situated, and the Signalman must be told by the Guard that the

train has work to do at the siding.

At sidings where it is arranged for the signals to be lighted in readiness for the train, the Guard must put them to "Danger" as soon as his train arrives at the siding, and put them to "All Right" again immediately before he leaves. If there is any practical difficulty in lighting or putting out the signals, they need not be lighted if so authorised by the Divisional Superintendent or District Traffic Manager, meeting or accompanying the train, and the work is done by who will make special arrangements for the protection of the train whilst it is occupied at the siding.

Except in the case of sections worked by Pilotman, trains calling at such sidings must in all cases stop dead at the Signal Box at the commencement of the Block section in which the siding is situated, and the Signalman must be told by the Guard

that the train has work to do at the siding.

On single lines, worked by Train Staff and Ticket whether the Signals are LIGHTED OR NOT, the train calling at the Siding must carry the Train Staff, or be accompanied by a Pilotman.

#### "LONG BURNING " SIGNAL LAMPS.

In addition to the General Instructions on page 281 as to the precautions to be taken in connection with the cleaning and trimming of Petroleum Lamps, the following regulations must be strictly observed in dealing with "Long Burning" Signal

Lamps :-

1. The lamps must be taken to the lamp room every seventh day to be cleaned and trimmed. Unless instructions are issued to the contrary, on no account must the lamps be cleaned and trimmed at the foot of the signal post. The lamps and the feeders used for filling them must be kept perfectly clean. The Signal Lampmen, who attend to the signal lamps in districts embracing several groups of signals must trip the lamps in each group on the day appointed, and must not vary the day except by special instruction. special instruction

Method of

When and where lamps are to be cleaned and trimmed.

2. When retrimming, the charred wick must be entirely cut off, and any carbon which may have collected in the burner code must be removed. In removing crust from the wick and cone the burner which be taken out of the lamp and held sideways so that no charred substance shall fall into the burner and the wick must be cut square by means of scissors. When this has been done the top of the wick must be slightly spread with the inger in order to broaden the flame and eliminate smoking. The burner must be well cleaned before it is replaced in the lamp. The lamps must be quite filled with oil, and the outsides afterwards wiped dry. The back and front classes of the inner lamp cases also should be carefully cleaned with cotton waste on each occasion. Should the inside become stained with a brownish deposit, cotton waste moistened with a little methylated spirit will remove the discoloration.

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oloration.

Long Burning Signal Lamps-Pages 282-285.

The instructions under this heading to be deleted and substituted by the following:-

### LONG BURNING SIGNAL LAMPS-EQUIPMENT.

The undermentioned articles may be ordered by Signal Lampmen from the Stores Department on Requisition Form No. 224 through the Station Master at their Home Station, and arrangements should in future be made accordingly:—

Glasses for Lamp Case Interiors.

Standard Pattern Interior Circular Tank.

Front,  $4\frac{3}{4}'' \times 4\frac{1}{2}''$ . Back,  $3\frac{1}{4}'' \times 2\frac{3}{4}''$ 

Old Pattern Interior Shallow Circular Tank.

Back and Front,  $3\frac{7}{8}" \times 3\frac{3}{8}"$ .

Semaphore 9" and 11" Cases.

Back and Front, 51 × 5". (Bottom corners cut away.)

#### "LONG BURNING" SIGNAL LAMPS-Continued.

Oil containers to be com-pletely emptied at commence ment of eve alternate month

Of to be drawn direct from steel barrels.

Washers for taps of barrels and stoppers of oil containers.

Unsatisfactor of to be returned at once to Stores Department.

Cleanliness essential.

round in turn,

Spare Lamps to

Station Masters and District Inspectors to supervise work.

Failures to be reported on Special Form

During the first week in each alternate month every lamp must be completely emptied before being refilled with oil, in order to get rid of any sediment, water, &c., which may accurate in the oil containers. This practice, if regularly followed, will keep the wick clean, and promote the satisfactory burning of the lamps. The oil emptied out of the lamps should be used for cleaning purposes, etc. Spare two-gallon cans may be ordered in which to collect his oil and hand it over to the station staff.

9 Every effort should be made to prevent water, dust o dirt of any kind becoming mixed with the oil.

No others oil than the special oil provided for the purpose must be used in these lamps, and the oil must be drawn direct from the steel barrels or cans where used, in the trib supplied to enable this to be done. Taps must be properly tightened-up in the barrels when in use with a view to preventing loss of petroleum by leakage. For this purpose small lother washers to fit the taps, and the stoppers of oil containers, may be obtained direct from the Tavelling Stores Van Attendants, or on application to the General Storekeeper, Stores Department, Swindow.

If it is found that the petroleum is any barrel is not CLEAR or, although apparently clear, does not burn well, the barrel should be put on one side and the Stores Superintendent must at once be communicated with so that the matter may be investigated and, if necessary, fresh supply obtained. The circumstances must also be reported to the Superintendent of the Line. When returned to Stores the barrel must be labelled with the name of the sending Station and the date of the report complaining of the oil. It such cases special arrangements must be made by the Station Master for more frequent examination of the lamps until the difficulty has been removed. removed.

removed.

10. WART OF CLEANLINESS WILL CAUSE LAMPS TO BURN UNSATISECTORILY.

IT IS, THEREFORE, OF VITAL IMPORTANCE THAT ABSOLUTE CLEANLINESS SHOULD BE OBSERVED IN ALL OPERATIONS AFFECTING THE LONG BURNING LAMPS.

11. Damage will also occur if chisels are used to unscrew the bungs (both the side bung and end tap plug), and for this purpose the standard pattern barrel spanner which is fitted with a box end, must be used. Should a spanner be found defective or fitted with a chisel end it should be sent to the Stores Department, Swindon, accompanied by a special requisition (Form No. 224) so that a regulation spanner can be supplied. can be supplied.

The spanner must be used for screwing and unscrewing the bungs of the parrels, and the side bung and tap plug must be properly secured when a barrel is empty to avoid moistare entering the barrel.

avoid moistaire entering the barrel.

12. The Spare lamps kept in the lamp huts must be used round in turn with the other lamps, and one of each kind, viz., Ordinary, Repeater, and Disc, must always be left trimmed and ready for immediate use in the Office of the Station Master or Inspector, or signal box at each station. The Lampman must collect these lamps on his next round and, having satisfied himself that they are in order, must bring them into use and replace them with others which have been taken out of signals and retrimmed. The object of this arrangement is that in the event of the failure of a lamp at any time, another ready-trimmed lamp will be instantly available to replace the lamp which has failed.

13. Station Masters and District Inspectors must pay special attention to the arrangements in connection with the trimming and lighting of the long burning

arrangements in connection with the trimming and lighting of the long burning lamps, and satisfy themselves that the work is being properly carried out and that the lamps give a good light.

14. If the event of any long burning lamps going out or failing in any way, a report on form 5030 must be forwarded the same day to the Divisional Superintendent, District Traffic Manager or Dock Manager, and a copy sent of the same time to the Superintendent of the Line or Chief Docks Manager, and to the Stores Superintendent, Swindon, so that prompt steps may be taken to investigate the cause of failure.

The lamp or lamps that have failed must be taken charge of by the Station Master who must retain them in his possession in the same condition as when lamp or lamps failed until they have been inspected by the Superintendent of the Line's Lamp Inspector or a representative of the Stores Superintendent.

When sending lamps to Signal Department for repairs they must be despatched intact, i.e. with tank, burner, feeder screws and reflector complete, and be addressed to the the Signal Inspector of the district concerned.

(G.A.15-12/44 L.K.1)

Old Pattern Interior Shallow Circular Tank. Back and Front,  $3\frac{7}{8}'' \times 3\frac{3}{8}''$ .

Semaphore 9" and 11" Cases.

Back and Front,  $5\frac{1}{2}'' \times 5''$ . (Nottom corners cut away.)

ordered by District Lampmen.

Articles to be 15. The undermentioned articles may be ordered by Signal Lampmen from the Stores Department on Requisition Form No. 224 through the Station Master at their Home Station, and arrangements should in future be made accordingly :-

GLASSES FOR LAMP CASE INTERIORS. Standard Pattern Interior Circular Tank.

Front, 43" × 41". Back, 31" × 23".

Old Pattern Interior Shallow Circular Tank. Back and Front, 37" × 33".

Semaphore 9" and 11" Cases. Back and Front,  $5\frac{1}{2}'' \times 5''$ . (Nottom corners cut away.)

#### GLASSES FOR LAMP CASE INTERIORS.—Page 285.

Delete the entry in respect of Route Indicator and Permanent Speed Indicator Lamps and insert the following:—

Route Indicator Lamps
47/n. x 5½n.

Permanent Speed Indicator Lamps Front glasses 55in. x 5in. Side glasses 3in. x 2½in.

(G.A.30 Op.—9/54 LKI/10676/26).

# INSTRUCTIONS FOR USING PARAFFIN VAPOUR LAMPS—"TILLEY" TYPE-Pages 285-287.

The instructions under this heading to be deleted and substituted by the following:-

# PARAFFIN VAPOUR LAMPS—"TILLEY" TYPE—SPARE PARTS.

The Operating Department Staff can order the following, but care must always taken to quote the correct number of the respective part when ordering:—

Part No. Name of Part.

123-90 Globes, Clear (outside lighting).

123-91 Globes, Clear and Frosted (inside lighting).

133-40 Mantles, Inverted, Large, No. 2.

133-42 Mantles, Inverted, Small, No. 1, Indoor Lamp (I.L. 37).

133-39 Mantles, Inverted, Floodlight Projector.

133-79 Washers, Vaporiser, Black, No. 153.

133-19 \*Caps, Screw, Filler "F."

133-25 \*Connections, Pump, 15".

133-37 \*Lighters, Paraffin.

Part No. Name of Part.

133-46 \*Pumps, without connections.

133-74 \*Vaporisers, complete.

133-81 Washers, XN Cock Black, No. 160.

119-31 \*Cans, Oil, with spout.

119-165 \*Angle Funnels, Tin, Gaussianer,

133-22 \*Carriers, Wire, "A," for P.L. = and 56 lamps.

133-23 \*Carriers, Wire, "B," for a other lamps.

130-10 Balls, 82". No. 147.

109-31 Special Brush for cleaning burne

Spare Parts must be kept by the person in charge of the place where the lamp are installed, i.e., Station, Yard, Depot or Signal Box.

(G.A.31-7/56 B.R. 29611)

<sup>\*</sup> These items are obtainable only on Repairs Requisitions (Form No. 224), and the old articles must be returned to Swindon.

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TYPE-

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k Black, No.

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Gauze Tin, ," for P.L. 55

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#### STATION INSTRUCTIONS.

GLASSES FOR LAMP CASE INTERPORS-Centinued.

Repeater 9" and 11" Cases. Back and Front,  $4\frac{3}{4}$ "  $\times$   $4\frac{3}{4}$ ".

Route Indicator and Permanent Speed Indicator Lamps.

Taff Vale Section B "Type.

51 × 12 thin.

Taff Vale Section "C" Type.

42 × 4½" thin.

Barry Section Signal.

Curved,  $5_4^3$  high  $\times$   $4_8^3$ 

Barry Section Disc. Curved, 4½" high × 25.

Rhymney Section.  $2^{3} \times 3^{3} \times 10^{4}$  thin.

Rhymney Section. Elevated Disc,  $5\frac{1}{2}'' \times 3\frac{7}{8}''$  thin,  $5\frac{1}{2}'' \times 3\frac{1}{8}''$ .  $5\frac{1}{2}'' \times 3\frac{3}{8}''$ .

B. & M. Section Signal. 55"

Wicks for Ordinary and Disc Long Burning Lamps.
Ditto for Standard "Serex" (T.V.) Lamps.
Feeding, Bound, 11" long.
Wicks for Repeater Long Burning Lamps.

Burning, 12'' by  $\frac{3}{8}''$  wide. Feeding, 6'' by  $\frac{1}{2}''$  wide. Tins, Safety, for Methylated Spirit. Scissors Lamp.

Tins, locket, wick.
Polish, Metal, for cleaning reflectors.
Special attention is directed to the fact that motal polish instead of whitening

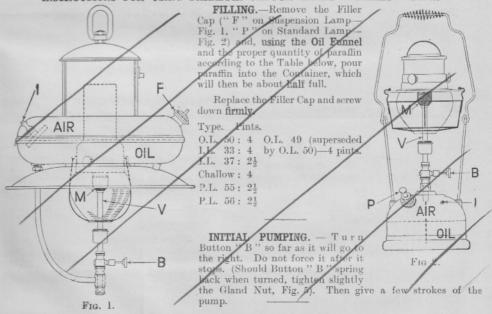
may be ordered for cleaning tamp reflectors.

10. Leather washers should be used on the screw stoppers of these lamps, and if any are required at any time they can be requisitioned by Station Masters through the District Signal Inspectors.

#### LONG-BURNING LAMPS FOR SPEED INDICATORS AND ROUTE INDICATING SIGNALS.

The burners of these lamps are 5" wide, and wick of similar width is supplied for use in the lamps.

#### INSTRUCTIONS FOR USING PARAFFIN VAPOUR LAMPS-"TILLEY" TYPE.



### INSTRUCTIONS FOR USING PARAFFIN VAPOUR LAMPS-TILLEY"

TYPE-Continued.

WICK

Fro. 4.

LEDGE

FIXING MANTLE.—When it is necessary to fix a new mantle "M" (see Fig. 3 for detail) stretch the top over the rim at a" so that it rests in the groove. Then stretch the bottom into the groove at "c."

The mantle should not be twisted; it will shape itself correctly when the

lame is lighted.

## PREHEATING BY SELF-FILLING LIGHTER.

(See Fig. 4).

1. Slide the lighter on to the vaporiser "V" as shown. Slacken the cap nut "L" and press the end of the feed tube "A" into it as far as it will go. Then tighten the cap nut firmly with the fingers

2. Avoid a draught whilst lighting.

3. Lift the perforated hood by means of the handle "H," and allow the projection in the handle to rest on the rim of the cup "£."

4. Open the tap "E" just sufficiently to allow the oil to flow slowly into the Cup "C," and shut off when the oil reaches the level of the ledge marked with an arrow in the illustration. (See N.B. below.)

5. Apply a light to the wick, and when the flame is burning evenly all round, lower the hood slowly on to the cup "C" to void extinguishing the flame. After about a minute, the flame should reach the mantle.

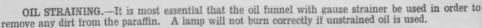
6. Let the flame burn until the paraflin in the wick is nearly con.

sumed, then gradually turn on the vapour by means of the button "B."

N.B.—The cup" C" should not be filled completely, otherwise
the paraffin will oreep over the edge, causing smoke. When filled up
to the ledge, there is ample paraffin for heating the burner. If, however, too small a charge has been used, the cup may be roulled when the first charge is exhausted.

FINAL PUMPING.—When the lamp is properly alight, continue pumping until the centre part of Indicator "I" is level with its outer

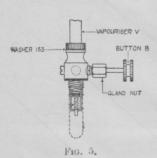
EXTINGUISHING .- Turn button "B" as far as it will go to the right, when the lamp will immediately go out. The air pressure need not be released,



BURNING HOURS .- Once the lamp is half filled with oil it will burn for the following time :-

O.L. 50	 	32 hours	 	Outdoor Lamp.
Challow	 	32 ,,	 	,, . ,,
I.L. 33	 	22 ,,	 	Indoor Lamp.
1.L. 37	 	20 ,,	 	Indoor "General Utility" Lamp.
P.L. 55	 	20 ,,	 	Outdoor "General Utility" Lamp.
DI 56		20		

Occasional pumping is sometimes necessary. There are some O.L. 49 lamps on the G.W. System which are being superseded by the O.L. 50 type. O.L. 49 Lamps must not be used indoors.



DIRT IN VAPORISER.—To clear dirt which may collect in the hole at the top of vaporiser "V" turn button "B" backwards and forwards quickly; this can be done while the lamp is alight. Should the lamp "light-back" inside the burner when this is done, causing a roaring noise, turn button "B" again and re-light. (See Fig. 5.)

#### IMPORTANT.

CLEANING BURNER. - When fixing a new mantle, unscrew the brass cap at the top of the Burner and thoroughly wipe away dust which has collected inside. See that the small holes at the bottom of the burner are free from dirt and obstruction.

RISING and FALLING of the light is an indication that the oil is nearly exhausted.

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The following sentence to be added to the Note at the end of paragraph (C.A.23—7/49. L.K.1/6717/Gen.4.)

# LIGHTING OF LAMPS IN LAMP HUTS.

The number of lamps which may be lighted at a time in a lamp hut must not exceed 4. Where more than this number are required to be dealt with they should be dealt with in batches not exceeding 4 in number. Except where special authority is given the trimming of paraffin vapour lamps must only be undertaken in a lamp hut or a lamp room properly constructed of either brick or corrugated iron and having a brick or concrete floor. The lamps must not be left unattended.

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### DEFECTIVE CONTAINERS.

Should a leak be discovered in the container of a paraffin vapour lamp this must be reported immediately to the Engineering Department and under no circumstances must the lamp be used. (G.A.3.—12/37. L.K.1/4391/23.)

## INSTRUCTIONS FOR USING PARAFFIN VAPOUR LAMPS-"TILLEY" TYPE-Continued.

FIXING AND RENEWAL OF VAPORISERS .- Vaporisers are to be fixed with one washer only finger-tight; pliers are not to be used. Should pre-heating become difficult when the lamp has been in use for a considerable time, this can usually be traced to the formation of a carbon deposit inside the vaporiser. The vaporiser is then to be replaced.

MAINTENANCE PROCEDURE.—The exchange of globes, mantles, vaporisers and vaporiser washers will be undertaken by the Traffic Department Staff, who are instructed in this work by the Lighting Inspectors when lamps are installed.

# ALL OTHER REPAIRS ARE TO BE REFERRED TO THE DIVISIONAL ENGINEER.

In all cases the defective lamp to be forwarded complete, with name of Home Station shewn on label.

DAMAGE.—Breakage of the nipples on the filler caps used with these lamps is caused by screwing the pump direct on to the nipple. The proper connection must be used in every case when employing the pump with these lamps.

SPARE PARTS .- The Traffic Department staff can order the following, but care must always be taken to quote the correct number of the respective part when ordering :-

Name of Part. 123-90 Globes, Clear (outside lighting). Globes, Clear and Frosted (inside 123-91 lighting). Mantles, Inverted, Large, No. 2. 133-40 Mantles, Inverted, Small, No. 1, 133-42 Indoor Lamp (I.L. 37).

133-39 Mantles, Inverted, Floodlight Projector.

133-79 Washers, Vaporiser, Black, No. 153.

133-19 \*Caps, Screw, Filler "F." 133-25 \*Connections, Pump, 15".

Name of Part. Part No. 133-37 \*Lighters, Paraffin.

133-46 \*Pumps, without connections.

133-74 \*Vaporisers, complete.

119-31 \*Cans, Oil, with spout. 119-165 \*Angle Funnels, Tin, Gauze strainer. 133-22 \*Carriers, Wire, "A," for P.L. 55 and

56 lamps. "B," for all other 133-23 \*Carriers, Wire,

lamps. 130–10 Balls, %, No. 147.

109-31 Special Brush for cleaning burner.

\* These items are obtainable only on Repairs Requisitions (Form No. 224), and the old articles must be returned to Swindon.

Spare Parts must be kept by the person in charge of the place where the lamps are installed, i.e. Station, Yard, Depot or Signal Box.

# STEEL PETROLEUM BARRELS.

Damage to bungs of steel petroleum barrels, is caused by the use of chisels to unscrew the bungs (both the side bung and end tap plug) instead of the standard pattern barrel spanner, which is fitted with a box end and the spanner is to be used for screwing and unscrewing the bungs of the barrels.

A defective spanner or one fitted with a chisel end should be sent to the Stores Department, Swindon, accompanied by a special requisition (Form No. 224) so that a regulation spanner can be supplied.

The side bung and tap plug must also be properly secured when a barrel is empty.

# SPARE LAMPS FOR SIGNALS, LEVEL CROSSINGS, &c.

The Signal Department is to be requisitioned when spare oil lamps are required, either for gas burning signal lamps or for signal lamps in which oil is ordinarily used, and the Signal Department will supply them without debiting the Traffic or Docks Departments with the cost. The requisitions are to be sent to the Signal Department by the Divisional Superintendents, but the number of spare signal lamps must not be increased more than is absolutely necessary.

When gas or electricity is used in the signal lamps, spare oil lamp tanks and petroleum burners must be kept in the proportion of one-third of the total number of gas burning or electrically lighted

ed.

The Signal Department will carry out all repairs to signal lamps, whether spare or otherwise, but the Superintendents will be responsible for seeing that these lamps are kept in repair, and sent to the Signal Department's Inspector for the purpose when necessary. When sending lamps for repairs they Signal Department's Inspector for the purpose when necessary. When sending lamps for remust be forwarded intact, that is, with tank, burner, feeder, screws and reflector complete.

The Traffic or Docks Departments will continue to provide and see that all lamps which may be

necessary for lighting signal boxes where gas is not provided are properly maintained.

All lamps used in connection with the train staff exchanging apparatus are provided and maintained by the Signal Department, and the local Signal Inspector should be advised of any repairs which may be required to same.

With respect to lamps required for lighting level crossings, the Engineering Department will provide and maintain the posts and outer cases, and the Traffic or Docks Departments will be responsible for the interiors.

#### FIRE APPLIANCES.

Where fire appliances are provided the Station Master, Goods Agent, Fire Warden or person in charge must see that the regulations for the guidance of the staff are thoroughly understood by all concerned, and that they are carried out when the necessity arises.

The hydrants, stand pipes and hose must be tested not less than every three months, and all appliances after being used should be thoroughly dried before being replaced in the receptacles provided

for their accommodation.

Where fire buckets only are supplied, the Station Master, Goods Agent or person in charge must appoint a man to keep the buckets filled with water, and examine them daily and satisfy himself they are kept ready for use and in good order. The water in the buckets should be changed at least once a month, and during frosty weather measures must be taken to prevent the water becoming frozen.

Fire buckets, dip tanks or tubs, which normally contain water, are to be kept clear of ice during frost. In severe frost, buckets which are in exposed situations should be placed in sheltered or warm positions where practicable. Where this cannot be done, the buckets should be emptied and placed near the most suitable taps or hydrants for quickly filling them in case of fire.

Station Masters, Fire Wardens or other persons in charge are specially requested to see that these arrangements are applied whenever weather conditions suggest the desirability of adopting this course. (See General Manager's Circular No. 3136, dated October, 1930, for full details as to fire arrange-

#### REPORTING OF FIRES.

All cases of fire, whether upon the railway or at the Company's premises in city or town, must be immediately reported to the superior officer concerned, so that, in those instances where insurance is involved, a proper notice may be promptly given to the Insurance Company.

See also General Manager's Circular No. 3136, dated October, 1930, in regard to reporting fires immediately by wire to the Secretary, Paddington Station, and Superintendent of the Line.

#### CONVEYANCE OF MOTOR VEHICLES BY RAIL.

Petrol may be left in the tanks of Motor-Cars conveyed by Passenger trains providing that:-

(i.) The flow of petrol to the carburettor has been stopped.

(ii.) All pressure has been released from the tank.

(iii.) The Motor Car is free from leakage of petrol. Where the flow of petrol to the carburettor is stopped by means of a shut-off cock, the engine must be run by the person in charge of the car until the petrol in the carburettor is exhausted and the engine stops automatically.

When, however, it is necessary for petrol or other inflammable liquid to be discharged from the tanks of Motor Cars the special attention of the Company's Staff is directed to the precautions necessary in connection therewith.

Vapour from Petrol is highly dangerous and liable to be ignited by a flame at a considerable distance. Being heavier than air the vapour sinks, and lies close to the ground or in any pit or depression.

When practicable the liquid should be discharged from the vehicles by daylight. If the operation

cannot be performed by daylight, a man must be specially detailed to put out any lights, including hand lamps and those on vehicles, other than electric lights, within 15 yards, which are less than 8 feet from the ground level, warn anyone approaching with a light of any kind, and stop persons smoking within the prescribed distance. No lighted lamp must be placed on the ground or platform within 20 yards of a motor vehicle from which petrol is being withdrawn.

Where the transit of cars from or to a station is frequent, a special point at which cars must be loaded or unloaded must be arranged in conjunction with the Road Transport Department. Where no special spot is appointed the discharge must be made in an open space and not in proximity to any building or inflammable material, and the liquid should not be withdrawn from the vehicles while standing in a position where draught is likely to blow the vapour into any pit or depression. Discretion must be used where circumstances are exceptional, e.g., if the movement of petrol takes place between high walls where the vapour is not readily dispersed, or if there is any kind of ditch along which the vapour would be readily carried. The direction of the wind should also be noted.

At places where the Company have no Agent appointed to do the work, the Company's servant in charge of the loading of the vehicle will be responsible for seeing that all petrol withdrawn is taken away from the station premises. On no account must it be thrown down a drain.

Conveyance of Motor-Cars with Spare Wheels attached. - When a spare wheel forms part of the equipment of a motor-car, in addition to utilising the holder provided on the motor-car, senders must secure the wheel by a strap or other equally suitable fastening, or remove and place it in the body of the car in such a position that it cannot fall out.

#### HANDLING OF MOTOR CYCLE TRAFFIC.

The following instructions must be observed when dealing with motor cycle traffic:-

(a) In loading, the front wheel must be placed on the van floor, one loader to hold handle bars and another to lift rear wheel into van.

(b) When more than one machine is placed in a van, they must be loaded front to rear alternately.

(c) The machines must not be placed on their stands when loaded in train vans, as the oscillation is likely to cause them to fall,

(d) When unloading, the rear wheel must be lowered on to the platform by one loader, another loader holding the handle and lowering the front wheel.

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loader,

# HANDLING OF MOTOR CYCLE TRAFFIC.—Page 288.

The following to be inserted as clause (e):-

At stations where special motor cycle loading boards are provided they must always be used for loading or unloading motor cycles into or out of vans. The boards must be carefully handled and after use they must be returned to their storage point.

# PROTECTION OF GAS AND WATER PIPES AND SANITARY FITTINGS AGAINST FROST.

During frosty weather full use must be made of stop valves where provided. These must be turned off during the night and, if the frost be severe, during the day, and tanks and service pipes

Gas and water service pipes, pumps, etc., whether for w.c.'s, urinals, or taps, must, wherever practicable, be protected with bands of hay or straw. Where fire devils can conveniently be used Station Masters must see that there are sufficient available and that a supply of smokeless coke is on hand. Supplies must not, however, be ordered locally unless authorised by the Stores Department.

Open louvre windows must be covered with canvas, sacking, or other suitable material.

Straw, old sacks, or other suitable material should be placed over flush cisterns, and stop cock

hydrant and meter pits must be filled with straw.

When the water in a tank or pipe freezes the assistance of the Engineering Department should be obtained if practicable, but where this is not immediately available care must be taken to avoid damage in thawing. Hot water should be used for the purpose as far as practicable. In some instances half-burnt waste has been found choking the flush pipes, and ball cocks and valves have been damaged in breaking ice, and these practices must be avoided.

#### ELECTRIC TRAIN LIGHTING-DYNAMO BELTS.

In the event of dynamo belts of electrically-equipped coaches being found on the line, they should be forwarded to the Carriage and Wagon Works Foreman in the Division in which they are found.

#### ARTICLES FOUND ON LINE.

It is the duty of the permanent way staff to collect any loose ironwork that may be found on the line, and all belonging to engines, carriages or wagons must be sent to the nearest Locomotive or Carriage Depot, properly labelled, showing the date found, the nearest mile post and station, and whether

picked up on the Up or Down Line.

When any other article is found on the line it must be conveyed to the nearest station, and the circumstances immediately reported to the Station Master, Goods Agent or Inspector, who must state in the report to his Superintendent, Goods Manager or Docks Manager, as the case may be, the place and time at which the article was discovered, and whether it was found on the Up or Down side of the Line, so that the vehicle from which it fell may be traced, and the cause at once ascertained.

# REGULATIONS FOR DEALING WITH MAIL BAGS ON NON-STOPPING TRAINS.

1. The G.W.R. mail trains by night are distinguished by the MAIL VEHICLE carrying white lights at the side by the mail apparatus, and the L.M.S. Mail by a small white light showing towards the engine, so that the muil train can be easily recognised before it arrives by the Signalmen and Post Office Officials attending to the mail apparatus. Persons in charge of stations from which mail rains are started must see that the mail train is so equipped.

2. It sometimes happens, however, that from various causes the mail trains are running out of course, or are divided, and in order that the practice in such cases may be as uniform as possible,

the following regulations must be carried out :-

3. At those places at which electrical or mechanical gongs are fixed between the signal box and the hut near the mail apparatus, the gong must be worked for every mail train carrying the mail apparatus during the time the signal box is open, the following boll code being used:—

 Down Mail approaching
 ...
 ...
 2 beats.

 Up
 ,,
 ...
 ...
 4
 ,,

 Cancel Signal
 ...
 ...
 8
 ,,

and on each occasion when the Mailman comes on duty at the hut a test signal of one beat must be exchanged between the signal box and the mail hut.

4. The bell signals must be sent steadily by the Signalman, and must be returned by the Mailman in each case to denote that they are understood. If incorrectly returned, the Signalman must send the signal again until it is correctly returned.

At each place a time at which the Mailman must be at the apparatus must be fixed upon with

the local Postmaster so that the former may be present to receive the gong signal from the box.

Where telephones are provided for communicating with the mail apparatus Attendant the Mailman must inform the Signalman on each occasion when he comes on duty by calling him up on the telephone, and the Signalman will announce the approach of the mail train by passing the appropriate one of the following messages:—

Down Mail approaching. Up Mail approaching.

My advice in regard to Up Mail approaching is cancelled.

These messages must be repeated by the Mailman to shew that they are understood.

#### REGULATIONS FOR DEALING WITH MAIL BAGS-Continued.

5. Immediately the "Is Line Clear?" signal (or "Train entering Section" signal where that is early enough) is received from the next station in circuit in the rear, for a train which carries the mail van, the Signalman must send the number of beats on the bell, as per bell code, to the mail hut, and the man in charge of the latter must at once return the same number of beats to the Signalman.

6. The bell signal sent to the mail hut will simply be a warning to the Mailman that the mail train is approaching, but in no way is it to serve as a guide as to the time he shall prepare the pouches or mail apparatus, as the time occupied by the train in reaching the latter will depend upon the station from which the mail train is signalled.

7. When the signal, as per bell code, has been sent to the mail hut for an up or down train, and it is found to be necessary to cancel the signal, the Signalman must send the "Cancel" signal to the mail hut, which signal must be acknowledged by being returned. If the Mailman has already set the apparatus when he receives the "Cancel" signal, he must place the apparatus in its normal position clear of the main line.

8. When the mail trains are running in duplicate, Stations starting the first part must telegraph to the places at which the mail apparatus is fixed, stating whether the mail van is on the first or second part of the train, and if on the second, how late it is running. A telegraph message must also be sent to the mail apparatus station in those cases where the mail is running late and another passenger train is allowed to go in front of the mail and in its running times. The message must be despatched by the officer in charge of the station from which the other train starts in front of the mail train.

Such messages must be shortened, as under :-

When mail is divided send the words "Mail first part (—) mins. late."

Or "Mail second part (—) mins. late."

When mail is late and out of course, send the words:—
"Mail (—) mins. late (—) Train in front."

9. Station Masters or persons in charge of stations, excepting places covered by clause 3, should arrange with the local Postmaster for the Mailman who sets the mull apparatus to call at the signal-box whenever possible, and ascertain how the mail trains are running, and whether divided or not, and which part of the train the mail van is on.

10. At those places where gongs are not fixed, but where it is possible, owing to the short distance between the mail hut and the signal-box or station, for the Officer in charge of the station to advise the Mailman when the mails are running late or in duplicate, this must always be done, and particular care must be taken to make the Mailman clearly understand which part of the train is conveying the Mail Van.

11. When the mail trains are divided by special notice (either printed or written), Station Masters or persons in charge must make the necessary arrangements with the local Post Master to ensure the apparatus being set for the proper train, the instructions in Clause 8 being observed in addition.

12. At those places where gong communication is not in existence, and which are too far from the station to be advised when trains are running in duplicate without written or printed notice, it will be the duty of the Post Office attendants to watch the trains and set the apparatus for the proper one, but in those cases in which the mail trains are divided by notice, the instructions laid down in Clause 11 must be strictly carried out.

13. These arrangements do not in any way relieve the Mail Attendant from responsibility in adopting every precaution for the safe working of the apparatus, neither do the Company accept any responsibility for any damage done to the apparatus, nor delay that may occur to the despatch, or receipt of the mails, which may arise from any failure to advise the Mail Attendant as to the running of the mail trains.

14. Whenever it is necessary for a train that picks up or sets down mail bags by means of the apparatus, to be diverted from the line upon which it usually runs and for which the apparatus is fixed, the Station Master or other person in charge of the station where mail bags are thus dealt with must take steps to stop the train for the purpose of making the exchange of mail bags by hand, instead of by the apparatus. In all such cases the Post Office Officials must be previously advised if it is possible to do so.

### LAMPS ON POST OFFICE WAYSIDE MAIL BAG APPARATUS STANDARDS.

Brackets have been fixed to the standards of the Post Office mail apparatus, on which a lamp shewing a white light will be exhibited at night, when the arm of the apparatus is in working position. The lamp is about five feet above the ground.

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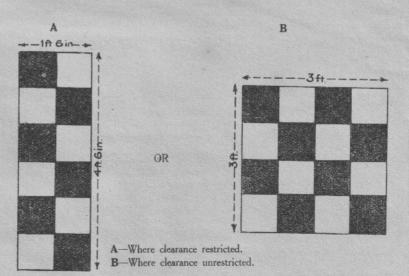
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lamp ition. Reference to the following to be made on page 290:-

### MAIL BAG APPARATUS.

Enamelled plates with 9 inch yellow and black squares, as per diagram below, are erected vertically at ground level, about 20 yards on the approach side of Post Office mail apparatus.



The enamelled plates, which are illuminated at night when pouches are swung towards the Line, are provided in order to give warning to Enginemen and others of the position of the mail apparatus.

(G.A. 5.—2/39. L.K.1/4326/13.)

# TELEGRAPH AND TELEPHONE MESSAGES.

Telegrams should be worded as briefly as possible and use made of the code Excessive Wording of ially designed to reduce the length of telegrams. specially designed to reduce the length of telegrams.

Telegrams should not be handed in at a late hour unless it is certain they Telegrams not will reach their destination in time to be dealt with the same day.

to be handed in at late hour.

Telegrams for stations only a short distance from the office of origin, or between Urgent Train points where a good train service is available, which could reach their destination Messages quickly by train should be sent as Urgent Train Messages. Between 5.0 p.m. and 6.0 p.m. a good many offices close for the day, consequently

communications cannot be delivered after that hour for these places, and would reach their destination just as quickly by ordinary letter as they do by Telegraph or Urgent Train Messages.

The need for strictly reserving the use of the telegraph system for matters which Telegrams cannot be dealt with by post, train letters, or Urgent Train Messages, is important to be confined and the telegraphic system should be kept as free as possible for the more urgent munications. communications, so that prompt despatch can be ensured.

Telegrams sent under frank.—A certain number of words is allotted to the Post Office Company each year, and if this number is exceeded, payment at the usual inland Wires. rates for all excess words has to be made. Telegrams must not be sent over the Post Office wires if there are Company's circuits available.

Railway Trunk Telephone Service.—The Railway trunk telephone service prophone Messages vided on the Company's system for purposes of rapid exchange of communications to be confined on matters of importance should only be used for messages which are urgent or of an to urgent important character.

Public Telephone Service.—The Post Office Telephone Exchange connections which exist on the Company's system provide the following facilities:-

Telegrams sent on the Company's business must be written on the special Railway Telephoning Telegrams to Pass forms provided for the purpose, and may be telephoned to any Post Office which Post Office may be reached through the local Telephone Exchange system, provided that the Despatch. pass forms are forwarded to the particular Post Office on the same day the messages are despatched. It is most important that the pass forms should be sent in by the appointed time, otherwise the telegrams will be charged for at the usual inland rate.

When it is desirable to make use of the telephone to any considerable extent for this purpose, application should be made to the Superintendent of the Line, who will make arrangements whereby a supply of Railway Pass forms may be deposited at the Post Office periodically, in order to avoid the necessity of sending them day by day.

In the case of Paddington, and the London Receiving Offices, the necessary passes have already been deposited, so that pass telegrams can be sent direct from all offices where an exchange connection exists.

Communications which it is desired should be dealt with at the Post Office as Telephoning express letters and ordinary letters by Post can be telephoned, and the services of tions for an express messenger for the express delivery of letters and parcels can be obtained express by telephoning to the nearest Post Office connected with the exchange.

Delivery.

The usual fees for the express delivery of letters or the services of an express messenger will be charged. Further information on the subject will be found in the Telephone Directory which is supplied to stations having telephone exchange connections.

A careful record of all trunk calls should be kept on Form No. 4193 and forwarded Record of to the Superintendent of the Line at the end of each month so that the charges can be properly certified.

Telephone accounts should not be paid locally, but be sent direct to the Superin- Accounts tendent of the Line, in order that they can be properly checked and certified for payment.

A Trunk fee is a charge for the use of the Post Office Trunk Lines.

Fees for Trunk Calls.

The time occupied by a trunk call should be limited to 3 minutes conversation wherever possible. Double fees are charged for 6 minutes' conversation.

A reduction is made in trunk call charges between the hours of 5.0 a.m. and 9.0 a.m. and 2.0 p.m. and 7.0 p.m. and after 7.0 p.m., and it should be arranged as far as possible for trunk calls to be made during these times provided this can be done without detriment to the Company's business

It should be borne in mind that these charges are made to the Company only in the case of originating calls, and not in respect of inward calls.

It is of importance that trunk calls should be restricted to matters of pressing Restriction for prey and they may only be made on the authority of responsible persons. urgency and they may only be made on the authority of responsible persons. Calls on the Public Telephone must be confined to the Company's business.

#### CUSTODY OF WORKING NOTICES, PROGRAMMES, INSTRUCTIONS AND OTHER DOCUMENTS RELATING TO RAILWAY BUSINESS.

It is desired that the staff should very clearly understand the importance of retaining in safe custody all notices, programmes, instructions or other documents relating to Railway business.

Under no circumstances must printed, written or verbal information as to working or train arrangements (other than those notified to the public) be communicated in any quarter outside the service.

Notices or instructions marked "Private and not for publication" must not be exhibited in

positions accessible to the Public in Parcels Offices, etc.

Any member of the staff who becomes aware, either directly or through information received, of circumstances which point to the leakage of confidential matter relating to the railway, or of attempts made on the part of unauthorised persons to obtain such information, is requested to report the case to his superior Officer, in order that immediate action may be taken in the matter.

#### BICYCLES PROVIDED FOR USE OF STAFF.

- (1) Each machine when not in use must be kept in the station buildings or office under lock and key, and in a place safe from interference. Bicycles are only to be used on the authority of the Officer in charge.
- (2) Machines must be used only on the Company's business, care being observed that they are not subjected to improper treatment.

(3) Small repairs (such as punctures and repairs to outer covers of tyres, adjustment, and cleaning) should be carried out by a member of the Staff where possible.

- (4) Occasional repairs or replacements involving expenditure of not more than 15/0d, may be authorised by the Station Master, Goods Agent, or other officer in charge, details of the work, together with the account, being subsequently forwarded to the Stores Superintendent. Should the cost involved exceed 15/0d, a detailed estimate must be forwarded to the Stores Superintendent before the work is put in hand.
- (5) The Stores Superintendent will stock all necessary accessories such as spanners, oil cans, repair outfits, lamps, bells, and lamp wick, and supplies of these where needed may be obtained by Stores Requisition or by Repairs Requisition, in the latter case the old article being forwarded to General Stores, Swindon.
- (6) ALL accounts must be forwarded to the Stores Superintendent and will be paid from Swindon, where a record of repairs will be kept.
- (7) Applications for additional machines to be made to the General Manager through the head of Department concerned. All communications as to repairs to be addressed to the Stores Superintendent, Swindon.
- (8) Care must be taken to see that all bicycles in addition to being fitted with a red reflector are equipped with "a white surface" of not less than 12 square inches in accordance with "The Pedal Cycles (White Surface) Provisional Regulations, 1934."

#### STORAGE OF CARBIDE OF CALCIUM.

The attention of all concerned is directed to the necessity for strictly carrying out the Regulations in regard to the storage of carbide of calcium, as laid down in the General Manager's Circular No. 3136, dated October, 1930, which reads as follows :-

The quantity of Carbide of Calcium that may be kept without a licence shall be as follows:-Where the carbide is kept in separate hermetically closed metal vessels containing not more

than one pound-5 lbs. may be kept.

Where the following conditions are observed, 28 lbs. may be kept:-

- (a) The Carbide shall be kept only in a metal vessel or vessels hermetically closed at all times at which the Carbide is not actually being placed in or withdrawn from such vessel or vessels.
- (b) The vessels containing Carbide shall be kept in a dry and well ventilated place.
- (c) Due precautions shall be taken to prevent unauthorised persons from having access to
- (d) Notice shall be given of such keeping to the Local Authority and free access shall be afforded to their duly authorised Inspector to inspect the portion of the premises where the Carbide is kept and the generator is situated.

In the event of it being found necessary to store more than 28 lbs. of Carbide an advice must be sent immediately to the Stores Superintendent, Swindon, in order that the necessary licence may be obtained, as laid down in Clause 5 of the General Manager's Circular.

Every effort should be made to keep the stock of Carbide within the limits laid down, i.e. 28 lbs.,

in order to avoid more licences than necessary being obtained.

# GREAT WESTERN RAILWAY.

# Alterations and additions to the "General Appendix to the Rule Book."

To come into operation on 26th July, 1937.

EXAMINATION, MAINTENANCE, TESTING AND WORKING OF LIFTING AND HAULING APPLIANCES. Pages 293 to 298.

The Regulations under this heading have been revised as follows:-

### Definitions.

Lifting and hauling appliances include:—
Cranes (Fixed, Movable, Portable, and Travelling); Gantries; Coal Tips; Shoots; Hoists and Lifts; Capstans and Hauling Appliances; Traversers; Overhead Runways; Chains and Ropes, and all loose tackle for lifting and

(a) Fixed Cranes are Cranes that are unable to travel.

(b) Movable Cranes are those that can travel within limits on a prepared

(c) Mobile Cranes are those that can be moved by their own power and independent of any track.

(d) Portable Cranes are those that can be moved from place to place.

(e) Travelling Cranes are Cranes that can travel on their own wheels in a

Maintenance includes Painting, Examination and Lubrication, also Removal and Refixing of Ropes and Chains.

Ownership.—The Department for whose use an appliance is provided is deemed the "Owning" Department.

# Instructions to Staff respecting Working and Transit.

Instructions to Staff respecting Working and Transit.

Cranes and other appliances, including chains, ropes, slings, etc., must be be used only by a substitution of the control of the co used only by authorised persons, who must satisfy themselves that they are in good working order.

persons.

- 2. ANY DEFECT MUST BE PROMPTLY REPORTED TO THE STATION MASTER, GOODS AGENT OR OTHER PERSON IN CHARGE, WHO WILL COMMUNICATE WITH THE DEPARTMENT RESPONSIBLE FOR MAINTENANCE. ON NO ACCOUNT IS ANY CRANE, ROPE, SLING, ETC., FOUND DEFECTIVE, TO BE USED UNTIL THE DEFECTS HAVE BEEN MADE GOOD. THE INSTRUCTIONS ON EACH CRANE MUST BE STRICTLY OBSERVED. WHEN CRANES ARE OUT OF USE, ALL PARTS MUST BE LOCKED OR OTHERWISE SECURED SO AS TO PREVENT THEIR BEING MOVED BY WIND OR OTHER FORCE.
- 3. All Steam Lifting and Hauling Appliances will be worked by staff provided by the Chief Mechanical Engineer's Department (excepting those belonging to and used exclusively by the Civil Engineering, Signal, and Marine Departments, who will provide their own staff).
- 4. ON NO ACCOUNT MUST ANY CRANE OR OTHER LIFTING OR HAULING APPLIANCE BE USED FOR A HEAVIER LOAD THAN THE MAXIMUM MARKED UPON IT WITHOUT EXPRESS PERMISSION FROM THE ENGINEER RESPONSIBLE FOR MAINTENANCE—see Clause 42. No person must use an appliance without first ascertaining its maximum load.
- 5. When doubt exists as to the weight of a load being within the marked lifting capacity of an appliance, the load must not be lifted, without the special authority of the person in charge of the appliance, who must, if necessary, consult the Maintaining Department.
- 6. WITH REGARD TO TIMBER, IT MUST BE BORNE IN MIND THAT THE WEIGHT Varying weight VARIES WIDELY FROM VARIOUS CAUSES; THE EXCESS OF ACTUAL WEIGHT OVER WEIGHT ESTIMATED BY MEASUREMENT MAY VARY FROM 50 PER CENT. IN THE CASE OF DRY, WELL-SEASONED TIMBER, TO 100 PER CENT. IN THE CASE OF RECENTLY CUT TREES.
- 7. Cranes must NOT be used for shunting or towing trucks nor for lifting articles out of plumb.
- Note.—Care must be exercised to see that a load to be lifted from a stack, such as timber, is quite free and not pinned down or jammed in any way.
- 8. The slewing gear must be used where provided. In other cases manual Use of slewing power must be employed for pulling round the jibs of cranes.

Staff for working steam lifting appliances.

Maximum marked loads exceeded.

Care with loads of unknown

Cranes not to be used for loads out of plumb.

Load or jib not to foul any structure. 9. When a load is suspended from a crane, it is very important that neither the load nor the jib be allowed to foul any structure, as this may cause the crane to slew, resulting in the handles rotating and striking the operator, and may also damage the load or structure.

Use of brake power.

10. Great care must be exercised in using the brake power, particularly in damp weather, to prevent the brake slipping. When loads are to be lowered by means of a hand or foot brake, the brake must be applied, the gearing released, and the brake kept on until the loads are in position for lowering, and sufficient brake power must be applied and maintained to lower slowly. On no account must the lowering be stopped suddenly; and the man in charge must not release control of the brake lever unless it is properly secured or the load is being lowered by hand. The handles of cranes must be taken off or the handle shaft put out of gear before the operation of lowering is commenced.

When a load is being landed, care must be taken to obviate the risk of its heeling over and causing the crane to slew.

Use of Pawls.

11. PAWLS MUST ALWAYS BE ENGAGED IN THE RATCHET WHEEL WHEN LIFTING, AND ONLY DISENGAGED DURING LOWERING OPERATIONS.

Loads not to remain suspended. 12. Loads must not remain suspended beyond the time necessary to adjust the crane for lowering.

13. Travelling and portable cranes bear a caution plate embodying instructions in regard to user, and these must in all cases be strictly observed. The special attention of all concerned is directed to the necessity for seeing that when such cranes are used whilst stationary for lifting heavy weights, the extension girders on both sides, when provided, are drawn out and properly packed and the crane secured by means of the rail clips when provided. If these precautions are impracticable, other means of securing the stability of the crane must be taken.

Whenever it is necessary for the crane chains to be used doubled, the snatch blocks must be made use of.

Use of Travelling

Strict observance of instructions

on caution plates.

14. Crenes which are incapable of moving under their own power must not be

14. Cranes, whether capable of moving under their own power or not, must not be moved, when on running lines the gradient of which is 1 in 150 or steeper, unless coupled to a locomotive.

Delete clause 15.

16. With either class of crane, the locomotive should, in all cases where no serious inconvenience to the operation would result, be placed below, rather than above the crane, i.e., the locomotive should stand lower on the gradient than the crane.

17. Where cranes are working on a gradient steeper than I in 260, a baulk of timber must in every case be fixed across the line and secured to the rails by means of chains at the lowest point on the gradient to which the crane, or the locomotive attached to the crane, may be required to travel, unless the locomotive is at the lower level.

18. Where the track on which the crane is working is broken, as is frequently the case where a bridge is under reconstruction, a baulk of timber is in every case to be fixed to the rails as described above, independently of whether the line is level or on a gradient, and short of the point at which the break of track occurs.

Working of Privately Owned Steam Cranes on Running Lines. 19. Before the crane is allowed to work on G.W. lines it must be examined by a competent Locomotive Department Mechanical Inspector who will certify that the crane is fit to travel on G.W. lines or on the particular lines on which it is required to work

20. Whenever the crane is required to work under load on running lines, a locomotive must be provided to work with it. Clauses 14 to 18 to be applied strictly.

21. The driver of such steam crane must, before the permission is given, be required to pass a similar examination by the Locomotive Inspector to that for drivers of private owners' locomotives.

#### Petrol-electric Mobile Cranes.

22. The arrangements for the maintenance and working of these cranes are set forth in Joint Departmental Instructions issued by the Chief Mechanical Engineer and Superintendent of Road Transport jointly with the Chief Goods Manager for Traffic cranes and jointly with the Chief Docks Manager for Docks cranes.

Cranes of this type that are used generally for Traffic purposes must not be moved from station to station, or outside station premises, without the Chief Goods Manager's authority.

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HAULIN

Instructions t

Staff not to expose themselves to danger.

CHAINS, W

multiple slings.

# EXAMINATION, MAINTENANCE, TESTING AND WORKING OF LIFTING AND HAULING APPLIANCES .- Page 293.

Instructions to Staff respecting Working and Transit.

The following to be added as clause 13a:-Staff not to expose themselves to danger.

motion, or where they may be struck by rotation of crane handles which, through some defect, cannot be removed or disconnected as required by Clause 10. When withdrawing chains or slings clear of loads, care must be taken to ensure that they do not catch in the load and cause it to overturn. Staff must not ride on a hook or load, nor interfere with a load except to guide or prevent it swinging.

The averaginate of value avera 2 leasting instead of a 4 leasting an average leasting

by standing where the load may fall, be lowered on to them, or strike them during

Staff must see that they do not expose themselves to danger during lifting operations

business in the state of the st

(G.A.30 Op.-9/54. C.R.O.-W.36027/5),

ple

e following to be added to Clause 29:-

Use of

# CHAINS, WIRE ROPES AND THEIR ATTACHMENTS.

The expedient of using two 2-leg slings instead of a 4-leg sling or two single-leg slings in place of a 2-leg sling must not be resorted to, as this entails two rings or end links being placed on the crane hook. There is a possibility of one of these slipping off the hook when the load is being lifted, particularly when the load is

out of balance and takes up an inclined position. When a load to be lifted requires the use of a multiple leg sling, only the standard type of sling which has one ring for placing on the crane hook should be (G.A.29.Op.-5/52.) or a tablet on the sting is being discontinued owing to the

multiple slings.

Clause 29 to be amended to read:

Chains, wire ropes and their attachments.

When using double or four leg slings, the horizontal distance H, between opposite hooks, should not be greater than 1½ times L, the length of the sling, and when using three leg slings the horizontal distance H, between hooks, should not exceed 1½ times L. Under these conditions the vertical angle between the sling legs will be about 90°

and the working load and 90° will be marked on the main ring of the slings.

This angle should not be exceeded except in special cases, when the load must

not exceed that shewn on the table posted on the premises where the sling is used.

When using multiple leg slings care must be taken that each leg is carrying an

equal share of the load and where this is not possible a larger size sling must be used.

Note.—The fitting of a tablet on the sling is being discontinued owing to the tablet becoming detached in user.

Use of multiple

slings.

(G.A. 18. 11/47. G.M.-W. 36. 027/5.) ans

(CA 19 11/17 CM W. 20

Ropes-wire and fibre.

Each rope (wire or fibre), including slings, used for lifting, must bear a ferrule giving its registered number and its maximum working load, and in no circumstances must the working load be exceeded.

24. Wire ropes cannot be used with the same freedom as chains or fibre ropes, and must NOT be bent round small objects such as crane hooks, or used in any way likely to produce a sharp bend or kink. This practice injures the wires and causes them to break under load.

25. It is very important that the Jib Head Sheave of a crane, or the hook of an overhead crane, should be directly over the load before lifting is commenced, otherwise the WIRE ROPE is liable to be jammed at the side of the Sheave, causing it to break.

To maintain WIRE ROPES in good condition they must be kept well lubricated internally and externally. Supplies of suitable lubricant (described as "Lubricant; wire ropes") can be obtained from the Stores Department.

27. Loose ropes and fittings (wire and fibre) must be kept under cover in suitable places.

Chains, Wire Ropes, and their attachments.

28. The Company's own slings, hooks, chains, etc., should be used as far as practicable. Appliances belonging

traders should not be used unless special authority is given by the Company, and in all such exceptional cases a responsible man must first see that they are in good condition and of ample size, and the

must accept all responsibility for

cover. appliances to

Ropes to be

kept under

Numbers and maximum loads

indicated on

sharp bends.

Care in lifting

Lubrication of wire ropes.

with wire ropes.

rones. Wire ropes; avoidance of

be used only on special authority.

Use of double slings.

ling-chain rings must be large enough to pass easily over crane hooks. Sling-chain found to be too small, a shackle, large enough to fit the crane hook, must shackles.

rerane should be equipped with a suitable shackle, which will be supplied hef Mechanical Engineer on receipt of a requisition on form 2432.

avelling by train on their own wheels.

Steam and hand travelling cranes must be conveyed from place to place, as far as possible, on Transit of slow moving freight trains, and then subject to the following conditions:-

(a) All cranes to be placed next in front of the rear brake van. (b) Not more than one crane, with match truck, of combined dead weight exceeding 30 tons may be

attached to any suitable freight train, and only then provided no other vehicle conveying an exceptionally heavy consignment is also attached. 2 or more of these cranes may be conveyed (c) Cranes of greater lifting capacity than 25 tons must not be conveyed by ordinary freight services. Severe the When not forming part of "breakdown" or "Engineering Department train," they must always be worked where

from point to point as a special train. No hand travelling crane of 10 tons capacity or over, and no steam travelling crane must be allowed to travel by freight train without an attendant (supplied by the Chief Mechanical Engineer's Department on application). The attendant will ride in the Guard's brake van, except when the crane is in steam, or

steam is required to be raised en route, when he will ride in the cab of the crane. The crane attendant, with the Guard, will be responsible for safe running. 32. The transit of steam travelling cranes on certain lines and branches is

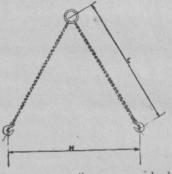
subject to limitations necessitated by consideration of axle weights and clearances. The owning Department is responsible for ensuring the observance of the conditions governing the transit of all steam travelling cranes.

33. The jibs of cranes not fitted with derricking gear must not be lifted or lowered with the Snatch Block in position; the hook of the crane chain must be engaged in the shackle on the jib or crane post before the jib is lifted or lowered.

34. The movable parts of travelling cranes must always be securely fastened before the cranes are allowed to travel. The jib must be properly lowered and secured so as to pass under the load gauge, and in cases where the jib is not carried on the tie rods, it must be lowered on to the match truck.

precautions for securing jibs without derricking gear. Safety precautions for securing movable parts, etc.

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Position of jibs of cranes when travelling.

Cranes with jibs carried by tie rods. special Precautions.

Movement of cranes in yards, Cranes removed from running lines.

Travelling cranes via Severn Tunnel.

35. Steam and hand travelling cranes with the jib properly secured on a specially constructed match truck fitted with roller on which the jib can rest and traverse may be permitted to travel with the jib leading or trailing. Other steam or hand travelling cranes must, when practicable, travel with the jib trailing.

36. CRANES WITH THE JIBS CARRIED BY THE TIE RODS MUST TRAVEL WITH THE JIBS POINTING TO THE REAR OF THE TRAIN. If, however, a crane is at a terminal or other station where facilities do not exist for turning, it may be allowed to travel with the jib pointing in the direction the train is travelling, to the nearest point at which it can be turned. Special care must be taken (whether jib is leading or trailing) to see that the jib is securely chained or roped to a match truck or wagon; sufficient allowance being made to prevent rigidity in passing round curves.

37. When moving travelling cranes in station yards, the jibs must be lowered to be within the load gauge and properly secured.

If travelling cranes have to be removed from running lines or sidings, this must be done only by the Chief Mechanical Engineer's Department, who will furnish the necessary assistance unless timber loaders or other experts are available. After replacement, the cranes MUST NOT be allowed to travel over running lines or sidings until they have been examined by a representative of the Chief Mechanical Engineer's Department. Such examinations will be made by wagon Examiners where available; at other places the nearest Divisional Locomotive Superintendent will arrange on receipt of an advice.

39. Steam travelling cranes with the jib properly secured on a specially constructed match truck fitted with roller on which the jib can rest and traverse may pass through the Severn Tunnel with the jib leading or trailing. Trains on which these cranes are conveyed, whether ordinary freight trains or special trains, must not

exceed a speed of 25 m.p.h. through the Tunnel. Other steam cranes and hand cranes must not be allowed to pass through the Severn Tunnel, except on trains specially arranged for the purpose, and provided the crane is properly adjusted for travelling, and the jib is pointing towards the rear of the train, but the speed of the train conveying such vehicles must be limited to 25 m.p.h. through the Tunnel.

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Chief Mechanical

Engineer.

Civil Engineer.

Chief

Docks Manager.

Register of Appliances.

40. The Station Master, Goods Agent or other person in Charge must keep a register in approved form of all Lifting and Hauling appliances, loose or fixed, under

The register must be kept posted up-to-date by the entry of any additional appliances provided and by marking off any that are sent away.

Quarterly Inspection.

41. The Station Master, Goods Agent, or other person in charge must see each individual article on the Register at least once every three months. He should, as a rule, be accompanied on this inspection by the man appointed to make the weekly examinations (paragraph 61), and should satisfy himself that these instructions are properly understood and carried out.

# Responsibility of Departments for Maintenance, Testing and Registration.

Departmental maintenance.

42. The departmental responsibility for maintenance is as under:-(a) All appliances, including structures and foundations,

used by the Locomotive, Carriage and Wagon and Stores Departments at the Works and Running Sheds at Swindon and Wolverhampton, and Severn Tunnel Pumping Stations. All Travelling and Portable cranes.

All appliances (including those on floating craft) other than provided for in (b) and (c).

(b) Manual appliances (including those on floating craft) used exclusively by the Engineering Department,

All foundations, including masonry, metal and timber, and all tracks other than provided for in (a)

(c) All appliances on floating craft under the supervision of the Docks Department, including those subject to Lloyds' survey.

Fixed appliances provided for the Signal and Marine Departments will be painted by the owning departments.

It is recognised that in certain cases it will be convenient for one department to carry out work for and at the expense of another department.

43. THE WEIGHT TESTING OF ALL APPLIANCES WILL BE CARRIED OUT BY THE CHIEF MECHANICAL ENGINEER,

Weight testing.

#### 45. The date of the testing and examination of Crance --CHAINS-ANNEALING AND TESTING.

References to "Rail Clips for Travelling Cranes," in Clause 50, to be deleted. The following to be added to Clause 50:-

(9) Any chain or litting tache

Wrought iron chains are in some cases being replaced by steel chains which do not require annealing after use but normalising after manufacture and before use. It will, there- or fore, only be necessary to forward steel chains and their attachments to the Testing House, Locomotive Works, Swindon, for testing at three-yearly intervals, except those on hand cranes fixed and movable and on manual appliances of all kinds which should be sent in for testing at six-yearly intervals. To enable examiners to identify steel chains these will be stamped with the letter S at the end of the register number on the tab attached to the chain or on the ring of multiple leg slings. The tab will be attached to the chain by a steel wire in the form of a closed "S". A similar piece of wire will also be attached to the ring of multiple not (G.A.29.Op.—5/52.) (G.A. 18. 11/47. G.W.—w. 2002. 5.) leg slings made of steel.

49. The date of the testing and examination of fireman s of testir appliances must be la-"17 The following to be added to Clause 50: The following classes of chain and lifting tackle are exempt from annealing: marked nes. (1) Chains made of malleable cast iron Per Per (3) Chains, rings, hooks, shackles and swivels made of steel or of any non-ferrous metal lly Ani and Rings, hooks, shackles and swivels permanently attached to pitched chains, pulley blocks or Pitched chains working on sprocket or pocketed wheels (6) Hooks and swivels having screw-threaded parts or ball-bearings or other case-hardened parts Socket shackles secured to wire ropes by white metal capping (9) Any chain or lifting tackle which has been subjected to the heat treatment known as "normalising' instead of annealing. Boards, etc., as rorming part of the chains with which attachments.

44. Each crane must bear a registered and consecutive number, indicated by Registration means of a cast iron plate. The registered numbers will be appropriated, and plates supplied, by the Chief Mechanical Engineer. As new numbers are allocated, all old number plates and painted numbers must be deleted. Prefix letters as under, will be used in connection with the numbering, to indicate the Group and Power of each

GROUP. POWER. (Fixed). M. (Manual). (Movable). S. (Steam). M E (Electric). (Portable). P. (Travelling). H. (Hydraulic). (Petrol, Oil, etc.) 0. Fixed Manual. P.S.100 Portable Steam. . .

For the guidance of the users, the Maximum Load must be clearly indicated Indication of upon each appliance.

maximum load on appliances.

## Instructions respecting Testing and Examination.

46. All appliances will be weight tested when new, or when re-erected after Weight testing extensive repairs. It is recognised that there may be exceptional cases in which of appliances. special weight tests are desirable; such tests will be a matter of arrangement between the department concerned and the Chief Mechanical Engineer.

47. A THOROUGH EXAMINATION OF ALL CRANES, OTHER LIFTING AND HAULING APPLIANCES (EXCEPT Examination LIFTS, FOR WHICH, SEE BELOW) AND ALL LOOSE TACKLE MUST BE MADE ONCE EVERY TWELVE MONTHS BY A Maintaining

48. Records of the testing and examination of all lifting and hauling appliances must be kept by the Engineers responsible for the maintenance of these appliances.

49. The date of the testing and examination of Cranes and all lifting and hauling Dates of testing appliances must be legibly marked upon them.

and testing. and examination to be marked

on cranes.

Chains-Annealing and Testing.

50. All chains, fittings and their attachments (including Shackles, Slings, Hooks, Dog-Hooks, Periodical 50. All chains, fittings and their attachments (including blacket) by the owning department periodically Annealing and Testing, and Testing, to the Testing House, Locomotive Works, Swindon, for annealing and testing, as set out below:

The intervals between Annealing and Testing not to exceed-

Chains 1 diameter or less in use at Docks, Wharves, Quays and premises subject to the Factories Act, also all chains used in connection with molten metal or slag ...

6 months

51. Hooks and other attachments, such as Coal Grabs, Scale Boards, etc., Chains and having chains attached, must be treated as forming part of the chains with which attachments.

Any other loose tackle to which chains are not permanently attached, such as tubs, buckets, trays, etc., must be tested, stamped with the working load and a certificate issued, which will cover these items until such time as they may require heavy repairs, when they would again be tested and a fresh certificate issued.

52. If a chain is found on examination (see paragraphs Nos. 60-64) to be badly Defective worn or to shew defects, it must be sent to Swindon for testing forthwith, even though not due for testing as specified above.

53. Every Station Master, Goods Agent, or other person affected must keep a register of the chains, slings, and other loose lifting tackle under his charge (a special book being provided for large stations), and care must be taken to ensure that the dates on which chains, etc., are sent for testing and returned are accurately recorded. The register must be periodically examined by the Station Master, or other person having charge of chains, etc., so as to ensure that every appliance requiring testing is dealt with in accordance with instructions. This does not refer to chains 1/2 in. diameter or smaller, in use at Docks, etc., which must be specially dealt with as directed in Statutory Order No. 279 issued by the Home Office.

Register of chains, etc.

54. Every chain, after being tested, will have its number stamped on the end link, also a small metallic label attached to the end link bearing the number of the chain as recorded in the Testing House register, and the maximum working load when used as a single chain. This label must on no account be removed.

Chains to bear metallic number labels.

The following to be added as Clause 54 A:-

Unregistered lifting tackle.

Unregistered lifting tackle should not be used, but, if found, should be sent to Swindon for registration.

(G.A.29.Op.-5/52.)

Chains ½" diameter or less in use at Docks, Wharves, Quays and premises subject to the Factories Act, also all chains used in connection with molten metal or slag	
Chains over ½" diameter on Travelling and Portable Cranes, and on all power appliances.	
Chains ½" diameter and under on Hand Cranes, Fixed and Movable, and on manual appliances of all kinds	12 months
Chains over 1 diameter used on Cranes and other hoisting appliances worked by hand	
at the Docks, and subject to Statutory Rules and Orders, 1934, No. 279	2 years
· Chains over ½" diameter on Hand Cranes, Fixed and Movable, and on manual appliances	
of all kinds	3 years

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Requisitions for replacements during repairs and testing. 55. When a crane chain, a drag sling, or other loose lifting tackle requires testing, and cannot be spared for the time necessary for it to be sent to Swindon and returned, application must be made to the Chief Mechanical Engineer, Swindon, on Form No. 2432 for another to take its place; a tested chain will then be supplied, and when received, the chain requiring testing must be sent to Swindon (together with an advice on Form No. 2432-1) the one supplied in its stead being retained as the permanent chain until it, in turn, requires testing. Form No. 2432 must be used in ordering new chains and loose lifting tackle, and the progressive number quoted as shewn on diagram of "Standard Lifting Tackle." All requisitions for sling chains should quote the length of each leg and the maximum load of the complete appliance.

Requisitions for the testing of chains.

56. Form 2432-1 must always be used when a chain is forwarded for testing. If a duplicate chain has been received in response to a request on Form 2432, the form relating to the one which is due for testing must be endorsed—"Chain not required to be returned."

Loan of lifting

57. Loose lifting tackle required for temporary use should be requisitioned on Form 2432 amended as necessary, and the numbers quoted as appearing in the diagram of "Standard Lifting Tackles". Tackle borrowed must be returned to the Chief Mechanical Engineer, Swindon, an advice being sent (Form 2432–1 amended as necessary) quoting the registered numbers of the articles which are being returned.

In the case of tackle for loan to outsiders, the Chief Mechanical Engineer must be furnished with full particulars to ensure that the appropriate charges are rendered In urgent cases application may be made by telephone to the Chief Mechanical Engineer.

Chains, etc., not to be cut, heated, or altered.

58. Chains, wire ropes, and their attachments, drag-hooks attached to tow-ropes and other loose fittings must be used in the same condition as received from Swindon; they must not be heated, cut, nor altered in any manner. Should any chain, hook, or fitting require alteration it must be sent to Swindon for this purpose,

Chains sent Swindon to bear sheet iron address labels. 59. All chains and loose lifting tackle sent to Swindon must be addressed to the Chief Mechanical Engineer, to whom application should be made for the necessary sheet iron address labels to accompany them. An advice of despatch must be sent, and the chains or other tackle booked free of charge. The sheet iron address labels received from Swindon must be returned to the Chief Mechanical Engineer immediately.

Examinations of chains, ropes, and loose tackle.

60. Examination of chains, etc., as set out below, will be arranged by the Engineers responsible for maintenance, who will notify the owning Department when replacements are necessary.

To be examined at intervals not exceedingChai

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Chains, ropes (wire or fibre) and other fittings on all cranes and lifting appliances worked by power, including all loose chains and other tackle

1 month.

Wire ropes on manual appliances ... Chains, and fibre ropes on hand cranes 3 months.

Chains, and fibre ropes on hand cranes and on all manual appliances, including loose chains and other tackle

6 months.

# Examination by Owning Department.

Chains, etc., to be kept under continual observation. 61. In addition to the periodical testing and examination it is necessary for all chains, ropes and loose fittings to be kept under the continual observation of the using department. A competent man must be appointed at each station by the Station Master, Goods Agent, or other person in charge, to examine, each week (preferably on Monday morning), all lifting and hauling appliances, including chains, ropes, slings, tow-ropes, rings having rope attachments, rail clips for cranes, and all other fittings covered by these instructions and report upon their condition.

Reports of weekly examination.

When making his weekly reports, the person so appointed must state whether the loose lifting tackle indicated on list supplied to him is safely stored and complete in every respect.

Procedure with defective ropes (wire and fibre). 62. Any fibre rope found defective must be at once cut in two so as to prevent its being used. The old rope must be sent to Swindon, and application made on Chain Requisition Form No. 2432 to the Chief Mechanical Engineer for a new one. If any wire rope, wire sling, etc., is found on examination to have 10 per cent. of the total number of wires broken in any length equal to eight times its diameter, it should be condemned and sent to Swindon, and application made to the Chief Mechanical Engineer's Department, on Wire Rope Form No. 5171-1, for a replacement.

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revent de on w one. of the should anical 63. The weekly examination will be independent of the periodical testing and examination of chains referred to in paragraphs 50 and 60.

Weekly examination independent

64. All Dock Buoys and all Dock Mooring Chains are to be examined every twelve months by a Diver, and must be tested, repaired and renewed as and when found necessary.

Weekly examination independent of periodical examination.

Examination of Dock Buoys,

Chains-Annealing and Testing.

Amend the first sentence of clause 56 to read:-

Form 2432-1 must always be rendered in duplicate when a chain is forwarded for testing.

(G.A.30 Op.-9/54. C.R.O.-W.36027/5).

Amend the first sentence of clause 57 to read:-

Loose lifting tackle required for temporary use should be requisitioned on form 2432 rendered in duplicate amended as necessary, and the numbers quoted as appearing in the diagram of "Standard Lifting Tackle."

(G.A.30 Op.—9/54. C.R.O.—W.36027/5).

#### JAMES MILNE,

July, 1937.

General Manager.

Each member of the Staff receiving a copy of this Circular is required to read carefully and note the contents, and, if supplied with a copy of the General Appendix, to alter, or cancel in ink the present instructions on the subject appearing therein, afterwards pasting the Amendments in the proper places in the General Appendix.

Station and Depot Masters are responsible for seeing that copies of the General Appendix supplied to Signal Boxes, etc., under their supervision, are corrected in accordance with this Circular.

(This form must be detached and for waste)	
	1937;
Station.	
RECEIVED copy of Circular (G.A. 2) dated July, 1937, General Appendix to the Rule Book.	containing Alterations and Additions to the

1 1 - 1-teched and forwarded to the Head of Department

# EXAMINATION AND TESTING OF CHAINS.

The instructions in Clauses Nos. 48 and 50 of General Manager's Circular No. 2932, dated November 1st, 1924, as shewn below, must be strictly observed by all concerned:-

48. When a crane chain, a drag sling, or other loose lifting tackle requires Requisitions for 48. When a crane chain, a drag sling, or other loose litting tackie requires acquires acquired testing, and cannot be spared for the time necessary for it to be sent to Swindon replacements during repairs and returned, application must be made to the Chief Mechanical Engineer, Swindon, and testing. on Form No. 2432 for another to take its place; a tested chain will then be supplied, and when received, the chain requiring testing must be sent to Swindon (together with an advice on Form No. 2432/1), the one supplied in its stead being retained as the permanent chain until it, in turn, requires testing. Form No. 2432 must be used in ordering new chains and loose lifting tackle, and the progressive number quoted as shown on diagram of "Standard Lifting Tackle."

Form 2432/1 must always be used when a chain is forwarded for testing. If a duplicate chain has been received in response to a request on Form 2432, the form relating to the one which is due for testing must be endorsed—"Chain not required

50. All chains and loose lifting tackle sent to Swindon must be addressed to Chains sent the Chief Mechanical Engineer, to whom application should be made for the necessary sheet iron sheet iron address labels to accompany them. An advice of despatch must be sent, address labels, and the chains or other tackle booked free of charge. The sheet iron address labels Requisitions for the chains of the chains of the chains of the continuous the continuous the continuous the continuous the continuous the chain of the continuous the cont received from Swindon must be returned to the Chief Mechanical Engineer immediately. the test

It should be particularly noted that, in every case, the chain requisition must be forwarded to Swindon on the same day as the chain is despatched.

# EXAMINATION, MAINTENANCE, TESTING AND WORKING OF LIFTING AND HAULING APPLIANCES. See G. A2.

Definitions.

Listing and hauling appliances include :-Cranes (Fixed, Movable, Portable and Travelling); Cantries; Coal Tips; Shoots; Hoists and Lifts; Capstans and Hauling Appliances; Traversers; Overhead Runways; Chains and Ropes, and all loose tackle for lifting and

(a) Fixed Cranes are Cranes that are unable to travel.

(b) Movable Cranes are those that can travel within limits on a prepared track.

(c) Portable Cranes are those that can be moved from place to place.

(d) Travelling Cranes are Crapes that can travel on their own wheels in a Maintenance includes Painting, Examination and Lubrication, also Removal

and Refixing of Ropps and Chains, Ownership.—The Department for whose use an appliance is provided is deemed the "Owning" Department.

Instructions to Staff respecting Working and Transit.

1. Cranes and other appliances, including chains, ropes, slings, etc., must be Cranes, etc., to used only by authorised persons, who must satisfy themselves that they are in good be used only by authorised

persons.

- 2. ANY DEFECT MUST BE PROMPTLY REPORTED TO THE STATION MASTER, GOODS AGENT OR OTHER PERSON IN CHARGE, WHO WILL COMMUNICATE WITH THE DEPART-MENT RESPONSIBLE FOR MAINTENANCE. ON NO ACCOUNT IS ANY CRANE, ROPE, SLING, ETC., FOUND DEFECTIVE, TO BE USED UNTIL THE DEFECTS HAVE BEEN MADE GOOD. THE INSTRUCTIONS ON EACH CRANE MUST BE STRICTLY OBSERVED. WHEN CRANES ARE OUT OF USE, ALL PARTS MUST BE LOCKED OR OTHERWISE SECURED SO AS TO PREVENT THEIR BEING MOVED BY WIND OR OTHER FORCE.
- 3. All Steam Lifting and Hauling Appliances will be worked by staff provided Staff for by the Mechanical Engineering Department (excepting those belonging to and used exclusively by the Civil Engineering, Signal and Marine Departments, who will provide their own staff.)

4. ON NO ACCOUNT MUST ANY CRANE OR OTHER LIFTING OR HAULING APPLIANCE Maximum BE USED FOR A HEAVIER LOAD THAN THE MAXIMUM MARKED UPON IT. No person marked not to b must use an appliance without first ascertaining its maximum load.

5. When doubt exists as to the weight of a load being within the marked lifting Care with loads capacity of an appliance, the load must not be lifted without the special authority of unknown of the person in charge of the appliance, who must if recessory consult if Windows of the person in charge of the appliance, who must, if necessary, consult the Maintaining Department.

# EXAMINATION, MAINTENANCE, TESTING AND WORKING OF LIFTING AND HAULING APPLIANCES—Continued.

Varying weights of timber.

6. WITH REGARD TO TIMBER, IT MUST BE BORNE IN MIND THAT THE WEIGHT VARIES WIDELY FROM VARIOUS CAUSES; THE EXCESS OF ACTUAL WEIGHT OVER WEIGHT ESTIMATED BY MEASUREMENT MAY VARY FROM 50 PER CENT. IN THE CASE OF DRY, WELL-SEASONED TIMBER, TO 100 PER CENT, IN THE CASE OF RECENTLY CUT TREES.

Cranes not to be used for loads out of plumb. 7. Cranes must NOT be used for shunting or towing trucks nor for lifting articles out of plumb.

(Note.—Care must be exercised to see that a load to be lifted from a stack, such as timber, is quite free and not pinned down or jammed in any way.)

Use of slewing gear.

8. The slewing gear must be used where provided. In other cases manual power must be employed for pulling round the jibs of cranes.

Use of brake power.

9. Great care must be exercised in using the brake power, particularly in damp weather, to prevent the brake slipping. When loads are to be lowered by means of a hand or foot brake, the brake must be applied, the gearing released, and the brake kept on until the loads are in position for lowering, and sufficient brake power must be applied and maintained to lower slowly. On no account must the lowering be stopped suddenly; and the man in charge must not release control of the brake lever unless it is properly secured or the load is being lowered by hand. The handles of cranes must be taken off or the handle shaft put out of gear before the operation of lowering is commenced.

Use of Pawls,

10. PAWLS MUST ALWAYS BE ENGAGED IN THE RATCHET WHEEL WHEN LIFTING, AND ONLY DISENGAGED DURING LOWERING OPERATIONS.

Loads not to remain suspended. Strict 11. Loads must NOT remain suspended beyond the time necessary to adjust the crane for lowering.

Strict observance of instructions on caution plates.

12. Travelling and portable cranes bear a caution plate embodying instructions in regard to user, and these must in all cases be strictly observed. The special attention of all concerned is directed to the necessity for seeing that when such cranes are used whilst stationary for lifting heavy weights, the extension girders on both sides, when provided, are drawn out and properly packed and the crane secured by means of the rail clips when provided. If these precautions are impracticable, other means of securing the stability of the crane must be taken.

Whenever it is necessary for the crane chains to be used doubled, the snatch

blocks must be made use of.

Use of Travelling Cranes on running lines. 13. Cranes which are incapable of moving under their own power must not be moved, when on running lines, the gradient of which is 1 in 160, or steeper, unless coupled to a locomotive.

Special Precautions.

- 14. Cranes which are capable of moving under their own power must not be moved, when on running lines, the gradient of which is 1 in 100, or steeper, unless coupled to a locomotive.
- 15. With either class of crane, the locomotive should, in all cases where no serious inconvenience to the operation would result, be placed below, rather than above the crane, i.e. the locomotive should stand lower on the gradient than the crane.
- 16. Where cranes are working on a gradient steeper than 1 in 260, a baulk of timber is in every case to be fixed across the line and secured to the rails by means of chains at the lowest point on the gradient to which the crane, or the locomotive attached to the crane, may be required to travel.
- 17. Where the track on which the crane is working is broken, as is frequently the case where a bridge is under reconstruction, a baulk of timber is in every case to be fixed to the rails as described above, independently of whether the line is level or on a gradient, and short of the point at which the break of track occurs.

  \*Ropes—wire and fibre.\*

Numbers and maximum loads indicated on ropes.

18. Each rope (wire or fibre), including slings, used for lifting, must bear a ferrule giving its registered number and its maximum working load, and in no circumstances must the working load be exceeded.

Wire ropes: avoidance of sharp bends,

19. Wire ropes cannot be used with the same freedom as chains or fibre ropes, and must NoT be bent round small objects such as crane hooks, or used in any way likely to produce a sharp bend or kink. This practice injures the wires and causes them to break under load.

Care in lifting with wire ropes.

20. It is very important that the Jib Head Sheave of a crane, or the hook of an overhead crane, should be directly over the load before lifting is commenced, otherwise the WIRE ROPE is liable to be jammed at the side of the Sheave, causing it to break.

The following to be substituted for paragraph 25 :-

No hand travelling crane of 10 tons capacity or over, and no steam travelling crane must be allowed to travel by freight train without an attendant (supplied by the Chief Mechanical Engineer's Department on application). The attendant will ride in the Guard's brake van, except when the crane is in steam, or steam is required to be raised en route, when he will ride in the cab of the crane. The crane attendant, with the Guard, will be responsible for safe running.

(G.A. 1. 3/37. LK.1/3946/12.)

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#### EXAMINATION, MAINTENANCE, TESTING AND WORKING OF LIFTING AND HAULING APPLIANCES-Continued.

21. To maintain wire ropes in good condition they must be kept well lubricated Lubrication of nternally and externally. Supplies of suitable lubricant (described as "Lubricant; wire ropes." wire ropes") can be obtained from the Stores Department.

22. Loose ropes and fittings (wire and fibre) must be kept under cover in suitable Ropes to be kept under cover. places.

places.

Chains, Wire Ropes, and their attachments.

23. The Company's own slings, hooks, chains, etc., should be used as Traders' far as practicable. Appliances belonging used only on to traders should not be used unless special authority. and in all such exceptional cases a responsible man must first see that they are in good condition and of ample size, and the owners must accept all responsibility for

24. When using double slings, care Use of double must be taken that the horizontal dis-slings. tance H, between the hooks, is not greater than L., the length of the sling.

If in any case it is found that H is greater than L, a stronger sling must be used such as will, in the opinion of the person in charge, give the necessary security.

Cranes travelling by train on their own wheels.

25. No hand travelling crane of 10 tons capacity or over, and no speam Traver. Transit of cranes. LING GRANE, must be allowed to travel by freight train without an attendant (supplied by the Mochanical Engineering Department on application), who with the Guard of the train will be responsible for safe running.

26. The jibs of cranes not fitted with derricking gear must not be lifted or Safety precautions lowered with the Snatch Block in position; the hook of the crane chain must be without derricking engaged in the shackle on the jib or crane post before the jib is lifted or lowered.

27. The movable parts of travelling cranes must always be securely fastened before the cranes are allowed to travel. The jib must be properly lowered and secured securing movable so as to pass under the load gauge, and in cases where the jib is not carried on the tie parts, etc. rods, it must be lowered on to the match truck.

28. Steam and hand travelling cranes, with the jib properly secured on a specially Positions of jibs constructed match truck fitted with roller on which the jib can rest and traverse, travelling. may be permitted to travel with the jib leading or trailing. Other steam or hand travelling cranes must, when practicable, travel with the jib trailing. (See Note at foot of page.)

29. CRANES WITH THE JUS CARRIED BY THE TIE RODS MUST TRAVEL WITH THE Crane with jibs JIBS POINTING TO THE REAM OF THE TRAIN. If, however, a crane is at a terminal or special through the resulting where facilities do not exist for turning it may be allowed to travel. other station where facilities do not exist for turning, it may be allowed to travel Precautions. with the jib pointing in the direction the train is travelling, to the nearest point at which it can be turned. Special care must be taken (whether jib is leading or trailing) to see that the jib is securely chained or roped to a match truck or wagon; sufficient allowance being made to prevent rigidity in passing round curves.

30. When moving travelling cranes in station yards, the jibs must be lowered Movement of to be within the load gauge and properly secured.

Cranes of greater lifting capacity than 25 tons must not be conveyed by ordinary freight services. When not forming part of a breakdown or Engineering Department train, they must always be worked from point to point as a special train. Two or more of these cranes may be conveyed by the same special train where it is convenient to do so and provided the route is authorised for the passage of such cranes.

No hand travelling crane of 10 tons capacity or over, and no steam travelling crane must be allowed to travel by freight train without an attendant (supplied by the Chief Mechanical Engineer's Department on application). The attendant will ride in the Guard's brake van, except when the crane is in steam, or steam is required to be raised en route, when he will ride in the cab of the crane. The crane attendant, with the Guard, will be responsible for

Note.—An addendum to Circular No. 3450 will be issued at a future date.

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red to appliam is Suard, 12.)

NOTE. - See Transit of Sieums and mine trans

32. Steam travelling cranes with the jib properly secured on a specially con. Travening cranes structed match truck fitted with roller on which the jib can rest and traverse may Tunnel. pass through the Severn Tunnel with the jib leading or trailing. Trains on which these cranes are conveyed, whether ordinary goods trains or special trains, must not

exceed a speed of 25 m.p.h. through the Tunnel. Note. - See " Transit of Steam and Hand Travelling Cranes," page 186.

Chief Mechanical Engineer.

Civil Engineer.

#### STATION INSTRUCTIONS.

# EXAMINATION, MAINTENANCE, TESTING AND WORKING OF LIFTING AND HAULING APPLIANCES—Continued.

Other steam cranes and hand cranes must not be allowed to pass through the Severn Tunnel, except on trains specially arranged for the purpose, and provided the crane is properly adjusted for travelling, and the jib is pointing towards the rear of the train, but the speed of the train conveying such vehicles must be limited to 25 m.p.h. through the Tunnel.

Register of Appliances. /-

33. The Station Master, Goods Agent or other person in charge must keep a register in approved form of all lifting and hauling appliances, loose or fixed, under his control.

The register must be kept posted up-to-date by the entry of any additional appliances provided and by marking off any that are sent away.

Quarterly Inspection.

34. The Station Master, Goods Agent or other person in charge must see each individual article on the Register at least once every three months. He should, as a rule, be accompanied on this inspection by the man appointed to make the weekly examinations (paragraph 52), and should satisfy himself that the instructions contained in this Circular are properly understood and carried out.

# Responsibility of Departments for Maintenance, Testing and Registration.

35. The departmental responsibility for maintenance is as under:-

(a) All appliances, including structures and foundations, used by the Locomotive, Carriage and Wagon and Stores Departments at the Works and Running Sheds at Swindon and Wolverhampton, and Severn Tunnel Pumping Stations, and:

All Travelling and Portable cranes and all

appliances (including those on floating craft) other than provided for in (b) and (c).

All electrical appliances.

(b) Manual appliances (including those on floating craft) used exclusively by the Engineering Department, and:—

All foundations, including masonry, metal and timber, and all tracks other than provided for in (a)

(c) All appliances on floating craft under the supervision of the Marine Department, and subject to Lloyds' Superintendent.

Fixed appliances provided for the Signal and Marine Departments will be painted by the owning departments.

It is recognised that in certain cases it will be convenient for one department to carry out work for and at the expense of another department.

36. THE WEIGHT TESTING OF ALL APPLIANCES WILL BE CARRIED OUT BY THE

CHIEF MECHANICAL ENGINEER.

37. Each crane must bear a registered and consecutive number, indicated by means of a cast-iron plate to be supplied by the Chief Mechanical Engineer. As new members are allocated, all old number plates and painted numbers must be deleted. Prefix letters as under, will be used in connection with the numbering, to indicate the Group and Power of each crane:—

GROUP. (Manual). (Fixed). (Movable). (Steam). M (Electric) P. (Portable). (Hydraulic). (Travelling). (Petrol, Oil, etc.). e.g. F.M.100 ... Fixed Manual. Portable Steam. P.S.100

38. For the guidance of the users, the Maximum Load must be clearly indicated upon each appliance.

Indication of maximum load on appliances.

Instructions respecting Testing and Examination.

39. All appliances will be weight tested when new, or when re-erected after extensive repairs. It is recognised that there may be exceptional cases in which special weight tests are desirable; such tests will be a matter of arrangement between the Department concerned and the Chief Mechanical Engineer.

Weight testing.

Registration of

Departmental responsibility for maintenance.

Weight testing of appliances.

### EXAMINATION, MAINTENANCE, TESTING AND WORKING OF LIFTING AND HAULING APPLIANCES-Continued.

40. A THOROUGH EXAMINATION OF ALL CRANES, OTHER LIFTING AND HAULING Examination by APPLIANCES AND ALL LOOSE TACKLE MUST BE MADE ONCE EVERY TWELVE MONTHS Department. BY A COMPETENT MECHANIC PROVIDED BY THE MAINTAINING DEPARTMENT, AND J CERTIFICATE OF SUCH EXAMINATION FORWARDED TO THE OFFICE OF THE ENGINEER RESPONSIBLE FOR MAINTENANCE.

41. Records of the testing and examination of all lifting and hauling appliances Records of must be kept by the Engineers responsible for the maintenance of these appliances.

42. The date of the testing and examination of Cranes and all lifting and hauling.

Dates of testing

to be marked

42. The date of the testing and examination of Cranes and all lifting and hauling and examination appliances must be legibly marked upon them.

Chains-Annealing and Testing.

43. All chains, fittings and their attachments (including Shackles, Slings, Periodical Hooks, Dog-Hooks, Rail Clips for Travelling Cranes, Pulleys and Snatch Blocks) annealing and testing. must be sent by the owning department periodically to the Testing House, Locomotive Works, Swindon, for annealing and testing, as set out below :-

Chains 3in. diameter or less in use at Docks, Wharves and Quays (see Circular No. 2,000, December, 1904)

Chains on Travelling and Portable Cranes, and on all Power appliances and Dredging Plant ...

Chains on Hand Cranes, fixed and movable, and on Manual appliances of all kinds .. .. ..

The intervals between testing not to exceed-

6 months. 12 months

3 years.

44. Hooks and other attachments, such as Coal Grabs, Scale Boards, etc., Chains and having chains attached, must be treated as forming part of the chains with which they are used.

45. If a chain is found on examination (see paragraphs Nos. 51-55) to be badly Defective chains, worn or to show defects, it must be sent to Swindon for testing forthwith, even though etc., to be tested. not due for testing as specified above.

46. Every Station Master, Goods Agent or other person affected must keep a Register of register of the chains, slings, and other loose lifting tackle under his charge (a special chains, etc. book being provided for large stations), and care must be taken to ensure that the dates on which chains, etc., are sent for testing and returned are accurately recorded. The register must be periodically examined by the Station Master, or other person having charge of chains, etc., so as to ensure that every appliance requiring testing is dealt with in accordance with instructions. This does not refer to chains in diameter or smaller, in use at Docks, etc., which must be specially dealt with as directed in General Manager's circular No. 2,000.

47. Every chain, after being tested, will have its number stamped on the end Chains to bear link, also a small metallic label attached to the end link bearing the number of the labels. chain as recorded in the Testing House register, and the maximum working load when used as a single chain. This label must on no account be removed.

48. When a crane chain, a drag sling, or other loose lifting tackle requires testing. Requisitions for and cannot be spared for the time necessary for it to be sent to Swindon and returned, during repairs application must be made to the Chief Mechanical Engineer, Swindon, on Form and testing. No. 2,432 for another to take its place; a tested chain will then be supplied, and when received, the chain requiring testing must be sent to Swindon (together with an advice on Form No. 2432-1), the one supplied in its stead being retained as the permanent chain until it, in turn, requires testing. Form No. 2432 must be used in ordering new chains and loose lifting tackle, and the progressive number quoted as shewn on diagram of "Standard Lifting Tackle."

49. Chains, wire ropes, and their attachments, drag-hooks attached to tow Chains, etc., not ropes and other loose fittings must be used in the same condition as received from to be cut, heated, Swindon; they must not be heated, cut, or altered in any manner. Should any chain, hook or fitting require alteration it must be sent to Swindon for this purpose.

50. All chains and loose lifting tackle sent to Swindon must be addressed to the Swindon to bear Chief Mechanical Engineer, to whom application should be made for the necessary sheet-iron address sheet-iron address labels to accompany them. An advice of despatch must be sent, labels. and the chains or other tackle booked free of charge. The sheet-iron address labels received from Swindon must be returned to the Chief Mechanical Engineer immediately.

# EXAMINATION, MAINTENANCE, TESTING AND WORKING OF LIFTING AND HAULING APPLIANCES—Continued.

Examinations of chains, ropes and loose tackle. 51. Examination of chains, etc., as set out below, will be arranged by the Engineers responsible for maintenance, who will notify the owning Department when replacements are necessary.

To be examined at intervals not exceeding—

1 month.
3 months.

Chains, and fibre ropes on hand cranes and on all manual appliances, including loose chains and other tackle ...

6 months.

Examination by Owning Department.

Chains, etc., to be kept under continual observation. 52. In addition to the periodical testing and examination, it is necessary for all chains, ropes and loose fittings to be kept under the continual observation of the using department. A competent man must be appointed at each station by the Station Master, Goods Agent or other person in charge, to examine, each week (preferably on Monday morning), all lifting and hauling appliances, including chains, ropes, slings, tow-ropes, rings having rope attachments, rad clips for cranes, and all other fittings covered by this Circular and report upon their condition.

Reports of weekly examinations.

When making his weekly reports, the person so appointed must state whether the loose liftint tackle indicated on list supplied to him is safely stored and complete in every respect.

Procedure with defective ropes (wire and fibre). 53. Any fibre rope found defective must be at once cut in two so as to prevent its being used. The old rope must be sent to Swindon, and application made on Chain Requisition Form No. 2432 to the Chief Mechanical Engineer for a new one. If any wire rope, wire sling, etc., is found on examination to have 10 per cent. of the total number of wires broken in any length equal to eight times its diameter, it should be condemned and sent to Swindon, and application made to the Chief Mechanical Engineer's Department, on Wire Rope Form No. 517-1, for a replacement.

Weekly examination ndependent of periodical examination.

Dock Buoys, etc.

54. The weekly examination will be independent of the periodical testing and examination of chains referred to in paragraphs 43 and 51.

55. All Dock Buoys and all Dock Mooring Chains are to be examined every twelve months by a Diver, and must be tested, repaired and renewed as and when found necessary.

# SALVAGING OF SCRAP METALS.

Items of Scrap Metals, which have not been returned to the senders, should not be thrown away, but must be collected and disposed of in the following manner:—

THE MORE VALUABLE METALS, such as Brass, Copper, Lead, Zinc, are to be sent to the Salvage Warehouse, Park Royal, at short intervals; but the Baser Metals, such as Iron or Steel, must be allowed to accumulate at the stations, and a report sent periodically to the Salvage Department, Paddington, but at not greater intervals than twelve months, when instructions will be given as to disposal.

# APPLICATIONS FROM PUBLIC FOR INFORMATION RE TRAFFIC.

Information regarding the traffic of a trader must not be given to any other trader or person not in the employ of the Company, and the public are not to be allowed access to the books and papers of the Company, with the exception of the Rate Books and the General Railway Classification.

All requests for information in respect of traffic, other than from the parties immediately interested, i.e., senders or consignees, must be declined, but, if pressed, the matter must be submitted to a superior officer.

#### THEFTS AT STATIONS.

It is desired to impress upon all concerned the importance of the Station lock-up being utilised to the fullest possible extent, with a view to the prevention of robberies. Articles of a valuable character frequently have to remain on hand over night, and at week-ends, at places where the Goods Shed is an open one, and no efforts must be spared to make use of such protection as is available.

It must be understood that, even in the case of Goods Sheds which can be properly closed up, the additional measure of protection must be taken of placing all articles of value in the lock-up. The space must not be utilised for the storage of the private property of the Staff.

#### NEATNESS AND CLEANLINESS OF STATIONS.

The attention of Station Masters is specially directed to Rule 17 (vi).

There is need for the exercise of special care in regard to all Sanitary equipment.

Station Duty Sheets should be examined in order to ensure that due provision is made for the regular performance of the necessary cleaning work, and those upon whom this devolves should be specially reminded of the importance of the matter.

#### CLEARING UP STATION YARDS.

WORK TO BE DONE BY TRAFFIC DEPARTMENT.—Station Offices, Waiting Rooms, Platforms (whether under cover or not), Goods Sheds and all areas under cover, must be swept and kept clean, and the Platforms weeded, by the Traffic Department, who will also be responsible for the cleansing and disinfecting of Cattle Pens, and for depositing the rubbish, manure, etc., in a receptacle provided for the purpose, or at an agreed point.

WORK TO BE DONE BY THE ENGINEERING DEPARTMENT.—Station Yards, Sidings, Roads, Wharves and other places not under cover (except platforms and private Wharves let at rentals) must be swept scraped, repaired and kept clean by the Engineering Department, who are to have the disposal of all rubbish, manure, etc., swept together in the course of cleaning up by the two Departments. Any cash

obtained for disposal of refuse must be credited to the Traffic Department.

Station Masters, Goods Agents and all other persons concerned are reminded of the necessity of keeping the Stations, Offices, etc., clean and tidy; and they must advise the Divisional Engineers if any instances occur in which the Lines, Yards, Roads or other places, for the cleansing of which the Engineering Department is responsible, are not properly looked after and kept in a neat state.

Refuse from the Platforms must not be swept on to the Permanent Way, but must be collected and

conveyed to the dust-bin; or be burnt.

#### DISPOSAL OF STATION AND OFFICE REFUSE.

Where practicable, arrangements should be made for the Local Sanitary Authorities to remove refuse from Stations, Offices, etc., proper receptacles being provided, where needed, for the deposit of refuse in places convenient for its removal by the Local Authorities' carts. Where fixed receptacles are required these will be provided by the Engineering Department, but in cases where it is necessary to have portable bins these must be requisitioned from the Stores Department.

#### OLD BOOKS, FORMS, AND WASTE PAPER.

1. All books, consignment notes, invoices, correspondence and documents must Preservation of be preserved with care, kept in good order and conveniently arranged for reference. Records.

2. Books, correspondence, etc., must be kept for the periods prescribed.

Period for which Records to be kept.

3. Old books, papers, etc., at every station must be overhauled annually and Annual Overhaul those over age forwarded to the Stores Department as follows, application being made and Disposal. to the Storekeeper concerned for the necessary sacks:—

Northern Division .. .. Wolverhampton Stores.

London Offices and Depots . . . See Clause 8.
All other Stations . . . . Swindon General Stores.

In special cases, arrangements will be made for paper to be sent direct to Contractors, and in all cases where the quantity on hand is one ton or over application should be made to the Stores Department for instructions regarding disposal.

4. Waste paper must be collected daily from the various offices, etc., at the Daily Collection station or depot and placed in a suitable receptacle until a sufficient quantity has accumulated to warrant an application to the Stores Department for sacks and forwarding instructions. The responsible staff must satisfy themselves that suitable accommodation is provided for waste paper.

Upon application, the Stores Department will supply small sacks for storage purposes and exchange them for empty sacks when received full.

5. The forms shown at the end of the Appendix must be carefully packed if in a Method of clean condition and in all cases forwarded to Swindon separately from other waste paper in order that they may be re-used.

Old envelopes, bundles of correspondence and other papers must be packed tightly in sacks. Correspondence backs, pins and paper fasteners must be carefully removed from old papers and retained at the station or depot for further use.

Large books must not be put into sacks but tied securely in bundles.

## OLD BOOKS, FORMS, AND WASTE PAPER-Continued.

Old tissue and foolscap books must be packed separately, the former being sent to the Stationery Superintendent, Westbourne Terrace, Paddington, G.W.1 and the latter to the Storekeeper, General Stores, Swindon.

If the quantity is more than is required for parcelling or packing purposes at the station or depot, old time bills and posters are to be returned whole, if possible, and not torn up as is often the case. This class of paper is used to considerable advantage at Swindon for packing purposes in lieu of brown paper.

Old truck labels must not be packed with waste paper, but forwarded in separate

sacks and the nature of the contents marked on the label.

Care must be taken to see that **no rubbish** is packed with waste paper. Instances have occurred where detonators, broken glass, pieces of tin, etc., have been found in the sacks, involving risk of serious accidents to the staff handling the waste.

6. When sacks for waste paper are received they must be filled and disposed of promptly as directed by the Stores Department. They must not be used for storage purposes nor kept more than seven days except as provided in the second paragraph of clause 4 under the heading "Daily Collection of Waste Paper."

If more sacks are received than are required for immediate use, the surplus must be returned at once to the Stores Department, Swindon, a delivery note stating the

number of sacks returned being sent at the same time.

7. Every sack and bundle of books, etc., sent away must have a label attached giving the address of the consignee, the date sent, and the name of the sending station or depot.

Travelling stores vans must be used whenever possible.

 The instructions relating to the collection and disposal of old books, forms and waste paper from London offices and depots are set out in Circular No. 3010 of 17th November, 1926.

Note.—Books and documents relating to accounts for the year 1913 should be retained for the present, as they comprise the first and only complete year of accounts rendered in accordance with the Accounts and Returns Act of 1911.

## DISPOSAL OF OLD BOOKS AND PAPERS.

TO BE RETAINED PERMANENTLY.

Comparative Returns.

D.G.M.'s Weekly or Periodical printed Circular.

Dock Rate Book.

Fare Books.

Use of Sacks.

Advice of

London Offices and Depots

Dispatch.

General Instructions relating to Merchandise Traffic.

General Railway Classification and Supplements.

Guard Books containing circulars, balance sheets, etc.

Handbook of Stations and Appendix.

Important correspondence where questions of Agreements, history, principle and other similar questions have been reported upon and settled. To be specially indexed and kept apart.

Instructions to Country Stations re London Traffic. Irish Traffic—General Instructions.

Merchandise Route Books.

Pier Head Arrivals & Sailings Book.

R.C.H. Coaching Arrangements Books.

R.C.H. Instructions to Railway Companies' Staffs relating to Passenger Train Traffic and Merchandise Traffic.

Rate Books.

Regulations, General Instructions, and Scales of Charges for Warehouse Rent, Wharfage, Demurrage, Labourage, etc.

Season Ticket Rate Books.

Special Arrangements Book.

Staff Records, and correspondence connected therewith.

Station Accounts Instruction Book and Supplements,

Statistical Books.

Stemming Book.

Summary of Coal shipped at each Appliance.

Tables for ascertaining Railway Charges on Mcrchandise.

Towns, Villages, etc., adjacent to and served by G.W.R., and Supplements.

Other printed publications and important documents not specifically enumerated, in use in General Offices, etc.

Description	Period forms retained at present.	Period forms to be retained in futures
Income Tax Records Carmen's Delivery Sheets Consignment Notes Rolling Stock Requisition	 6 years 6 years 6 years 1 year	4 years 3 years 3 years 6 months

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### DISPOSAL OF BOOKS AND PAPERS-Continued.

#### GOODS DEPARTMENT FORMS, ETC.

#### TO BE RETAINED ONE YEAR AFTER COMPLETION.

Abstract forms No. 5512/3/4.
Carmen's Daily Record.
Carting Agents' Account Return.
Cheque and Note Books.
Coal Order form 134.
Collection Orders.
Insecuracy Sheet Register.
Invoice Pro Books.
Loading and Unloading Slips (Provincial).

Ministry of Transport statistics.
Paid on Certificates.
Porters' Loading Book (Provincial).
Repairs requisition book 206.
Returned Goods Book.
Rolling stock daily report 5781.
Rolling stock requisition 5823.
Shipment Declaration.
Staff Time Books.
Wagon Labels from local and foreign stations.

#### TO BE RETAINED THREE YEARS AFTER COMPLETION.

Abstracts. Arrival Book. Carmen's Bonus Record (Carters' daily returns only). Cartage Loading Books. Cash and Ledger Transfer Summaries (copies). Cash Transfer Voucher Copies Clothing Requisition. Correspondence (ordinary station). Credit and debit transfer book 7743. Daily Cash Account. Despatch Book. Half-yearly Statement of Outstandings (copies). Insurance Form. Ledger Account List 1586. Not to Hand Book. P.T.F. Book. Paying-in Slips for Banks. Pickford's Account Forms (copies).
Porters' Bonus Record (Checkers' daily list only).

Porters' Loading Book (London District). Railway statistics. Rebate Vouchers (copies). Remittance Book 257. Station Account List-1386A (copies). Station Book 165 (when not used as signature book). Station Truck Lists. Summary Book. Summary of Debits. Summary Delivery Sheets 5297. Summary Cash Book. Tissue Copying Books :-Abstracts and Summaries. Ledgers. Station Accounts. Undercharge Book. Unentered Book. Unloading records. Unpaid Wages Book. Wagon Repairs Book. Weekly Revenue Returns (copies).

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### TO BE RETAINED SIX YEARS AFTER COMPLETION.

Advice Notes. Advice Notes Register. Carmen's Delivery Sheets. Cash Book. Claims Register. Consignment Notes. Delivery Book. Depot Sheets. Excess Register. General Account 1103. General Account Analysis. Grain Book. Tax Records Invoices (Inwards and copies of Outwards). Ledger Account Book. Ledger Deductions Book,

Letter Register. Number Takers' Books. Overcharge Book. Paid-on Book. Postage Book. Rebate Register. Receipt Book. Short credit account register 1543A, Siding Rent and Demurrage Book. Station Account Book. Station Book 165 when used as signature book. Sundry Charges Book. Transfer Orders. Wagon Book. Warehouse Book. Weighing Book.

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#### DISPOSAL OF BOOKS AND PAPERS-Continued.

## PASSENGER DEPARTMENT FORMS, ETC.

TO BE RETAINED ONE YEAR AFTER COMPLETION.

Brood Mares and Stallions Certificates, Carmen's Journals, Cheque Register. Delays to Train correspondence, Guards' Time Sheets, Guards' and Porters' Memo, Books, Horse Stock and Harness Book, Inaccuracy Sheet Register. Passenger Guards' Train Book 750.
P.L.A. Forms,
Property Found Books.
Receiving Office Settlements,
Return Fare Claims, Copies of
Rolling Stock Returns.
Stable Stores requisition.
Staff Time Books.

# TO BE RETAINED THREE YEARS AFTER COMPLETION.

Accounts. Passenger General Account; Form No. 874 (copies). Parcels General Account; Form No. 109 (copies). Blank Card Registers. Booking Clerks' Train Book. Carted Luggage Book. Cash Registers. Cash Remittance Slips. Cloak Room Book to-pay. Cloak Room Indemnity Book. Cloak Room Ticket Book. Clothing Requisition. Copying Book. Correspondence (ordinary station). Dockets, Record of Excess Pads. General Summary for Overcharge Vouchers. Insurance Book.

Milk Register. News Label Register. Paper Ticket Book. Parcels Account Book. Parcels O/S Book. Parcels Stamp Book. Parcels Transfer Book. Particulars to Follow Book. Passenger Excess Returns. Proof Book—Daily. Proof Book—Monthly. Revenue Returns. Season Ticket Registers. Station Account (O/S). Summary Cash Book. Ticket Collectors' paying-in books. Ticket Requisition Form. Train Register Book. Unpaid Wages Book. Warehouse Rent Book.

# TO BE RETAINED SIX YEARS AFTER COMPLETION.

Carmen's Delivery Sheets.
Claims Register.
Consignment Notes.
Counter Sheets.
Credit Account Book.
General Summary Book (Ledger).
Income Tax Records.
Inward Parcels Book.
Letter Register.
Outward Parcels Book.
Outwards Receiving Office Sheets.

Parcels Cart Bills.
Parcels Cash Book.
Parcels Excess Book.
Paybill Tissues.
Postage Book.
Receipt Book.
Receipt Book Counterfoils.
Receipt Pads.
Record of Excess.
Waybills.

Paid on Book.

#### DOCKS DEPARTMENT FORMS, ETC.

TO BE RETAINED ONE YEAR.

Berthingman's Daily Report.
Cheque Registers.
Coal Stocktaker's Handbook.
Delays Return.
Dock Pass (Admission to Dock Premises).
Dock Pass (Vessels).
Foreman's Daily Report.
Note Books at Machine Houses.

Passes for taking Goods Off the Dock. Record of Orders. Special Services Book. Staff Time Book. Tonnage Book (Tippers). Traffic Order Form. Weighing Machine Daily Report.

Paybill Tissues				
Counter Sheets	***			
Cradit Account Book				
General Summary Book (Ledger	)		6 years	3 years
Inwards Parcels Book			0 years	
Outwards Parcels Book	***			
Outward Receiving Office Sheets				
Parcels Cart Bills	***			
Waybills	***	)	3 years	2 years
Unpaid Wages Book	***		o jouro	
Carted Luggage Book		}	3 years	1 year
Insurance Book	***	)	3 years	13 months
Train Register Book	•••	***	1 year	6 months
Rolling Stock Returns	•••		1,000	

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#### DISPOSAL OF OLD BOOKS AND PAPERS-Continued.

#### TO BE RETAINED THREE YEARS.

Accounts.
Berthing Note.
Cargo Labour Order Book.
Clothing Requisition.
Coal Orders.
Coal Passing various Junctions.
Coal Shipped U/E Book.
Colliery Permits & Invoices (Uncleared Book).
Correspondence (ordinary).
Crane Sheet.
Dock Toll Collector's Daily Return.
Empty Coal Sheets,
Forwarding Instructions (Empty Wagons).
Hirers Shunting Orders (In and Out).
Journal of Domestic Engines.
Landing Orders.

Licenses, Ships, Canvassers (Counterfoils).
Marine Controllers' Daily Record.
Particulars of Current Overcharge Vouchers.
Record of Mixing Forms.
Record of Train and Traffic Advices.
Removal of Vessels Form.
Return of Hirers Wagons shunted.
Shipping Instructions Form.
Stemming List.
Stemming List.
Stemming Note.
Train Advice Form.
Tugboat Order Book.
Unpaid Wages Book.
Waterman's Overtime Book.
Water Order Book.
Working List.

## TO BE RETAINED SIX YEARS.

Accounting Books. Arrivals and Sailings Forms. Berthing & Working Book & Lists. Berthing and Working Orders. Clearance Requests. Coal and Coke Traffic Transferred form. Coal Shipments Day Book. Coal Wharfage Book Commercial Dry Dock Charges Book. Crane Order form. Credit Slips. Daily Towage Return. Docks Daily Traffic statement. Dry Dock Stemming Diary. Electric Lighting Order. Graving Dock Stemming Book. Hirers Empty wagons standing on Sidings, Income Tax Records. Letter Register. Manifest forms (Outwards and Inwards). Masters' Reports. Mates' Receipts.

Night Order Book. Orders for Unshipping Hatchbeams. Paybill Tissues. Postage Book. Receipt Book. Record of Arrivals & Sailings Book. Register of Licenses. Retared Wagons Permits. Royalties Book. Screening Invoices. Shipment Certificate. Shipping List. Ship Store Journal. Tally Books. Time Register of Vessels Book. Tipping Invoices. Coal Shipping Appliance Time Sheets. Vessels Changing Docks. Vessels Expected Book. Watermen's Daily Return. Water Receipt Book. Weighing Machine Permits.

The list, so far as the books and documents in use at Stations generally are concerned, can be regarded as comprehensive, but there may be special forms in use at some stations, in regard to which no difficulty need arise as to the classification under which they should come.

Having regard to the varying periods for the retention of books and papers, it will be necessary for a proper system of filing to be instituted at each station in order to avoid confusion and prevent unnecessary handling.

#### FORMS TO BE CAREFULLY PACKED FOR RE-USE AND FORWARDED TO SWINDON (SEE CLAUSE 5).

Abstracts			 No. 1109	Guard's Time Sheet		No. 41
Abstracts			 No. 1110	Half Yearly Statement		No. 3547
Account form	ns		 No. 964	Ledger Account		No. 1586
Clothing Rec	uisition	-	 No. 2635	Porter's Bonus Record		No. 3731
Dockets, Records of No. 5004		Time Table Notices and Pos	ters.			
Cananal Agas	ment An	Innia	No 9744			

Note.—The above periods to be observed by Divisional and District Officers as far as comparable books and papers are retained by them.

## LEGAL AND OTHER NOTICES AT STATIONS.

The following Notices must be exhibited at each Passenger Station :--

BY-LAWS NOTICE EXPLOSIVES "
INSURANCE PENALTIES FEES "
SMOKING "

EXAMINATION OF CHANGE NOTICE.

At Goods stations which are altogether separate from the Passenger stations, the legal Notices must be exhibited. On the Monmouthshire Section of the Line Toll Notices must be displayed in addition to the foregoing.

The Divisional Superintendent, District Traffic Manager on Dock Manager must be notified when

the notices become dilapidated and he will arrange for new copies to be supplied.

In no circumstances are old Notices to be taken down until new ones are received.

#### WINDING AND REGULATING CLOCKS.

1. The winding (and as far as possible the regulating) of all the clocks at the stations must be done by, or under the personal directions of, the Station Masters, who will keep the keys and see that the clocks are wound, either daily or weekly as may be required, and that the correct Greenwich time is shown.

2. Greenwich time will be transmitted daily from Paddington to all stations at which a telegraph instrument is fixed.

3. Stations in circuit with Paddington will receive the signal direct from that station, and the Telegraph Operators or other persons in charge of the instruments at those Stations will, simultaneously with the receipt of the signal from Paddington, signal the stations with which they are in circuit, and so on, till every Station having telegraph communication is reached at the same time.

4. At places where "Time" has to be transmitted by block telegraph or telephone bell, the signal must be made by giving 18 beats in the following manner:—8 pause 5 pause 5.

5. Guards must set their watches daily by the clock of the first station having telegraphic communication at which they stop or start from after 11.0 a.m., and they must give the time to all stations having no telegraph to which their trains form the first available communication. The Station Masters are responsible for seeing that such is done.

6. The clocks at outlying signal boxes which are not on through single needle circuits are to be set by the clock of the nearest adjoining station, and the Station Master through whose pay-bill the Signalmen are paid must make arrangements for the proper time to be supplied to them by telegraph or otherwise, so as to ensure their having the correct time.

7. The winding and regulating of the clocks, whether done by the Station Master or by some person deputed by him, should always be performed by the same person. The Station Master must do it himself in all cases where it is practicable for him to do so; but when it is not possible for him to wind them himself, he must depute a competent person to perform the duty, and see that it is properly done.

8. Defective fixed clocks and time recorders should be reported to the Signal Engineer at Reading who will arrange for attention to be given.

9. Defective movable clocks should be sent to the Signal Engineer at Reading for attention in accordance with the instructions issued.

10. Defective time pieces and watches should be sent to the Divisional Officer concerned who will despatch them to the Signal Engineer at Reading for attention.

(For general instructions regarding the maintenance of clocks, etc., see General Manager's Circular No. 3172—1/10/30).

#### PROTECTION OF WORKSHOP VANS

The following instructions are exhibited in Workshop Vans for the protection of the men engaged at work therein:

"The man in charge of this van must, immediately on arrival at a station, inform the Station Master (or other person then in charge of the station) of the presence of the van. As soon as the van has been placed in a siding, and until it leaves, the man in charge must exhibit a red flag during daylight and a lamp with a square glass shewing a red light after dark, if in a loop siding, at each end of the van; if in a dead-end siding, at the opposite end of the van to the stop-block. These flags and lamps must be fixed on the side of the van furthest from the running lines, so as not to interfere with passing trains."

Station Masters or other persons in charge when advised of the presence of a workshop van should arrange, if possible, to have it placed in a dead-end siding, towards the stop-block end, and warn their Shunters not to shunt vehicles against it.

In the case of Messrs. Pooley's workshop vans, the Station Master or person in charge of the station will be held responsible for seeing that red flags or lamps are exhibited, except at those places where the van has been placed on a siding which is protected by wheelstops or by the points leading to such siding being clipped in such a manner as to protect the van.

JAMES MILNE, General Manager. -

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# GREAT WESTERN RAILWAY

Circular No. 3714. (Amending Circular No. 3680.) General Manager's Office Paddington Station, November 1st, 1947.

The instructions shewn on pages 304 to 308 of the General Appendix to the Rule Book dated July 1936 in regard to the instructions to be observed for the protection of Workshop Vans, Carriage Cleaners, Gasmen, etc., are revised as follows:

# (A) REGULATIONS FOR THE PROTECTION OF CARRIAGE CLEANERS, GASMEN, LAMPMEN AND OTHERS WORKING ON COACHING STOCK.

# Where an engine is not attached to the vehicles.

1. Before any of the above-named men commence work:

- (a) Upon the outside of vehicles on any line or siding on which it is possible for other vehicles to be shunted against them;
- (b) Which necessitates the use of steps or ladders inside the vehicles or involves the men placing themselves in such a position that they might lose their balance if the vehicles are moved,

a red flag by day, or a red light during darkness, fog, or falling snow, must be exhibited at the end of the last vehicle nearest the direction from which vehicles might be shunted against those on which the men are at work. If it is possible for vehicles to be shunted against both ends of the vehicles on which the men are at work, the same precautions must be taken at both ends.

When the vehicle or vehicles stand wholly inside a shed, the flag or light must be exhibited at the entrance or entrances to the shed on the line upon which the vehicles are standing.

- 2. If the vehicles are standing on a line adjoining a running line, the red flag or red light must be exhibited on the side farthest away from the running line. When the vehicles are standing at a platform the flag or light must be placed on the platform side in such a position as to be plainly visible therefrom.
- 3. Before commencing work on the vehicles each man concerned is responsible for seeing that he is protected by a red flag or a red light in accordance with the foregoing Regulations, but should more than one man or set of men be separately at work on the same vehicles, or on the same line or siding, each man or set of men must be separately protected by additional red flags or lights. Each man working alone will be held responsible for carrying out these Regulations. Where a set of men is engaged one of them must be made responsible for carrying out these Regulations.
- 4. The man responsible for carrying out Regulation 3 must, before work is commenced, also satisfy himself that no shunting operations are in progress affecting the vehicles on which work is to be done.
- 5. When vehicles in a siding adjoining a running line, or on a running line adjoining another running line, are being cleaned, gassed or watered, a good lookout must be kept and care must be taken to see that doors are not left projecting on the running line side. Planks, steps, or long-handled brushes must not be used on the side of vehicles next to a running line except when authorised by the Chief Mechanical Engineer and Superintendent of the Line.

# Where an engine is attached to the vehicles.

- 6. Before commencing the work referred to in (a) and (b) of Regulation 1, or supplying gas or water to a train of vehicles, the man concerned must place a red flag by day, or a red light during darkness, fog or falling snow, on the side of one of the vehicles for the guidance of the Traffic Department Staff and Enginemen. In the case of a train standing at a platform, the flag or light must be placed on the platform side in such a position as to be plainly visible
  - 7. Passenger trains or vehicles must not be moved whilst men are at work on the top of the vehicles.

#### General Instructions.

- 8. The red flag or red light exhibited for the protection of the men must not be removed until the work has been completed, or has been suspended to admit of the vehicles being moved, and the man before removing it must satisfy himself that all the men concerned are clear. (See also Regulation 3.)
- 9. Vehicles protected by the red flag or red light must 'not be moved, nor must others be shunted against them except as provided in the second paragraph of Regulation 10. Persons responsible for starting trains must be careful to see that no red flag or red light is exhibited before giving the signal for the train to start.
- 10. Enginemen and Shunters are particularly warned to satisfy themselves when approaching, and before coming into contact with, vehicles standing on platform and other lines, or sidings, and before backing on to or attaching or detaching vehicles to or from trains at platforms, that no red flag or red light is exhibited for protection purposes in accordance with these Regulations.

Should, however, it be necessary to attach or detach vehicles or to change engines after the red flag or red light has been placed on the platform side of a through train at a passenger station this must be done under the supervision of the person responsible for starting the train but great care must be taken to see that the train is not moved until all the men concerned have ceased work and are clear of the train.

11. Shunters and others must keep a good lookout when shunting on lines and sidings adjacent to those occupied by vehicles on which men are at work.

12. Carriage Cleaners and others working on coaching stock must not pass under, over, or between the buffers of vehicles, nor between the stop block and the nearest vehicle, when less than a carriage length apart. If necessary, men may pass through a van or third class compartment when the vehicle is stationary but must take care to close and fasten the doors after them.

13. Carriage Cleaners and others working on coaching stock must not stand on the lines between vehicles or between a vehicle and the stop block unless they are properly protected in accordance with the foregoing Regulations.

14. Special attention is directed to Rule 11 in the Rule Book.

Note.—Should it be necessary for work to be done underneath a vehicle the provisions of Regulation 10, relative to the protection of Brake Fitters, etc., must be observed.

# (B) REGULATIONS FOR THE PROTECTION OF BRAKE FITTERS, LIFTERS, REPAIRERS AND OTHERS WORKING ON CARRIAGE OR WAGON STOCK.

These Regulations must also be observed by the employees of Private Carriage or Wagon Repairing Firms.

In repairing shop sidings, and sidings specially set apart for the purpose of carrying out repairs or other work on carriage or wagon stock, where protection is afforded against shunting by means of padlock and key.

1. During the time that men are at work in the sidings referred to above, the points leading to such sidings must be kept padlocked so as to protect the men, and the key of the padlock must be held by the repair staff, who will be responsible for securing and releasing the points.

2. Before any work is commenced or resumed on such a siding it is the duty of the workman, or where more than one man is engaged of the man in charge, to examine the points giving access to the siding and satisfy himself that they are securely padlocked in such a manner that a shunt cannot be made into the siding on which they intend to work.

3. Before the points of such sidings are unlocked for shunting purposes, or to enable vehicles to be placed in or removed from the sidings, it must be ascertained whether any men are working on vehicles in the sidings, and the padlock must not be taken off until such men have been warned to place themselves in a position of safety, nor must work be resumed by them until the points have again been padlocked.

4. If Railway Company's repair staff are employed in the siding they will be responsible for carrying out these Regulations. If no Railway Company's staff are employed the duties must be carried out by the staff of the

private firm or firms concerned.

When the siding is required for shunting purposes or on the termination of all repair work, the man holding the key of the padlock must hand it to the person in charge of the shunting, but this must not be done until the man holding the key has satisfied himself that there are no men at work in the sidings and the work on the vehicles has reached a stage when shunting can safely be permitted.

## In sidings and on lines other than those referred to above.

5. Before any work is commenced it is the duty of the workman, or where more than one man is engaged of the man in charge, to go to the Station Master, Inspector, or other person in charge of the line or sidings, advise him what work is required to be done and obtain his permission for the work to be carried out.

6. A red flag by day or a red light during darkness, fog, or falling snow, must be exhibited at the end of the last vehicle nearest the direction from which vehicles might be shunted against those on which the men are at work. If it is possible for vehicles to be shunted against both ends of the vehicle on which the men are at work, the same precautions must be taken at both ends.

7. If the vehicle is standing on a line adjoining a running line the red flag or red light must be exhibited on the side farthest away from the running line. When the vehicle is standing at a platform the flag or light must be placed on the platform side in such a position as to be plainly visible therefrom.

8. Before commencing work on the vehicle each man concerned is responsible for seeing that he is protected by a red flag or a red light in accordance with the foregoing Regulations, but should more than one man or set of men be separately at work on the same line or siding, each man or set of men must be separately protected by additional red flags or lights.

9. The wheels of the vehicle upon which work is to be carried out must be secured by sprags or scotches and if other vehicles are standing on the same line or siding, one or two brakes must be applied on the vehicles both in front and in rear of the one upon which work is to be carried out, or the wheels of such vehicles must also be secured by sprags or scotches.

10. Before examination or other work is commenced underneath a vehicle attached to an engine, the Driver, and Guard or Shunter, as well as the Station Master, Inspector or other person in charge of the line or sidings, must be advised, and instead of the flag or light being exhibited as prescribed in Regulation 6, a man with a red hand signal must stand in the best possible position and keep a good lookout for the protection of the workmen.

When examination or other work is required to be carried out underneath a vehicle and there is not an engine attached, the instructions in Regulations 5, 6, 7, 8, 9, 11, 12 and 13 must be observed.

11. The red flag or red light exhibited for the protection of workmen must be removed only under the instructions of the man in charge of the work, after the work has been completed or has been suspended to admit of the vehicle or vehicles being moved. The person in charge of the shunting must be duly advised. Care must be taken to see that all concerned are clear and all scotches and sprags have been taken away before the flag or light is removed.

12. Each man working alone will be held responsible for carrying out these Regulations. Where more than one man is engaged one of them must be made responsible for carrying out these Regulations.

13. Whilst the red flag, or red light is exhibited, the vehicles so protected must not be moved, nor must others be shunted against them.

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General Instructions.

14. Where there are repairing shop sidings, or where other sidings are set apart for the purpose of carrying out repairs, etc., the work on vehicles must, as far as practicable, be carried out in such sidings.

15. Shunters and others must keep a good lookout when shunting on lines and sidings adjacent to those occupied by vehicles on which men are at work, and must, before commencing such shunting, warn the men engaged on the vehicles what they are about to do.

16. The special attention of the Railway Company's employees is directed to Rule 11 in the Rule Book.

17. Except on Sidings or running lines which are protected in accordance with the provisions of clauses 1 to 6 or 10, employees are forbidden to go between the buffers of vehicles or between stop blocks and the nearest vehicle, when less than 50 feet apart, without first satisfying themselves that none of the vehicles is about to be moved by engine, capstan, horse or other power, and that no shunting is going on upon the lines which they are about to cross.

18. Each Private Carriage or Wagon Repairing Firm will supply to their own staff the red flags and lamps of a pattern approved by the Railway Company concerned.

# INSTRUCTIONS TO BE OBSERVED FOR THE PROTECTION OF MEN REPAIRING OR PAINTING STOP BLOCKS.

Before commencing to repair or paint a Stop Block, the man concerned must obtain the permission of the Station Master, Inspector or person in charge, who, if permission can be given, must arrange to place the line or siding temporarily out of use, during which time the points must be secured by clip and padlock. If the points are worked from a signal box, the Station Master, Inspector or person in charge must inform the Signalman of what is about to be done.

When it is not possible to place the line or siding out of use, the Station Master, Inspector or person in charge must arrange for a distance of not less than 50 yards to be maintained between the Stop Block and the nearest vehicle on the line or siding during the progress of the work. The man engaged in the work must, before commencing, exhibit a red flag at the end of the vehicles nearest to the Stop Block in a position where it can be most readily observed by Shunters, Enginemen and others concerned, engaged in the movement of traffic, and must fix two detonators on the rail close to that vehicle in order that immediate warning will be given in the event of a movement towards the Stop Block.

#### PROTECTION OF WORKSHOP VANS.

The following instructions are exhibited in Workshop Vans for the protection of the men engaged at work therein:

"The man in charge of this van must, immediately on arrival at a station, inform the Station Master (or other person then in charge of the station) of the presence of the van. As soon as the van has been placed in a siding, and until it leaves, the man in charge must exhibit a red flag during daylight and a lamp with a square glass shewing a red light after dark, if in a loop siding, at each end of the van; if in a dead-end siding, at the opposite end of the van to the stop-block. These flags and lamps must be fixed on the side of the van furthest from the running lines, so as not to interfere with passing trains."

Station Masters or other persons in charge when advised of the presence of a workshop van should arrange, if possible, to have it placed in a dead-end siding, towards the stop-block end, and warn Shunters not to shunt vehicles against it.

In the case of Messrs. Pooley's workshop vans, the Station Master or person in charge of the station will be held responsible for seeing that red flags or lamps are exhibited, except at those places where the van has been placed on a siding which is protected by wheelstops or by the points leading to such siding being clipped in such a manner as to protect the van.

November 1st, 1947.

JAMES MILNE, General Manager.

(Each man receiving a copy of this Circular is required to carefully read and note the contents, and keep it for reference.)

This Circular amends Circular No. 3680, dated 1st November 1945.

20,000—L.T.X.T. 9/47 6,000 (Reprint) 12/49.

(This form must be detached and forwarded to Head of Department.

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Received copy of General Manager's Circular No. 3714, dated November 1st, 1947, respecting the Protection of Carriage Cleaners, etc.

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#### CARRIAGE CLEANING.

The attention of Station Masters, Inspectors, Guards, the Travelling Staff and others is specially directed to the subject of Carriage Cleaning, it being of the utmost importance that the Company's Trains, Motor Cars and Trailers shall at all times present a smart appearance outwardly and that the interiors shall be scrupulously clean.

All concerned are requested to be observant in this matter and to report any instances where a proper standard of cleanliness is not maintained and where there is cause for complaint in regard to the condition of the coaching stock.

At Stations where the Carriage Cleaning is performed by the Traffic Department, Station Masters are reminded that they are responsible for the efficient performance of this duty, and that constant personal supervision is necessary to ensure the proper standard of cleanliness being maintained.

Instructions to Carriage Cleaners.—In order to avoid any misunderstanding on the part of Carriage Cleaners as to the proper method to be adopted in cleaning Carriages, the following instructions are issued for their guidance:—

#### 1.—INSIDES OF CARRIAGES.

The insides must first be dealt with in the following manner:-

(a) The rugs must be removed and shaken outside the compartment, and if the cushions require beating the work must be done sufficiently far away from the carriages to prevent the dust entering the compartments. (This applies to Stations not equipped with vacuum cleaning plant.)

(b) The cushions, upholstered backs, and seat beds must be thoroughly brushed in every case, the floor then swept, particular attention being given to that portion under the seat. The floor-cloth in all compartments and corridors must be scrubbed if necessary, or washed with water containing a small quantity of disinfecting fluid, i.e. Carbo Cleanser. The lavatory pans, basins, metal fittings must be thoroughly cleansed, and all metal fittings polished, mirror polished, floors swept and scrubbed or washed with water and disinfecting fluid.

(c) The windows, lamp globes, shades and reflectors must also be well cleaned and polished.

(d) When the compartment has been swept, the rug and cushion replaced and time allowed for the dust to settle, the dusting of the compartment must be proceeded with, for which dusters are provided. This duty must be thoroughly performed, attention being given to the window-ledges, racks and brackets, ventilators, polished doors and panels, and every part of the interior where dust is likely to accumulate.

(e) Particular attention is to be given at all large Depots and Stations to the periodical cleaning of the white painted and polished woodwork in compartments, corridors and lavatories with the special solution supplied from Swindon for this work.

Note.—Carriage Cleaners must not walk through Restaurant Cars for the purpose of getting from one vehicle to another in the performance of their duties.

#### 2.—OUTSIDES OF CARRIAGES.

The outsides of Carriages must be cleaned with a special preparation supplied from Swindon, and no other must be used.

Any carriages which cannot be properly cleaned by the usual men after these instructions have been carried out, should be sent to the nearest Carriage Repairing Depot where the Carriage Department Staff will attend to them; and all vehicles sent in for repair will be thoroughly cleaned before they leave the shops.

(a) The first duty of the men appointed to clean the outsides of carriages is to see that the doors, windows, and ventilators of every compartment which is about to be cleaned are properly closed.

(b) After the panels and windows of the Carriages have been attended to, the brass work must be cleaned and polished with Rotten Stone and water, or other authorised preparations.

(c) The buffers and buffer rods must also be cleaned by rubbing the rods with oily waste. The screw connections must also be regularly cleaned and oiled.

Every vehicle must be cleaned before it commences its first journey in the day; and when it makes more than one trip in the day it must be swept out and dusted before it commences its subsequent trips if it be practicable to do so.

The windows and ventilators of all empty carriages standing in Sidings or Carriage Sheds must be closed.

All articles belonging to the Refreshment Department found during cleaning operations must be collected and forwarded to the recognised Depot without delay. On account of the large amount of crockery broken, attention is directed to the necessity of carefully handling all such articles.

# (A) REGULATIONS FOR THE PROTECTION OF CARRIAGE CLEANERS, GASMEN, LAMPMEN AND OTHERS WORKING ON COACHING STOCK.

Where an engine is not attached to the vehicles.

1. Before any of the above-named men commence work :-

(a) Upon the cutside of vehicles on any line or siding on which it is possible for other vehicles to be shunted against them;

(b) Which necessitates the use of steps or ladders inside the vehicles or involves the men placing themselves in such a position that they might lose their balance if the vehicles

a red flag by day, or a red light during darkness, fog, or falling snow, must be exhibited at the end of the last vehicle nearest the direction from which vehicles might be shunted against those on which the men are at work. If it is possible for vehicles to be shunted against both ends of the vehicles on which the men are at work, the same precautions must be taken at both ends.

When the vehicle or vehicles stand wholly inside a shed, the flag or light must be exhibited at the

entrance or entrances to the shed on the line upon which the vehicles are standing.

If the vehicles are standing on a line adjoining a running line the red flag or red light must be exhibited on the side farthest away from the running line. When the vehicles are standing at a platform the flag or light must be placed on the platform side in such a position as to be plainly visible

3. Before commencing work on the vehicles each man concerned is responsible for seeing that he is protected by a red flag or a red light in accordance with the foregoing Regulations, but should more than one man or set of men be separately at work on the same vehicles, or on the same line or siding, each man or set of men must be separately protected by additional red flags or lights. Each man working alone will be held responsible for carrying out these Regulations. Where a set of men is engaged one of them must be made responsible for carrying out these Regulations,

4. The man responsible for carrying out Regulation must, before work is commenced, also satisfy himself that no shunting operations are in progress affecting the vehicles on which work is to be done.

5. When vehicles in a siding adjoining a running line, or on a running line adjoining another running line, are being cleaned, gassed or watered, a good look-out must be kept and care must be taken to see that doors are not left projecting on the running line side. Planks, steps, or long handled brushes must not be used on the side of volicles next to a running line except when authorised by the Chief Mechanical Engineer and Superintendent of the Line.

Where an engine is attached to the vehicles.

6. Before commencing the work referred to in (a) and (b) of Regulation 1, or supplying gas or water to a train or vehicles, the man concerned must place a red flag by day, or a red light during darkness, fog or falling snow, on the side of one of the vehicles for the guidance of the Traffic Department Staff and Enginemen. In the case of a train standing at a platform, the flag or light must be placed on the platform side in such a position as to be plainly visible therefrom.

7. Passenger trains or vehicles must not be moved whilst men are at work on the top of the vehicles.

#### General Instructions.

8. The red flag or red light exhibited for the protection of the men must not be removed until the work has been completed, or has been suspended to admit of the vehicles being moved, and the man before removing it must satisfy himself that all the men concerned are clear. (See also

9. Vehicles protected by the red flag or red light must not be moved, nor must others be shunted against them, except as provided in the second paragraph of Regulation 10. Porsons responsible for starting trains must be careful to see that no red flag or red light is exhibited before giving the signal

for the train to start.

10. Enginemen and Shunters are particularly warned to satisfy themselves when approaching, and before coming into contact with, vehicles standing on platform and other lines, or sidings, and before backing on to or attaching or detaching vehicles to or from trains at platforms, that no red flag or red light is exhibited for protection purposes in accordance with these Regulations.

Should, however, it be necessary to attach or detach vehicles or to change engines after the red flag or red light has been placed on the platform side of a through train at a passenger station this must be done under the supervision of the person responsible for starting the train but great care must be taken to see that the train is not moved until all the men concerned have ceased work and are clear

11. Shunters and others must keep a good look-out when shunting on lines and sidings adjacent to those occupied by vehicles on which men are at work,

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WESTERN RAILWAY Private and not for publication. General Manager's Office, Circular No. 3680. Paddington Station. (Amending Circular No. 3405.) November 1st, 1945. The instructions shewn on pages 304 to 308 of the General Appendix to the Rule Book dated July 1936 in regard to the instructions to be observed for the protection of Workshop Vans, Carriage Cleaners, Gasmen, etc., are revised as follows: (A) REGULATIONS FOR THE PROTECTION OF CARRIAGE CLEANERS, GASMEN, LAMPMEN AND OTHERS WORKING ON COACHING STOCK 1. During the time that men are at work in the sidings referred to above, the points leading to such sidings must be kept padlocked so as to protect the men and the key of the padlock must be held by the repair staff, who will be responsible for securing and releasing the points. 2. Before any work is commenced or resumed on such a siding it is the duty of the workman, or where more than one man is engaged of the man in charge, to examine the points giving access to the siding and satisfy himself that they are securely padlocked in such a manner that a shunt cannot be made into the siding on which they intend to work. 3. Before the points of such sidings are unlocked for shunting purposes, or to enable vehicles to be placed in or removed from the sidings, it must be ascertained whether any men are working on vehicles in the sidings, and the padlock must not be taken off until such men have been warned to place themselves in a position of safety, nor must work be resumed by them until the points have again been padlocked. 4. If Railway Company's repair staff are employed in the siding they will be responsible for carrying out

these regulations. If no Railway Company's staff are employed the duties must be carried out by the staff of the private firm or firms concerned. When the Siding is required for shunting purposes or on the termination of all repair work, the man

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holding the key of the padlock must hand it to the person in charge of the shunting, but this must not be ced done till the man holding the key has satisfied himself that there are no men at work in the sidings and the ted work on the vehicles has reached a stage when shunting can safely be permitted. (G.A.18, 11/47, Gen. Man. Min. 3765.)

protected by additional red flags or lights. Each man working alone will be held responsible for carrying out these Regulations. Where a set of men is engaged one of them must be made responsible for carrying out these Regulations.

4. The man responsible for carrying out Regulation 3 must, before work is commenced, also satisfy himself that no shunting operations are in progress affecting the vehicles on which work is to be done.

5. When vehicles in a siding adjoining a running line, or on a running line adjoining another running line, are being cleaned, gassed or watered, a good lookout must be kept and care must be taken to see that doors are not left projecting on the running line side. Planks, steps, or long-handled brushes must not be used on the side of vehicles next to a running line except when authorised by the Chief Mechanical Engineer and Superintendent of the Line.

#### Where an engine is attached to the vehicles.

6. Before commencing the work referred to in (a) and (b) of Regulation 1, or supplying gas or water to a train or vehicles, the man concerned must place a red flag by day, or a red light during darkness, fog or falling snow, on the side of one of the vehicles for the guidance of the Traffic Department Staff and Enginemen. In the case of a train standing at a platform, the flag or light must be placed on the platform side in such a position as to be plainly visible therefrom.

7. Passenger trains or vehicles must not be moved whilst men are at work on the top of the vehicles.

## General Instructions.

8. The red flag or red light exhibited for the protection of the men must not be removed until the work has been completed, or has been suspended to admit of the vehicles being moved, and the man before removing it must satisfy himself that all the men concerned are clear. (See also Regulation 3.)

9. Vehicles protected by the red flag or red light must not be moved, nor must others be shunted against them except as provided in the second paragraph of Regulation 10. Persons responsible for starting trains must be careful to see that no red flag or red light is exhibited before giving the signal for the train to start.

10. Enginemen and Shunters are particularly warned to satisfy themselves when approaching, and before coming into contact with, vehicles standing on platform and other lines, or sidings, and before backing on to or attaching or detaching vehicles to or from trains at platforms, that no red flag or red light is exhibited for protection purposes in accordance with these Regulations.

Should, however, it be necessary to attach or detach vehicles or to change engines after the red flag or red light has been placed on the platform side of a through train at a passenger station this must be done under the supervision of the person responsible for starting the train but great care must be taken to see that the train is not moved until all the men concerned have ceased work and are clear of the train.

11. Shunters and others must keep a good lookout when shunting on lines and sidings adjacent to those occupied by vehicles on which men are at work.

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12. Carriage Cleaners and others working on coaching stock must not pass under, over, or between the buffers of vehicles, nor between the stop block and the nearest vehicle, when less than a carriage length apart. If necessary, men may pass through a van or third class compartment when the vehicle is stationary but must take care to close and fasten the doors after them.

13. Carriage Cleaners and others working on coaching stock must not stand on the lines between sections of between a vehicle and the stop block unless they are properly protected in accordance with the foregoing Regulations.

14. Special attention is directed to Rule 11 in the Rule Book.

Note .- Should it be necessary for work to be done underneath a vehicle the provisions of Regulation 10, release to the protection of Brake Fitters, etc., must be observed.

# (B) REGULATIONS FOR THE PROTECTION OF BRAKE FITTERS, LIFTERS, REPAIRERS AND OTHERS WORKING ON CARRIAGE OR WAGON STOCK

These Regulations must also be observed by the employees of Private Carriage or Wagon Repairing Firms.

In repairing shop sidings, and sidings specially set apart for the purpose of carrying out repairs or other work on carriage or wagon stock, where protection is afforded against shunting by means of

1. The points leading to the sidings referred to above must be kept padlocked in such a manner as to protect the men working in the sidings.

2. Before any work is commenced on such a siding it is the duty of the workman, or where more than one man is engaged of the man in charge, to examine the points giving access to the siding and satisfy himself that they are securely padlocked in such a manner that a shunt cannot be made into the siding on which they intend to work.

3. Should the points be found not properly padlocked so as to afford the necessary protection, the Inspector or other person in charge of the shunting must be applied to and the points padlocked before the work is commenced.

4. Before the points of such sidings are unlocked for shunting purposes, or to enable vehicles to be placed in or removed from the sidings, it must be ascertained whether any men are working on vehicles in the sidings, and the padlock must not be taken off until such men have been warned to place themselves in a position of safety, nor must work be resumed by them until the points have again been padlocked.

## In sidings and on lines other than those referred to above.

5. Before any work is commenced it is the duty of the workman, or where more than one man is engaged of the man in charge, to go to the Station Master, Inspector, or other person in charge of the line or sidings, advise him what work is required to be done and obtain his permission for the work to be carried out.

6. A red flag by day or a red light during darkness, fog, or falling snow, must be exhibited at the end of the last vehicle nearest the direction from which vehicles might be shunted against those on which the men are at work. If it is possible for vehicles to be shunted against both ends of the vehicle on which the men are at work, the same precautions must be taken at both ends.

7. If the vehicle is standing on a line adjoining a running line the red flag or red light must be exhibited on the side farthest away from the running line. When the vehicle is standing at a platform the flag or light must be placed on the platform side in such a position as to be plainly visible therefrom.

8. Before commencing work on the vehicle each man concerned is responsible for seeing that he is protected by a red flag or a red light in accordance with the foregoing Regulations, but should more than one man or set of men be separately at work on the same line or siding, each man or set of men must be separately protected by additional red flags or lights.

9. The wheels of the vehicle upon which work is to be carried out must be secured by sprags or scotches and if other vehicles are standing on the same line or siding, one or two brakes must be applied on the vehicles both in front and in rear of the one upon which work is to be carried out, or the wheels of such vehicles must also be secured by sprags or scotches

0. Before examination or other work is commenced underneath a vehicle attached to an engine, the Driver, and Guard or Shunter, as well as the Station Master, Inspector or other person in charge of the line or sidings, must be advised, and instead of the flag or light being exhibited as prescribed in Regulation 6, a man with a red hand signal must stand in the best possible position and keep a good lookout for the protection of the workmen.

When examination or other work is required to be carried out underneath a vehicle and there is not an engine attached, the instructions in Regulations 5, 6, 7, 8, 9, 11, 12 and 13 must be observed.

11. The red flag or red light exhibited for the protection of workmen must be removed only under the instructions of the man in charge of the work, after the work has been completed or has been suspended to admit of the vehicle or vehicles being moved. The person in charge of the shunting must be duly advised. Care must be taken to see that all concerned are clear and all scotches and sprags have been taken away before the flag or light is removed.

12. Each man working alone will be held responsible for carrying out these Regulations. Where more than one man is engaged one of them must be made responsible for carrying out these Regulations.

13. Whilst the red flag, or red light is exhibited, the vehicles so protected must not be moved, nor must others be shunted against them.

#### General Instructions.

14. Where there are repairing shop sidings, or where other sidings are set apart for the purpose of carrying out repairs, etc., the work on vehicles must, as far as practicable, be carried out in such sidings.

15. Shunters and others must keep a good lookout when shunting on lines and sidings adjacent to those occupied by vehicles on which men are at work, and must, before commencing such shunting, warn the men engaged on the vehicles what they are about to do.

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- 16. The special attention of the Railway Company's employees is directed to Rule 11 in the Rule Book.
- 17. Except on Sidings or running lines which are protected in accordance with the provisions of clauses 1 to 6 or 10, employees are forbidden to go between the buffers of vehicles or between stop blocks and the nearest vehicle, when less than 50 feet apart, without first satisfying themselves that none of the vehicles is about to be moved by engine, capstan, horse or other power, and that no shunting is going on upon the lines which they are about to cross.

18. Each Private Carriage or Wagon Repairing Firm will supply to their own staff the red flags and lamps of a pattern approved by the Railway Company concerned.

# INSTRUCTIONS TO BE OBSERVED FOR THE PROTECTION OF MEN REPAIRING OR PAINTING STOP BLOCKS

Before commencing to repair or paint a Stop Block, the man concerned must obtain the permission of the Station Master, Inspector or person in charge, who, if permission can be given, must arrange to place the line or siding temporarily out of use, during which time the points must be secured by clip and padlock. If the points are worked from a signal box, the Station Master, Inspector or person in charge must inform the Signalman of what is about to be done.

When it is not possible to place the line or siding out of use, the Staffon Master, Inspector or person in charge must arrange for a distance of not less than 50 yards to be maintained between the Stop Block and the nearest vehicle on the line or siding during the progress of the work. The man engaged in the work must, before commencing, exhibit a red flag at the end of the vehicles nearest to the Stop Block in a position where it can be most readily observed by Shunters, Enginemen and others concerned, engaged in the movement of traffic, and must fix two detonators on the rail close to that vehicle in order that immediate warning will be given in the event of a movement towards the Stop Block.

# REGULATIONS FOR THE PROTECTION OF CARRIAGE CLEANERS, ETC.—Continued.

12. Carriage Cleaners and others working on coaching stock must not pass under, over, or between the buffers of vehicles, nor between the stop block and the nearest vehicle, when less than a carriage length apart. If necessary, men may pass through a van or third class compartment when the vehicle is stationary but must take care to close and fasten the doors after them.

13. Carriage Cleaners and others working on coaching stock must not stand on the lines between vehicles or between a vehicle and the stop block unless they are properly protected in accordance

with the foregoing Regulations.

14. Special attention is directed to Rule 11 in the Rule Book.

Note. Should it be necessary for work to be down underneath a vehicle the provisions of Regulation 10, relative to the protection of Brake Fitters, etc., must be observed.

# (B) REGULATIONS FOR THE PROTECTION OF BRAKE FITTERS, LIFTERS, REPAIRERS, AND OTHERS WORKING ON CARRIAGE OR WAGON STOCK.

These Regulations must also be observed by the employees of Private Carriage or Wagon Repairing Firms.

In repairing shop sidings, and sidings specially set apart for the purpose of carrying out repairs or other work on carriage or wagon stock, where protection is afforded against shunting by means of padlock and key.

1. The points leading to the sidings referred to above must be kept padlocked in such a manner

as to protect the men working in the sidings,

2. Before any work is commenced on such a siding it is the duty of the workman, or where more than one man is engaged of the man in charge, to examine the points giving access to the siding and satisfy himself that they are securely padlocked in such a manner that a shunt cannot be made into the siding on which they intend to work.

3. Should the points be found not properly padlocked so as to afford the necessary protection, the Inspector or other person in charge of the shunting must be applied to and the points padlocked

before the work is commenced.

4. Before the points of such sidings are unlocked for shunting purposes, or to enable vehicles to be placed in or removed from the sidings, it must be ascertained whether any men are working on vehicles in the sidings, and the padlock must not be taken off until such men have been warned to place themselves in a position of safety, nor must work be resumed by them until the points have again been padlocked.

In sidings and on lines other than those referred to above.

- 5. Before any work is commenced it is the duty of the workman, or where more than one man is engaged of the man in charge, to go to the Station Master, Inspector, or other person in charge of the line or sidings, advise him what work is required to be done and obtain his permission for the work to be carried out.
- 6. A red flag by day or a red light during darkness, fog or falling snow, must be exhibited at the end of the last vehicle nearest the direction from which vehicles might be shunted against those on which the men are at work. If it is possible for vehicles to be shunted against both ends of the vehicle on which the men are at work, the same precautions must be taken at both ends.

7. If the vehicle is standing on a line adjoining a running line the red flag or red light must be exhibited on the side farthest away from the running line. When the vehicle is standing at a platform the flag or light must be placed on the platform side in such a position as to be plainly visible therefrom.

- 8. Before commencing work on the vehicle each man concerned is responsible for seeing that he is protected by a red flag or a red light in accordance with the foregoing Regulations, but should more than one man or set of men be separately at work on the same line or siding, each man or set of men must be separately protected by additional red flags or lights.
- 9. The wheels of the vehicle upon which work is to be carried out must be secured by sprags or scotches, and if other vehicles are standing on the same line or siding, one or two brakes must be applied on the vehicles both in front and in rear of the one upon which work is to be carried out, or the wheels of such vehicles must also be secured by sprags or scotches.
- 10. Before examination or other work is commenced underneath a vehicle attached to an engine, the Driver, and Grard or Shunter, as well as the Station Master, Inspector or other person in charge of the line or sidings, must be advised, and instead of the flag or light being exhibited as prescribed in Regulation 6, a man with a red hand signal must stand in the best possible position and keep a good look-out for the protection of the workmen.

When examination or other work is required to be carried out underneath a vehicle and there is not an engine attached, the instructions in Regulations 5, 6, 7, 8, 9, 11, 12 and 13 must be observed.

# REGULATIONS FOR THE PROTECTION OF BRAKE FITTERS, ETC .- Continued.

11. The red flag or red light exhibited for the protection of workmen must be removed only under the instructions of the man in charge of the work, after the work has been completed or has been suspended to admit of the vehicle or vehicles being moved. The person in charge of the shunting must be duly advised. Care must be taken to see that all concerned are clear and all seetches and sprags have been taken away before the flag or light is removed.

12. Each man working alone will be held responsible for carrying out these Regulations. Where more than one man is engaged one of them must be made responsible for carrying out these Regulations.

13. Whilst the red flag or red light is exhibited, the vehicles so protected must not be moved, nor must others be shunted against theme

# General Instructions.

14. Where there are repairing shop sidings, or where other sidings are set apart for the purpose of carrying out repairs, etc., the work on vehicles must, as far as practicable, be carried out in such sidings.

15. Shunters and others must keep a good look out when shunting on lines and sidings adjacent to those occupied by vehicles on which men are at work, and must, before commencing such shunting, warn the men engaged on the yehicles what they are about to do.

16. The special attention of the Railway Companies' employees is directed to Rule 11 in the Rule Book.

18 Each Private Carriage or Wagon Repairing Firm will supply to their own staff the red flags and lamps of a pattern approved by the Railway Company concerned.

## INSTRUCTIONS TO BE OBSERVED FOR THE PROTECTION OF MEN REPAIRING OR PAINTING STOP BLOCKS.

Before commencing to repair of paint a Stop Block, the man concerned must obtain the permission of the station Master, Inspector or person in charge, who, if permission can be given, must arrange to place the line or siding temporarily out of use, during which time the points must be secured by clip and padlock. If the points are worked from a signal box, the Station Master Inspector or

person in charge must inform the Signalman of what is about to be done.

When it is not possible to place the line or siding out of use, the Station Master, Inspector of person in charge must arrange for a distance of not less than 50 yards to be maintained between the Stop Block and the nearest vehicle on the line or siding during the progress of the work. The man engaged in the work must, before commencing, exhibit a red flag at the end of the vehicle nearest to the Stop Block in a position where it can be most readily observed by Shunters and Enginemen engaged in the movement of traffic, and must fix two detonators on the rail close to that vehicle in order that immediate warning will be given in the event of a movement towards the Stop Block.

# COACHES SET ON FIRE FROM WASTE LEFT ON ROOFS.

Waste must not be left on the roofs of coaches, between the large label boards and the roofs or in other places where it is likely to be fired by sparks, etc.

# COACHES REQUIRING TO BE DISINFECTED.

Any carriage in which it is known that a person suffering from an infectious disease has travelled must be stopped at the Station where the discovery is made, and placed in an isolated part of the yard or sidings, and a telegraphic advice immediately dispatched to the Chief Mechanical Engineer, Swindon, to that effect, when a competent person will be sent for the purpose of disinfecting it on the spot.

In the event of infected passengers leaving the Company's premises before it is known that they are suffering from an infectious disease, and the information is subsequently conveyed to the Company by Sanitary or other authorities, steps must at once be taken to trace the Coach in which the infected person travelled, so that it may be stopped, the Chief Mechanical Engineer being then communicated with as explained above.

It is also very important that, if possible, the number of the compartment should be taken, as in some extreme cases it may be necessary to strip the compartment and burn the trimmings.

The windows of infected coaches must be closed as speedily as possible, the doors locked, and paper pasted over the keyholes and other apertures.

If from any exceptional cause it should become necessary to send an infected coach to Swindon to be dealt with, the Chief Mechanical Engineer should be wired accordingly, giving the number of the coach, and the train by which it is being forwarded. The nature of the contamination should be written on the label, i.e. whether verminous or contagious, and if the latter, the name of the complaint must also be stated, as the methods of disinfecting with the new plant at Swindon vary accordingly. 17. Except on sidings or running lines which are protected in accordance with the provisions of clauses 1 to 6 or 10, employees are forbidden to go between the buffers of vehicles or between stop blocks and the nearest vehicle, when less than 50 feet apart, without first satisfying themselves that none of the vehicles is about to be moved by engine, capstan, horse or other power, and that no shunting is going on upon the lines which they are about to cross.

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Red-banded lamps must not be used for ordinary traffic and any of these lamps on hand after the passing of the special holiday traffic must be promptly returned to Swindon Stores. (G.A. 7.-3/40. LK1/2639/23.)

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Except in cases of Tubercular patients, in regard to which refer to instructions below, when a compartment is reserved for an infectious party, other passengers must not be permitted to travel in the remaining compartments in the same coach, unless it is non-corridor. All the compartments in a corridor coach must be labelled and locked, and Guards must take steps to ensure that the unoccupied compartments remain locked and are not opened up to passengers. When the journey involves changes en route a through reserved coach should be arranged if practicable.

The whole coach or coaches used are to be disinfected after use, unless non-corridor; if non-corridor

only the compartments actually used are to be dealt with.

For Tubercular infectious patients the exclusive use of a coach is not necessary; the reservation of one compartment only is sufficient. In such cases the remaining compartments in the coach are to be available for other passengers. When the journey involves changes en route, a through coach containing the reserved compartment, should be arranged if practicable. (When in the notification to the Company of the journey of a Tubercular patient requiring reservation of a compartment, a competent Medical Authority states in writing there is no risk to the public on account of infection and no necessity for disinfection, no disinfection arrangements need be made. It will be necessary, however, to allow a current of air to be passed through the compartment for some hours before being used again.)

In cases of Whooping Cough, providing the Medical Officer responsible does not consider disinfection necessary, it will be sufficient if the compartment is thoroughly cleaned after use, and a current of air is passed through the compartment for some hours, before being used again; no disinfection fee is chargeable in such

cases.

Platform staff at entraining and detraining stations to ensure that the Guard is aware of the circumstances: in cases where Guards change en route, the Guard booking off to ensure that the relieving Guard is advised of the reservation.

In no circumstances must labels be removed, or doors unlocked, by unauthorised persons, either in the case of the compartment actually infected or the remaining compartments retained in the coach, until disinfec-

tion has been carried out.

After detraining the patient the coach passing to the disinfection point should be clearly labelled throughout "FOR DISINFECTION". When only one compartment is to be disinfected the compartment concerned must be clearly labelled on both sides (red or other distinctive coloured ink to be used) "FOR DISINFEC-TION". The onus of carrying this out must rest upon the Station Master at the detraining point, and if suitable labels are not on hand steps must be taken to secure or improvise.

The third paragraph on page 309 of the existing instructions to be amended to read:

The following are the charges for the disinfection of coaching vehicles:

£2 8s. 10d. \*Saloon or complete corridor coach

\*Compartment in corridor or non-corridor coach

Vehicle containing the corpse of a person deceased as a result of an infectious or contagious £1 12s. 7d. disease

The charge to be credited to the Company performing the disinfection.

(\*-Where one or more changes of train are necessary and the Saloon or coach does not work through, the above charges apply to each train. No reservation fee is charged.) (G.A. 18, 11/47, T.87530, M.)

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# COACHES REQUIRING TO BE DISINFECTED-Continued.

When application is made to convey the body of a person who has died from an infectious disease, the authority of the Superintendent of the Line must be obtained. No such application must, however, be entertained until the Medical Officer of Health certifies that the body may be removed, and if necessary the vehicle used must be subsequently sent to Swindon to be disinfected.

Bodies of persons who have died from Spotted Fever must not be accepted unless coffins are hermetically sealed and a certificate is received from the Local Medical Officer of Health certifying that there is no danger in handling and conveyance by rail. The vans used must, in all cases, be disinfected

The following are the charges agreed generally by Railway Companies for the disinfection of coaching vehicles :-

Saloon or corridor carriage or compartm	ents in	corrio	ior car	rriage .	or con	Threre	Hon-	0.	
corridor carriage								30	0
Compartment in non-corridor carriage								10	0
Guard's van conveying corpse								20	0

The charge to be credited to the Company performing the disinfection.

It is imperative that all carriages used for the conveyance of Foreign Emigrants should be thoroughly disinfected immediately after the journey is completed, and before the vehicles are utilised for other traffic.

This applies not only to Foreign Emigrants travelling in large numbers, but also to small parties for whom one or more compartments are reserved. In the latter case it is essential that any small

number should not be allowed to mix with ordinary passengers.

To ensure all carriages, or odd compartments in carriages, being thoroughly disinfected, it will be necessary for the forwarding station to advise the receiving station so that the latter can make the necessary arrangements. The compartments should be plainly labelled to assist the staff at intermediate and terminating points.

# COACHES FOR THE CONVEYANCE OF HOP-PICKERS.

Only THIRD Class coaches of the oldest type must be used for the conveyance of Hop-pickers. In no circumstances are lavatory carriages or carriages with first class compartments to be provided.

# SIDE, TAIL AND HAND LAMPS.

The number of side, tail and hand lamps necessary to meet normal traffic Authorised Stock. requirements has been fixed and the authorised allotment to any station must not be exceeded.

Stores vans—each serving different Districts, and in charge of an Attendant—Replacement of will carry a sufficient supply of Side, Tail and Hand lamps to replace damaged lamps. lamps as required. Station Masters must arrange for all damaged lamps to be ready on the platform when the van arrives.

If a lamp requires repairs at a station not served by one of the Stores vans, it must Stations where be forwarded to the nearest station at which the van calls, where the exchange will not call. be made on demand. A requisition will not be necessary, but a special label showing the forwarding station must accompany the lamp.

A stock of Tail lamps marked with two white bands is kept at Swindon, and will White-banded be supplied at Holiday Periods to equip Special Trains and the divided portions of Lamps. ordinary trains.

RED WHITE BANDED LAMPS MUST NOT BE USED FOR ORDINARY TRAFFIC PURPOSES.

Any station requiring extra lamps for special events must apply to the Extra Lamps for Divisional Superintendent or District Traffic Manager, who will arrange to meet the special events. requirements from the reserve stock of lamps in the Division. When this cannot be done, the Divisional Superintendent or District Traffic Manager will countersign the application and pass it forward to the Superintendent of the Line, when arrangements will be made for the lamps to be supplied.

ADDITIONAL LAMPS ARE NOT ISSUED FROM SWINDON STORES Additional Lamp EXCEPT ON THE AUTHORITY OF THE SUPERINTENDENT OF THE LINE. Stores.

Tail lamps working on ordinary passenger trains are usually balanced with the Balancing of coaches, but where this is not so the Station Master will be held responsible for fixing Train Lamps. a balance.

Under no circumstances must the number of tail lamps on hand exceed the Surplus lamps. authorised supply. Any surplus lamps must be forwarded immediately to the Stores Superintendent, Swindon, and an advice sent to the Divisional Superintendent or District Traffic Manager.

The following paragraph to be added :-

Great Western tail lamps are being painted white instead of red. Side lamps will continue to be painted black.

Tail and Side Lamps.

SIDE, TAIL AND HAND LAMPS-Continued.

Handling of Lamps. Hand Lamps on Joint Lines. The utmost care is needed when handling lamps, so as to avoid damage.

When it is necessary for stations on Joint Lines or other stations which have Joint accounts, to send in hand lamps for exchange, the lamps must in all cases be accompanied by the special green requisition No. 224, in order that the Joint account may be correctly charged. Lamps for repairs should be handed to the Stores Van Attendant at stations at which the Van calls, or, in the case of stations at which the Van does not call, sent to the nearest exchanging station or Depot, accompanied, in all cases, by a green requisition.

TAIL LAMPS. (L.M. & S., L. & N.E. AND SOUTHERN RAILWAYS.)

The outer cases of the L.M. & S. Company's train tail lamps are aluminium colour instead of red.

The L. & N.E. Company's tail lamps are plated and allocated to certain stations and depots.

L.N.E. lamps which are painted **Red** must be used on Passenger Trains only, whilst those painted **White** must be used on Freight Trains only.

The Southern Company's passenger train tail lamps, Freight train side-tail lamps and Freight train tail lamps are being painted white instead of red.

## CLEANING, TRIMMING AND LIGHTING OF TRAIN LAMPS, AND MODE OF WORKING.

All trains must be provided at starting point with tail lamps and, where required, side lamps, cleaned, trimmed and in every respect fit for use.

side lamps, cleaned, trimmed and in every respect fit for use.

The Station Masters must satisfy themselves that the following regulations for cleaning and trimming lamps are carried out:

Side, Tail and Hand lamps.—Only petroleum must be used in these lamps; great care must be taken in trimming the lamps to fill the tanks, which must be wiped quite dry before being replaced in the lamp every part of which must also be well cleaned, especially the burners and vent holes.

Only § inch wick must be used for these lamps; it should be cut into lengths of six mehes, and be quite dry before being put into the burners. The top of the wick must be cut perfectly level aite it has been put into the burners, and the lamps must be lighted 10 minutes at least before they are required to be used, so that the flame may be properly regulated; the wink must reach the bottom of the tank, and if too short to do this, it must be replaced by a fresh length. The wick must also be changed directly it is found to have become discoloured or hardened.

Care should be taken to see that the tanks are free from water; this, when in the tank, can be seen moving at the bottom in globules, or its presence is indicated by the tank having specks of rust; if there should be any water in the tank it must be emptied and resh oil and wick put in.

Special attention must be paid to the insides of the tops of the lamps in order

Special attention must be paid to the insides of the tops of the lamps in order that any soot which may have accumulated may be promptly removed, and soot brushes are provided for this purpose. The tops of the lamps must on no account

be cleaned with oily waste.

All lamps found to be defective or dimly burning must be examined by the Station Master and, if necessary, exchanged for another lamp from the lamp-room. Side and tail lamps must be removed from the trains as soon as practicable in the lamp in the lamp in the lamp in the lamp in the lamp.

after the arrival of the latter at destination; if not required for further use, the lights to be blown out, and the lamps taken to the lamp-room by the lampmen; in no case must the light be extinguished by nolently jerking or knocking the lamps on the benches or floor.

The utmost care must be taken of the articles supplied to the lamp-room for use, strict economy must be exercised in the use of stores.

Every care must be exercised on the part of the lampmen to prevent damage

(by scratching or otherwise) to lamps. The barrows, lamp-racks, trimming-benches, floors and all places and articles in connection with the lamp-room must be kept clean, and dirty waste and old wick, &c., must be kept together and removed from the lamp-room daily.

Every lampman must strictly attend to the duties assigned to him, in accordance with his duty sheet, which should be posted in the lamp-room.

The outside of all tail lamps should be kept a bright red colour, and with this in riew the outsides should be frequently cleaned with cleaning oil, which can be obtained from the Stores Department.

The lampmen must not permit lamps to be used which are in any manner defective, or require repainting, particularly in the case of tail lamps. All lamps requiring repairs must be exchanged at the next visit of the stores van.

Guards must examine the lamps on their trains at the starting stations and report defects, if any, to the Station Master, also note the condition of the lamps at intermediate stations and, if any defects are observed, have the lamps changed, if circumstances will permit. If not, in order to avoid delay to trains, the Station Master must advise the next lamping station to be ready to do this. GP31

Trimming petroleum side and tall and hand lamps.

Wick for side and tall lamps.

Water in tank.

Soot in tops.

Defective lamps.

Lamps to be removed by lampmen from train and extinguished.

Care and economy in stores. Damage to lamps, and cleaning rooms

Duties.

Repairs to lamps.

Guards to report defects.

CLEANIN

CLEANING, TRIMMING AND LIGHTING OF TRAIN LAMPS AND MODE OF WORKING.—Page 310.

The first paragraph of these instructions to be amended to read:-

Side, Tail and Hand Lamps.—Only petroleum must be used in these lamps; great care must be taken in trimming the lamps to ensure that an adequate quantity is poured into the tank but that the tanks are not filled above the level of the bottom of the burner collar. The tanks must be wiped dry before being placed in the lamp, every part of which must be well cleaned especially the burners and vent holes.

(G.A.30 Op .-- 9/54 LKI/10688/229).

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#### INSTRUCTIONS AS TO CHARGING VEHICLES WITH OIL GAS AND ALSO FOR LIGHTING AND EXTINGUISHING OIL GAS LAMPS.

Oil Gas Works are situated at Paddington, Swindon, Bristol, Exeter, Cardiff, Stations at Newport, Ebbw Junction, Llandarcy Siding (National Oil Refineries), Worcester, which Coaches Wolverhampton and Birkenhead. Travelling gas tanks are also kept at most of the with gas. principal stations (other than those above named) at which trains are formed. These tanks, being constructed to work on Passenger Trains only, must not be conveyed by Freight Trains, and great care must be exercised with them when shunting.

(See page 237.)

"A" Head Lamp Trains

which Coaches

GAIR

Gas tanks must not be worked on Express Passenger Trains, except in cases of emergency. When Gas Tanks are regularly worked between stations, particulars of the trains by which they should be conveyed must be recorded in the Coach Working Programme.

Before coaches leave the Station at which they should be charged, care should Coaches, &c., be taken to see that the gas holders are properly filled with gas, as otherwise there charged. will not be sufficient for the out and home journey. At stations where travelling gas tanks are stationed the Carriage Department examiners will, when requested, charge the reservoirs of vehicles, upon the tank and vehicles being placed in position for this to be done. After vehicles have been charged, the valves at the ends of the indiarubber filling tube should be closed, but the gas remaining in the pipe should not be discharged into the atmosphere.

When it is necessary to use a lamp during the operation of charging the reservoirs, the safety lamp specially provided for the purpose must be used and under no circum-

stances must an ordinary hand lamp be brought near the filling valve.

The men employed to do this work must previously be properly instructed to ensure that they are competent to do gas filling, and a person at each station where this work is carried out must be made responsible for seeing that the safety lamps are kept clean and in proper condition, and that the screw connections on the hydrants and gas charging pipes are in proper condition and a good fit.

Should the gauze in a safety lamp become pierced or otherwise damaged, or any damage occur to the body of the lamp, which would allow air to be admitted, other than through the gauze, the lamp must on no account be used, but must be sent to

Swindon for repairs.

In the event of fire breaking out, the gas must at once be shut off by closing the ground filling valves and the stop valves on the ends of the reservoirs.

Before vehicles commence their journey the globes and reflectors must be Cleaning and thoroughly cleaned and polished with chamois leathers, whether the light is required Lighting. or not. Care must be exercised in order to avoid damage to the incandescent mantles. In the event of the apparatus being found to be defective, the attention of the Carriage examiner must be called to the matter at once.

The stop valve is placed in the corridor of Corridor vehicles, and in the guard's Incandescent compartment of Vans. A lever to operate the stop valve for turning the gas "off" Lighting. 'on" is placed at one end of Ordinary coaches. In Double-ended Slips a stop valve is fixed in each of the guards' compartments, but it is only necessary to open one of the valves before lighting the lamps. The same valve must be operated to extinguish the light throughout, and on no account must the other valve be interfered with. On Milk Vans, etc., the lever is fixed to the sole-bar, on which the "on" and "off" positions are marked.

The Bye-pass arrangement is to a great extent independent of the main lighting, Bye-pass. and the small bye-pass lights burn continuously.

In order to raise or lower the lights, the handle of the stop valve must be turned Regulating, right over to the "on" or "off" position, as the case may be, and not left intermediately, otherwise a waste of gas and damage to mantles will result.

Accidents have occurred through glass bowls in gas-lit coaches cracking and Broken Gas falling. In most cases the trouble has been traced to defective bye-pass nipples, Carriage which have projected a point of flame in the direction of the glass and so caused the Stock. globe to crack.

When nipples are found to be damaged, the Examiner should be advised, and the lamp should not be lighted until it is in order.

### SUPPLY OF GAS IN COACHES.

Gas coaches, collected to form Special and Excursion trains, must be furnished with sufficient gas to supply light for the outward and home journeys.

When coaches are ordered by the Rolling Stock Department of the Superintendent of the Line's Office to be sent to any other Station, and gas coaches are sent, steps should be taken by the person in charge at the station from which the coaches start to ascertain the amount of gas in the reservoirs, and if not sufficient, instructions should be given for the coach, or coaches, to be properly charged at

#### SUPPLY OF GAS IN COACHES-Continued.

the nearest convenient Station en route, and an advice by wire should be sent by the Station Master to the charging station, stating the number of coaches to be charged, in order that proper arrangements may be made with the Locomotive Department for the reservoirs to be filled without delay. When it is arranged for a Special to be run with the empty coaches, the Station Master issuing the order for the train to be run will be responsible for sending an advice by wire giving the number of coaches, if any, to be gassed.

Should the Guard of the train pick up any such coaches on the journey, he will be responsible for arranging with the Station Master at the station where the coaches are attached, for a telegraphic advice to be forwarded to the gassing station, if the coaches require gas.

The Stations at which trains, or coaches, may be charged while standing at the platforms are :-

Paddington Swindon Bristol, Temple Meads Exeter

Worcester Wolverhampton Birkenhead.

Any gas coaches ordered to be sent from a station at which gas can be supplied must be fully charged before they are sent away.

# INSTRUCTIONS RESPECTING THE USE AND REPAIR OF GAS LIGHTING TORCHES.

Gas Lighting Torches are to be used in all cases for lighting square and hexagon gas lamps.

If not already supplied, a torch and oil can painted red (with serew top) to contain benzoline or petrol is to be supplied to each Station or Depot lighted by square and hexagon gas lamps. Stations and Depots not supplied with a torch must at once apply to the Stores Department, Swindon, for a torch and can, by special requisition through the Divisional Superintendent or District Goods Manager.

In the event of a torch requiring repair, a telegram must be immediately forwarded to the Stores Department at Swindon requesting that a replace torch should be despatched by the next train. If not received after a reasonable time has elapsed, repeat telegrams should be sent to Swindon Stores.

Torches sent by train from Swindon will be attached to a board to prevent damage.

On receipt of the new torch, that requiring repair must be at once returned to the Stores Department, Swindon Stores, the packing received with the new torch being used so as to prevent damage to the torch during transit and repairs requisition (Form 224) in respect of the torch requiring repairs should be rendered, and forwarded under separate cover to Swindon Stores.

#### Use of Torch.

Fill upper reservoir with torch oil which is supplied ready mixed by the Stores Department, Swindon.

Charge lower reservoir with a dessertspoonful of common benzoline or petrol. (To get best results re-charge each evening.)

Use only the wick supplied for the torches by the Stores Department.

Keep flame low, as this is not used to light the burner, but simply to ignite the benzoline or petrol vapour which is flashed across oil flame by a pressure on metal valve.

The torch lights from above or below glass of burner, and without coming near to the burner, mantle, or chimney shade.

Turn lever tap, insert top of torch a few inches into lantern, and press trigger of valve. The projected flame lights the burner. Torches when not in use to be kept upright in a suitable place and cleaned and trimmed daily

## CARRIAGES STORED UNDER COVER.

Carriages stored under cover must always be coupled together, and the hand-brake nearest to the exit from the shed must be tightly applied, also the automatic brake released on each coach throughout the train, in order that, in the event of fire breaking out, they may be drawn out without delay or risk to life and property.

## LOCKING DOORS OF EMPTY CARRIAGES IN SIDINGS AND SHEDS AND LOWERING BLINDS OF DINING CARS, &c.

The doors of all empty carriages must be kept locked on both sides, whilst the vehicles are standing in sidings or carriage sheds; the windows must also be closed, the ventilators being left open, When the vehicles are left standing for any length of time, the blinds must also be lowered, in order to prevent the sunlight fading the trimmings.

All concerned must be careful to see that the window blinds are lowered when dining cars or saloons are left standing exposed to sunshine so as to prevent the cloth table covers being scorched and burned, owing to the concentration of the rays of heat caused by the sun shining through the water bottles.

#### MAINTENANCE OF WEIGHING MACHINES, WEIGHBRIDGES, ETC.

1. The Station Master or Goods Agent is responsible generally for the carrying Every Machine to out of the following regulations, and each machine must be placed under the charge Responsible Man. of one of the permanent staff.

Oiling of Machines.

At some stations one man may be placed in charge of all machines, but at others it may be convenient for several men to be appointed to attend to particular machines; in either case the responsible men must make themselves thoroughly acquainted with these instructions.

2. EVERY MACHINE MUST BE BALANCED FIRST THING EVERY MORNING, AND AS Balancing. OFTEN AS MAY BE NECESSARY THROUGHOUT THE DAY, AND KEPT READY FOR USE.

AT DOCKS THE MACHINES MUST BE BALANCED AT THE COMMENCEMENT OF EACH TURN OF DUTY BY THE RESPECTIVE WEIGHERS.

A machine is properly balanced when the steelyard slowly rises, from the bottom,

and remains at the top of its carrier.

Should there be a failure to balance, or any reason to suppose that a machine is inaccurate, or if the Government Inspector's stamp on machines should be illegible or have become obliterated, such machine must immediately be placed out of use, and Messrs. Pooley & Son be at once notified at their nearest depot. (See clause 15.)

Machines, such as weighbridges, constantly in heavy use, must be closely watched to see that they are working correctly.

3. The cleaning and oiling of machines is an important matter, and the following Cleaning and instructions must be strictly observed :-

Every part of each machine, except those parts involving displacement or interference with working parts, to be kept cleaned and oiled when required. EMERY CLOTH NOT TO BE USED.

Steelyards and dials to be kept bright, so that the letters, figures and indicators and the Government stamps are legible. Oil or spirits must not be used for cleaning

The spaces between the platform and frame of weighbridges, deck and dormant platform machines to be kept clear of gravel, dirt, or other obstacle by the Department using such machines.

The surface of plates to be kept free from mud and water.

Weighbridge pits, walls, floors and corners of Traffic and Engineering Department machines, as well as frames and castings which involve no displacement, must be cleaned by the Engineering Department not less frequently than once in six months and whenever the machines are overhauled. The person in charge must advise the Permanent Way Inspector when machines are being overhauled by the contractors. The pits of machines which have bad drainage, or which are subject to tidal waters, are to be inspected and, if necessary, cleaned out every month, and in any case, after a heavy storm.

The Locomotive and Carriage Department and the Signal Department are responsible for cleaning out the pits of weighbridges under their charge, as well as

frames and castings which involve no displacement.

Messrs. Pooley & Son are responsible for keeping clean all under parts of weighbridges involving displacement of other parts, as well as for keeping all machines properly and efficiently painted, except bright parts. Where this is not carried out, and rust results, the Engineering Department, so far as the under parts of weighbridges are concerned, and the using Department, so far as the parts above ground are concerned, to report the cases to the Stores Superintendent. The cleaning of rust from any parts of weighing machines which should be painted is the responsibility of Messrs. Pooley & Son.

4. In case of frost binding the plates of weighbridges, fires are not on any Frosty account to be lighted thereon or underneath, but, if necessary, salt may be used, any Weather. salt water to be brushed off as quickly as possible, and great care taken to prevent salt water getting into contact with the gearing surfaces.

5. Should the Inspector of Weights and Measures deface the Stamp Defacement of of any Machine, or make any complaint with regard to it, a Report of the Stamp. CIRCUMSTANCES MUST BE SENT BY NEXT TRAIN TO THE STORES SUPERINTENDENT, SWINDON, AND TO MESSES. POGLEY & SON, AT THEIR NEAREST DEPOT.

6. Particular care must be taken that each machine has its own weights, and no Use, Inspection others. Weights must not be used as hammers, or for any other purpose than as and Custody of Use and if the lead though its weights. Each weight must be carefully examined daily, and if the lead therein is found to be loose, the fact must be reported at once to Messrs. Pooley & Son. WHEN NOT IN USE ALL WEIGHTS MUST BE PLACED IN PROPER CUSTODY. ACCOUNT MUST ANY WEIGHT BE REMOVED FROM ONE MACHINE TO ANOTHER.

All proportional weights are to bear the number of the machines with which they are used, and they are not to be used with any other machine.

# MAINTENANCE OF WEIGHING MACHINES, WEIGHBRIDGES, ETC.-Continued.

MAINTEN	NANCE OF W		
Steelyards.	7. No a machines.	rticles of ar	ny kind must be hung on the steelyards or any other part o
Relieving Apparatus.	8. Ever	vcent at the	provided with a relieving apparatus must be carefully kep moment of weighing. Such machines must be put in gear
Passage of Engines over Weighbridges.	NOT TO BE R	UN OVER V	be unloaded before being put out of gear. Engines are Weighbridges, if it can possibly be avoided. Should the passage of Engines over Weighbridges, the evilually be put out of gear, and the speed of the
	ENGINE MUST	NOT EXCE	ED FOUR MILES PER HOUR.
Weigh-houses.	clean, free from	m lumber as tment using	to be kept in good repair by the Engineering Department and confined to their proper use, and locked when not in use the machine.
Interference with Machines.	10. Pass must not, und with any weig	sengers, Coa ler any prete phing instru	al Dealers, Dealers' Clerks or other unauthorised person- ext, be allowed to make use of the weigh-offices or to interfere ment.
Permanent Way Staff.	11. The assistance as cooperate to be made	Station ar convenience oley & Son' de by Messr	nd Permanent Way Staff must afford every facility and may permit, in lifting, loading and unloading when required 's men. Applications for assistance of Permanent Way Staff s. Pooley & Son direct to the Permanent Way Inspector.
Weighbridges out of use.	12. As s lengthy period at once comm On the 31	soon as it is d the Stores nunicate wit 1st December	known that weighbridges are to be taken out of use for any Superintendent at Swindon must be advised so that he may the Messrs, Pooley & Sons on the matter of maintenance, er of each year the Stores Superintendent, Swindon, must be all machines nermanently out of service at that date and o
	those which a	re out of u	use for the lengthy period referred to above, giving in each machines ceased to be used.
Weigh Tickets.	12 No t	ticket or ne	armit of weight is to be given for any loaded wagon, cart of
	other road veh	ricle until th	ie tare has been duly taken and deducted from the gross load
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### MAINTENANCE OF WEIGHING MACHINES, WEIGHBRIDGES, ETC .- Continued.

Any failure by Messrs. Pooley & Son to do what is required promptly, or to keep machines efficiently repaired and maintained, must be immediately reported to the Stores Superintendent, Swindon. All machines are required to be inspected by Messrs, Pooley & Son at least once in every 12 months. If a machine has not been examined for 12 months, the attention of Messrs, Pooley & Son should be called to it, whether it is in need of repairs or not, and the fact reported to the Stores Superintendent.

The officers responsible for all docks machines are the Dock Managers. In the event of any such machine getting out of order, the Dock Manager will communicate at once with Messrs. Pooley & Son, at their nearest depot, and with the Resident

Docks Engineer.

The responsibility for all other machines is with the using Department, whether the machines are within the Dock areas or otherwise, and such users must communicate direct with Messrs. Pooley & Son in the event of repair work being needed.

16. When a machine is examined or repaired Messrs. Pooley & Son's workmen Certificates. will present for signature a certificate specifying the nature of the work done. If the certificate presented is a PINK one, it denotes that an extra charge will be made against the Company; and in this case the person in charge must investigate and explain fully on the back of form the circumstances necessitating the repairs. All certificates, after being certified, must be handed to Messrs. Pooley's workman on the day the repairs are completed.

Experience shows that a large number of certificates do not reach the Stores Superintendent, and persons in charge are required to see that the above instruction

is strictly observed.

As a rule, no machine should be in a condition that it cannot be used for more than three days. Any such cases should be immediately reported to the Stores Superintendent.

17. All communications respecting the maintenance, condemnation, replace. Communications. ment, inaccuracy, etc., also requisitions for the supply of weighing appliances must be forwarded to the Stores Superintendent, Swindon.

18. The Company undertake to convey the Contractors' repairing vans and Contractors' their contents, as also materials for renewing, repairing and testing the machines, and Materials. no charge must be made when such materials pass over the line; but, in order to prevent irregularity, the following instructions must be observed :-

With every consignment, whether by Freight or Passenger train, which Messrs. Pooley & Son, Ltd., hand to the Company for transit under the contract, they will hand in a consignment note, No. 3104, which will be overprinted as follows :-

- "MATERIALS FOR REPAIR OR TESTING WEIGHING MACHINES.
- "We certify that the traffic mentioned herein is to be used under the

"Agreement respecting weighing machines between the G.W.R. "Company and Messrs. Pooley & Son, Ltd.

"H. POOLEY & SON, LTD. " per :...

When traffic is to be forwarded by passenger train, Messrs. Pooley & Son will so

endorse the Consignment Note 3104.

It will be the duty of all stations to stamp, waybill or invoice this traffic "paid." The vans and contents must be charged at the rate of 1d. per van per mile, and the loose materials at the ordinary rates, and in the case of goods traffic the invoice must be endorsed "contract."

Amounts in respect of Passenger train traffic must be taken credit for through Column "A" or "B," as the case may be, of the Parcels General Account "109," and those in respect of Goods train traffic on the Goods General Account "1103" in the space provided thereon.

The Accounts for the traffic, on form 964 Goods, or form 971-1a or 971-2a Parcels, together with the special consignment notes, must accompany the returns to the

Chief Accountant's Office (Audit Section).

The staff at stations must, as far as possible, exercise a check as to the purpose for which the materials, etc., conveyed under this arrangement are used, as it covers only the articles to be used in connection with the repair and testing of weighing machines upon the Company's system.

The "Contract" Accounts must be kept separate from the accounts for traffic dealt with for Messrs. Pooley & Son in their ordinary business, and the latter should be collected from the firm.

# MAINTENANCE OF WEIGHING MACHINES, WEIGHBRIDGES, ETC .- Continued.

Station Records.

19. A memorandum book must be kept containing a description of each machine, the dates of overhauling and adjusting by Messrs. Pooley & Son's workmen, and dates and particulars of any visits or complaints by the Inspector of Weights and Messures or traders.

Spare Parts.

20. All parts of weighing machines replaced in the course of maintenance are the property of the contractors, but parts replaced, due to accident or to negligence of the Company's servants are to remain the property of the Company, and are to be sent to the General Stores, Swindon. Persons in charge are also required to see that any parts taken away by Messrs. Pooley & Son as patterns for the purposes of repairs under the last-mentioned category are returned to the Company at Swindon.

# HIRE OF RUGS AND PILLOWS.

Hire of Rugs and Pillows to Passengers. Men appointed to take charge of

stock of articles.

Number of Rugs and Pillows allotted to each Station.

- 1. Rugs fastened with paper bands, and Pillows in paper bags, may be hired at certain Stations at a fixed charge of one shilling per article.
- 2. Men appointed to take charge of the rugs and pillows at the stations referred to in the next paragraph, will be held responsible, under the Station Master, for the proper care and issue of the stock under their charge.
- 3. The rug and pillow hiring stations, and the authorised stocks at each station are as shewn below :-

	Authorised Stock.				Authorised Stock.		
Station.	Rugs.	ugs. Pillows Pillow Slips.		Station.	Rugs.	Pillows	Pillow Slips.
Banbury Birmingham Birkenhead Bristol (T.M.) Cardiff (Gen.) Exeter (St. David's) Fishguard H'br. Newport Newton Abbot	6 12 6 20 23 6 32 6 6	6 12 6 30 22 12 70 6 6	12 12 12 100 44 24 120 12 12	Paddington Penzance Plymouth (N.R.) (Millbay) Reading Shrewsbury Swansea (High St.) Torquay Truro	117 12 25 10 6 12 4 4 14	117 24 50 20 6 12 4 6 24	362 48 90 24 12 24 10 12 48

Tickets in connection with the Issue of rugs and pillows.

- 4. With the hiring of a rug or pillow a ticket must be issued. Tickets are in three sections and must be dealt with as follows:-
  - (a) RECEIPT FOR PASSENGER.

OBVERSE.	REVERSE.
G.W.R. No. (1843) PASSENGER'S TICKET.	
PAID FOR HIRE OF RUG - 1/- PILLOW - 1/-	Passengers are requested to kindly place the articles in the Carriage Rack when leaving the Train.
SPECIAL NOTICE. In order to ensure the provision of clean Rugs and Pillows, Passengers are reques- ted to report to the Station Master any case where the paper bands round Rugs or the paper bags in which Pillows are enclosed have been broken.  [SEE BACK.]	

This must shew exactly what articles have been issued, i.e. if a rug is supplied the word PILLOW should be crossed through in pencil, and vice versa if a pillow only is supplied. If two or more of the same article are issued a receipt must be given in respect of each.

#### HIRE OF RUGS AND PILLOWS-Continued.

(b) TICKET FOR GUARD.

OBVERSE.	REVERSE.
G.W.R. No.  GUARD'S TICKET FOR HIRE OF RUGS AND PILLOWS.	SPECIAL INSTRUCTIONS.  This Tickot must be given to the Guard, who, at the completion of his journey—unless the Passenger has alighted at an intermediate station—must hand it to the person appointed to collect the Rugs and Pillows, and Indicate the compartment in which the articles have
Date	been in use.  When a Guard is relieved en route he must hand this ticket to the Guard working the train forward and indicate the compartment in which the Passenger is travelling.  In the case of Passengers completing their journey at an intermediate station or changing into another train in order to reach their destination, the Guard must hand this ticket to the Station Master or Inspector at the Station at which the Passenger alights.

This must be made out immediately a rug or pillow is issued to a passenger, Guard's ticket care being taken to clearly state the number of the Rug Band or Pillow Bag, the final destination final destination of the passenger, and the train by which he is travelling. All such of the Passenger tickets must be handed to the Guard, who will deal with them in accordance with the instructions shown on the back of the ticket.

When a passenger with a rug or pillow, or both, changes at an intermediate station into another train in order to reach his destination, the Station Master or Inspector to whom the Guard hands this ticket must give it to the Guard working the train by which the passenger is going forward and indicate the compartment where the articles are in use.

- (c). COUNTERFOIL FOR ISSUING STATION. This must be carefully made out at the same time as the Guard's ticket, double-sided carbon paper being used.
- 5. THE RUGS, PILLOWS AND PILLOW SLIPS ARE BRANDED "G.W.R." AND ARE COMMON USER TO THE STATIONS CONCERNED.

A linen tab is attached to the rugs and pillows worded "Traffic Department Stock. To be returned to nearest hiring station," and in the event of any of these articles finding their way to Departments other than the Traffic Department they must be dealt with accordingly. The hiring stations are shewn in paragraph 3, page 316.

6. At Stations where men are appointed to collect Rugs and Pillows they Collection of must obtain from the Guard the tickets which have been handed to him by issuing Rugs and Pillows. station, and also particulars of the compartments in which the Rugs and Pillows are to be found. At other stations the Guard must hand the tickets to the STATION MASTER OR INSPECTOR, WHO WILL BE HELD RESPONSIBLE FOR THE COLLECTION AND DISPOSAL OF THE ARTICLES.

7. After rugs and pillows have been collected at the passengers' destination Disposal of stations, they must be dealt with as follows :-

If destination station is a Rug Hiring Station—to be taken into stock.

If destination station is NOT a Rug Hiring Station-to be sent to the nearest Rug and Pillow Hiring Station by the next train, securely packed in paper and booked as a parcel.

8. Station Masters must take a muster of the stock of rugs, pillows and pillow Maintenance of slips at their stations on the day the Stores Van calls, and enter the particulars on Stations. Form 5603, which, on completion, must be handed to the Stores Van Attendant, who will make up the authorised stocks at stations reporting shortages. Any articles in excess of the authorised stock must be handed to the Stores Van Attendant, from whom a signature must be obtained.

#### GREAT WESTERN RAILWAY.

(5603)

## WEEKLY RETURN OF RUGS, PILLOWS, AND PILLOW SLIPS.

	Stat	19		
	Number on hand.	Number in excess of authorised stock.	Number short of authorised stock.	Remarks.
RUGS				
PILLOWS				
PILLOW SLIPS				

\* The "Number on hand" to include soiled or damaged articles on hand for exchange.

The Stores Superintendent,

SWINDON.

G.W.6.

Signature of Station Master.

Station Masters must also examine the stocks of rugs, pillows and pillow slips at their stations on the 1st of each month to ascertain whether the articles are in good condition, clean and well aired.

Exchange of soiled and damaged articles. 9. Soiled or damaged articles should be securely tied in bundles, each bundle bearing label No. 5621-2 properly completed (except at Stations where the Stores Van does not call, in which case label No. 5621-3 should be used), and tendered to the Stores Van Attendant, who will exchange for clean articles. A separate label must be used for each class of article. In the event of its being impracticable, due to depletion of stock, to exchange articles handed in, arrangements will be made to forward clean articles as early as possible. If exchanges or additional supplies are required before the Stores Van is due, delivery will be made from Swindon by passenger train service upon the receipt of a telegram, and the soiled articles should be returned to the General Storekeeper, Swindon, by the same service, carefully packed (in hampers if available), under cover of washing advice No. 5148 duly completed. This practice must not be adopted unless absolutely essential.

Enquiries respecting errors or discrepancies in the exchange of articles must be addressed to the General Storekeeper, Swindon.

Rugs and Pillows held at Swindon for special require-

ments.

10. A small stock of rugs and pillows branded "Return to G.W.R. General Stores, Swindon," and numbered consecutively, is held at Swindon for special requirements. Stations requiring rugs and pillows to meet temporary requirements (such as football excursions, outings, etc.) should make application to their Divisional Superintendent, giving as much notice as possible. If approved, the Divisional Superintendent will pass the application forward to the Superintendent of the Line, who will make the necessary arrangements. After use, the articles should be collected and returned to the General Stores, Swindon, immediately, an advice of despatch being sent under separate cover.

Supplies of Labels and Forms to be used. 11. Supplies of the weekly return form (specimen above) and also label No. 5621-2 can be obtained from the Stores Van Attendants. Supplies of label No. 5621-3 and washing advice No. 5148 can be obtained by written application to the General Storekeeper, Swindon, and stations at which the Stores Van does not call can also obtain supplies of the weekly return form from Swindon.

Receipts.

12. The receipts for the hire of the articles must be accounted for through the "Passenger General Account, No. 874."

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white

After the line "Castings—give shape, marks and weight where possible," add "Articles of a specialised nature.—Dimensions shape, identification marks, colour, material, etc., also the trade name if known, should be given, and if the article is of such a specialised nature that the name would be unlikely to give a true idea of its outward appearance, or if the trade name is likely to be misleading, a sketch should be provided."

(G.A.15-12/44 C.P.50221)

Paragraph 7.

#### GENERAL INSTRUCTIONS TO BE OBSERVED IN DEALING WITH LOST AND UNCLAIMED LUGGAGE AND PARCELS.

The arrangements for dealing with lost and unclaimed property and parcels are as follow:-

The Lost Property Depots of the Railway Companies are situated at the following stations, to which, according to the circumstances of the case, enquiries must be sent :-

C.L.C. .. Liverpool (Central). East Kent Shepherdswell.

Great Northern (Ireland) Belfast (for Northern Ireland Stations).

Dublin (Amiens Street) for Irish Free State Stations. Dublin (Kingsbridge). Great Southern (Ireland)

Dublin (Broadstone). Dublin (Westland Row). Great Western ... Paddington.

Kent & East Sussex Rolvenden. . . Liverpool Street. L. & N.E. Marvlebone. King's Cross. Edinburgh (Waverley).

York.

Continental Dept. Liverpool Street. L.M. & S. Derby. . . .

Glasgow (Central). Belfast (York Road). N.C.C. Section

L.P.T.B. 200, Baker Street, London, W. M. & G.N. Joint King's Lynn.

S. & D. Joint .. Bath. Waterloo. L. & N.E. and L.M. & S. Joint Aberdeen. Leeds (New).

2. Searching of Premises.—Station Masters or Parcels Agents must inspect parcels offices, cloak rooms, etc., daily to assure themselves that no articles remain on hand irregularly or unreported.

3. Unclaimed Articles to be Taken to Lost Property Office. -Articles (excluding unaddressed booked packages) found on the Company's premises or in their trains must be taken at once to the station Lost Property Office. Unaddressed booked packages must be dealt with by the Parcels Department

4. Recording of Unclaimed Articles.—Upon receipt in the station Lost Property Office such articles must be promptly labelled with label No. 272 and recorded in progressive number order in Lost Property Register No. 20 or in a foolscap book ruled for the purpose.

Small articles, such as hand bags, attache cases, etc., should be opened by a responsible person, and the owners communicated with if an address is found.

5. Reporting Unclaimed Articles .- Particulars of unclaimed property must be reported to the Lost Property Depot, Paddington, on Form 2158, which should be despatched so as to reach Paddington not later than 11.0 a.m. the day following the finding of the articles.

Stations on branch lines should advise the junction station also of any luggage being unclaimed.

6. Reporting Luggage, Parcels, &c., Addressed "Till Called For."—Booked luggage and parcels received addressed "Till called for" and remaining unclaimed more than a week; also all consignments, way-billed or docketed, where delivery cannot be effected owing to the address label being lost, or where senders are unknown and consignee cannot be found within 48 hours of receipt, must be reported to the Lost Property Depot, Paddington, on Form 2158.

These articles must be registered separately and reported in progressive number order on the returns with prefix "P," a new series of numbers being commenced on January 1st in each year.

7. Description of Found Luggage and Parcels.-To assist identification a full and explicit description of articles must be given including names, addresses, initials, old labels, or other marks. which may appear thereon, and also the following special features:—

Paper parcels should be distinguished as white, brown, or newspaper, as the case may be, contents stated where possible.

Portmanteaux, Bags, Baskets, Boxes, Bundles, Cases, &c.-Give colour, material, and any diarity which may help to distinguish them, and, where possible, the contents.

Soldiers' Luggage.—Give regimental numbers or other marks.

Sailors' Bags. - Give duplicate numbers and any speciality in contents.

Bieveles and Tyres.-Give full description, make and number, also particulars of any

Castings - Give shape, marks and weight where possible.

The place where found to be stated, whether on platform or elsewhere, and if found in train, partirulars of train to be given.

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# LOST AND UNCLAIMED LUGGAGE AND PARCELS-Continued.

8. Disposal of Unclaimed Property.—Unclaimed property (except booked parcels and booked luggage bearing addresses, and perishables) must at the expiration of 3 days be forwarded to the Lost Property Depot, Paddington, and bear label No. 272 properly filled in.

Articles referred to in Clause 6 must be sent to the Lost Property Depot, Paddington, at the expiration of 1 month and be duly labelled with Label 272.

Cloak Room Deposits Unclaimed .- Any article remaining on hand in a Cloak Room more than a month, the owner of which is not known, must be reported to the Divisional Superintendent and to the Lost Property Depot, Paddington, full particulars, including date of deposit, being given. Any articles not claimed within six months must be opened by the Station Master or a responsible person deputed by him, in the presence of another, and full particulars of the contents taken. If an address can be found, the owner should be communicated with, but if there is no clue or instructions are not received within a week, the package, together with a list of the contents, must be sent to the Lost Property Depot, Paddington.

10. Perishables.—Perishable articles on hand without account, unclaimed or refused, must be dealt with in accordance with R.C.H., C.T. Regulation No. 57.

- 11. Clearance of Charges.—No carriage charges are to be raised on unclaimed property forwarded to the Lost Property Depot, Paddington. When an amount for carriage or warehouse is outstanding, particulars must be submitted to the Divisional Superintendent or District Traffic Manager.
- 12. Money, etc., Found.—Cash, Bank and Treasury Notes, Postal Orders, Stamps, etc., found and not claimed on the same day should be paid in and accounted for in the current day's receipts, the empty receptacles to be dealt with as unclaimed property. A separate advice to be sent on the day of finding to the Divisional Superintendent or District Traffic Manager.

13. Articles of Value.—Jewellery, cameras, field glasses, and other articles of value when sent to the Lost Property Depot must be treated as "Values."

- 14. Articles Sent for Inspection.—Articles sent to a station for inspection and not claimed within 48 hours must, if from a local station, be sent to the Lost Property Depot, Paddington, with particulars of receipt, and forwarding station advised. If received from another Company, they must be returned to that Company.
- 15. Articles to be Protected when Sent to Depot, Etc. -Articles sent to the Lost Property Depot or from one station to another for identification, must be reasonably protected and not sent in an open condition. Where parcels hampers are in use the packages should always be placed therein.
- 16. Luggage Miscarried.—When luggage labelled or addressed to one station is received at another, the receiving station must advise by wire the other station concerned.
- 17. Articles Reported and Subsequently Claimed by Owner.—Articles previously reported and subsequently delivered to the owner or otherwise disposed of, including articles forwarded to other stations for identification purposes, must be reported to the Lost Property Depot as early as possible on the "Disposal" Return (4807), and the following particulars given of each :-

Progressive number under which reported.

found.

Description of article.

How disposed of.

18. Naval Seamen's Luggage. Naval and Royal Marine Kit (kit-bags, chests, hammocks, etc.) found in Railway Carriages or on Railway premises must, if bearing a label showing destination, be sent to the destination station immediately. Kit found without destination labels must, if unclaimed, be forwarded to the Company's Lost Property Depot, and if at the expiration of one month the articles are still unclaimed, those bearing an indication of the owner's Port Division must be sent to the Port Division (see below); those bearing no indication of owner's Port Division must be sent to R.N. Barracks, Portsmouth, Chatham or Devonport, whichever is the nearest.

Naval steel helmets and respirators bearing no indication of the owner's Port Division must be sent to the Deputy Armament Supply Officer, Chatham. (Note.—Naval respirators will shortly bear a distinguishing mark, viz., a continuous horizontal black line one inch wide on the front and on the back of the haversack on the upper part of the reinforced canvas which forms the base of the haversack.)

An advice giving particulars of the articles together with any markings thereon, must be sent to the Commanding Officer at the Naval or Royal Marine Depot to which any articles are sent (see below).

The usual Lost Luggage charges must be made for all articles of Naval and Royal Marine equipment, etc., found on the Railways. Each consignment must be entered with charges at the authorised Naval Stores

The following to be substituted for paragraph 19:-

19. Military and Air Force Equipment.—Unclaimed articles of military equipment must be forwarded to Greenford Station for collection by the C.O.O. Greenford. Articles of R.A.F. equipment must be sent to the Central Depository at the R.A.F. Station,

Particulars on Form 2158 must be sent to the Lost Property Depot, Paddington, in all cases. Hornchurch, Essex.

(G.A.19-10/48. C.S.-C.R./0.)

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Unlabelled articles bearing an indication of the owner's Port Division must be disposed of as under:-

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### To be sent to:

*"C," "Ch." or "Ch.X"			The Commodore, R.N. Barracks, Chatham.
*"P," "Po." or "Po.X"			The Commodore, R.N. Barracks, Portsmouth.
*"D" or "Dev."			The Commodore, R.N. Barracks, Devonport.
"L.T."			The Commodore, Patrol Service Central Depot, Lowestoft.
"R"			The Superintendent of Boom Defence, E.M. Boom Dept., Rosyth.
"F" or "FX"			The Commodore, R.N. Barracks, Lee-on-Solent.
"T124" or "T124X"			The Commanding Officer, H.M.S. "Mersey." Liverpool, 3.
"T124T"			Commanding Officer, H.M. Rescue Tug "Minona," Campbeltown.
"R.M.E."			The Commandant, Eastney Barracks, Southsea, Hants.
"Ply.," "Ply.X" or "Aux."			The Commandant, R.M. Barracks, Plymouth.
"Ex."			The Commandant, R.M. Depot, Lympstone, Devon.
"R.M.B." or "R.M.B.X."			The Commandant, R.N. School of Music, Scarborough, Yorks.
"Depot"			The Commandant, R.M. Holding Battalion, R.N. Barracks, Deal-
*—Articles of Royal N must be sent to R.N. Barn	larine acks,	kit mar Chatha	ked (1) "Ch.," or "Ch.X," (2) "Po.," or "Po.X," and (3) "D" m, Portsmouth and Devonport respectively.

(G.A.12. 4/43. C.R.41139.)

New Paragraph 29. Official Documents.

Official Government Documents, Service Identity Documents, or Government Passes, found in trains or on the Company's property, should be dealt with as follows:-

Confidential official documents must be passed to the Civil Police.

Service Identity Documents must be passed to the R.T.O. (where available).

Government passes must be passed to the Establishment Officer of the Department concerned.

7(G.A.15-12/44 C.R.)

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#### INSTRUCTIONS AS TO TRACING MISSING LUGGAGE AND PARCELS.

- 20. Enquiries for Missing Luggage.—In all cases of complaint of loss of articles a full description of the exterior appearance with marks or initials and a general description of contents of the missing package must be obtained and registered at once. When luggage is concerned in addition to the particulars of the journey and train, it should be ascertained where the package was last seen and what labels and addresses, including particularly old labels, were on it. A sketch should be sent if in 21 3f. Steps to be Taken when Valuable Luggage Lost.—Full particulars should be wired immediately to the Divisional Superintendent or District Traffic Manager and to the Lost Property Depot, Paddington. The Police Department must also be advised.
- 22. Periodical Enquiries at Hotels, Police Stations, Etc.—Special enquiry for missing packages must be made at the hotels, police stations, steamship, dock and cartage offices, goods depots, etc., in the neighbourhood on the last weekday of each month, and a certificate sent to the Divisional Superintendent the day following that this has been done, with full particulars of any articles which may have been found.
- 23. Articles Lost whilst in Passenger's Own Care.—Enquiries for articles lost whilst in the passengers' own care should be made on the appropriate form and passengers must be informed that telegraphic enquiries can only be made at their expense, unless the Company's staff are at fault.
- 24. Luggage Not Found Day Following Its Loss.—If luggage is not found the day following its loss, Form No. 44 must be filled up and sent to the Divisional Superintendent or District Traffic Manager, with full particulars of the steps taken to recover the property, and the result.

A copy of Form No. 44, duly filled up, must also be sent to the sending station to ascertain whether the luggage has actually left the owner's place of residence, and if found to have been despatched in due course, full enquiry must be made at the starting station with a view to ascertaining how the missing luggage was dealt with.

25. Enquiries for Missing Parcels.—Persons complaining of loss of parcels must be asked to give:

A full description of the package and list of contents.

Name and address of sender and date of dispatch.

These particulars must be circulated immediately on enquiry note (4806) to the Lost Property Depot,

Paddington, and other likely points.

If these efforts are not successful within a week, full particulars must be sent to the Divisional Superintendent. A swetch should be sent if difficulty in identifying article

- 26. Two or More Stations in One Town.—Where there are two or more stations in a town, a system of daily advice should be established between them with regard to missing or found luggage.
- 27. Enquiries at Destination.—Missing luggage, when received at destination, should be carefully examined and a note taken of labels and address. If not entered through parcels, enquiry should be made of the guard to ascertain the forwarding station, and in all cases the person claiming the luggage should be asked if a passenger travelled and particulars of the journey made.
- 28. Stations Responsible for Enquiry.—The destination station is responsible for making enquiry for missing luggage; but if complaint be made at an intermediate or junction station, a note must be made and the destination station advised.

In the case of luggage for a foreign line, the sending station will be responsible for making enquiry. The forwarding station is responsible for tracing forward parcels, but this does not relieve destination station from the necessity for making enquiries at the Lost Property Office and other likely points when complaint is made by the consignee.

#### CONVEYANCE OF LETTERS BY TRAIN.

The attention of Station Masters and others concerned is specially called to the necessity of vigilance to prevent letters, not exempted from the exclusive privilege of His Majesty's Postmaster General as conferred by Act of Parliament, being conveyed over the Railway otherwise than through the Post.

The only letters that may be conveyed by train are official communications relating to the business of the Company; "Railway Letters" carried in connection with the Post Office, which must be signed for by all concerned in the same manner as Insured Parcels; and certain exemptions, including the following:—

- 1. Letter or Despatch bags for which the Company make a contract charge for conveyance by train between Traders' Head Offices and their Branch Depots, Works, Collieries, etc.
- 2. Letters sent by Traders and Carting Agents to Station Agents, and to their own Agents at the different Stations, when relating to the disposal of goods carried, or about to be carried over the Line.
- 3. Letters or Invoices, sent by Traders with, and relating to, goods and which are to be delivered with the goods for the consignce.

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cerned. C.R.)

# CONVEYANCE OF LETTERS BY TRAIN-Continued.

Letters sent to and from the Weighing Machine Contractors on matters relating to the Company's Machines.

Letters sent to and from the Company's Sack Contractors respecting the hire of Sacks for Grain, Seed or other traffic conveyed over the Line.

Contravention of the law upon this subject renders the offending person liable, in the event of

prosecution, to heavy cumulative penalties, and may place the Company in a like position.

Instances have come under the notice of the Company of members of the Staff sending unauthorised communications by train, and making improper use of the stationery of the Company in connection therewith. If any Servant of the Company should become aware that irregularities of this character are being committed, it will be his duty to bring the matter to the immediate knowledge of his superior Officer.

#### FREIGHT CHARGES ON TRAFFIC CONVEYED IN CONNECTION WITH COMPANY'S NEW WORKS, Etc., LET TO CONTRACTORS.

Contractors' materials, etc., passing in connection with contracts let by the Company to private firms, must be properly consigned and charged invoices issued.

Where a Contractor is given an "occupation" of a section of the line, a record must be taken

of the materials moved from point to point.

At both forwarding and receiving points the staff concerned must exercise every precaution to ensure that traffic passing is duly recorded by consignment note and invoice in respect of all traffic either in Company's or Contractor's trucks and worked either by Company's or Contractor's engines.

Guards must record upon their journals particulars of traffic dealt with at Contractor's Sidings or Depots situate beyond the environments of Stations in connection with specific "Contracts" or "Occupations," and the exact points between which the traffic was worked; the numbers and owners of the trucks conveyed; together with an approximate idea as to the quantity and contents thereof. A duplicate of this journal to be furnished to the District Goods Manager in whose district the traffic originated, who must satisfy himself that the traffic has been properly invoiced and charged.

Agents in charge of Stations adjacent to these outlying Sidings or Depôts must satisfy themselves as to the adequacy of the arrangement contained in the foregoing paragraph and, if necessary, arrange for a checker to be on duty at the Siding or Depôt to record the necessary particulars, so that all services which the Company may render in connection with such "Contract" or "Occupation" may be duly

charged for.

GAIG. and Railex PARCELS.

1. Insured or Value Parcels, Railway Letters, carried in connection with the Post Office, and "News Intelligence" Packets and News Photographs for Reproduction, must always be entered and signed for, in a book provided for the purpose, by the persons appointed to receive them in the Parcels Offices at Stations, who will be responsible for their guide-waybilling and safe custody until they are transferred to the Guards of the trains by which they are intended to be forwarded.

The Porter or other person who may be deputed to attend to this duty will also be held responsible for the parcels being taken to the trains in good time, in order to properly hand them (together with the guide-waybills) to the Guards and obtain their signatures. Each parcel must be signed for

separately.

Free "Value" letters must be accompanied by a guide-waybill and dealt with in the same way

as value parcels.

The name of the forwarding station and the date must always be shown upon value envelopes

and parcels, in order that destination stations may keep a proper record of receipt.

Guards must give special attention to the safe custody of such consignments, and wherever practicable lock them up. Before parting with them they must obtain the signature of a responsible official at the junction or destination station or, in the case of traffic for other Companies' lines or steamers, at the point where the traffic is handed over.

2. Ordinary Parcels.—All cases of damage, loose packing, or appearance of loss, must be noted by Guards in their Train Books, entered on the waybills (when such are issued) and particulars reported to the Divisional Superintendent on Form 534. Before handing such parcels to Parcel Porters, attention must be called to their condition, and Parcel Porters must note all particulars in

case of claim or enquiry.

3. Parcels handed out of Trains by Passengers or Parcels handed into Trains to Passengers. Passengers carrying parcels or market goods and handing them out of the trains to the Company's Staff or others, with instructions to deliver them to the Consignees, or to await being called for at the Station, must be charged for such parcels at the ordinary rates applicable to such traffic when unaccompanied. In the same manner parcels or market goods handed in to passengers at intermediate stations must be charged for.

4. Handling of Parcel Traffic. —Complaints are frequently received of damage in transit to parcels

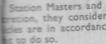
and perishable traffic due to lack of eare in handling.

All concerned must see that traffic is dealt with carefully and Staff at transfer points and destination stations are specially reminded of the necessity for keeping traffic off wet platforms and out of the rain.

5. Newspapers for Use of Staff.—Small parcels of newspapers sent by Messrs. Wyman & Sons to stations for use of Staff must be stamped.

# BULLION, SPECIE AND ARTICLES OF EXCEPTIONAL VALUE BY PASSENGER

These instructions do not apply to traffic conveyed in Bullion Vans, in respect of which special instructions are in operation.



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## CONDITIONS OF ACCEPTANCE.

Consignments of Bullion, Specie and other articles included in the Carriers' Act known to be of ater value than £25 must not be accepted for conveyance unless:—

1. The value is declared and the Executive's charge for insurance is paid;

or

2. The sender fills in and signs the special Consignment Note and Declaration (Form 894).

Station Masters and others at forwarding stations are reminded that if, in the exercise of their cretion, they consider it desirable to inspect consignments before insurance to ascertain if the icles are in accordance with the declaration, in good condition and well packed, they have the ht to do so.

Reasonable notice is required before acceptance of any consignment declared to be of a value reeding £500. Station Masters or other persons in charge must immediately communicate (by ephone, if necessary) full particulars to their Divisional Superintendent or District Traffic Manager and the received of such consignments, whether insured or not.

The conditions laid down under Regulations 64, 82, 83 and 84, Coaching Arrangements Book nuary, 1935, edition) must also be borne in mind.

# -INSURED CONSIGNMENTS EXCEEDING £1,000 IN VALUE.

Arrangements must be made, unless otherwise specially agreed, for a responsible caretaker to company insured consignments exceeding £1,000 in value from the starting-point, or transferation if from another Region, until delivered to the consignee, or, if for another Region, until ansfer to that Region has taken place.

The caretaker must travel with the guard in the brake van, and during the whole journey keep e consignments under his personal care. Before parting with them he must obtain the signature

the consignee or of a responsible official of the Region taking forward.

Transfer points and destination stations must be advised by telephone or telegram, so that any sistance necessary may be given to the caretaker.

Note.—In quoting services preference should be given to express trains, regard being also had to the desirability of arranging an early return home for the caretaker.

# —INSURED CONSIGNMENTS OF £1,000 IN VALUE OR LESS, OR UNINSURED CONSIGNMENTS OVER £25 IN VALUE IN RESPECT OF WHICH FORM 894 IS SIGNED.

Insured Consignments of £1,000 or less in value and uninsured consignments in respect of which he special Consignment Note and Declaration (Form 894) has been signed should, as far as possible, e sent by express trains, and must be handed to the guard personally, and his signature obtained, ut a caretaker in ordinary circumstances need not be provided. Guards must give special attention of the safe custody of such consignments, and wherever practicable lock them up. Before parting with them they must obtain the signature of a responsible official at the junction or destination tation, or, in the case of traffic for other Regions, at the point where the traffic is handed over.

Transfer points, destination stations, and such intermediate stations as considered necessary, nust be advised by telephone or telegram, and steps taken to protect the traffic at such stations.

# -TRAFFIC FROM OR TO OTHER REGIONS.

Where consignments are handed to the Western Region by employees of other Regions for transit to or over the Western Region, similar precautions must be taken from the transfer point to destination, or to the point of transfer with the Region taking forward.

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# 5.—SAFE CUSTODY AT FORWARDING, TRANSFER AND RECEIVING STATIONS.

During the time valuable consignments are at forwarding, transfer or destination stations, they must be placed in the custody of a trustworthy person, who will be held responsible for their safety. Upon receipt of the advice from the sending station, the receiving station must advise the

consignees in order to arrange delivery immediately on arrival.

Where such consignments have to be kept on hand over-night they should be locked in the safe, the night staff (where employed) told to keep special observation, and the police warned when considered necessary. The Divisional Superintendent or District Traffic Manager must be consulted in case of difficulty.

Where consignments are tendered for Banks and other business premises which would in all probability be closed at the time of arrival at destination, senders should be asked to communicate with consignees, so that if practicable someone may be available to receive and sign for the traffic.

#### 6.—SEA TRANSIT.

#### FISHGUARD AND ROSSLARE, AND FISHGUARD AND WATERFORD.

Valuable consignments for transit to Eire via Rosslare must be handed over by a trustworthy person to the ticket collector of the Steamer, who must immediately lock them up in the Purser's safe, and report particulars to the Chief Officer. The latter official will be responsible for seeing that such consignments are duly cleared, and on arrival at Rosslare the ticket collector must, before parting with them, obtain a signature from the Official to whom they are handed.

In the case of the direct Waterford service such consignments must be handed to the Chief Officer of the vessel, who, after giving a signature in acknowledgment, will place them in the ship's safe, and not part with them at the Irish port until he has obtained the signature of the Official to whom he

hands them.

Consignments from Eire to England must be similarly safeguarded.

It will not be necessary for a Western Region caretaker (where one is provided) in charge of a consignment to Eire to travel beyond Fishguard, but he must personally place the consignment in the hands of the proper ship's officer and obtain his signature.

#### CHANNEL ISLANDS.

Consignments of bullion, specie, value parcels or articles of exceptional value must be stowed in the ship's Parcel Room by or under the supervision of the Supercargo. Only small consignments consisting of a few boxes or packages can be stowed, and in the event of a consignment of any magnitude being presented for transport, prior advice must be sent to the Quay Superintendent, Weymouth Quay, by telephone or telegram, in order that such special arrangements as may be necessary can be made.

This Parcel Room is in charge of the Supercargo who must retain the keys in his custody and must personally supervise the stowing or landing of any of the articles enumerated above.

duty must not be delegated to another person.

Value parcels, etc., from shore must be signed for by the Supercargo on board ship, and in the reverse direction, by the person ashore authorised to receive and deal with them.

Mail Bags and value parcels are in all cases to be examined on receipt to verify the fact that the

seals are intact. It will not be necessary for a Western Region caretaker (where one is provided) in charge of

a consignment to the Channel Islands to travel beyond Weymouth, but he must personally place the consignment in the hands of the Supercargo and obtain his signature.

#### 7.—PRECAUTIONS AT DESTINATION.

Where it is observed that packages appear to be in loose condition or in any case where considered desirable, the caretaker or a responsible local representative of the Executive should be (G.A.19-10/48. C.S.-F.73278/181.H.) present at the unpacking of insured articles.

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#### CHARGING OF PARCELS TRAFFIC.

Carriage charges on Parcels and Miscellaneous Traffic (with certain exceptions in the case of Fish Traffic from ports in Scotland and Ireland and Live Stock from sales) must be prepaid, and except in those cases where waybills are required to be issued, the charges must be accounted for by means of prepaid stamps, which, before being affixed to the traffic, must be properly cancelled by means of the rubber cancelling stamp supplied for the purpose.

In the case of Ice, Sacks and Baskets covered with canvas upon which stamps will not adhere,

tie-on labels must be used to which the stamps can be affixed.

Parcel Stamps represent cash, and every care must be taken to ensure their safe custody.

All persons handling stamped traffic are required to notice whether the stamps are properly cancelled-in cases where this has been omitted the stamps are to be cancelled at once, and the irregularity reported to the Divisional Superintendent or District Traffic Manager, giving the number of the stamp and the name of station at fault.

All Traffic must be recorded by the Forwarding Station before despatch, and by the Receiving

Station before delivery.

Receiving Stations must be careful to see that the stamps affixed represent the correct carriage charges from the Station from which despatched. Special arrangements must be made with regard to Newspaper and similar traffic which is frequently taken away immediately on arrival of the trains. Parcels of Newspapers forwarded from Paddington by firms having a ledger account, are stamped with a rubber stamp "Ledger Account."

Market traffic under two cwt. for London, Bristol, Cardiff, Birmingham, Liverpool, Manchester, must be accompanied by Memo Waybills to which the Parcel Stamps must be affixed, except in the case of traffics exempted from the stamping arrangement, and the Staff at Forwarding and Junction stations must be careful to see that the documents are duly sent forward with the traffic.

Label 135A must be attached to all consignments to which stamps showing Sending Station and

route are not affixed.

All Traffic should be clearly addressed and bear the name of the Station to which it is intended to be conveyed. If a parcel is addressed to a place at which there is no railway station, the sender must be requested to give the name of the station to which it is to be forwarded.

The attention of all concerned is drawn to the necessity for seeing that all old Parcel Stamps are removed from Pigeon Hampers, Returned Empties and other packages before they are despatched, and Station Masters, Parcel Porters and others concerned are particularly requested to give this matter

Ledger Labelled Traffic.—In the case of certain traffics, special "Ledger Labels" are used in accordance with instructions issued from time to time. If parcels are received bearing Ledger Labels for which no authority is held, full particulars must be reported to the Divisional Superintendent or District Traffic Manager.

# LIGHT TARPAULIN SHEETS FOR PROTECTION OF PARCELS ON PLATFORMS.

Light Tarpaulin Sheets are supplied to various stations for the purpose of protecting parcels from inclement weather when being conveyed on barrows to and from the trains.

Care should be taken to see the sheets are kept in a convenient place ready for immediate use, and that they are only used for the purpose for which they are provided. When not in use the sheets must be carefully folded and kept in the Parcels Office.

# PLATFORM TROLLIES FORWARDED TO SWINDON STORES.

When platform trollies are forwarded to Swindon Stores an advice must be sent to the Stores Superintendent stating the number and type of trollies forwarded, reason for return, date and train despatched, and the number of the truck into which loaded.

A label must be attached to each trolley showing sending Station and reason for returning to

Swindon, also a memo-waybill or invoice must accompany all trollies.

#### MILK TRAFFIC.

Having regard to the highly perishable nature of this commodity and the severe road competition now obtaining, it is most important that Milk Traffic shall be afforded the best possible service and special attention paid to the working of returned empty churns. If any difficulties are experienced in this connection, the Divisional Superintendent or District Traffic Manager must be advised.

When churns are received at a station, they should be either loaded direct into a Milk Truck (when one is provided), or placed, as far as possible, upon trolleys, which should be wheeled to a position on the platform, as nearly as can be judged, opposite the spot where the van into which the Churns are to be loaded will come to a stand. In the event of there not being sufficient trollies, the Churns must be placed on the platform, in such a position as will cause the least possible delay on loading into the Milk Van. In either case the milk should be placed as far as possible in the shade.

Care must be taken to stand the trollies so that they will be well clear of the open doors of incoming

and outgoing Trains.

#### MILK TRAFFIC-Continued.

In loading Milk great care must be taken to keep together in one part of the van all churns for one Station, so that the Staff at the receiving and transfer Stations may be in a position to readily unload the Churns.

In accordance with the conditions upon which Milk is conveyed at the reduced rate at Owners' Risk, men in the employ of the senders and consignees of the Milk must assist in the loading and

unloading of the churns.

In the event of the van accommodation provided in the Trains for the conveyance of Milk being considered by the Station Staff to be at any time insufficient, or if the Milk to be despatched from the Station is sufficient to fully load a Milk Truck, the attention of the Divisional Superintendent or District Traffic Manager must at once be directed to the matter, so that revised arrangements may be made if necessary.

It is found that "Through" milk vehicles are often not fully labelled on Both Sides, with the result that delays and wrong sending arise, owing to the milk being loaded into wrong vehicles at intermediate stations. The staff at stations despatching "Through" milk vehicles must be careful to see that these are fully labelled ON BOTH SIDES, and any omission in this respect, which may be observed by the staff at stations en route, must be immediately reported to the Divisional Superintendent.

Cases have come under notice where milk churns not bearing the Sender's label or name-plate, are being forwarded, and considerable inconvenience has arisen in consequence. The staff at forwarding stations must see that all churns are properly plated and the Sender's attention must be specially drawn to cases where this is not done, and also to any case where milk is consigned to one station although bearing a Consignee's name-plate for another.

Instances of unnecessary haulage of vehicles and light loading of milk trucks have been observed,

and great care must be exercised by all to avoid such cases occurring.

Trucks used for the conveyance of Fish Traffic must not be used for the conveyance of Milk Traffic.

Milk Trucks should be kept in regular working for Milk Traffic only.

Station Masters, Parcel Porters, and others are responsible for seeing that Milk Trucks are properly cleansed before being loaded with Milk Traffic.

Other descriptions of traffic must not be loaded on top of the Milk Churns.

It is essential that Consignees' full names, together with the number of the vehicle into which the milk is loaded, should appear on the milk waybills, and no effort must be spared by the staff concerned to ensure these important particulars being shown.

Milk traffic must not be booked from Senders' consignment notes without being checked. All milk waybills must be scrially and progressively numbered, a fresh number being commenced

It is of the utmost importance that milk waybills be forwarded by the same train as the milk to which they refer, and Guards must see that the waybilis accompany the traffic throughout, that they are duly put out with the milk at transfer points and, also, that they are not put under the lids of the churns.

Complaints are frequently made that Milk is short measure at destination stations; it must, therefore, be checked as frequently as possible to see that the quantity consigned and waybilled agrees with the contents of the churns, and any shortage must be immediately pointed out to Sender so that the necessary alteration may be made in the consignment note before the milk is forwarded. In the case of sealed churns the weight of the churn must be legibly stamped on the outside and the contents may be arrived at by weighing, 101 lbs. being allowed for each Imperial gallon of milk, plus the weight of the churn.

Should complaint be made in respect of any particular consignment, that Sender's traffic must

be specially tested daily for a couple of weeks, and the result noted for reference.

Unclaimed empty milk churns must not be allowed to stand about at the stations without any effort being made to find the owner. When churns are found on hand unclaimed, or put out at stations in error, immediate steps must be taken to ascertain the owner, or the correct destination station, and the empty churns must be at once disposed of accordingly.

Milk churns must be examined to see that they are in good order before they are accepted for conveyance, so as to prevent possible injury to the staff owing to cans with defective handles, &c.

# INSTRUCTIONS TO BE OBSERVED IN DEALING WITH THE RECEIPT, LABELLING, FORWARDING AND DELIVERY OF PASSENGERS' LUGGAGE, ALSO WITH LOST AND FOUND. LUGGAGE AND PARCELS.

# Receipt, Forwarding and Delivery of Luggage.

Acceptance and Labelling of Lug-

Passengers are required to attach securely to each article of luggage a label with the owner's name and destination legibly written thereon, and to see it properly labelled by the Company's staff. No article must be accepted unless this condition

All luggage, other than such hand luggage as may be retained by the passengers under their own care, must be labelled to the station to which tickets have been taken. by the staff appointed for this work.

#### RECEIPT, LABELLING, FORWARDING AND DELIVERY OF PASSENGERS' LUGGAGE. ETC .- Continued.

In all cases where there are alternative routes, the label must show by which route the passenger is travelling. Before affixing the new label, all old labels, including address labels, must be removed or unmistakably defaced. Special care should be taken in this respect in dealing with commercial travellers' luggage.

Duplicate labels, which must always be used, are provided for sailors' luggage, Sailors' Luggage, one part being given to the passenger, and the other tied to the article.

3. Cycles must not be accepted unless plainly addressed, and in order to meet any Cycles. cases where passengers have not provided themselves with labels, a supply must be

kept by the luggage labellers.

4. THE COMPANY'S STAFF ARE NOT PERMITTED UNDER ANY CIRCUMSTANCES TO TAKE Staff not to take CHARGE OF LUGGAGE FOR ANY PASSENGER EXCEPT FOR THE PURPOSE OF LABELLING, Charge OR AT ONCE PLACING IT IN A COMPARTMENT, ON A CAB OR OTHER VEHICLE, IN CHARGE OF THE OWNER, OR DEPOSITING IT IN THE CLOAK ROOM. WHEN LUGGAGE HAS TO BE DEPOSITED IN THE CLOAK ROOM, THE OWNER MUST BE REQUESTED TO ACCOMPANY IT AND RECEIVE A CLOAK ROOM TICKET.

5. Care must be exercised to ensure luggage being stowed in the prescribed Stowing of Lugpositions, and Guards are required to see that it is placed ready for unloading at the gage.

6. Before luggage is handed over at the destination station, the staff must require Delivery of Lugpassengers to identify their property, and when engaging cabs must not place any. gage to Owner. thing in or on the vehicle, without first noting the number or being otherwise able to identify it, unless the owner is in attendance to see personally to the safety of his or

7. Tickets for cycles, perambulators, mail carts, &c., and the passenger's portion Instructions to of the duplicate label used for sailors' or registered luggage must be collected before Guards.

the articles are given up.

Guards should in all cases be advised of unclaimed articles handed out of trains by passengers, or taken out by the Company's servants, and should make a note of the fact. A signature must be given for articles found in Restaurant Cars and handed to the Traffic Department staff, and for articles handed to the Lost Property Office by guards or porters.

The Guard's attention should be directed by the Platform staff to luggage out

of repair or in any way damaged.

On arrival of trains at terminal stations, the Guards must ascertain whether the whole of the luggage is claimed or not; and call the attention of the Station Master or person in charge to any irregularity, luggage out of repair, or in any way damaged.

8. Any luggage not claimed must be taken immediately to the Lost Property Unclaimed Luggage. Office, and full particulars of the train by which it was received, &c., recorded.

#### REGULATIONS BEARING UPON CLAIMS ON COACHING TRAFFIC. DISPOSAL OF CONSIGNMENTS, SALVAGE, ETC.

1. When consignments are lost, pilfered, or damaged, the destination station must Packages lost, advise the sending station immediately. On receipt of such advice, the sending philered or damaged, station must make the necessary inquiries forthwith, but the destination station

must also make inquiries as may be necessary.

2. A note must be made and initialled by the writer on consignment notes or Record of Packages! waybills, where such are used, of any case of pilferage, loss or damage, etc., coming Packages lost, under the notice of the staff, and, where known, the cause must be given. When made damaged. on waybills, the name of the station where the loss or damage is discovered must be

At stations, transfer or otherwise, where discovery of loss, pilferage or damage is made, the facts must be reported to the Station Master or other person in charge of the Parcels Department, who must at once advise the forwarding and destination stations.

All such cases discovered by Guards or Travelling Ticket Collectors, either on the journey or at stations, should be reported on the special "Bad Order" form, No. 534, which must be attached to the journal.

3. In cases of serious damage to valuable traffic conveyed at Company's risk, the Traffic at services of an expert should be obtained at once, so that reliable information may Services of expert. be obtained of the nature and extent of the damage, and the approximate amount of liability, if any.

4. The instructions as to the weighing of traffic at both the forwarding\* and Weighing of receiving stations must be strictly carried out. Particular care must be exercised traffic. in weighing packages of tobacco, butter, sausages, rabbits, or other commodities specially liable to loss by pilferage.

<sup>\*</sup> A Receiving Office where parcels are booked to be regarded as a station.

#### CLAIMS ON COACHING TRAFFIC, ETC .- Continued,

Notice or receipt of claim.

5. When notice of claim is given, or a claim is made, the Station Master must, when practicable, inspect the consignment, check the quantity, ascertain if the packing was sufficient and proper, and the extent of damage or loss, and see that the amount claimed is reasonable. The invoice from the sender to the customer should be inspected where possible.

Liability not to be admitted. 6. In the course of preliminary inquiry, or upon receipt of a claim, no admission must be made as to the Company's liability, nor until proper inquiries have been made and the Company's position determined.

Salvage.

7. When a claim is made for the full value of a damaged consignment and the salvage is said to be worthless, claimant should be requested not to destroy or otherwise dispose of it, as, in the event of the Company admitting liability, they may require to take possession of the salvage.

Responsibility for dealing with Claims. 8. A claim made at either the original forwarding or the destination station must be dealt with by the station receiving it. Claims made upon another Company in which this Company is intermediate must be dealt with by the station where the transfer is made to this Company.

In the case of traffic from a foreign line to a Great Western station, the claim should be dealt with by the destination station.

Advice to Police Department. 9. The Company's Police Department must be advised of all cases of total loss, or pilferage as laid down in the General Manager's Circular No. 2,696, dated 1st May, 1922.

Inquiry into Claim.

- 10. Claims must be inquired into immediately by means of Report Form (3476), and otherwise as may be necessary. The following points must be specially observed:—
  - (a) The checking of delivery sheets for proof of delivery (including the number of parcels for each consignee) by a competent and responsible person.
  - (b) "No trace" replies must be signed by a responsible person.
  - (c) Possible misdeliveries to branches of Co-operative Stores, multiple shops, or other wrong consignees, must be looked for.

Both the Forwarding and the Receiving stations must fully and accurately record on the form the facts relating to the transaction. Special care must be taken to state the nature, extent and cause of the damage or loss (if known), and other grounds of claim (if any), also whether the amount is reasonable. Any other information likely to be useful should also be given, with an expression of opinion from the ascertained facts by the Station Master or Chief Parcels Clerk.

In cases of delay, the proper time of delivery and the actual time must be given on the report. In cases of pilferage, particulars of any evidence of theft and the time of delivery and time at which complaint is made must be furnished. The date of first complaint must in all cases be shown in the space provided on the claims form.

In the case of claims upon Milk traffic, it should be stated whether the churns were plated or labelled in accordance with the conditions of carriage. Where labels are used a description of the label must be given.

Claims made by Managers of Branch shops.

11. Claims received from Managers of Branches of multiple shop firms should be settled through the Head Offices of the Firms, and not through the Branch Managers.

Claims to be dealt with promptly. 12. Prompt dealing with claims is essential, not only to enable the facts of a case to be ascertained, but to avoid annoyance to traders. If any difficulty is encountered in obtaining quick replies from stations, the Divisional Superintendent or District Traffic Manager should be advised without delay.

Rates to be quoted in claim reports.

13. When reporting claims on traffic consigned at Owner's risk, or by traders who have signed a General Request for the traffic to be conveyed at the Owner's risk, the O.R. and C.R. rates must be shown in the report, and any discrepancy between the O.R. and the rate charged must be explained.

Prevention of Damage, etc. 14. It is of the first importance to traders and the Company that traffic should be carried free from damage, loss, etc., as far as possible. In all cases the causes of damage, loss, etc., such as improper handling, opportunities for theft, or pilferage, etc., must be thoroughly investigated, and statements obtained from all concerned, and such measures as may be necessary to remove the causes promptly taken. In the event of claims on any particular traffic, or from certain traders, being numerous and out of proportion to the receipts, or of repeated cases of loss or pilferage on particular sections of the line, special representations should be made to the Divisional Superintendent or District Traffic Manager.

#### CLAIMS ON COACHING TRAFFIC, ETC .- Continued.

15. Subject to limitations mentioned hereafter, Station Masters are authorised to Station Masters settle claims up to and including Forry Shillings (40s.) made on traffic conveyed at up to and Company's risk, where, after exhaustive inquiry, there is no doubt :-

including £2.

(1) as to the liability of the Company;

(2) of the reasonableress of the claim; and

(3) that no vital principle is involved.

This authority does not extend to :-(a) Claims on traffic charged and conveyed at Owner's risk rate—such Station in

Limitations of Station Masters'

claims having to be submitted to the Joint Claims Committee for decision. (b) Claims on articles and goods comprised in the Carriers Act, 1830, viz. :-Gold or Silver Coin, Gold or Silver in a manufactured or unmanufactured state, Precious Stones, Jewellery, Watches, Clocks, or Timepieces of any description, Trinkets, Bills, Bank-notes, Orders, Notes, or Securities for Payment of Money, English or Foreign Stamps, Maps, Writings, Title Deeds, Paintings, Engravings, Pictures, Gold or Silver Plate or Plated Articles, Glass, China, Furs, or Lace.

(c) Claims in respect of street or other accidents involving personal injury or damage to private property.

(d) Claims upon Horses, Cattle, and other traffic, of which the Company are not common carriers by passenger train.

17. Immediately a claim is paid, a Compensation Voucher (202) in duplicate Compensation must be prepared by the Station Master, and sent, with all papers, to the Divisional Vouchers. Superintendent or District Traffic Manager for signature, one copy being retained by the latter as a record.

In cases where claims are sent by one Divisional Superintendent or District Traffic Manager to another for settlement, the Divisional Superintendent or District Traffic Manager paying the claim must prepare the Compensation voucher.

In cases where claims are sent by one Divisional Superintendent to a station in another Division for settlement, the station paying the claim must prepare the compensation voucher and send it to the Superintendent from whom the claim was received for certification.

18. Divisional Superintendents or District Traffic Managers are authorised to Divisional settle claims on coaching traffic ("Local" and "Through") conveyed at C.R. rates, to settle claims and claims arising out of street accidents not involving personal injury, provided up to and includ-(a) that the liability of the Company or Companies interested is clearly established, ing £25. (b) that no vital question of principle is involved, and (c) that the amount paid does

oucher to be

19. The following claims must be reported by the Divisional Superintendent or District Traffic Manager as shewn :-

I. To the Commercial Superintendent.

not exceed TWENTY-FIVE POUNDS (£25).

- (a) Claims for payment of sums exceeding TWENTY-FIVE POUNDS (£25);
- (b) Claims for submission to the Claims Com

and

20. Claim (i.e., demand note) and receipt must accompany the compensation attached to Dally Cash Account (110) voucher when the latter is taken credit for through the Daily Cash Account (110).

be collected and paid into the credit of Parcels Compensation in such cases.

A Return of Coaching Traffic Compensation to be prepared on Form 2577 and Monthly Return forwarded by the Divisional Superintendent or District Traffic Manager to the Superintendent of the Live section. tendent of the Line not later than the 8th day of each month.

23. Perishable Traffic.—Efforts should in all cases be made to induce Consignees Salvage. to accept traffic which has been delayed, damaged, or pilfered in transit, but if negotiations fail and senders' instructions cannot be obtained, traffic of an unimportant character should be sold to the best advantage under the personal sanction of the Station Master.

Perishable traffic must not be sold to the Company's staff if better prices can be obtained elsewhere.

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19. The following claims must be reported by the Divisional Superintendent or District Traffic
Manager as shewn :-
                                                                                                                                                                                                                                                                                                                                                                   at o
                1. To the Commercial Superintendent.
                                (a) Claims for payment of sums exceeding TWENTY-FIVE POUNDS (£25):
                                (b) Claims for submission to the Claims Committee;
                                (c) Claims in respect of Cattle, Sheep and other animals killed or injured whilst trespassing
                                             on the line :
                                (d) Claims raising questions of principle; and
                                (e) Claims on Livestock, and on insured Carrier's Act traffic.
               2. To the Operating Superintendent.
                                   (i) Claims in respect of street or other accidents involving personal injury;
                                (ii) Claims arising out of collisions between locomotives or rolling stock and privately owned
                                                                                                                                                                                                                                                            (G.A.23-7/49. C.S.-C.P.)
                                              vehicles.
                                                                                                                                                                                                                        and agreed proportions must of claims.
                                                                                                                                                         Total Control of the Party of t
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#### CLAIMS ON COACHING TRAFFIC, ETC .- Continued.

Before large or valuable consignments of perishable traffic are disposed of the circumstances must be telegraphed to the Divisional Superintendent or District Traffic Manager whenever possible and authority obtained as to the disposal of the traffic.

Particular attention is drawn to the Standard Terms and Conditions on this

point.

Other Traffic—Not Perishable.—When traffic not of a perishable nature is refused by the Consignee the Sender must be advised, and if no instructions can be obtained, notice should be given sender and consignee that it remains on hand at their sole risk and the facts reported to the Divisional Superintendent or District Traffic Manager. If there are special features in connection with its manufacture or nature which render it desirable that it should be disposed of locally in the district concerned, arrangements may be made to sell the same under the authority of the Divisional Superintendent or District Traffic Manager, the Assistant Superintendent or Manager, or in their absence, the Chief Clerk.

Whole proceeds of Sales to be paid in. 24. The proceeds of all sales by or on behalf of the Company must be paid in on the day of receipt through the Daily Cash Account (110), to the credit of Parcels Compensation, no part thereof to be applied to the reduction of claims.

Compensation Vouchers must be issued for the full amounts paid in settlement of claims, and reference must be made thereon to the date of the Cash Account (110)

in which the amount of the salvage credit appears.

Proceeds of Sales to be advised monthly. 25. Particulars of the amounts paid in, through the Cash Account, to the credit of Parcels Compensation, must be forwarded by the Station Master to the Divisional Superintendent or District Traffic Manager as they arise and a copy of the advice sent to the Police Department, Paddington. Divisional Superintendents must advise the Chief Accountant of such amounts at the close of each month.

# REGULATIONS BEARING UPON CLAIMS ON MERCHANDISE TRAFFIC, DISPOSAL OF REFUSED GOODS, SALVAGE, ETC.

Acceptance of Traffic from Traders and others. 1. Persons receiving traffic from traders, the public generally, or from other Rail or Road Carriers, must carefully examine same at time of tender. If any packages are not in proper state, the attention of the sender, or carriers or their representatives must be called to their condition. If the defect be not remedied, the goods must be signed for with a qualifying remark describing the state of the goods, and a similar remark must be made upon the consignment note. When packages are repaired, record must be made when signing for the goods and on the consignment note, thus: "Package repaired, condition of contents not known."

When Goods are Lost.

- 2. When goods are lost, the station to which they are invoiced must so advise the station at which the goods were last transhipped or transferred as well as the invoicing station immediately. On receipt of such advice, the last station from which the goods are alleged to have been fowarded (whether it be the transfer or original forwarding station) must make the necessary inquiries for the missing goods.
  - Note.—(i) See R.C.H. Regulation re Reporting the Loss of Goods within 24 hours after the loss has been ascertained.
    - (ii) See General Manager's Circular 2696, of 1st May, 1932, re Reporting Goods Stolen to Special Police Department.

When Goods are Damaged or Pilfered.

- 3. Checkers, Loaders, and all others concerned must record upon the consignment notes or invoices, and in their note books, all cases of Pilferage, Loss or Damage, etc., and the cause of the damage (whether due to pilferage, bad loading, improper shunting, or as the case may be), in such manner as to convey a correct idea of what was amiss; the remarks must be initialled by the writer, and, when made on invoices, the name of the station where made must be written against them.
- 4. All cases of Damage or Pilferage of serious nature, or of Bad Loading, must be reported to the forwarding and last transfer stations.
- 5. In cases of serious damage to Goods, the services of one of the Company's experts should be obtained immediately, so that whilst the facts are fresh the expert may obtain such evidence as may clear the Company from liability or enable him to advise what ought to be done so as to reduce the monetary loss to whomsoever may be concerned and also to suggest, when possible, measures for preventing similar damage.

#### CLAIMS ON MERCHANDISE TRAFFIC, ETC .- Continued.

- 6. Damages of comparatively unimportant character need not be reported to either the forwarding or transfer station, whether local or foreign, excepting when the Manchester Ship Canal Company is concerned, which Company must be advised of all damages (excepting those only discovered by consignees after delivery) within 24 hours after they have been ascertained (vide R.C.H. Regulation, 158, as amended July, 1930).
- 7. Packages of Wines, Spirits, Cigarettes, Tobacco, Tea, Silks, Boots, Shoes or Wines, Spirits, other goods specially liable to loss by Pilferage, Leakage, etc., must be accurately Tobacco, etc., to be weighed. weighed at both forwarding and receiving stations, and in the case of "Through' traffic, at stations of transfer from one Company to another also. In many cases, the weight is the only means of determining whether loss has taken place in transit.
- 8. In the event of loss by leakage or destruction of wines, spirits, tobacco, or other When Merdutiable merchandise, the facts must be at once reported to the nearest Inland Revenue chandise liable to Government Officer, so that he may testify as to the circumstances and loss, otherwise the Company Duty is lost or will be unable to obtain a remission of the duty. When a leakage is discovered within destroyed by a wagon, lorry, or other vehicle, the vehicle concerned must be detained for the accident. Officer's inspection, in order that he may satisfy himself in regard to any evidence which may exist as to the extent of the leakage. In the absence of an Inland Revenue Officer, a Railway Police Officer or, if one is not available, a Public Police Officer, should be called in as a witness. The case must also be reported immediately to headquarters. The Agent or person in charge must take prompt steps to prevent further loss, and to make certain that no part of the merchandise passes into consumption. The names of all persons present when such damages are discovered must be recorded, so that, if required, their evidence may be obtained.

Note. - See the General Manager's Circular 2696, 1st May, 1922, re Reporting Pilferages to Special Police Department.

#### Method of dealing with Goods Compensation Claims.

9. When notice of claim or claim is received, the Goods Agent or Station Master, When notice of must, whenever practicable, inspect the merchandise, check the quantity, ascertain is received. the extent of damage or loss and take every reasonable means to see that the claim is correctly stated, that the amount claimed is in accordance with the terms of sender's invoice to his customer, and record particulars on Goods Inspection Report (Form 5198).

10. Whilst inspecting the merchandise and receiving the account for damage When Liability or loss, admission must not be made that Company is liable, because it is only after admitted. proper enquiries have been made that the Company's position can be determined.

11. When a claim represents the full value of damaged merchandise, and salvage Salvage said to id to be worthless, claimants should be teld not to destroy on atherwise dispasse be worthless. is said to be worthless, claimants should be told not to destroy or otherwise dispose of the goods, as in the event of the Company admitting liability, they may require to take possession of the salvage.

12. A Station receiving a claim, if it be either the original Forwarding or the Enquiry into and Reporting (i.e.—destination) Station, must deal with it. RECEIVING (i.e.—destination) Station, must deal with it.

13. When a claim is received, it must be enquired into immediately by means of the approved Goods Claims Report Form (1396) and otherwise as may be necessary.

14. Both the Forwarding and Receiving Stations must fully and accurately ascertain and record on the form the facts relating to the transaction, and special care must be taken to state the nature, extent, and particularly the cause of the damage, loss, or of whatever the claim may be for, whether the amount is reasonable or not, and any other useful information; and reference to the correspondence must be made upon the original invoice and upon the copy thereof.

15. Subject to the exceptions recorded in the Note to this Clause, when dealing with Net Claims (i.e.—Claims less salvage or other credits) not exceeding £7 for Damage, Pilferage, or Partial Loss (i.e.—Loss of part of the contents of a package) on "Through" Traffic forwarded to or received from Stations on Railways in Great Britain, it will not be necessary to communicate with Intermediate Stations or Companies concerning such claims; it will only be necessary for the Terminal Station or Company to communicate with the other Terminal Station or Company to ascertain the facts and determine the extent of the Carrier's liability (if any) in reference thereto, because all such compensation payments not exceeding £7 will be scheduled for through mileage division irrespective of the point at which the damage, pilferage, or partial loss arose.

Note. - Claims arising from accidents to trains, or fire, or claims in which Railways in Ireland, and Steamship Companies are concerned are excepted from this agreement.

# CLAIMS ON MERCHANDISE TRAFFIC, ETC .- Continued.

When dealing with claims on "Through" traffic, other than those referred to in the preceding paragraph, it will be necessary for the Station or Manager dealing with the claims to communicate with the Intermediate Stations and Companies according to the requirements of each case as heretofore.

16. Promptness in dealing with claims being essential to the obtainment of reliable information and the furtherance of the Company's interests with traders, if any difficulty is encountered in obtaining prompt replies from stations, the attention of your District Goods Manager must be directed to the irregularity.

17. When reporting claims on traffic consigned for conveyance at Owner's Risk, the O.R. rate, C.R. rate and rate charged must be quoted in the report.

18. It is of the highest importance to traders and the Company that goods should be carried as free from damage, loss, etc., as possible; that the cause of damage loss, etc., should be duly and accurately recorded; that such measures as are necessary to prevent the recurrence of those causes should be promptly taken; and that whenever it is found that claims on any particular traffic or from certain traders are numerous, and the percentage of claims to the receipts on the traffic is high, special representations should be made to your superior officer.

19. Subject to certain limitations, mentioned in the "Notes" below (see Clause 20), Goods Agents and Station Masters are authorised to settle claims up to Forty Shillings (40s.) made on Goods, etc., CONVEYED AT CARRIER'S RISK in cases where, after full enquiry has been made, there is no doubt as to the liability of the Company or Companies interested, or of the reasonableness of the claims, and that no principle is involved.

Care must be used in the exercise of this authority, as if found to have been

mis-used it may be withdrawn. 20. Immediately the settlement of a claim is made, the whole of the papers, including the claim and receipt together with the report of the facts, must be sent by London District Stations to the Chief Goods Manager and by other Stations to their District Manager who, if satisfied, will issue compensation voucher for the amount paid. Those Stations being authorised so to do will issue and send compensation vouchers with the papers.

Notes.—This Authority is subject to the following Reservations:—

- (a) Railway Clearing House Regulation 159, 1926 Edition, stipulates. in reference to all claims on Goods Train traffic FORWARDED FROM G.W.R. STATIONS to Stations on other Railways, that the concurrence of the other Terminal Company to the proposed settlement shall be obtained first.
  - (b) (i) Claims on Owner's Risk traffic and claims on goods not properly protected by packing, whether or not the consignment note is so endorsed.
    - (ii) Claims for delay.
    - (iii) Claims on Live Stock.
    - (iv) Claims on Carriers' Act Goods.
    - (v) Claims by Commission Salesmen on goods sent to them to sell on commission.
    - (vi) Claims for street or other accidents involving injury to persons or damage to private property.

Must not be entertained without special authority.

(c) Claims on Carriers' Act Goods; claims on Statuary and claims in respect of street or other accidents involving damage to private property, must not be entertained without special authority.

21. District Goods Managers are empowered to settle claims on Merchandise and Live Stock Traffics "Local" and "Through" (excepting claims which have to be submitted to the Joint Claims Committee, and claims involving personal injury) for such sums as, after due inquiry, it is clearly established the Company or Companies interested are liable to pay, PROVIDING that no important question of principle is involved, and that no payment exceeds twenty five pounds (£25).

22. Claims which have to be submitted to the Joint Claims Committee; claims involving personal injury or questions of principle; and claims involving payments exceeding twenty-five pounds (£25), must be reported by the District Goods Manager to the Chief Goods Manager upon the approved Claims Report Forms numbered (2437), (2439), (2956).

23. Whenever several "Local" traffic claims by one trader are settled at one time, the multiple claim receipt forms (4176 A providing for 10 claims; or 4176 providing for 20 claims) should be used. These forms may also be utilised for foreign

Rates to be quoted when re-Prevention of Claims.

Powers of Goods Agents and Station Masters in reference to settlement of Claims.

Powers of Managers in reference to Claims.

What Claims, etc., are to be reported to Chief Goods Manager.

Receipts for Claims.

# CLAIMS ON MERCHANDISE TRAFFIC, ETC .- Continued.

claims, but when this is done a separate form must be used for the items with each constituent Company of another group. When claims are settled individually receipts must be obtained on the claims and must specify the amount paid in settle-

24. Goods Compensation payments must be cleared by Local Goods Compensation payments must be cleared by Local Goods Compensation tion Voucher (Form 457), by R.C.H. Goods Compensation Voucher (Form 849), or Goods compensation payment Cartage Compensation Voucher (2496), as the circumstances require.

25. The Claim and Receipt must, in every instance, accompany the Compensa- Attachment of tion Voucher when the latter is taken credit for through the Daily Cash Account Claim, Receipt.

Voucher to Daily CashAccount(110).

26. Carting Agents, Road Carriers (e.g.—Pickfords, Carter Paterson, Sutton Cartage Agents', and others), Railway, Shipping and Dock Companies, have to bear certain agreed Road Carriers', &c. proportions proportions of claims on merchandise in connection with the services of collection of Claims. or delivery performed by them. In the case of Railway Companies the agreed proportions must be scheduled to the Railway Clearing House for apportionment in the usual way; in all other cases the agreed proportions must be collected and paid into the credit of Goods Compensation.

27. When settling claims for loss from or damage to goods carted by Senders, Consignces, or their Agents, to or from the Railway Station within the vicinity of Sender's and/or Consignee's premises which pass all hands without remark the deductions as agreed with the Mansion House Association must be made from the net

If goods are conveyed by Senders, Consignees, or their Agents greater distances than are indicated in the foregoing paragraph and there is no evidence to show where the loss or damage occurred, instructions must be obtained from the District Manager.

28. An Abstract of Goods Compensation, prepared on Form (2577a), must be Abstract of Goods forwarded by the District Goods Manager to the Chief Goods Manager not later than be sent to Chief

Goods Manager monthly.

### Disposal of Refused Goods, Salvage, etc. Perishable Goods.

29. If perishable goods have been damaged, pilfered, or delayed in transit Disposal of and consignees raise objections to receive them, efforts should be made to induce Refused Goods, the consignees to accept the goods and deal with them promptly and to the best Salvage, &c. the consignees to accept the goods and deal with them promptly and to the best advantage on behalf of whom it may concern.

- 30. If the consignees decline to do this, and senders' instructions cannot be obtained without risking further deterioration of the goods, goods of an unimportant character should be sold to the best advantage, under the personal sanction of the Goods Agent or Station Master.
- 31. Goods must not be sold to the Company's staff if better prices can be otherwise obtained.
- 32. Before important consignments of Perishable Goods are disposed of, the circumstances must be immediately reported to the District Goods Manager, by telegraph or telephone who will give his assent to the course proposed to be adopted or other instructions after consulting the Salvage Department.

# Other Goods-Not Perishable.

33. When goods, not of a perishable character, are refused by the consignee, or owing to any other reason cannot be delivered and disposal instructions cannot be obtained from either consignee or sender, if there are special features in connection with their manufacture or their description which render it desirable that they should be disposed of locally in the district concerned, report must be made by telephone or telegram to District Goods Manager who will give instructions after consulting the Salvage Department. If such special features do not obtain, the goods must be sent to the Salvage Department, Park Royal, and full particulars of the goods must be shewn on Form 675 which must be despatched to that Department on the same date. Every package and article forwarded to the Salvage Department, Park Royal, must bear Label No. 1093 properly filled up and securely fastened.

#### CLAIMS ON MERCHANDISE TRAFFIC, ETC .- Continued.

Notes: (i) See also Standard Terms and Conditions A.16, B.15 and C.16

(ii) The foregoing instructions, re "Other Goods-not Perishable," do not apply to such goods received from other Carriers when the compensation payable thereon has to be shared by such other Carriers. In such cases, arrangements must be made with the other Carriers concerned before any sale is effected

Whole proceeds of Sales to be paid in.

34. The proceeds of all sales of goods by or on behalf of the Company must be at once paid in through the Daily Cash Account (110), to the credit of Goods Compensation; no part thereof may be applied in reduction of claims. Compensation Vouchers must be issued for the full amounts paid in settlement of the claims for such goods, and reference must be made thereon to the date of the Cash Account (110), in which the amount of the sales credit appears.

Statement of Sales to be ren dered monthly.

35. Particulars on Form 2190 of the amounts paid in, through the Cash Account, to the credit of Goods Compensation, must be forwarded by the Station Agent on the 1st of each month to his District Manager, and by the latter to the Chief Goods Manager on the 4th of the month.

#### GOODS NOT TO HAND AND GOODS RECEIVED WITHOUT ACCOUNT.

failures to connect "Overs with "Shorts."

The Company incur serious monetary loss owing to failures or delay in connecting goods received at Stations without account with the consignment to which they belong. The goods largely concerned are those forwarded by the senders either unaddressed or under mark and those which from various causes have lost their address labels.

These failures and delays are largely contributed to by inadequate and incorrect information being given in official communications concerning such goods. Unless the goods required or the goods on hand without account are promptly reported and correctly and adequately described connections cannot be properly made.

Station Agents will be held responsible for seeing that the following instructions are properly carried out.

#### Goods not to hand.

Goods not to hand to be recorded.

1. Full particulars of goods not to hand must be recorded in the "Goods not Received as Invoiced" Book (2021).

Efforts to be made to trace them.

2. Every effort must be promptly made to trace missing goods. "Not to Hand" advice (Form 197) must be issued to the invoicing station and to any transfer station, and this advice must be followed up by such other enquiries as each case demands, enquiry forms (2319) and (4079) being used as may be necessary.

Goods to be traced for enquiries.

Shipment, Valuable, Perishable, or are Perishable, Valuable, or Dangerous, the telegraph or telephone must be used

Fullest descrip-

4. It is particularly important that the best and fullest description of missing tion and marks to goods and the marks they bear should be given in all tracing communications.

Duty of forwarding and transfer stations.

5. On receipt of "Not to Hand" advices it is the duty of the invoicing and transfer stations to satisfy themselves that the missing goods have been duly forwarded, and whether they have been reported by any station as received without account, It is the duty of the last station from which the goods are alleged to have been forwarded (whether it be the transfer or original forwarding station) to also make the necessary enquiries for the missing goods, and to give prompt and complete replies to the communications about them.

Supplementary enquiries,

6. All enquiries of the Salvage Department for missing goods must be made by Form 2319.

#### Goods received without account.

Goods received at corded.

7. Full particulars of GOODS RECEIVED WITHOUT ACCOUNT, including G.W. and Foreign Stations without goods found on line, must be entered in the "Goods Received Unentered Book account to be re- (2020). If the goods are damaged record must be made accordingly.

> 8. Goods advised to G.W. Stations as received at Foreign Stations without account must also be recorded in the "Goods Received Unentered Book" (2020), or in the "Goods Reported Unentered Book" (216) where kept, but these entries must be distinguished from the entries of goods received at the Recording Station without account. (See also Clause 16.)

Efforts to be made to trace owners of goods.

9. Every effort must be promptly made to trace the owners of goods received without account. "Received Unentered" form (381) must be issued, and if necessary the telegraph or telephone must be used, and these enquiries must be followed by such other enquiries as each case demands.

# GOODS NOT TO HAND AND GOODS RECEIVED WITHOUT ACCOUNT-Continued.

10. It is very important that the best and fullest information respecting goods Fullest description and marks to received without account should be given, the packages and their contents and the begiven. marks they bear should be clearly described.

11. Stations receiving these advices must make immediately such enquiries as Duties of stations are necessary to connect the goods with the consignments to which they belong. 12. Valuable Goods, and Goods evidently intended for Shipment, the owners of Reporting of which cannot be traced promptly, must be reported to the Salvage Department by ment goods.

Perishable Goods must also be reported to the Salvage Department by wire Perishable goods.

(See Instructions 16 to 19 as to the disposal of Perishable Goods received without account).

14. When Dangerous Goods, e.g. EXPLOSIVES, INFLAMMABLE OILS.

received without

MATCHES, etc., are received without account the station they were received from, and when known the original forwarding station, must be so advised by wire, and if disposal instructions are not obtained immediately the District Manager must be

applied to by wire for his instructions. Full description of the goods and of the marks thereon must also be telegraphed to the Salvage Department, Park Royal.

Dangerous goods must not be sent to the Salvage Department without special

authority from that department.

The General Regulations contained in the "Special Classification of Explosives and Other Dangerous Goods" Section of the General Railway Classification of Goods must be observed.

15. Each Station must forward to the Salvage Department, daily, the "RETURN Chief Goods OF GOODS RECEIVED WITHOUT ACCOUNT" (Form 446), excepting when there is Manager of Goods nothing to report.

This return must contain full and correct particulars of:

Perishable, Dangerous, and Valuable Goods received without account on the Information the day the return is issued. Goods (not perishable) received without account and which have been on hand

three working days awaiting disposal instructions. Note. The instructions apply to all goods including those received in trucks with-

out labels. Unaddressed goods received in excess of invoiced quantities in through trucks

or otherwise with the following exceptions: (a) Flour from Barry; District Goods Manager, Cardiff, will give instruc-

tions for disposal. (b) Continental Iron from South Wales Ports; Forwarding Station will

give instructions for disposal. (c) Traffic from Avonmouth Docks; District Goods Manager, Bristol, will

give instructions for disposal, (d) Traffic to or from Manchester Ship Canal; in addition to usual reports, advices of all excesses or shortages to be sent to District Goods Manager, Liverpool,

# Goods Found on Line.

Note.—For instructions to Permanent Way men respecting "Goods found on line," see page 289.

Goods advised to the reporting stations as received at Foreign Stations without

The Return must also contain the best and fullest description of the goods, and the marks they bear must be given, and when the written description is inadequate a sketch of the articles should be sent.

The entries on the return must be numbered consecutively commencing No. 1 on Entries to be 1st January in each year.

Each return must bear the last "entry number" shown on the preceding return.

"Nil" returns must not be issued.

who will give instructions for disposal of excesses.

#### Disposal of Perishable Goods received without account.

16. If the owners of Perishable Goods received without account cannot be Perishable goods notly discovered, and delay would involve land the delay would be received without account cannot be received and account cannot be received without account cannot be received and cannot be received account cannot be received account cannot be received account cannot be received and cannot be received account cannot be received a promptly discovered, and delay would involve loss by deterioration, goods of an account. unimportant character should be sold to the best advantage under the personal sanction of the Goods Agent or Station Master.

17. Goods must not be sold to the Company's staff if better prices can be otherwise obtained.

18. Before important consignments of Perishable Goods are disposed of the circumstances must be reported to the District Manager by telegraph or telephone, who will give his consent to the course proposed to be adopted or other instructions after consulting with the Salvage Department.

# GOODS NOT TO HAND AND GOODS RECEIVED WITHOUT ACCOUNT-Continued.

Butter, Cheese, Margarine, Bacon, Lard, etc.

19. Butter, Cheese, Margarine, Bacon, Lard, and such like goods may only be sold locally if the condition of the goods demands immediate disposal. Otherwise if disposal instructions are not received within three days from the day of receipt such goods must be forwarded to the Salvage Department, Park Royal, and loaded to Paddington Goods, invoiced free.

# Disposal of Non-perishable Goods received without account.

Non-Perishable goods received without account. 20. If disposal instructions for non-perishable goods received without account are not received within *fourteen* days from the day of receipt the goods must be forwarded to the Salvage Department, Park Royal, invoiced free, subject to the following exception:—

EXCEPTION.—Timber, Iron, Stone, and other goods in large quantities, very bulky articles (e.g., sheets of wool), and empties, must not be forwarded to the Salvage Department until authority from that Department has been received, but disposal instructions must be regularly pressed for until obtained.

# Returns and Labelling of Goods despatched to Salvage Department.

Returns and labelling of goods forwarded to Salvage Department, Park Royal. 21. When goods are despatched to the Salvage Department, Park Royal, full particulars of them must be duly and properly given on Form (675), which must be forwarded to that Department on the same day as the goods are despatched.

22. Every package and article forwarded to the Salvage Department must bear label (No. 1093) properly filled up and securely fastened to the package or article.

# Disposal of Proceeds of Sale of Goods.

23. The proceeds of all sales of goods by or on behalf of the Company must be at once paid in through the Daily Cash Account (110) to the credit of Goods Compensation; no part thereof may be applied in reduction of claims.

24. Particulars on Form 2190 of the amounts paid in, through the Cash Account, to the Credit of Goods Compensation, must be forwarded by the Station Agent on the 1st of each month, to his District Manager, and by the latter to the Chief Goods Manager on the 4th of the month.

25. Foreign Ropes, etc., on hand without account.—Other Companies' Ropes, Chains, Scotches, etc., on hand without account, i.e. forwarding Stations unknown; must be forwarded immediately to the Salvage Department, Park Royal, advised and labelled in accordance with Instructions 21 and 22.

# INSTRUCTIONS TO BE OBSERVED IN DEALING WITH THE RECEIPT, STORAGE, AND DELIVERY OF ARTICLES DEPOSITED IN CLOAK ROOMS.

Charges to be made.

1. The charges to be made for Cloak Room deposits are as follows:—

(a)			(b)		
Bags.		Bass Viols.			
Baskets.		Bath Chairs.			
Boxes.		Bicycles (ordina	ary).		
Bundles.		Cash Registers.			
Cases.		Hand Carts.			
Coats.		Harps.			
Hat-boxes.		Ice Cream Cart	S.		
Hucksters' Lugg	age.	Ice Cream Free	zers.		
Mail Carts, Fold	ing (folded).	Invalid Chairs.			
Packmen's Lugg	age.	Mail Carts (chil			
Parcels.		Organs (street).			
Portmanteaux.		Perambulators. Pianos (street).			
Rugs.	1 1				
Sewing Machines	s, nand.	Scissor Grinder			
Typewriting Mac Umbrellas.	enines.	Sewing Machines, treadle. Side Cars.			
Walking Sticks.					
warking bucks,		Violoncellos.			
If removed on day of deposit or next day.	For each other day than the days of de- posit and removal.	If removed on day of deposit or next day.	For each other da than the days of d posit and removal.		
Each article.	Each article.	Each article.	Each article.		
8d. 4º	1½d. (min.) 2d.	6d. 4 d	3d. 11 de		

#### ARTICLES DEPOSITED IN CLOAK ROOMS-Continued.

uncharged wit	nd Motor Scooters, the electricity, gas, inflammable liquid laration to be signed	(d)  Bicycles with more seats than one. Harmoniums. Pianos (other than street).			
If removed on day of deposit or next day.	For each other day than the days of de- posit and removal.	If removed on day of deposit or next day.	For each other day than the days of de- posit and removal.		
Each article.	Each article.	Each article.	Each article.		

Notes. - (i.) Sunday is not to be counted in the period, except when the articles are deposited or taken out on that day.

> (ii.) The following are charged half rate, minimum deposit charge, 2d. per package :-

Commercial Travellers' luggage (including Bicycles with handlebars, pedals and saddles removed).

Personal luggage of passengers engaged in Theatrical profession, and luggage of Music Hall Artistes (vide C.A.B. Reg. 40).

Luggage of Lecturers (vide C.A.B. Reg. 22).

(iii). When the length and girth together of any article-

Exceed 15 feet, and do not exceed 25 feet.... Double charge is made. Exceed 25 feet ...... Treble charge is made.

(iv.) Packages containing Liquid Air; Cylinders containing Compressed or Liquefied Gases; Motor Cycles and Motor Scooters charged with electricity, gas, oil or other inflammable liquid or vapour, are not accepted for deposit in Cloak Rooms.

2. No article should be accepted for deposit in the Company's Cloak Rooms unless Articles to bear the Cloak Room fees are paid at the time of deposit. In special cases where this may of Warchouse not be possible, Warehouse Form No. 4472 must be used. Any articles found on the label. platforms must be treated as unclaimed and the Lost Property fee charged.

Every article accepted for storage in a Cloak Room must have either the usual Cloak Room numbered label or a Warehouse label affixed.

The name of the depositor is to be recorded in the space provided on the ticket, which must be initialled by the person issuing it.

3. No package of an offensive, dangerous, or otherwise objectionable character offensive and dangerous articles, is to be accepted for storage in Cloak Rooms.

Live animals or birds taken charge of in exceptional circumstances are accepted and birds, at the Owner's risk only and the Cloak Room ticket should be endorsed accordingly.

4. In the case of a bag or box of any description being tendered to the Cloak Room, the lock should be tried, and if found to be unlocked, the depositor's attention should unlocked. be called to it with a request to lock it, and if, from any cause, he should omit to do so, the ticket must be marked "Unlocked, owner's risk," which the depositor should be requested to sign.

Similar precautions must be taken in cases of apparent damage.

 Cloak Room tickets in rolls, printed in distinctive colours and in face-values of Cloak Room
 3d., 4d., 6d., 9d., 1s., 1s. 3d., and 1s. 6d., will be supplied as the existing stocks of cash. books are exhausted, and the values must be requisitioned to meet the general requirements.

Pads of face-value tickets will be supplied to stations not having a large issue.

When a charge of more than the highest value of ticket in use is due on deposit, two or more tickets to cover the total must be issued. Each ticket is provided with numbered labels, and when more than three articles are deposited under one ticket, label 522 must be used.

#### ARTICLES DEPOSITED IN CLOAK ROOMS-Continued.

Supplies must be ordered on Form 5746 from the Accountant (Audit Section), the closing number of the stock on hand being stated. Particulars of tickets received must be entered on the first page (inside cover) of the proof book.

The proof book (5063) of value tickets must be kept and made up daily and totalled monthly. The daily total should agree with the cash remitted.

A Return shewing the commencing and closing numbers and amount of each value used must be sent to the Accountants Office (Audit Section) each month with the Parcels Accounts. The total of the Return is to be carried to Column "D" of the Parcels General Account 109.

All Cloak Room receipts must be paid in daily, and at stations where books remain in use the person receiving the cash must initial the counterfoil of the last ticket issued and insert the amount thereon.

6. Each ticket issued must be clearly filled up in ink, and, when handed back by the holder, be cancelled with the station date-stamp and warehouse rent (if any), as per clause 7, collected. The person delivering the package should sign or initial the ticket.

Surrendered Cloak Room tickets must be withdrawn from the Cloak Rooms each morning by the Station Master or other responsible person deputed by the Divisional Superintendent, and a record made by him of those not surrendered at expiration of period covered by time of deposit. If not already done corresponding entries must be made in the Warehouse Register, either by or under the direct supervision of the Station Master or deputy referred to.

In no circumstances must the name of the depositor on a cloak room ticket be altered. If an error is made, the ticket must be cancelled and attached with counterfoil complete, in support.

7. After expiry of the **second** day of deposit, warehouse rent must be charged at the rates set out in clause 1, and the amount accounted for as Warehouse revenue, Warehouse receipt (4472) being issued in every case.

No extra charge is made when Commercial Travellers' luggage is deposited after Friday 12 noon and taken out on the following Monday, nor when deposited after 5,0 a.m. on the Thursday before Good Friday or Christmas Day when the latter falls on a Friday, or after 5.0 a.m. on the Fridays before Whit-Monday and August Bank Holiday and taken out on the following Tuesday.

8. When an article for which a Cloak Room ticket has not been issued, or a package left to be called for by a person other than the depositor, is claimed, the applicant should be required to give satisfactory proof of ownership. Any fees chargeable must then be collected and a Warehouse receipt (4472) issued.

9. A book, ruled as shewn below, must be kept in each Cloak Room, and all articles that have been on hand over two days (or beyond the time allowed in the case of Commercial Travellers) entered therein daily. When the articles are claimed the Warehouse charges which have accrued since date of entry must be collected and Warehouse receipt (4472) issued.

Printed books (5780) are stocked for issue to stations where the entries are numerous.

No. of Cloak Room Ticket.	No. of Articles.	Date Deposited.	Date Delivered.	Days Chargeable.	Ware- house Receipt Form 4472.	Amount.	Signature of Person Delivering Articles.

Tickets to be filled up in ink, and cancelled, with date stamp.

Warehouse Charges.

" To Pay "
Deposits.

Register of Warehouse Charges.

# ARTICLES DEPOSITED IN CLOAK ROOMS-Continued.

10. No article for which a Cloak Room ticket has been issued should be given up Lost Cloak unless the ticket is produced, but in the event of loss of a ticket by the holder the person claiming the articles must be requested to produce satisfactory evidence of ownership, such as keys to open, and/or description of contents, and to sign the Lost Ticket Declaration (88), the latter being retained in the book.

11. When an article is accepted in the Cloak Room to be called for by other than Articles to be the depositor the depositor should be made clearly to understand that the Company other than can accept no risk with regard to it if wrongly claimed, and the Cloak Room ticket depositor. must be marked "At owner's risk," the depositor's signature being obtained thereto.

12. When personal request is made for luggage deposited in the Cloak Room to Delivery of be delivered to an address, no objection must be made to this being done, the applicant luggage. being requested to write the address to be attached to the article or articles, which must be transferred to the Parcel Office in the ordinary course for delivery.

In the case of requests received by telephone or through the post without the Cloak Room ticket being enclosed; (a) if the address of the Owner is within the Company's delivery, the carman must be instructed to collect the Cloak Room ticket when delivering the package; (b) if outside the Company's delivery, or the luggage has to be delivered to a Cloak Room at another Company's Station, the ticket must be obtained before the despatch of the luggage.

13. Articles handed in as Commercial or Professional luggage can only be Commercial accepted as such upon satisfactory evidence being submitted that the owners are Professional entitled to the lower charges.

Professional Luggage.

14. Sticks, umbrellas, tennis racquets and fishing rods must not in any case be sticks and accepted when put under the straps of bags, portmanteaux, &c., but are to be treated as separate articles. This does not apply to holdalls containing umbrellas, &c., provided they are properly packed.

15. Motor cycles and motor scooters must not be accepted in the Cloak Room Motor-cycles unless all petrol, gas, oil, or other inflammable liquid or vapour has been discharged. The owners of such machines must not be permitted to discharge oil or petrol upon the Company's premises or property.

Cycles belonging to "professionals" travelling on tour with theatrical parties must not be accepted in the Cloak Rooms at other than the ordinary charge.

In dealing with cycles and also in transferring them to the Parcels Office, it must

be noted what accessories, i.e., lamp, pump, wallet, &c., are attached.

This should also be done as regards those received from the Parcels Office. The Company will not undertake the insurance of cycles of any description deposited in the Cloak Rooms; such articles can only be taken charge of at ordinary risk in accordance with the conditions shewn upon the Cloak Room tickets. insurance is desired, depositors should be advised to take a Baggage Insurance ticket

16. A register must be kept of cycles stored under the contract system in the Cycles stored under contract following form :-

system.

Name. Address.	. Address. Description.		Dat	tes.	Agreement No.	Amount.	Date Accounted
			From	То	No.	( )	For.

and a tab giving the number of the contract note and date of expiry attached to each machine. Care should be taken to see that contracts are promptly renewed or surrendered on expiry.

17. As little gum as possible should be used in affixing dockets to cycles, leather Affixing of lags, portmanteaux, and other articles liable to disfigurement, and the attendants dockets. should discriminate when it may be advisable to attach the numbered label by other means.

#### ARTICLES DEPOSITED IN CLOAK ROOMS-Continued.

Supervision of Cloak Rooms.

18. The Cloak Room tickets and the Warehouse Charges register must be checked by the Station Master or some responsible person deputed by him at least once a week, and the general conduct of the Cloak Room business should receive the personal supervision of the Station Master as far as possible.

Articles not

19 Any article remaining on hand in the Cloak Room more than a month after deposit must be reported to the Lost Property Office and Divisional Superintendent or District Traffic Manager, full particulars being given.

Any article not claimed within 6 months must be opened by a responsible person in the presence of another, and if the owner's name and address can be ascertained, instructions for disposal should be asked for. If no address can be found or no reply is received to an application within a month, the article must be sent to the Lost Property Depot, Paddington, a full list of the contents of all packages being kept.

Lost Luggage charges. 20. Cloak Room tickets must not be drawn to cover fees charged for the recovery of passengers' lost luggage. Such amounts must be paid in separately, and, if collected at the Station at which the article is found, Warehouse receipt (4472) must be issued.

#### TRAINMEN'S INTERVALS FOR REST.

With the exceptions which follow, the roster must provide for a period of 12 hours rest being shown in the case of all regular duties for men when at their home stations. In all other cases, a minimum of 9 hours is to be allowed from the time of signing off for one turn of duty to signing on for the next turn on duty.

Exceptions :-

(a) The rest interval agreed on shall not be adhered to when it is necessary to call men out for emergencies, such as breakdowns, failures, fog, etc. This is not intended to vary the agreed arrangements in regard to the intervals of rest between train duties.

(b) The twelve hours' interval for rest may be reduced to eight hours by mutual agreement in the case of men working in shifts of eight hours when changing turns of duty at the week-ends.

(c) The nine hours' interval shall not necessarily be adhered to in respect of trainmen working race trains, excursion trains, or other similar Special Trains; in such cases it is agreed that an interval of seven hours' rest only need be given where the outward working has not exceeded seven hours. In such cases the men shall be released from work as soon as possible after completion of the return journey.

When from any cause a freight train arrives late at its destination, and it is seen that the Guard cannot work the return train unless he resumes duty with less than the required interval for rest, arrangements must be made by the Station Master, Inspector, or person in charge, either to put the Guard on to a later train, or to put the train back so as to enable the Guard (or Guards) to have the proper interval for rest. It will be necessary, however, for the Guard before going off duty to see the person making the arrangements, or receive from him written instructions as to the time duty is to be resumed.

Before altering or putting back a train, the Traffic Officer must come to a clear understanding with the Locomotive Department, so that proper arrangements may be made by both Departments.

When trains are put back, an advice must be sent to the terminal and principal stations on the route, giving particulars of the arrangements.

GUARDS AND ENGINEMEN TRAVELLING FROM POINT TO POINT TO RELIEVE TRAINMEN, AND TRAINMEN PROCEEDING TO THEIR HOMES AFTER BEING RELIEVED in route FOR REST. ETC.

Trainmen's Order Form No. 676 is to be recognised as the authority for Guards in Uniform to travel by passenger or freight trains, without exchange at the booking office for an ordinary ticket in the case of a passenger train journey.

case of a passey yer train journey.

The person issuing the order must insert the station at which the Guard is stationed.

When used for a passenger train journey, the form is to be punched when examined and collected in the same way as an ordinary passenger ticket, and after collection is to be forwarded on the same day to he Superintendent of the Division in which the man is stationed, and not to the Audit Office with the collected tickets.

In the event of its being necessary for a man to travel the first portion of a journey by passenger train and the remainder by freight train, the form must be collected at the end of the journey and attached by the Guard in charge of the freight train travelled by, to the sectional or through journal rendered to the Superintendent of the Division in which the man leaves the train; and the Superintendent receiving it must forward it to the Superintendent in whose division the man is stationed.

Deleted

GH 30

### DUTIES WHICH MUST NOT BE ALLOTTED TO LADS.

Working signals or points.
Working block telegraph instruments.
Roping vehicles.
Coupling and uncoupling vehicles.
Shapting

Shunting. Look-out men. Handsignalling.
Acting as pilotman for single line working.
Dealing with Train Staffs, Tokens or Tablets.
Carrying full sacks of corn, flour, milk churns, &c.
Getting on tops of carriages.
Starting engines of road motor vehicles.

#### REFRESHMENT ROOMS AT STATIONS.

## At Stations where the Refreshment Rooms are held by tenants.

The Station Masters to see that :-

The rooms are properly conducted, and to bring under notice any complaints made respecting them.

A printed copy of the authorised tariff of charges is suspended in a conspicuous position in each of the public rooms and is the only tariff exhibited.

Each room is kept neat and clean.

# At Stations where the Refreshment Rooms are under the Company's own management.

The Station Masters to keep the rooms under general observation, and to call the attention of the Divisional Superintendent to any complaint or any other matter affecting them, which it may be desirable to bring under notice.

# At all Stations where there are Refreshment Rooms.

The staff to co-operate with, and assist, the persons in charge of the refreshment rooms in any measures that may, from time to time, be necessary to preserve order. Any persons evidently under the influence of drink or known to be habitual drunkards must, while at the station, be kept under observation, and, as far as practicable, prevented from entering the refreshment rooms, especially as, in the event of a prosecution for permitting drunkenness on licensed premises, the responsibility of proving that all reasonable steps were taken to prevent drunkenness is placed, by the Licensing Act of 1902, upon the licence holder.

# SUPPLY OF LUNCHEON AND TEA BASKETS AND CUPS OF TEA TO PASSENGERS IN THE TRAINS.

1. Breakfast, Luncheon and Tea Baskets.—Baskets containing Breakfast, Hot or Cold Luncheons or Dinner or Tea are obtainable at the Refreshment Rooms of the following Stations:—

‡Aberystwyth	‡Hereford Kidderminster	‡Shrewsbury *Slough
Banbury		Stourbridge Junction
*Barmouth Junction	‡Leamington Spa	Stratford-on-Avon
‡Bath	#Machynlleth	
‡Birmingham (Snow Hill)	*Maidenhead	‡Swansea, High Street.
†Bristol (Temple Meads)	Malvern, Great	‡Swindon
*Bristol (Stapleton Road)	*Moat Lane	‡Taunton
*Builth Road	†Neath	Torquay
†Cardiff (General)	Newbury	Trowbridge
Cardiff (Queen Street)	!Newport (Mon.)	‡Truro
†Carmarthen	Newton Abbot	Wellington (Salop)
Chester	Oswestry	‡Welshpool
Chippenham	Oxford	Westbury (Wilts)
Corwen	Paddington	Weston-super-Mare
Craven Arms	Penzance	*Weymouth Quay
Didcot	†Plymouth (Millbay)	*Windsor
Dudley	†Plymouth (North Road)	†Wolverhampton .
†Exeter (St. David's)	Pontypool Road	Worcester (Shrub Hill)
*Eichauard Harbour	†Reading	Wrexham
*Fishguard Harbour	Ruabon	
†Gloucester	Trumpon .	

† Hot Luncheons supplied. \* Tea Baskets only.

Hot Breakfast baskets can be supplied at all stations except those marked \*

The charges for Breakfast, Luncheon and Tea Baskets are as shewn below:-			
Breakfast Baskets.—Eggs and Bacon (or Cold Ham), Bread, Butter, Preserves, Tea,	_	d.	
Coffee or Cocoa	.,	U	
Butter, Cheese, Salad, &c			
Cold Chicken and Ham, Bread, Butter, Cheese, Salad, &c	3	6	
Tea Baskets.—Pot of Tea, &c., Bread and Butter, Cake or Bun and Fruit. Per person	1	3	

#### LUNCHEON AND TEA BASKETS, ETC .- Continued.

2. When empty baskets are handed out by passengers, the persons receiving them must satisfy themselves that the fittings—a list of which is given on a card in the inside of the baskets—are complete; and if any article should be missing they must, before the train leaves the station, advise the Station Master or person in charge.

3. All such baskets handed to the Company's Servants by passengers must be taken immediately into the parcel office by the person receiving them, and booked free of charge by next train to the station marked upon them. In the case of Guards, however, whose time does not permit of their taking the basket to the parcel office, they must immediately deliver it to a member of the Station Staff, who must take it there at once.

Luncheon and Tea Baskets, or cups and saucers, removed from carriages by searchers or others must be taken immediately to the parcel office, or deposited in a special receptacle when provided

for the purpose.

4. At Junction stations where through carriages (whether belonging to this Company or to other Companies) are taken on by, or transferred to, the trains of other Companies, such a search must be made in the through carriages as the circumstances will admit, with the object of removing Luncheon or Tea Baskets, or cups and saucers; and, if practicable, this must be done by a particular member of the Staff as part of his duties.

5. In addition to Tea Baskets, cups of tea, &c., are supplied on the platforms at the following stations, and passengers are allowed to take the cups and saucers into the carriage with them:—

Aberystwyth Afonwen Hereford Reading Leamington Spa Ruabon Banbury Moat Lane Shrewsbury Neath Swansea Bath Newbury Swindon Birmingham Bristol Newport Taunton Cardiff (General) Newton Abbot Trowbridge Carmarthen Oswestry Truro Chester Oxford Welshpool Chippenham Paddington Westbury (Wilts) Didcot Plymouth (Mill Bay) Plymouth (North Road) Weston-super Mare Wolverhampton Dovey Junction Exeter Pontypool Road Wrexham Gloucester

6. The Staff must remove cups, saucers, &c., from carriages after passengers have finished with them, and at those stations where the Rooms are under the management of the Company, at once take them to the Refreshment Rooms, or place them in the baskets provided on the platforms for their reception, while at stations where the Refreshment Rooms are controlled by Contractors, they must be promptly taken to the parcel office, whence they must be returned to Refreshment Department Stores, Paddington.

7. To prevent damage to crockery during transit, baskets are worked between the principal stations for its conveyance, and every attention must be given to the proper working of these baskets.

8. To prevent extensive loss of crockery, Station Masters must see that the best possible arrangements are made to ensure the due collection of empty Luncheon and Tea Baskets and cups and saucers, and that no delay takes place in returning them to the Refreshment Rooms.

9. It is hoped that the Staff generally will take an interest in these arrangements, which are made for the comfort and convenience of the travelling public, and will see that these instructions are properly observed, reporting any irregularity which may come to their notice.

10. Folding tables.—For the convenience of First-Class Passengers, small folding tables for luncheon or other purposes are provided for hire at the stations at which Luncheon and Tea Baskets are supplied, and also at Llanelly. Passengers ordering Luncheon Baskets should be asked whether they also require a Folding Table, and if so the words "Folding Table" should be added to the telegram. Folding tables are also provided in certain cases for the use of invalid passengers or special parties, first or third class, and in such cases authority for the hire thereof must be obtained from the Divisional Superintendent.

A charge of 1/- is made for the use of a folding table.

#### ADVERTISING ARRANGEMENTS.

1. Posting of Bills on Station Boards, &c.—Only posters authorised for exhibition should be displayed at stations, and Station Masters should report the circumstances to the Divisional Superintendent or District Traffic Manager if posters reach their stations through unauthorised channels.

Current time bills and posters affecting each station must be posted neatly on the station boards on the line of sight and renewed as soon as they become damaged, illegible, or begin to look shabby. A red line must be ruled on each bill under the name of the Station and the times applying to it.

Slips issued extending the period for which bills are dated, or announcing alterations in the services, must be promptly and neatly pasted upon the bills to which they refer.

#### ADVERTISING ARRANGEMENTS-Continued.

Great Western Pictorial Posters should occupy prominent positions, and should be renewed as soon as they become faded, dirty or damaged. No pictorial poster must be displayed on any board longer than two months.

All out-of-date bills must be promptly washed off the boards. Station Masters must see that this

is done and that current bills are properly displayed.

The best positions on boards should be retained for the Company's own advertisements, and

other posters authorised for display should be confined to less important positions.

Posters advertising Excursion and Cheap Ticket arrangements should be displayed upon boards in positions where they are most likely to be seen by the travelling public. At some stations "Special Notice Boards" are supplied with one panel for a bill and a black panel for chalk notices. At other stations black boards or black paper bills are provided for Chalk Notices. Special attention should be given to this valuable form of publicity.

Where boards are exhibited in situations at a distance from the stations, e.g. at Hotels, Institutes or on Company's property away from stations, special steps must be taken to ensure posting with up-

to-date bills.

The attention of the Divisional Superintendent should be called to all boards needing repair

or repainting.

Poster boards and poster board sites at stations are sometimes required for the purposes of the Commerical Advertising business, either temporarily or permanently, and when this is the case all concerned are requested to review the propositions made in the most sympathetic manner possible consistently with the necessity for adequately advertising the Company's excursions, &c.

2. Handbills, Pamphlets, &c.—All Excursion and other handbills, pamphlets, &c., should be strung and hung in Booking Halls or other places at the stations most frequented by the public, and should be examined regularly to ensure no out-of-date matter being displayed.

In the event of the supply of posters, handbills, pamphlets or other advertising matter being

too large, or insufficient, the Divisional Superintendent should be notified of the fact at once.

The effective distribution of advertising matter is very important, and Station Masters should personally interest themselves in it and lists should be drawn up for each station providing for the distribution of :-

(a) Race, Football and other sporting trip bills mainly to public houses, sporting clubs, works, workmen's clubs, &c., and a few copies to Libraries, Hotels, Institutes, Post Offices, &c.

(b) Ordinary day and half-day trip bills mainly to Libraries, Hotels, Institutes, Post Offices, Works, Newsagents, &c., and a few to public houses, sporting and workmen's clubs, &c.

Outlying villages and neighbouring towns should be specially considered. In some cases distribution would have to be by post. Now that so many places are linked up by the services of Associated or other Road Companies, a very much wider field is opened up for the attractive excursions arranged by the Railway, and Station Masters should make themselves acquainted with these services, and when Rail excursions are arranged for such times as make it quite practicable for road passengers to travel out and home by the excursions, the bills should be distributed at every suitable place along the road service route.

At seaside and holiday resorts, generally, the distribution lists require to be of a still more extensive character, and should comprise all hotels, boarding houses and apartment addresses in addition to the places in clauses (a) and (b). Even in small houses accommodating only two or three holiday makers at a time, the publicity is valuable. The visitors at these addresses are just the class likely to make trips to neighbouring places if the facilities are brought to their notice.

Tourist and Holiday Ticket Pamphlets and general descriptive literature issued by the Company gratuitously, must be distributed as widely as possible in all quarters where the utmost benefit may be expected in the Company's interest.

Opportunity should be taken of the delivery of parcels to place a few handbills or a pamphlet or two under the string, &c.

- 3. Advertising in Waiting Rooms, Booking Halls, &c.—Attention is directed to the necessity of doing all possible to keep Waiting Rooms, Booking Halls, &c., as tidy as possible. All shabby and out-of-date G.W.R. advertisements, and old untidy Shipping Co.'s advertisements, should be reported to the Divisional Superintendent who will, after communicating with Paddington, instruct as to disposal.
- 4. Gratuitous Time Tables. (Large Edition only.) The gratuitous time tables must be delivered to the Hotels, Firms, Clubs, &c., on the agreed list as soon as possible after receipt at the stations. In no case should the delivery be delayed after the date each time table comes into operation.

If the supply is not received wholly or in part at least 10 days before the date of operation, the attention of the Divisional Superintendent or District Traffic Manager is to be drawn to the matter. Opportunity should be taken of the distribution of gratuitous time tables to include local cheap

tickets bills or pamphlets.

Other Companies' gratuitous time tables sent to stations in addressed wrappers must be delivered equally promptly.

#### COMMERCIAL ADVERTISING ARRANGEMENTS.

All communications relating to commercial advertising arrangements should be addressed to the Commercial Advertising Agent, Paddington Station, W.2.

Six canvassing areas are each operated by a responsible representative, with Headquarters as under :-

Paddington Station. .. Divisional Superintendent's Office, Birmingham ,, Gloucester .. .. Cardiff .. .. 22 Bristol .. District Traffic Manager's Office. Plymouth ..

The Company's Billposting Staff is responsible for the display, erection and proper maintenance of all Commercial advertisements.

The duties of the Representatives and Billposting Staff are controlled direct from Head Office. All sites on and within the immediate precincts of a station are considered "free" for letting for general advertising matter, but Station Masters will be consulted when necessary before a special site is definitely let.

Proposals for sites on the Company's buildings or land in the occupation or maintenance of another

Department will be referred to the Officer concerned for agreement.

Station Masters must see that all Commercial advertisements exhibited at stations are in good condition, and to advise the Commercial Advertising Agent when "renewals" or other attention is necessary.

It is very important that no delay should occur to bills, boards and other material of the Department during transit, and Station Masters and others concerned must carefully watch this.

#### SALE OF NEWSPAPERS, TOBACCO AND CONFECTIONERY AT STATIONS.

Messrs. Wyman & Sons Ltd. have the right to sell (other than by automatic machines) newspapers, books, periodicals, pamphlets, prints and stationery, and other articles as may be subsequently agreed

with the Company and which will be notified from time to time.

The Contractors are required to keep the bookstalls clean and tidy; no trade advertisements must be exhibited thereon without the Company's consent, and contents bill spaces should not bear announcements other than those of the newspapers, etc., on sale at the bookstall. The newsboys employed by the Contractors are expected to be neat in appearance, and all persons employed by the Contractors must not in any way obstruct or impede the servants of the Company in the performance of their duties, but obey all reasonable orders or instructions of the Company's Station Masters. In the event of misconduct or refusal to obey such orders or instructions, the Station Master or other authorised Official may withdraw from any person in the employ of the Contractors the right of access to stations to which they otherwise are entitled under the terms of the contract.

The Contractors' stock in trade forwarded by or for them to the Company's stations should be charged at the ordinary rate, but unsold newspapers and other "returns" after having been exhibited for sale will be conveyed free on the Company's system. The Company have the right to inspect such

parcels of "returns."

At stations where no bookstalls are provided, the Contractors may, subject to the Company's approval, arrange with a local newsvendor for the sale of newspapers, etc.

Members of the Contractors' staff not holding passes or season tickets must pay ordinary fare. The attention of Station Masters and others in authority is directed to the necessity for ensuring that the terms of the contract are observed by Messrs. Wyman & Sons Ltd., and any breach of the arrangement should be reported.

Particular care and attention must be exercised by the Guards and others in dealing with the parcels of newspapers, etc., in transit, and at stations, in order that they may arrive safely and in

good condition, at their respective destinations.

Trestles or other temporary stands upon which newspapers are sorted must be removed from the

platforms each day before the busy traffic commences.

Station Masters must satisfy themselves that adequate arrangements are in force for the regular disposal of waste paper, which must not be allowed to accumulate at the bookstalls, particularly at the back or on the top.

In some cases outsiders have been permitted to sell race-cards on the platforms at the Company's stations. This is an infringement of the rights of the Bookstall Contractors, and none but Messrs.

Wyman & Sons' employees must be allowed to sell the cards on the Company's premises.

Instances have also occurred where platform barrows have been allowed to run against the bookstalls, and in consequence a considerable amount of damage has been caused, which, with ordinary care, could easily have been avoided. The attention of the staff concerned is specially drawn to the matter, and serious notice will be taken of any irregularity that may be discovered.

A certificate must be sent by Station Masters or others concerned to their Divisional Superintendent or District Traffic Manager on December 15th of each year that these regulations are being strictly

carried out.

SALE OF NEWSPAPERS, TOBACCO AND CONFECTIONERY AT STATIONS-Continued.

### Sale of Tobacco and Confectionery.

Messrs. Wyman & Sons are permitted to sell confectionery and tobacco at certain stations, which

are advised as and when agreed,

At all other stations where the sale of confectionery, tobacco and eigarettes, etc., is undertaken by the Company by means of trolleys or basket-boys under the control of the Refreshment Department, Station Masters should exercise supervision over the boys when on duty and, at Stations where there are no refreshment rooms, sign them on and off; receive and pay in cash; and sign the boys' daily time sheets and pay their wages. The paybills will be prepared by the Refreshment Rooms Department, who will also be responsible for checking the stocks. Cupboards to contain the stock sold from trolleys and baskets are provided by the Refreshment Rooms Department, and Station Masters must take all possible steps to prevent damage or pilferage.

#### RAILWAY SKETCHING.

Occasions frequently arise when a descriptive report dealing with the locality of a particular Occasions irequently arise when a descriptive report dealing with the locality of a particular occurrence, detailing shunting or other operations, outlining suggested structural alterations, or describing some other matter pertaining to the railway may be made far more intelligible if accompanied by a rough sketch. It will be found that very little skill is required so long as the recognised way of depicting railway objects is followed, and the diagrams given on the following page may be taken as examples of the correct way of showing these.

100						
	Fig. No.				Fig. N	0.
A. Jill standling game	 14	Lamp-post				11
Audible signalling ramp .	 	Level crossing				3
Barrow Crossing	 1	Load gauge				11
	 2	Locking bar				9
	 12	nothing but				
	 1, 5, 6	Mile posts				15
	 1					~
Dundings, station		Overbridge				5
Carriage landing	 10					10
Catch point	9	Parish boundary				12
Cattle-pens	 10.	Platforms				2
Company's boundary	 12	Points				9
Compound points	 9	Point discs				15
Crossing, barrow	 1					
Crossing (level) gates	 3	Ramp				1
1	 7					8
Cross-over roads, scissors	8	Seissors crossing				9
	 3, 4	Sidings			0	-
	 16	Signal-box			7.1	
Cutoning		Signals				
Dead end	 13	Steps Stop block				1 13
	 15	Stop block				
	 14	Stop, wheel				13
	 12	Subway				1
	 1					11
Double compound points	9	Telegraph pole				11
Double compound points		Track circuit				15
Embankment	 16	Track circuit indica				14
	 1	Truck weighbridge				11
		Tunnel				4
Facing point locking bar	 9	Turntable				11
Fences, &c	 12	1 di nombio				
Footbridge	 1	Underbridge				6
-		Urinals				1
Gantry	 15	Ormais				
Gate, level crossing	 3	Veranda coverings				1
Gate, platform	 1	veranda coverings	• •			
Gate, siding	 11	337-11		1		12
Goods shed	 10	Wall				11
Gradient Posts	 15	Water column				13
		Wheel stop				3
Hedge and ditch	 12	Wicket				3
-						

### RAILWAY SKETCHING-Continued.

